

**U.S. Department of
Homeland Security**

**United States
Coast Guard**



Commander
Eighth Coast Guard District

1222 Spruce Street
St. Louis, MO 63103-2832
Staff Symbol: dwb
Phone: (314)269-3792
Email: Giselle.T.MacDnald@uscg.mil

16591.1/392.5 ARW
May 12, 2021

Mr. Brian Linam, PMP
Preconstruction Manager, District 1
Oklahoma Department of Transportation
200 NE 21st Street, 1-C6
Oklahoma City, OK 73105

Subj: U.S. 62 Highway Bridge, Mile 392.5, Arkansas Waterway

Dear Mr. Linam:

Your application requesting approval of the location and plans for a proposed bridge at mile 392.5 over the Arkansas Waterway has been approved. Bridge Permit (4-21-8) dated April 14, 2021, is enclosed.

It is essential that we be kept informed of any events that could affect navigation during construction of the bridge. A representative from your office should be specifically instructed to keep our office informed of the progress of construction with particular reference to any and all events that may have an effect on navigation. The information received regarding the work will be disseminated to mariners by radio and written notices.

Your attention is directed to Condition 2 of the permit that requires plans for cofferdams, temporary causeways, work bridges, falsework bents, or any other temporary structures that will be placed in the water to facilitate construction of this bridge to be approved by this office. Construction of this bridge must be accomplished with minimal interference with navigation. To this end, construction schedules, plans and erection schemes should be submitted to us at the earliest possible time so that approval can be assured prior to commencing work.

Navigation lights and reflectors for the bridge are prescribed in enclosure (2). The lights, prescribed in accordance with Title 33, Code of Federal Regulations, Part 118, shall be installed at the locations and in the manner shown on the enclosed marked-up copies of the Permit drawings.

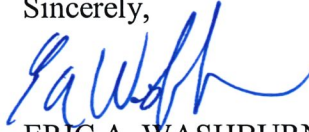
You are required by Condition 5 of the Permit to install vertical clearance gauges. The gauges shall be installed as prescribed in enclosure (2). Enclosure (3) is a copy of Coast Guard instructions for installing vertical clearance gauges on bridges.

Subj: U.S. 62 Bridge, Mile 392.5, Arkansas Waterway

16591.1/392.5, ARW
May 12, 2021

Please acknowledge receipt of the Permit. If you have any questions concerning our jurisdiction or requirements, please contact us at the above referenced extension.

Sincerely,



ERIC A. WASHBURN
Bridge Administrator
By direction of the District Commander

Enclosures: (1) USCG Bridge Permit No. (4-21-8) dtd April 14, 2021
(2) Navigation lights req'd w/marked-up permit dwgs Sheet (2) of 4
(3) Vertical clearance gauge instruction sheet

Copy: CDRUSAED Tulsa District w/encl (1)
USCGC MUSKINGUM
CG SECTOR Lower Mississippi River



BRIDGE PERMIT

(4-21-8)

APR 14 2021

WHEREAS by Title V of an act of Congress approved August 2, 1946, entitled "General Bridge Act of 1946," as amended (33 U.S.C. 525-533), the consent of Congress was granted for the construction, maintenance and operation of bridges and approaches thereto over the navigable waters of the United States;

AND WHEREAS the Secretary of Homeland Security has delegated the authority of Section 502(b) of that act to the Commandant, U.S. Coast Guard by Department of Homeland Security Delegation Number: 0170.1;

AND WHEREAS before construction is commenced, the Commandant must approve the location and plans of any such bridge and may impose any specific conditions relating to the construction, maintenance and operation of the structure deemed necessary in the interest of public navigation, such conditions to have the force of law;

AND WHEREAS the Commandant of the Coast Guard has further delegated to the District Commanders, by Section 1.01-60(b) of Title 33, Code of Federal Regulations, authority to issue permits of the construction, reconstruction, or alteration of bridges across navigable waters of the United States.

AND WHEREAS the - STATE OF OKLAHOMA - has submitted for approval the location and plans of a bridge to be constructed across the Arkansas Waterway near Muskogee, Muskogee County, Oklahoma;

NOW THEREFORE, This is to certify that the location and plan sheets 1, 2, 3, and 4 (of 4), dated 24 February 2021 are hereby approved by the Commander, Eighth Coast Guard District, subject to the following conditions:

1. No deviation from the approved plans may be made either before or after completion of the structure unless the modification of said plans has previously been submitted to and received the approval of the District Commander.

2. The construction of falsework, cofferdams or other obstructions, if required, shall be in accordance with plans submitted to and approved by the District Commander, prior to construction of the bridge. All work shall be so conducted that the free navigation of the waterway is not unreasonably interfered with and the present navigable depths are not impaired. Timely notice of any and all events that may affect navigation shall be given to the District Commander during construction of the bridge. The channel or channels through the structure shall be promptly cleared of all obstructions placed therein or caused by the construction of the bridge to the

APR 14 2021

P(4-21-8)

satisfaction of the District Commander, when in the judgment of the District Commander the construction work has reached a point where such action should be taken, but in no case later than 90 days after the bridge has been opened to traffic.

3. Issuance of this permit does not relieve the permittee of the obligation or responsibility for compliance with the provisions of any other law or regulation as may be under the jurisdiction of any federal, state or local authority having cognizance of any aspect of the location, construction or maintenance of said bridge.

4. The pier protection fender system shall be maintained as shown on the approved plan sheets 2, 3, and 4 (of 4), dated 24 February 2021 for the safety of navigation.

5. Clearance gauges shall be installed and maintained in a good and legible condition by and at the expense of the owner of the bridge when so required by the District Commander. The type of gauges and the locations in which they are to be installed will be submitted to the District Commander for approval.

6. All parts of the existing, to-be-replaced U.S. 62 Bridge across the Arkansas River, Mile 392.5, not utilized in the new bridge shall be removed down to or below a minimum of two feet below the natural ground line. The waterway shall be cleared to the satisfaction of the District Commander. A period of 90 days subsequent to the opening to traffic of the new bridge, mile 392.5, will be allowed for such removal and clearance. The proposed method and schedule for demolishing the existing bridge shall be submitted to the District Commander, for approval prior to commencing such removal.

7. When the proposed bridge is no longer used for transportation purposes, it shall be removed in its entirety or to an elevation deemed appropriate by the District Commander and the waterway cleared to the satisfaction of the District Commander. Such removal and clearance shall be completed by and at the expense of the owner of the bridge upon due notice from the District Commander.

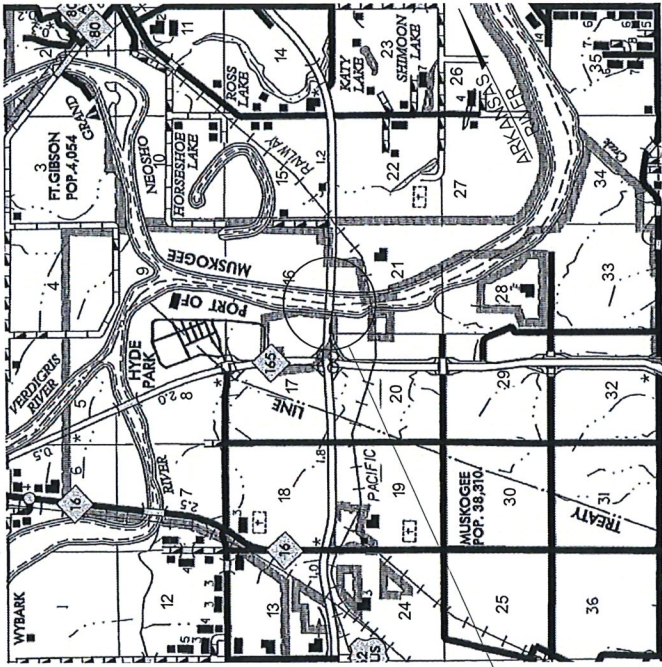
8. The approval hereby granted shall cease and be null and void unless construction of the bridge is commenced within three years and completed within five years after the date of this permit.



JOHN P. NADEAU
Rear Admiral, U.S. Coast Guard
Commander, Eighth Coast Guard District

NO.	DATE	REVISIONS

R 18 E | R 19 E

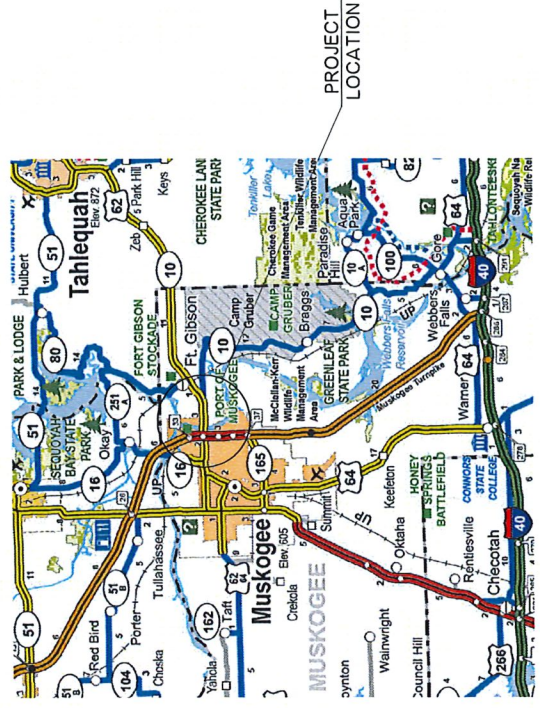


PROJECT LOCATION

PROJECT LOCATION MAP



VICINITY MAP



PROJECT LOCATION



STATE MAP

PREPARED BY: **Colin J. Osborne**
 PROFESSIONAL ENGINEER
 COLIN J. OSBORNE
 28477
 OKLAHOMA

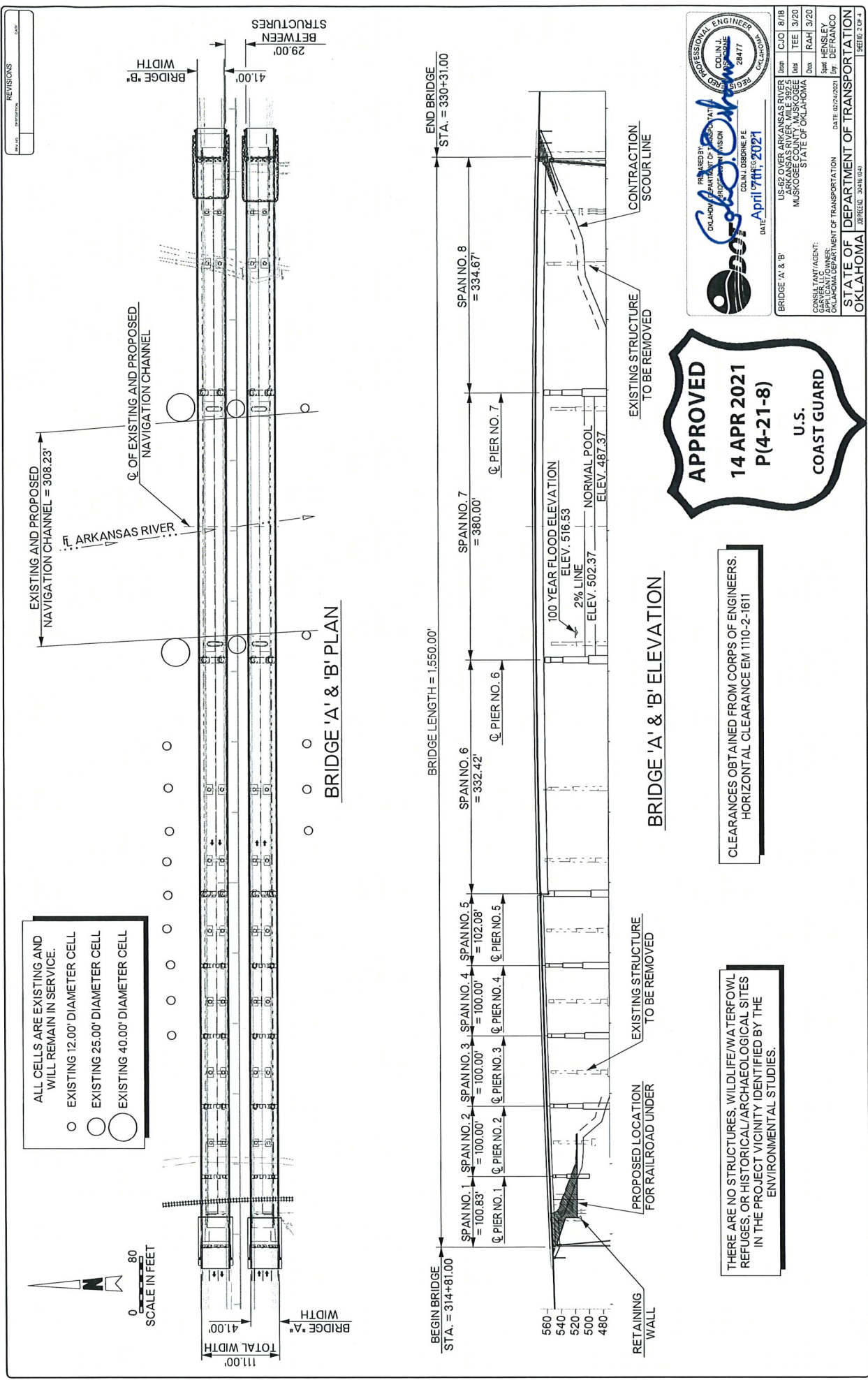
DATE: **April 7th, 2021**

BRIDGE: A & B'
 US-62 OVER ARKANSAS RIVER
 MUSKOGEE COUNTY, MUSKOGEE
 STATE OF OKLAHOMA

CONSULTANT/AGENT:
 HENSLEY
 APPLICANT/OWNER:
 OKLAHOMA DEPARTMENT OF TRANSPORTATION

DATE: 07/24/2021
 BY: DEFRANCO

STATE OF OKLAHOMA
 DEPARTMENT OF TRANSPORTATION
 OKLAHOMA



APPROVED
 14 APR 2021
 P(4-21-8)
 U.S.
 COAST GUARD

OKLAHOMA PROFESSIONAL ENGINEER
 COLIN L. OSBORNE
 28477

DATE: April 14th, 2021

BRIDGE 'A' & 'B'

US-62 OVER ARKANSAS RIVER
 MUSKOGEE COUNTY, OKLAHOMA
 STATE OF OKLAHOMA

CONSULTANT/AGENT:
 OKLAHOMA DEPARTMENT OF TRANSPORTATION

DATE: 02/24/2021

BY: HENSLY DEFRANCO

OKLAHOMA DEPARTMENT OF TRANSPORTATION

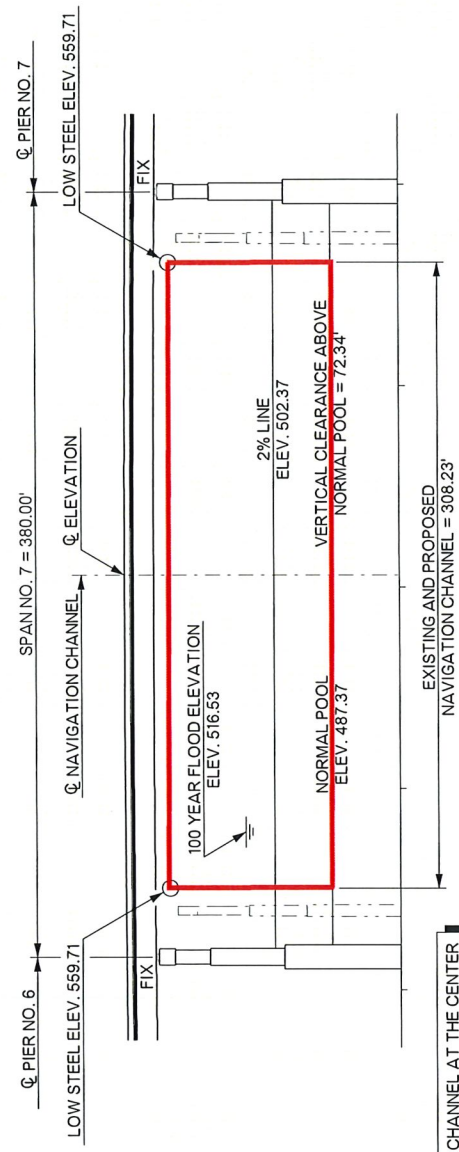
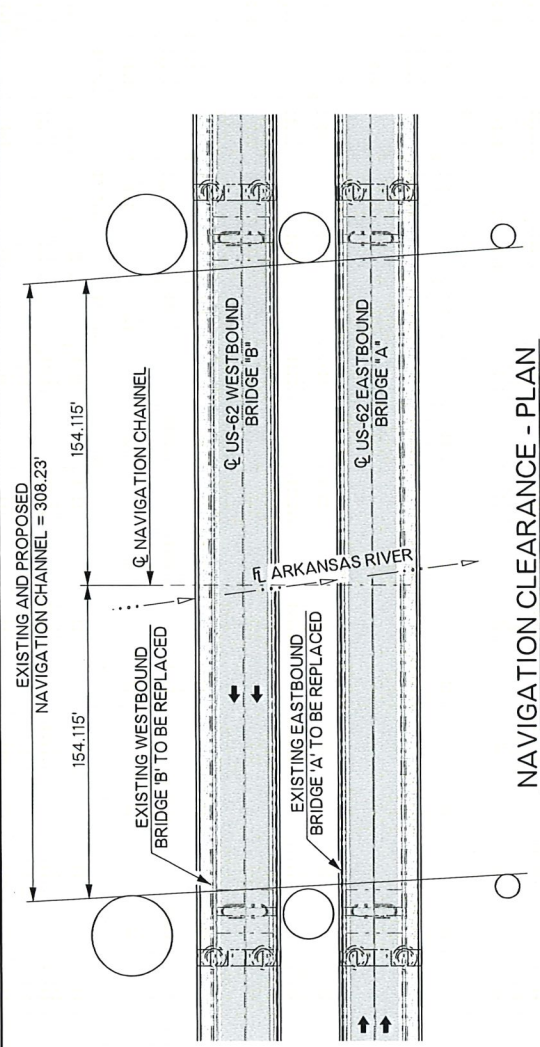
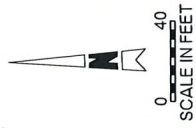
OKLAHOMA

SECTION 2 OF 4

CLEARANCES OBTAINED FROM CORPS OF ENGINEERS.
 HORIZONTAL CLEARANCE EM 1110-2-1611

THERE ARE NO STRUCTURES, WILDLIFE/WATERFOWL
 REFUGES, OR HISTORICAL/ARCHAEOLOGICAL SITES
 IN THE PROJECT VICINITY IDENTIFIED BY THE
 ENVIRONMENTAL STUDIES.

NO.	DATE	REVISIONS
1	04/14/2021	ISSUED FOR PERMIT



THE ESTIMATED DEPTH OF THE CHANNEL AT THE CENTER OF THE CHANNEL IS 12.37 FEET. ALL CHANNEL DEPTHS NOTED ABOVE ARE BASED ON THE NORMAL POOL ELEVATION UTILIZING NAVD 88 LEVEL DATUM.

HORIZONTAL CLEARANCE NORMAL TO SAILING LINE = 300FT

NAVIGATION CLEARANCE - ELEVATION

NOTE: LEVEL DATUM IS NAVD88. ELEVATIONS SHOWN FOR NORMAL POOL AND THE 2% FLOWLINE ARE ESTABLISHED BY CONVERTING NFVD TO NAVD88. A CONVERSION FACTOR OF 0.371 HAS BEEN APPLIED TO ELEVATIONS TAKEN FROM USACE MCCLELLAN-KERR ARKANSAS RIVER SYSTEM NAVIGATION CHARTS.

CLEARANCES OBTAINED FROM CORPS OF ENGINEERS.
HORIZONTAL CLEARANCE EM 1110-2-1611

CHANNEL BOTTOM ELEVATION 475.00' AT CENTER
OF CHANNEL ESTIMATED FROM AS-BUILT PLANS.

PREPARED BY: *Colin J. Osborne*

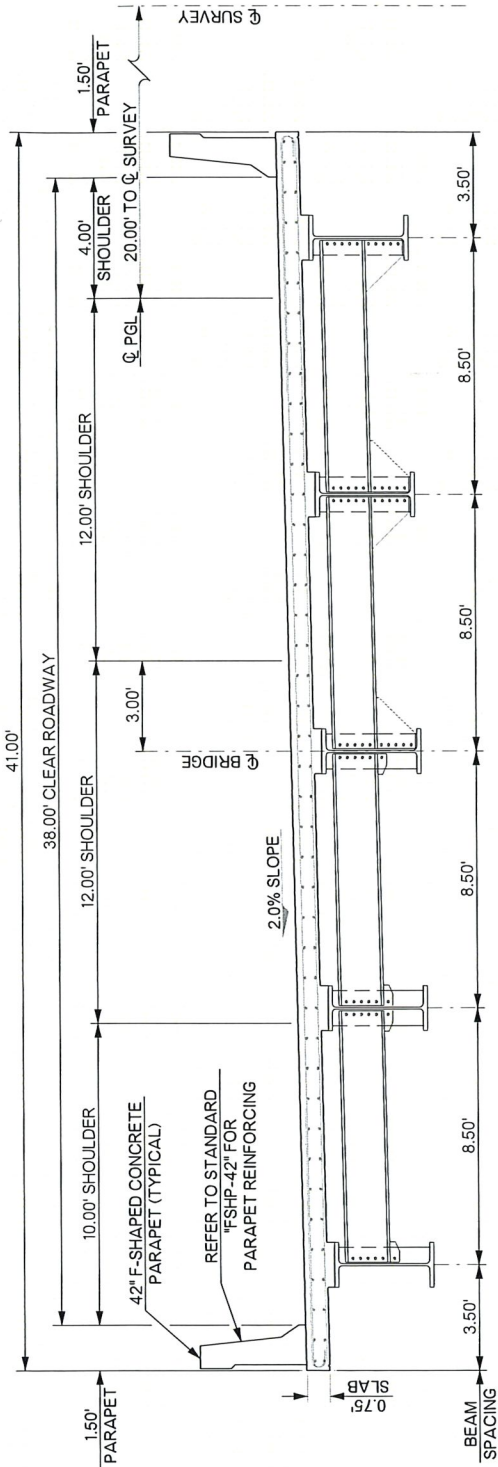
OKLAHOMA DEPARTMENT OF TRANSPORTATION
DIVISION OF HIGHWAYS
28477
OKLAHOMA

DATE: April 7th, 2021

PROFESSIONAL ENGINEER
COLIN J. OSBORNE
28477
OKLAHOMA

BRIDGE 'A' & 'B'	US-62 OVER ARKANSAS RIVER	DATE	8/18
CONSULTANT/AGENT:	OKLAHOMA DEPARTMENT OF TRANSPORTATION	DATE	3/20
APPLICANT/OWNER:	OKLAHOMA DEPARTMENT OF TRANSPORTATION	DATE	3/20
DESIGNED BY:	DAVID HENSLEY	DATE	3/20
CHECKED BY:	DAVID HENSLEY	DATE	3/20
APPROVED BY:	DAVID HENSLEY	DATE	3/20
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	DATE	3/20

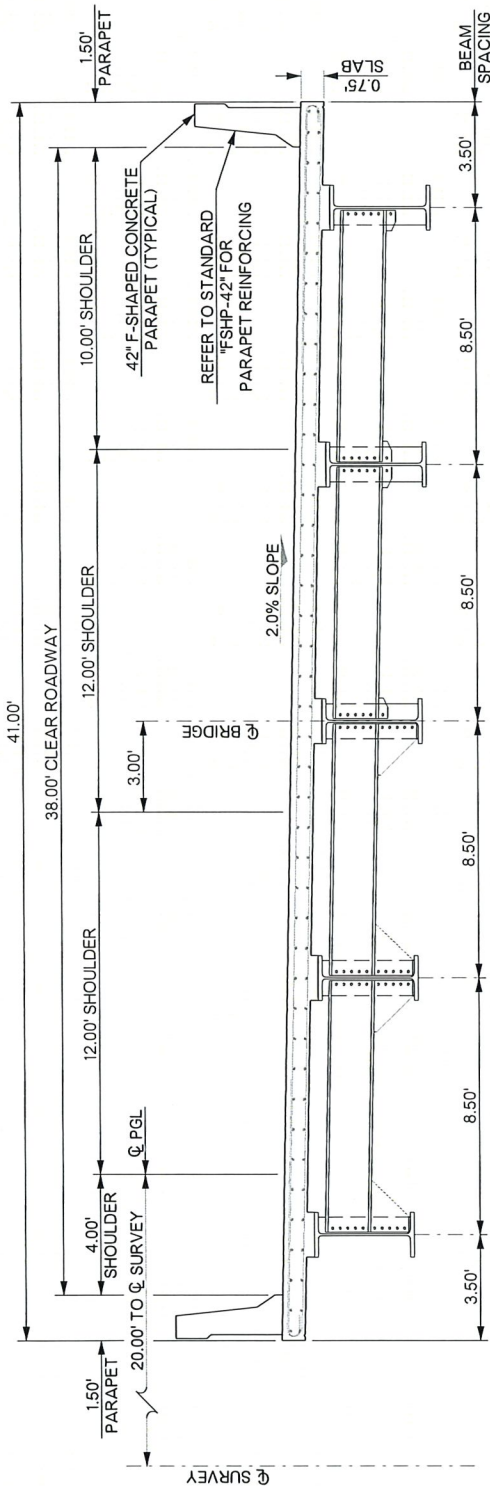
REVISIONS	DATE



HALF SECTION AT END DIAPHRAGM

HALF SECTION AT INTERMEDIATE DIAPHRAGM

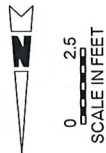
TYPICAL SECTION BRIDGE 'A'
NOTE: ALL DIMENSIONS ARE ALONG THE ϕ BEAM.



HALF SECTION AT INTERMEDIATE DIAPHRAGM

HALF SECTION AT END DIAPHRAGM

TYPICAL SECTION BRIDGE 'B'
NOTE: ALL DIMENSIONS ARE ALONG THE ϕ BEAM.



BRIDGE 'A' & 'B'	US-62 OVER ARKANSAS RIVER	DATE: April 7th, 2021
CONSULTANT AGENT:	ARKANSAS RIVER MILE 392.5	
GARVER LLC	MUSKOGEE COUNTY	
OKLAHOMA DEPARTMENT OF TRANSPORTATION	STATE OF OKLAHOMA	
DATE: 02/24/2021	BY: HENSLEY	
STATE OF DEPARTMENT OF TRANSPORTATION	OKLAHOMA	
28863.01 - 30481.04	28863.01 - 30481.04	






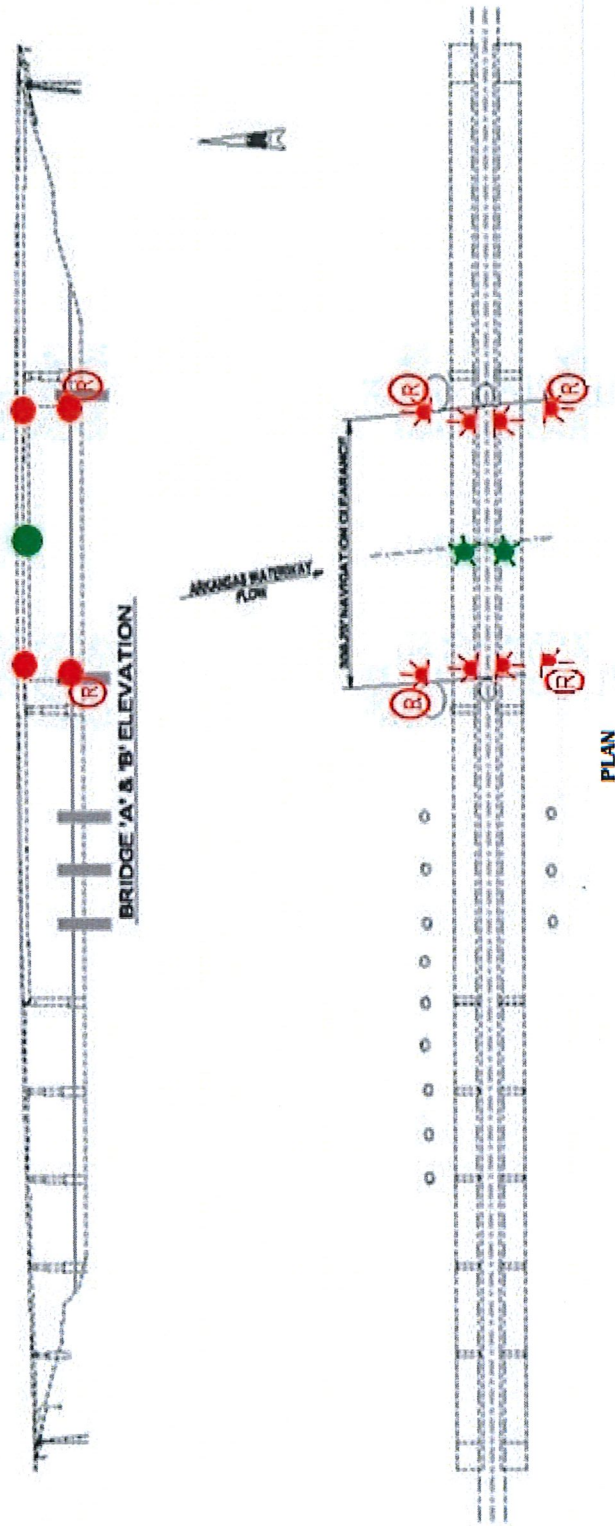
NAVIGATION LIGHTING PLAN

U.S. 62 HIGHWAY BRIDGE

MILE 392.5

ARKANSAS WATERWAY

-  = 360 DEGREE GREEN CHANNEL CENTER LIGHT
-  = 180 DEGREE RED MARGIN OF CHANNEL LIGHT AND PIER PROTECTION CELL LIGHTS
-  = RED RETRO-REFLECTIVE PANEL



Commander
Eighth Coast Guard District
1222 Spruce Street
St. Louis, MO 63103

Navigation lights and/or other signals
Prescribed per Title 33 CFR Part 118

Eric A. Washburn
ERIC A. WASHBURN (Date)
Bridge Administrator

NAVIGATION LIGHTS
FOR
U.S. 62 HIGHWAY BRIDGE
MILE 392.5
ACROSS THE ARKANSAS WATERWAY

The center of the channel shall be marked by a range of two green lights. Each green light shall show through a horizontal arc of 360 degrees and shall be securely mounted just below the outermost edge of the bridge span structure in line with the axis of the channel so as to be visible from an approaching vessel.

The upstream and downstream sides of the channel margins shall be marked with red lights. Each red light shall show through a horizontal arc of 180 degrees and shall be securely mounted no higher than the green navigation lights to show 90 degrees on either side of a line parallel to the axis of the channel so as to be visible from an approaching vessel.

Each pier protection cell shall be marked with a red light. Each red light shall show through a horizontal arc of 180 degrees and shall be securely mounted to the top of the cell on the channelward quadrant of the cell to show 90 degrees on either side of a line parallel to the axis of the channel so as to be visible from an approaching vessel.

The upstream and downstream sides of the channel pier protection cells shall be marked with red, high-intensity grade, retro-reflective square panels which measure 2 feet per side. This retro-reflective material should be placed near the top of the piers on the columns or pier caps so as to effectively reflect the searchlight of an approaching vessel.

The lights prescribed above shall be displayed from sunset to sunrise each night of the year and at other times when the visibility is less than one mile. They shall be of sufficient candlepower as to be visible against the background lighting at a distance of at least 2,000 yards 90% of the nights of the year.

Responsibility of the owner does not cease upon installation of the prescribed lights and reflectors. Provision for maintaining all lights and reflectors prescribed herein should be included in the planning for initial installation. A continuing program of inspection and maintenance is necessary to insure that the lights or retro-reflective material are properly displayed.

This is enclosure (2b) to CCGD8 (dwb) ltr dtd May 12, 2021

VERTICAL CLEARANCE GAUGES
FOR
U.S. 62 HIGHWAY BRIDGE,
MILE 392.5,
ARKANSAS WATERWAY

Vertical clearance gauges shall be installed on the upstream and downstream sides of the right descending channel pier. The E Series, 30-inch standard numerals shall be used unless the width of the piers will not accommodate this size gauge, in which case the largest numeral compatible with the width of the pier shall be used. The line indicating the topmost numeral of the gauge shall be at least 40 feet below low steel of the bridge. The line indicating the bottom numeral shall be at least 60 feet below low steel of the bridge.

The gauges prescribed herein shall read from top to bottom and measure the distance from low steel to the bottom of the foot marks.

Responsibility of the owner does not cease upon installation of the prescribed clearance gauges. Provision for maintaining the clearance gauges prescribed herein should be included in the planning for initial installation. A continuing program of inspection and maintenance is necessary to insure that the clearance gauges are properly displayed.

This is enclosure (3) to CGD8 (dwb) ltr dated May 12, 2021