

Oklahoma Dept. of Transportation - Bridge Inspection Report

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|--------------------------|--------------------------------------|------------------------|-------------------------------|-----------|
| NBI No.: 17051 | Structure No.: 6822 0000 X | Local ID: -1 | Suff. Rating: 82.90 | ND |
|--------------------------|--------------------------------------|------------------------|-------------------------------|-----------|

| Bridge Description: <div style="border: 1px solid black; padding: 2px;">3-125ft. P/S CONCRETE GIRDERS, 3-CONT. PLATE GIRDER SPANS (200ft.-330ft.-200), 4-125ft., 3-125ft. CONT. PLATE GIRDER SPANS</div> <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 1. State: Oklahoma 2. Division: Division 1 3. County: SEQUOYAH 4. City: Unknown Admin Area: Unknown 5a. On/Under: Route On Structure 5b. Kind of Hwy: Interstate Hwy 5c. Lvl of Srvc: Mainline 5d. Route No.: 00040 5e. Dir. Sufx: N/A (NBI) </div> <div style="width: 48%;"> 7. Facility Carried: I-40 6. Feat. ARKANSAS RIVER 9. SEQUOYAH-MUSKOGEE CC 11. Mile Post: NA 13. LRS / Sub Rte: 6800022HX / 00 16. Latitude: 35° 29' 16.49" 17. Longitude: 095° 05' 38.05" 98. Border: Unknown (P) % Responsible: 0.00 99. Border Brdg #: Unknown </div> </div> | INSPECTION <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th>Type</th> <th>Insp. Req.</th> <th>Insp. Done</th> <th>Freq.</th> <th>Insp. Date</th> <th>Next Insp.</th> </tr> </thead> <tbody> <tr> <td>NBI:</td> <td></td> <td>0</td> <td>24 months</td> <td>7/17/2023</td> <td>07/17/2025</td> </tr> <tr> <td>FC:</td> <td>Y</td> <td>0</td> <td>24 months</td> <td>7/17/2023</td> <td>7/17/2025</td> </tr> <tr> <td>UW:</td> <td>Y</td> <td>0</td> <td>60 months</td> <td>7/19/2020</td> <td>7/19/2025</td> </tr> <tr> <td>OS:</td> <td>Y</td> <td>1</td> <td>24 months</td> <td>7/19/2024</td> <td>7/17/2026</td> </tr> </tbody> </table> | Type | Insp. Req. | Insp. Done | Freq. | Insp. Date | Next Insp. | NBI: | | 0 | 24 months | 7/17/2023 | 07/17/2025 | FC: | Y | 0 | 24 months | 7/17/2023 | 7/17/2025 | UW: | Y | 0 | 60 months | 7/19/2020 | 7/19/2025 | OS: | Y | 1 | 24 months | 7/19/2024 | 7/17/2026 |
|---|---|------------|------------|------------|------------|------------|---|------------------------------|-------|-------|-----------|-----------|------------|------------------------------|-------|-------|-----------|-----------|-----------|-----|---|---|-----------|-----------|-----------|-----|---|---|-----------|-----------|-----------|
| Type | Insp. Req. | Insp. Done | Freq. | Insp. Date | Next Insp. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| NBI: | | 0 | 24 months | 7/17/2023 | 07/17/2025 | | | | | | | | | | | | | | | | | | | | | | | | | | |
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| OS: | Y | 1 | 24 months | 7/19/2024 | 7/17/2026 | | | | | | | | | | | | | | | | | | | | | | | | | | |
| STRUCTURE TYPE AND MATERIALS <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 43a/b. Main Span: Steel Cont. / Girder-Floorbeam 44a/b. Appr. Span: P/S Conc. / Stringer/Girder 45. # of Main Spans: 10 46. # of Appr. Spans: 3 107. Deck Type: Concrete-Cast-in-Place 108a. Wearing Surface: Low Slump Concrete 108b. Membrane: Unknown 108c. Deck protection: Unknown </div> <div style="width: 48%;"> 12. Base Hwy Net.: On Base Network 20. Toll Facility: On free road 21. Custodian: State 22. Owner: State 26. Function Class: 01 Rural Interstate 37. Historical Sig.: Not eligible for NRHP 100. Def. Hwy: On Interstate STRAHNE </div> </div> | CLASSIFICATION <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 101. Parallel Str.: No bridge exists 102. Traffic Dir.: 2-way traffic 103. Temp. Str.: Not Applicable (P) 104. Hwy System: On the NHS 105. Fed Land Hwy: IRR-Indian Res Rd 110. Defense Hwy: On Interstate STRAHNE 112. NBIS Length: Long Enough </div> <div style="width: 48%;"> 58. Deck: 6 Satisfactory 62. Culvert: N/A (NBI) Flowline Notes: <div style="border: 1px solid black; padding: 2px;">The Pier 5 footing is exposed up to 4"H x 5'L along the west face. 2020 Underwater Channel Notes: The channel in the vicinity of the bridge</div> </div> </div> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| AGE AND SERVICE <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 19. Detour Length: 5.0 mi 27. Year Built: 1967 28a/b. Lanes on/und: 4 / 0 29. ADT: 15,900 30. Year of ADT: 2022 42a/b. Type of Svc on/und: Highway / Waterway </div> <div style="width: 48%;"> 106. Year Reconst.: 1983 109. Truck ADT: 36% </div> </div> | LOAD RATING AND POSTING <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 31. Design Load: MS 18 (HS 20) 41. Post. Status: A Open, no restriction 70. Posting: 5 At/Above Legal Loads 63. Op / 65. Inv. Rating Meth.: 1 LF Load Factor / 1 LF Load Factor </div> <div style="width: 48%;"> <table border="1" style="width: 100%; border-collapse: collapse;"> <thead> <tr> <th></th> <th>H</th> <th>HS</th> <th>3-3</th> <th>EV3</th> <th>SHV</th> </tr> </thead> <tbody> <tr> <td>64. Operating Rating (tons):</td> <td>30.40</td> <td>54.70</td> <td>92.80</td> <td>0.00</td> <td>0.00</td> </tr> <tr> <td>66. Inventory Rating (tons):</td> <td>18.20</td> <td>32.80</td> <td>55.70</td> <td></td> <td></td> </tr> </tbody> </table> </div> </div> | | H | HS | 3-3 | EV3 | SHV | 64. Operating Rating (tons): | 30.40 | 54.70 | 92.80 | 0.00 | 0.00 | 66. Inventory Rating (tons): | 18.20 | 32.80 | 55.70 | | | | | | | | | | | | | | |
| | H | HS | 3-3 | EV3 | SHV | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 64. Operating Rating (tons): | 30.40 | 54.70 | 92.80 | 0.00 | 0.00 | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 66. Inventory Rating (tons): | 18.20 | 32.80 | 55.70 | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| GEOMETRIC DATA <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 10. Vert. Clearance: 99.99 ft 32. Appr Rwy Width: 69.91 ft 33. Median: Closed Med w/o Barri 34. Skew: 0.00° 35. Struct. Flared: No flare 47. Horizontal Clr: 30.00 ft 48. Length Max Span: 330.00 ft 49. Struct. Length: 2,003.16 ft </div> <div style="width: 48%;"> 50a. Curb/Sdwk Width L: 3.00 ft 50b. Curb/Sdwk Width R: 3.00 ft 51. Width Curb to Curb: 60.00 ft 52. Width Out to Out: 68.50 ft Deck Area: 136,247.76 sq. ft 53. Min. Vert. Cl. Ovr Brg: 99.99 ft 54a. Min. Vt. Undclr. Ref: N Feature not hwy c 54b. Min. Vert. Undclr.: 0.00 ft 55a. Min. Lat. Undclr. Ref: N Feature not hwy 55. Min. Lat. Underclr. R: 99.90 ft 56. Min. Lat. Underclr. L: 99.90 ft </div> </div> | APPRAISAL <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 36a. Brgd Rail: 1 Meets Standards 36b. Transition: 1 Meets Standards 36c. Appr. Rail: 1 Meets Standards 36d. Appr. Rail Ends: 1 Meets Standard 67. Str Evaluation: 6 Equal Min Criteria </div> <div style="width: 48%;"> 68. Deck Geom.: 4 Tolerable 69. Vert./Horiz. Undclr: Not applicable (NB) 71. Waterway Adeq: 8 Equal Desirable 72. Appr. Alignment: 7 Above Min Criteria 113. Scour Critical: 8 Stable Above Footin </div> </div> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| OKLAHOMA ITEMS <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 200c. Temperature: 91 200d. Weather: Clear 201. Struc. Stl. ASTM Desig.: A-36 / -1 202. Waterprf. Membrane: -1 Date Installed: 01/01/1901 203. Type Exp. Device: Modular Elastomeric Strip Seal: Sealed Expansion Joint 204. Type of Railing: PTR-1 (round hand rail) 205. Material Quantity: -3.00 208a. Type of Abutment: Skeleton b. Type of Found.: Steel Piling 209. Type of Pier/Found.: 2 / Yes Spread Footing 210. Foundation Elev.: <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>4,300.00</td> <td>4,240.00</td> </tr> <tr> <td>-1.00</td> <td>4,290.00</td> </tr> <tr> <td></td> <td>-1.00</td> </tr> </table> 211. Wear. Surf. Prot. Sys: Silane Date Installed: 01/01/1901 211c. Silane Reapplied 211d. Date: 213. Utilities Attached: Communication </div> <div style="width: 48%;"> 214a. Posted Weight Limit: NR b. Posted Speed Limit: 70 c. Narrow/1way Brgd Sign: NA d. Vertical Clr. Sign: NA Adv. Warning Sign: NA e. Navigation Lights?: Yes Working/Not Working: No 215. Overpass: INTERSTATE 218. Functionally Obsolete: - 220. Bridge Redecked: - 221. Substr. Cond. (U/W): Satisfactory Condition 222. Fill Over RCB: 223. Appr. Slab/Rwy Cond.: 6 225. Paint Type/Ovrct: Inorganic Zinc 3Coat Sys N/A 226. Date Painted: 2010 227. Paint Color: Gray 233. Deck Forming: 238. School Bus Rte.: Current & Desired route 240. Appr. Rwy Type.: Asphalt/Bituminous 243. Grdr Spacing/No.: / </div> </div> | 4,300.00 | 4,240.00 | -1.00 | 4,290.00 | | -1.00 | PROPOSED IMPROVEMENT <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 94. Bridge Cost: \$20,219,922 95. Roadway Cost: \$4,500,000 96. Total Cost: \$26,119,163 97. Yr. of Cost Est.: 2015 </div> <div style="width: 48%;"> 75. Type of Work: 31 Repl-Load Capacity 76. Lngth of Improvement: 1,989.0 ft 114. Future ADT: 25,440 115. Yr. of Future ADT: 2042 </div> </div> | | | | | | | | | | | | | | | | | | | | | | | | |
| 4,300.00 | 4,240.00 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| -1.00 | 4,290.00 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | -1.00 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | NAVIGATION DATA <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 38. Nav. Control: Permit Required 39. Vert. Clearance: 52.0 ft 40. Horiz. Clearance: 300.0 ft </div> <div style="width: 48%;"> 111. Pier Protect.: 2 In-Place, Function 116. Lift Bridge Vert. Clr.: 0.0 ft </div> </div> | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | <div style="display: flex; justify-content: space-between;"> <div style="width: 48%;"> 244. Span Lengths: <table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>131</td> <td>130</td> <td>130</td> </tr> <tr> <td>201</td> <td>330</td> <td>201</td> </tr> <tr> <td></td> <td>125</td> <td>125</td> </tr> </table> 245. Girder Depth: 246a. Type of Overlay: NA b. Overlay Thickness: 0.00 c. Overlay Date: 01/01/1901 d. Only Depth Changed >1": 247. Protective Systems: <div style="border: 1px solid black; height: 20px; width: 100%;"></div> </div> <div style="width: 48%;"> 248. # Field Splices w/ Corrosion: 249. Scour Crit. POA Exists?: - 250. Headwall: 258. Plans w/Found. in ODOT File: - 259. Scour Eval. in ODOT File: - 263. Interchange at Intersection: No 264. Interstate Milepoint: 290.66 </div> </div> | 131 | 130 | 130 | 201 | 330 | 201 | | 125 | 125 | | | | | | | | | | | | | | | | | | | | | |
| 131 | 130 | 130 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| 201 | 330 | 201 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| | 125 | 125 | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |

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| <u>NBI No.:</u> 17051 | <u>Structure No.:</u> 6822 0000 X | <u>Local ID:</u> -1 | <u>Suff. Rating:</u> 82.90 | ND |
|---------------------------------|---|-------------------------------|--------------------------------------|-----------|

Inspection Date: 7/19/24 Ed Cinadr

Invoice No.: 1160667 Inspected With:

BRIDGE NOTES:

Spans 1-3: Simple prestressed concrete girder approach spans (131.4 feet, 130 feet, 130 feet)
 Spans 4-6: Continuous variable-depth steel girder main spans (201 feet, 330 feet, 201 feet)
 Spans 7-10: Continuous steel girder approach spans (125 feet, 125 feet, 125 feet, 126 feet)
 Spans 11-13: Continuous steel girder approach spans (126 feet, 125 feet, 126.3 feet)
 w/ 18 inch safety curbs & 4ft median

Other/Special inspection items include:

- Modular joints support boxes and support beams at piers 3, 6 and 10.
- Cracks in floor beam 4, span 4:
 - o Stringer 1 (1 5/8 and 1 1/2 inches).
 - o Stringer 2 (3/4 and 1 1/8 inches).
- Cracks in lateral bracing gusset plates:
 - o Span 8, girder 3 at floor beam 4 (8 1/2-inch-long crack in gusset plate).
 - o Span 10, girder 2 at floor beam 2 (18-inch-long crack in gusset plate weld).
 - o Span 11, girder 2 at floor beam 1 (8-inch-long and 6-inch-long cracks in gusset plate weld).
- Horizontal web splice terminations in spans 4 near floor beam 5, span 5 near floor beams 3 and 11, and span 6 near floor beam 3. Cracks or cored hole noted at:
 - o Span 4, girder 2, near floor beam 5 (1-inch-long crack arrested by cored hole, 1 1/2-inch-long crack arrested by cored hole).
 - o Span 4, girder 4, near floor beam 5 (7/16-inch-long crack arrested by a cored hole).
 - o Span 5, girder 1, near floor beam 11 (3/4-inch-long crack stopping short of cored hole, 1 1/4-inch-long crack arrested by two cored holes).
 - o Span 5, girder 2, near floor beam 11 (2-inch-long crack arrested by a cored hole).
 - o Span 5, girder 3, near floor beam 11 (7/8-inch-long crack stopping short of cored hole).
 - o Span 5, girder 4, near floor beam 3 (1 1/8-inch-long crack arrested by a cored hole, 1-inch-long crack arrested by a cored hole).
 - o Span 5, girder 4, near floor beam 11 (1-inch-long crack arrested by a cored hole).
 - o Span 6, girder 1, near floor beam 3 (both cracks arrested by a cored hole).
 - o Span 6, girder 2, near floor beam 3 (1-inch-long crack arrested by a cored hole).
 - o Span 6, girder 3, near floor beam 3 (Paint crack originally noted, no cracks found during 2024 OS ins).
 - o Span 6, girder 4, near floor beam 3 (1 1/4-inch-long crack arrested by a cored hole).

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ND

INSPECTION NOTES: 7/19/24

2024/07/19 Other/Special inspection notes (no significant changes to OS items as noted in the Structure Notes)

PX – Recommendations:

- Replace missing north railing posts in span 8 and near pier 12.
- Splice gaps in metal rail of north railing in spans 5 and 9, and south railing in spans 1, 6 and 7.
- Patch spalls in driving surface.
- Repair cracks in bearing boxes at modular joint supports at pier 3. Also, install shim plates or additional support below bearing blocks to prevent future cracking.
- Replace modular joints at pier 3, 6 and 10.
- Replace pourable joint seals at west abutments and fixed joints and deck control joints.
- Replace strip seal expansion joint at east abutment.
- Replace missing or loose bolts at stringer connections.
- Arrest ends of cracks at:
 - o Floor beam 4, span 4 under stringer 1.
 - o Floor beam 4, span 4 under stringer 3.
 - o Stringer diaphragm over floor beam 5, span 6, between stringers 3 and 4.
- Repair cracks in lateral bracing gusset plates:
 - o Span 8, girder 3 at floor beam 4 – 8 1/2-inch-long crack in gusset plate.
 - o Span 10, girder 2 at floor beam 2 – 18-inch-long crack in gusset plate weld.
 - o Span 11, girder 2 at floor beam 1 – 8-inch-long and 6-inch-long cracks in gusset plate weld.
- Reattach lateral bracing vibration dampeners at:
 - o Span 4, between girders 1 and 2, between floor beams 5 and.
 - o Span 4, between girders 1 and 2 and between floor beams 8 and pier.
 - o Span 4, between girders 3 and 4 and between floor beam 7 and pier.
 - o Span 6, between girders 1 and 2 and between floor beams 2 and 3.
 - o Span 6, between girders 3 and 4 and between floor beam 0 and 1 – fractured spring not functioning as intended.
 - o Span 6, between girders 3 and 4 and between floor beam 6 and pier 6.
- Patch corrosion hole through lateral bracing gusset plate at girder 4, span 6 at pier 6
- Install crushed aggregate slope protection on east approach embankment.
- Remove debris from east abutment seat.
- Patching spall in pier 6, column 2 at top of web wall.
- Repair broken seismic cables at piers 6 and 10.
- Reset elastomeric bearings for girder 1 for span 4 at pier 3.
- Remove paint from stainless-steel sliding surfaces of elastomeric bearings at pier.

FX – Monitor:

- Terminations of horizontal web splices for crack propagation or initiation in spans 4 through 6.
- Lateral bracing gusset plate connections to web of girders at:
 - o Span 10, girder 1 at floor beam 4
 - o Span 11, girder 1 at floor beam 1
 - o Span 11, girder 2 at floor beam 3
 - o Span 12, girder 2 at floor beam 1
 - o Span 12, girder 2 at floor beam 2
- Ends of prestressed concrete beams for deterioration associated with exposed strands.

ELEMENT CONDITION STATE DATA

| Elem. / Env | Description | Unit | Total Qty | % 1 | Qty. 1 | % 2 | Qty. 2 | % 3 | Qty. 3 | % 4 | Qty. 4 |
|--|--------------------------|-------|------------|-----|-----------|------|------------|-----|--------|-----|--------|
| 12 / 4 | Re Concrete Deck | sq.ft | 119,340.00 | 70% | 83,518.00 | 30% | 35,802.00 | 0% | 20.00 | 0% | 0.00 |
| PX – Small spalls and patches are typical along control joints. Deck offset 1in relative to east approach railing. Deck in new portion (spans 1 through 3 and 70ft in span 4) has transverse cracks of 0.020in spaced at 3ft to 5ft, span 3 has 0.030in diagonal cracking. Deck in original portion has transverse cracks of 0.050in spaced at 5ft to 10ft. Raised pavement markers is missing. | | | | | | | | | | | |
| 107 / 4 | Steel Opn Girder/Beam | ft | 5,540.00 | 91% | 5,036.00 | 9% | 500.00 | 0% | 4.00 | 0% | 0.00 |
| FX – Cracks at horizontal web splice terminations in spans 4 near floor beam 5, and span 6 near floor beam 3 (Item 872 has splice terminations for span 5 near floor beams 3 and 11). Cracks or cored hole noted at: <ul style="list-style-type: none"> • Span 4, girder 2, near floor beam 5 (1-inch-long crack arrested by cored hole, 1 1/2-inch-long crack arrested by cored hole). • Span 4, girder 4, near floor beam 5 (7/16-inch-long crack arrested by a cored hole). • Span 6, girder 1, near floor beam 3 (both cracks arrested by a cored hole). • Span 6, girder 2, near floor beam 3 (1-inch-long crack arrested by a cored hole). • Span 6, girder 3, near floor beam 3 (Paint crack originally noted). • Span 6, girder 4, near floor beam 3 (1 1/4-inch-long crack arrested by a cored hole). FX – Lateral bracing gusset plates welded to girders web using backing bars. Cored holes through web with no visible crack at: <ul style="list-style-type: none"> • Span 10 (girder 1, FB 4). • Span 11 (girder 1, FB 1 and girder 2, FB 3). • Span 12 (girder 2, FB 1 and girder 2, FB 2). Lateral bracing gusset plates at FBs 3, 4, 5 in spans 4 and 6, and FBs 3 and 11 in span 5 have been flame-cut and reattached to the girder webs. Pack rust exists at isolated locations in the main girders between horizontal splices, FB connections, bolted bottom splice plates. Paint cracks at stiffener to girder bottom flange at span 6 (girder 3 at FB4 and girder 4 at FB7) and span 8 (girder 3 at FB 2 and girder 4 at FB 2). Welded nut or open hole on inboard web splice plate at span 5 (girder 1 near FB 9, and girder 3 near FBs 3, 5, and 9). Loose bolts in web splice plate at span 7 (girder 3 near FB 4) and span 11 (girder 4 near FB 4). | | | | | | | | | | | |
| 515 / 4 | Steel Protective Coating | sq.ft | 330,000.00 | 0% | 0.00 | 100% | 330,000.00 | 0% | 0.00 | 0% | 0.00 |

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| Paint is sound with corrosion reactivating under joints. | | | | | | | | | | | | |
| 109 / 4 | Pre Opn Conc Girder/Beam | ft | 4,140.00 | 100% | 4,140.00 | 0% | 0.00 | 0% | 0.00 | 0% | 0.00 | |
| No significant deficiencies were observed to the prestressed concrete girders in spans 1-3. | | | | | | | | | | | | |
| 113 / 4 | Steel Stringer | ft | 5,540.00 | 98% | 5,437.00 | 2% | 100.00 | 0% | 3.00 | 0% | 0.00 | |
| PX – Bolts and/or nuts are missing in the stringer to floor beam connections at the following locations: <ul style="list-style-type: none">Span 6, west side of floor beam 1, stringer 1 exhibits two missing bolts.Span 6, east side of floor beam 1, north face of stringer 3 exhibits a missing nut.Span 7, floor beam 2, stringer 4 exhibits four not fully seated bolts.Span 8, west face of floor beam 0, stringer 3 exhibits one missing bolt.Span 10, floor beam 0, stringer 3 exhibits four missing bolts. PX – Stringer diaphragm between stringers 3 and 4, span 6 over FB 5 has an 8 1/2in crack. Deck is lifting off stringers at several locations. Stringer 2 diaphragm connection to FB 1, span 8 has 5 of 7 connection bolts not fully seated. Stringer 3 connection to FB 1, span 6 has 1 broken bolt. Stringer 3 connection to FB at pier 9 is missing all 4 anchor bolts. Stringer 4 connection to FB 2, span 8 has all 4 bolts not fully seated. Stringers in span 7 near pier 6 and span 13 near the east abutment have a partial length welded cover plates. | | | | | | | | | | | | |
| 152 / 4 | Steel Floor Beam | ft | 3,536.00 | 0% | 0.00 | 95% | 3,346.00 | 5% | 190.00 | 0% | 0.00 | |
| PX – Span 4 FB 4 has cracks in web at top of vertical stiffener below stringer 1 (1 5/8in and 1 1/2in) and below stringer 3 (2 3/8in) Member Alignment – Span 6 FB 4 has 1/4in sweep between girders 1 and 2. Span 6 FB 2 between G3 and G4 has 11/16in local kink in lower strut. Span 9 FB 0 at G4 has slight bow in lower gusset plate. Painted over pack rust between FB components below deck control and expansion joints. Loose bolts or oversized bolt holes at FB to girder connections at span 5 FB 8 G3, span 7 FB 3 G2, span 9 FB4 G2, span 11 FB 3 at G2 and Span 12 FB 2 at G2. | | | | | | | | | | | | |
| 205 / 4 | Re Conc Column | each | 23.00 | 87% | 20.00 | 9% | 2.00 | 4% | 1.00 | 0% | 0.00 | |
| PX – Pier 6, column 2 exhibits a 5-square-foot spall with exposed and corroding reinforcing steel at the top of the web wall and a vertical crack extending the full height of the column. | | | | | | | | | | | | |
| 210 / 4 | Re Conc Pier Wall | ft | 94.00 | 68% | 64.00 | 32% | 30.00 | 0% | 0.00 | 0% | 0.00 | |
| Minor hairline cracking exists in the concrete pier walls. Pier 4 exhibits some water staining and vertical cracks to the stem wall. | | | | | | | | | | | | |
| 215 / 4 | Re Conc Abutment | ft | 152.00 | 97% | 147.00 | 3% | 4.00 | 1% | 1.00 | 0% | 0.00 | |
| PX – E abutment breastwall has undermining with 20in of penetration beneath flowable fill (2013 repair) and no exposure of piles. Erosion is from leaking joint. PX – Debris and ponding on E abutment seat (9in deep under median). Erosion exists under remnants of the original W abutment with no affect to the replaced abutment . Both abutments exhibit random hairline cracking with E abutment cracks up to 0.020in wide. | | | | | | | | | | | | |
| 234 / 4 | Re Conc Pier Cap | ft | 837.00 | 33% | 280.00 | 66% | 550.00 | 1% | 7.00 | 0% | 0.00 | |
| PX – Seismic restraints, consisting of cable anchorages attached between the girder bottom flanges and the pier caps, exist at piers 6 and 10. The cable anchorages for girders 1 through 4 at piers 6 and 10 are broken and are no longer functioning. Pier 6 cap exhibits rust staining throughout and a spall to the bottom east edge of the south cantilever. Pier 1 cap on the west and east faces at the south end exhibits 1/16-inch-wide x 20-foot-long crack 2-feet from the top. Pier 10 cap exhibits a wide crack and delamination to the bottom west edge between girders 3 and 4. | | | | | | | | | | | | |
| 300 / 4 | Strip Seal Exp Joint | ft | 69.00 | 0% | 0.00 | 0% | 0.00 | 0% | 0.00 | 100% | 69.00 | |
| Joint at east abutment. PX – The pourable expansion joint seal at the west abutment is missing the joint seal for 10 feet in the westbound lanes and throughout the southern lane of the eastbound lanes. Seal is missing in EB lane. | | | | | | | | | | | | |
| 303 / 4 | Assem Jnt With Seal | ft | 207.00 | 0% | 0.00 | 0% | 0.00 | 0% | 0.00 | 100% | 207.00 | |
| PX – Modular joints have fractured or missing components, joint seals bulging/torn, joints closed: Joint 3 Box 2 – W box unsupported. Box 3 – Both box bottom plates fractured and missing under W end of support bar 1. Bearing blocks dislodged. Support bar 2 partially unsupported. Box 4 – Both box bottom plates fractured. Boxes 5 and 6 – E bearing block for support bar 1 missing. Box 7 – W box bottom plate fractured under support bar 1. Box 8 – W box supported by 6 3/4in over 22 1/4in length. Box 15 – W box bottom plate fractured under support bar 1. Composite reinforcing above beam 10 exposed and box unsupported. Box 16 – W box bottom plate fractured away, support bar 1 unsupported. E box bottom plate fractured. Box 17 – W box not fully supported. Joint 6 – equidistant bars broken/dislodged/missing, transverse separation beam sagging. Box 2 – Transverse separation beam and repair plate fractured, sagging 1in. Longitudinal support bar broken free from transverse beam, dislodged. Box 3 – Longitudinal support bar broken free from transverse beam, dislodged. Joint 10 – equidistant bars broken/dislodged/missing. Box 3 – Longitudinal support bar broken free from transverse beam, dislodged. | | | | | | | | | | | | |
| 310 / 4 | Elastomeric Bearing | each | 76.00 | 97% | 74.00 | 0% | 0.00 | 3% | 2.00 | 0% | 0.00 | |
| PX – Span 4 bearings at pier 3, sliding surface painted: G 1 – Sheared 2in east, bearing split at N end of E face, debonded from sole plate at S end of E face, rotated with S end 3 1/2in E and N end 1 5/8in E, PTFE pad distorted, near limits of expansion at 91F. G 2 – Bulging along E edge, bottom edge lifting from pier seat. G 3 – Bearing assembly not parallel with girder (offset 5/16in over 16 5/8in length). | | | | | | | | | | | | |
| 311 / 4 | Moveable Bearing | each | 24.00 | 0% | 0.00 | 63% | 15.00 | 38% | 9.00 | 0% | 0.00 | |

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| 17051 | | 6822 0000 X | | -1 | | 82.90 | | | | | |
| <p>Bearings at piers 4, 6, 10 and E abutment have pack rust between rocker and masonry plate. Several bearing bolts broken/missing, pier 6 and E abutment worst.</p> <p>E abutment bearings have corrosion with 1/16in painted over pitting, anchor bolts bent/broken from over expansion.</p> <p>E abutment G1 and G2 bearings shifted 1in N, G3 and G4 bearings shifted 1in S.</p> <p>Span 6, pier 6 bearings rocked up to 14 degree in expansion at 91F, bearings at pier 10 rocked up to 9 degrees expansion. Pier 4 G3 rocked more than other girders.</p> | | | | | | | | | | | |
| 313 / 4 | Fixed Bearing | each | 24.00 | 0% | 0.00 | 100% | 24.00 | 0% | 0.00 | 0% | 0.00 |
| No significant deficiencies. | | | | | | | | | | | |
| 321 / 4 | Re Conc Approach Slab | sq.ft | 4.00 | 0% | 0.00 | 75% | 3.00 | 25% | 1.00 | 0% | 0.00 |
| <p>E approach slab recently overlaid with asphalt.</p> <p>1/16in longitudinal and map cracks in W approach slab.</p> | | | | | | | | | | | |
| 330 / 4 | Metal Bridge Railing | ft | 3,978.00 | 100% | 3,973.00 | 0% | 0.00 | 0% | 5.00 | 0% | 0.00 |
| <p>PX – Rail for N bridge railing missing adjacent posts in span 8 and near pier 12, spalls with exposed reinforcing at post location at pier 12.</p> <p>PX – Gaps in rail at N railing in spans 5 and 9, S railing in spans 5, 6 and 7.</p> | | | | | | | | | | | |
| 919 / 4 | St.(Rail) Prot. Coat | sq.ft | 7,500.00 | 0% | 0.00 | 100% | 7,500.00 | 0% | 0.00 | 0% | 0.00 |
| aluminum railing. | | | | | | | | | | | |
| 331 / 4 | Re Conc Bridge Railing | ft | 5,967.00 | 0% | 0.00 | 100% | 5,939.00 | 0% | 28.00 | 0% | 0.00 |
| <p>Concrete bridge railing and curb typically exhibits 0.020-inch to 0.030-inch-wide cracking spaced at 3 to 5 feet with leaching and minor rust staining and isolated locations of spalling with exposed reinforcing steel. The worst spalling exists to the south curb in span 6 with a 20-foot-long x up to 6-inch-deep spall.</p> <p>The following locations exhibit minor impact damage with heavy cracking and/or spalling with exposed reinforcement:</p> <p>-North railing in spans 3 and 4.</p> <p>-North face of the median railing over piers 10 and 12.</p> | | | | | | | | | | | |
| 819 / 4 | PS Conc.Gird.End(5Ft) | ft | 360.00 | 99% | 356.00 | 1% | 4.00 | 0% | 0.00 | 0% | 0.00 |
| <p>FX – End of beams spalling with exposed strands:</p> <ul style="list-style-type: none">Span 1, B1 at pier 1 - crack extending 30in up web and spall in bottom flange exposing 9 strands.Span 2, B12 at pier 2 - spall in bottom flange exposing 23 strands.Span 3, B1 at pier 2 – crack extending 31 inches down the web and spall exposing 3 strands.Span 3, B12 at pier 2 – 12in x 4in spall exposing 3 strands.Span 3, B1 at pier 3 - spall in bottom flange exposing 4 strands.Span 3, B12 at pier 3 - spall in bottom flange exposing 26 strands and in web exposing 4 strands. <p>Span 3, B6 at pier 3 bottom flange has the ends of the mild steel anchors for sole plate exposed.</p> | | | | | | | | | | | |
| 859 / 4 | Soffit | each | 1.00 | 0% | 0.00 | 100% | 1.00 | 0% | 0.00 | 0% | 0.00 |
| <p>Original spans exhibits full depth x full width transverse cracking up to 0.030-inch-wide spaced at 2 to 6 feet with minor efflorescence. Cracking is heavier over piers.</p> <p>Shallow spalls exist sporadically adjacent to girder top flanges throughout the main spans.</p> <p>Isolated areas of the soffit overhangs exhibit spalling with exposed reinforcement adjacent to the joints .</p> <p>Soffit between girders 2 and 3 exhibits rust staining and small pop-outs due to shallow cover of reinforcing steel chairs.</p> <p>Soffit below median in span 4 over pier 3 exhibits a 2SF spall with exposed and corroded reinforcing steel.</p> | | | | | | | | | | | |
| 865 / 4 | St.Open Gird End(5Ft) | ft | 120.00 | 50% | 60.00 | 50% | 60.00 | 0% | 0.00 | 0% | 0.00 |
| Pack rust up to 1/2in between connection stiffeners and FB lower strut gusset plates. | | | | | | | | | | | |
| 870 / 4 | Concrete Wingwall | each | 4.00 | 100% | 4.00 | 0% | 0.00 | 0% | 0.00 | 0% | 0.00 |
| No significant deficiencies. | | | | | | | | | | | |
| 872 / 4 | St.Gird Und Const.Jt | ft | 760.00 | 74% | 560.00 | 26% | 200.00 | 0% | 0.00 | 0% | 0.00 |
| <p>FX – Cracks at horizontal web splice terminations in spans 4 near floor beam 5, and span 6 near floor beam 3 (Item 107 has splice terminations for spans 4 and 6). Cracks or cored hole noted at:</p> <ul style="list-style-type: none">Span 5, girder 1, near floor beam 11 (3/4-inch-long crack stopping short of the cored hole, 1 1/4-inch-long crack arrested by two cored holes).Span 5, girder 2, near floor beam 11 (2-inch-long crack arrested by a cored hole).Span 5, girder 3, near floor beam 11 (7/8-inch-long crack stopping short of the cored hole).Span 5, girder 4, near floor beam 3 (1 1/8-inch-long crack arrested by a cored hole, 1-inch-long crack arrested by a cored hole).Span 5, girder 4, near floor beam 11 (1-inch-long crack arrested by a cored hole). | | | | | | | | | | | |
| 877 / 4 | St. Stringer End(5Ft) | ft | 120.00 | 50% | 60.00 | 33% | 40.00 | 17% | 20.00 | 0% | 0.00 |
| <p>Deck is lifting off the stringers at several locations . This separation can eventually cause an uneven riding surface increasing impact on the superstructure and further accelerating deck deterioration.</p> <p>The stringers are generally in good condition with negligible surface corrosion in isolated locations.</p> | | | | | | | | | | | |
| 879 / 4 | St.String.Un Const.Jt | ft | 760.00 | 100% | 760.00 | 0% | 0.00 | 0% | 0.00 | 0% | 0.00 |
| <p>Deck is lifting off the stringers at several locations . This separation can eventually cause an uneven riding surface increasing impact on the superstructure and further accelerating deck deterioration.</p> <p>The stringers are generally in good condition with negligible surface corrosion in isolated locations.</p> | | | | | | | | | | | |
| 890 / 4 | Steel SIP Form | each | 1.00 | 0% | 0.00 | 100% | 1.00 | 0% | 0.00 | 0% | 0.00 |
| Stay-in-place forms typically exhibit areas of surface and laminating corrosion near the interface with the original deck surface. | | | | | | | | | | | |
| 906 / 4 | Sealed Exp.Jt.(SEJ-3 | ft | 69.00 | 100% | 69.00 | 0% | 0.00 | 0% | 0.00 | 0% | 0.00 |
| <p>Joint at pier 1.</p> <p>No significant deficiencies.</p> | | | | | | | | | | | |
| 909 / 4 | Pourable Fix Jt.Seal | ft | 1,311.00 | 0% | 0.00 | 0% | 0.00 | 0% | 0.00 | 100% | 1,311.00 |

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| 17051 | | 6822 0000 X | | -1 | | 82.90 | | | | | |
| <p>Fixed poured seal joints at west abutment, pier 2, and deck control joints.</p> <p>PX – The compression joint seal at the east abutment has pushed through the full length and the joint is closed . The seal in the westbound lanes is impacted with soil and gravel in the inside shoulders and partially covered in a light amount of worn asphalt. The joint header in the westbound lanes exhibits a 24-inch-wide x 9-inch-long x 2-inch-deep spall in the northern lane. The seal in the eastbound lanes is missing causing the joint to leak and allowing drainage to pass onto the bearing seat at the east abutment. The joint armor in the eastbound lanes exhibits a 1/2-inch vertical offset with the bridge side joint armor lower than the approach joint armor .</p> <p>Several of the deck control joints exhibit minor spalling adjacent to the joints and missing joint seals.</p> | | | | | | | | | | | |
| 916 / 4 | St.Bearing Assembly | each | 76.00 | 97% | 74.00 | 0% | 0.00 | 3% | 2.00 | 0% | 0.00 |
| Bearings showed some surface corrosion and some pack rust between assemblies. | | | | | | | | | | | |
| 956 / 4 | St. Cracking/Fatigue | each | 1.00 | 0% | 0.00 | 0% | 0.00 | 100% | 1.00 | 0% | 0.00 |
| <p>FX – Cracks at horizontal web splice terminations in spans 4 near floor beam 5, span 5 near floor beams 3 and 11, and span 6 near floor beam 3. Cracks or cored hole noted at:</p> <ul style="list-style-type: none">Span 4, girder 2, near floor beam 5 (1-inch-long crack arrested by cored hole, 1 1/2-inch-long crack arrested by cored hole).Span 4, girder 4, near floor beam 5 (7/16-inch-long crack arrested by a cored hole).Span 5, girder 1, near floor beam 11 (3/4-inch-long crack stopping short of the cored hole, 1 1/4-inch-long crack arrested by two cored holes).Span 5, girder 2, near floor beam 11 (2-inch-long crack arrested by a cored hole).Span 5, girder 3, near floor beam 11 (7/8-inch-long crack stopping short of the cored hole).Span 5, girder 4, near floor beam 3 (1 1/8-inch-long crack arrested by a cored hole, 1-inch-long crack arrested by a cored hole).Span 5, girder 4, near floor beam 11 (1-inch-long crack arrested by a cored hole).Span 6, girder 1, near floor beam 3 (both cracks arrested by a cored hole).Span 6, girder 2, near floor beam 3 (1-inch-long crack arrested by a cored hole).Span 6, girder 3, near floor beam 3 (Paint crack originally noted).Span 6, girder 4, near floor beam 3 (1 1/4-inch-long crack arrested by a cored hole). | | | | | | | | | | | |
| 957 / 4 | Pack Rust Smart Flag | each | 1.00 | 0% | 0.00 | 0% | 0.00 | 100% | 1.00 | 0% | 0.00 |
| <p>Pack rust up to 1/2-in thick exists between some girder vertical web stiffeners and floor beam truss lower chord gusset plates and between horizontal splice flanges. Minor pack rust up to 1/4-in thick is developing at girder bottom flange splice plates where girder ends butt up against each other. Pack rust is active in many locations and worse at expansion joints.</p> <p>At floor beam 6 in span 4 between girders 3 and 4; 1/2-thick painted over pack rust exists between the floor beam web and the connection plate at girder 4. Similar condition at floor beam 8 at girder 4 over pier 6; in span 6. Similar conditions occur sporadically but with less severity.</p> <p>Floor beam 8 between girders 3 and 4 over pier 6; span 6; the floor beam truss lower chord exhibits pack rust up to 1/2 inch between the center gusset plate and the lower chord angle with 1/16-inch deep section loss to the gusset plate. Similar condition in span 7 over pier 6.</p> <p>FX – Pack rust is typically developing between the diaphragm top flange and the deck soffits ; up to 1/16-inch at random locations.</p> | | | | | | | | | | | |
| 958 / 4 | Concrete Cracking SF | each | 1.00 | 0% | 0.00 | 100% | 1.00 | 0% | 0.00 | 0% | 0.00 |
| <p>Deck in new portion (spans 1 through 3 and 70ft in span 4) has transverse cracks of 0.020in spaced at 3ft to 5ft, span 3 has 0.030in diagonal cracking. Deck in original portion has transverse cracks of 0.050in spaced at 5ft to 10ft.</p> | | | | | | | | | | | |
| 963 / 4 | Steel Section Loss SF | each | 1.00 | 0% | 0.00 | 100% | 1.00 | 0% | 0.00 | 0% | 0.00 |
| <p>Areas of painted over pitting are present throughout the bridge primary members. Minor section loss is active at areas of pack rust, generally at deck joints.</p> | | | | | | | | | | | |
| 968 / 4 | Erosion SF | each | 1.00 | 100% | 1.00 | 0% | 0.00 | 0% | 0.00 | 0% | 0.00 |
| <p>PX – E abutment breastwall has undermining with 20in of penetration beneath flowable fill (2013 repair) and no exposure of piles. Erosion is from leaking joint.</p> <p>Erosion exists under remnants of the original W abutment with no affect to the replaced abutment .</p> | | | | | | | | | | | |
| 969 / 4 | OutOfPlane Dist./Load | each | 1.00 | 0% | 0.00 | 100% | 1.00 | 0% | 0.00 | 0% | 0.00 |
| <p>Member Alignment – Span 6 FB 4 has 1/4in sweep between girders 1 and 2.</p> | | | | | | | | | | | |
| 974 / 4 | Straight Gird.Diaphr | each | 1.00 | 0% | 0.00 | 100% | 1.00 | 0% | 0.00 | 0% | 0.00 |
| <p>PX – Stringer diaphragm between stringers 3 and 4, span 6 over FB 5 has an 8 1/2in crack.</p> <p>Pack rust exists between diaphragm top flange and deck soffit .</p> | | | | | | | | | | | |

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Photograph 1 - Looking west at the bridge end view.

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Photograph 2 - Looking north at the bridge elevation.

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Photograph 3 - Looking northwest at the underside of the pier 3 modular joint at support box 2. Note: bottom plate of the support box is unsupported along the end of span 3. Equidistant spring between support bars is missing (typical).

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Photograph 4 - Looking south at the underside of the pier 3 modular joint at support box 3. Note: bottom plate of support box has fractured and west end of support bar 1 is unsupported. Bearing block for support bar 2 is dislodged. Stainless steel sliding surface is painted.

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Photograph 5 - Looking south at the underside of the pier 3 modular joint at support box 4. Note: the bottom plate of support box has fractured and support bar 2 is unsupported.

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Photograph 6 - Looking east at the underside of the pier 3 modular joint at support box 5. Note: bearing block for support bar 1 is missing at the east support box. Stainless steel sliding surface is painted (typical).

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Photograph 7 - Looking west at the underside of the pier 3 modular joint at support box 7. Note: bottom plate of bearing support box is fractured adjacent to vertical plate.

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Photograph 8 - Looking south at the underside of the pier 3 modular joint at support box 14. Note: stainless steel plate welded to support bar 1 at the east bearing box has fractured near the west weld.

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Photograph 9 - Looking west at the underside of the pier 3 modular joint at support box 15. Note: bottom plate of the west support box is fractured beneath the bearing block for support bar 1. Composite reinforcing extending up from beam 10 is exposed with no concrete under support box.

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Photograph 10 - Looking north at the underside of the pier 3 modular joint at support box 16. Note: bottom plate of the west support box has fractured away and the west end of support bar 1 is unsupported.

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Photograph 11 - Looking south at the underside of the pier 6 modular joint at support box 3. Note: support bar has broken away from the transverse separation beam.

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Photograph 12 - Looking north at the underside of the pier 6 modular joint at support box 10. Note: support bar has broken away from the transverse separation beam.

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Photograph 13 - Looking south at the underside of the pier 10 modular joint at support box 4. Note: support beam is no longer attached to the transverse support beam and has slid east. South equidistant bar has broken and is wedged in place.

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Photograph 14 - Looking northwest at span 4, floor beam 4, stringer 1. Note: no change to 1 1/2-inch-long horizontal crack.

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Photograph 15 - Looking southwest at span 4, floor beam 4, stringer 1. Note: no change to 1 5/8-inch-long horizontal crack.

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Photograph 16 - Looking east at span 4, floor beam 4, stringer 1. Note: cracks have now daylighted through to west face of web.

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Photograph 17 - Looking northwest at span 4, floor beam 4, stringer 2. Note: no change to 1 1/8-inch-long horizontal crack.

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Photograph 18 - Looking southwest at span 4, floor beam 4, stringer 2. Note: no change to 3/4-inch-long horizontal crack.

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Photograph 19 - Looking east at span 4, floor beam 4, stringer 2. Note: cracks have now daylighted through to west face of web.

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Photograph 20 - Looking southwest at span 8, girder 3 at floor beam 4. Note: no change to 8 1/2-inch-long crack in gusset plate.

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Photograph 21 - Looking north at span 10, girder 2 at floor beam 2. Note: no change to 18-inch-long crack in gusset plate weld.

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Photograph 22 - Looking east at span 11, girder 2 at floor beam 1. Note: no change to 6-inch-long crack at the lower lateral bracing gusset plate.

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Photograph 23 - Looking northeast at span 4, girder 2, near floor beam 5 at end of longitudinal bolted web splice. Note: no change to upper web 1-inch-long crack or 1 1/2-inch-long crack.

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Photograph 24 - Looking northeast at span 4, girder 4, near floor beam 5. Note: no change to 7/16-inch-long crack. No crack apparent in upper web, preventative arrestor hole drilled.

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Photograph 25 - Looking south at span 5, girder 1, near floor beam 11. Note: no change to 3/4-inch-long crack or 1 1/4-inch-long crack.

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Photograph 26 - Looking north at span 5, girder 2, near floor beam 11. Note: no change to 2-inch-long crack.

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Photograph 27 - Looking south at span 5, girder 3, near floor beam 11. Note: no change to 7/8-inch-long crack.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 28 - Looking north at span 5, girder 4, near floor beam 3. Note: no change to 1 1/8-inch-long crack or 1-inch-long crack.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 29 - Looking northeast at span 5, girder 4, near floor beam 11. Note: no change to 1-inch-long crack.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 30 - Looking south at span 6, girder 1, near floor beam 3. Note: no change to both cracks arrested by a cored hole.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 31 - Looking north at span 6, girder 2, near floor beam 3. Note: no change to 1-inch-long crack.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 32 - Looking south at span 6, girder 3 near floor beam 3. Note: paint cracks previously noted, no cracks found.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 33 - Looking north at span 6, girder 4, near floor beam 3. Note: no change to the 1 1/4-inch-long crack.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 34 - Looking south at span 6, girder 3 field splice near floor beam 11. Note: missing bolt.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 35 - Looking south at span 5, girder 3 field splice near floor beam 9. Note: missing bolt with localized deformation.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 36 - Looking southeast at span 4, girder bay 1-2 and floor beam bay 5-6. Note: fractured connection bolt at bottom of anti-vibration dampener (typical).

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 37 - Looking north at span 6, girder 3 rocker bearing at pier 6. Note: outboard anchor rod is missing.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 38 - Looking north at beam 1 at the west abutment. Note: southern bolt is separated from abutment.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 39 - Looking northwest at beam 2 at the west abutment. Note: southern bolt is separated from abutment.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 40 - Looking west at beam 4 at the west abutment. Note: both bolts separated from abutment.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 41 - Looking west at beam 9 at the west abutment Note: both bolts disconnected from abutment.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 42 - Looking west at beam 12 at the west abutment. Note: both nuts separated from abutment.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 43 - Looking north at beam 1 bearing at the east abutment. Note: bolts are disconnected at beam 1 bearing north, beam 2 both sides, beam 4 bearing south side.

| NBI # | Structure # | County | Fac. Carried | Fac. Intersected | Insp. Date |
|-------|-------------|----------|--------------|------------------|------------|
| 17051 | 6822 0000 X | Sequoyah | I-40 | ARKANSAS RIVER | 7/19/2024 |



Photograph 44 - Looking west at the west abutment beneath beam 5. Note: undermining up to 20 inches.