

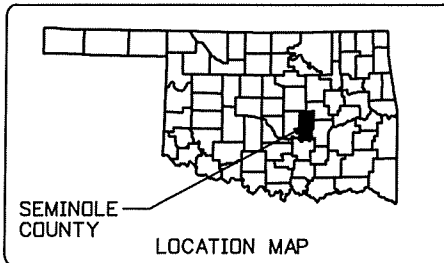
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	0001	44
REVISIONS					
NO.	DATE	DESCRIPTION			

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED
STATE HIGHWAY
FEDERAL AID PROJECT NO. FTP-167B(122)PM
BRIDGE AND APPROACHES
U.S. HIGHWAY 270 OVER WEWOKA CREEK & UPRR RAILROAD
SEMINOLE COUNTY

CONTROL SECTION NO. 270-67-02
STATE JOB NO. 21006(07)
SWO NO. 4879(1)

BRIDGE "A" LOCATION NO. 6702-0880 X
EXISTING NBI NO. 13653 ; NEW NBI NO. 31871
BRIDGE "B" LOCATION NO. 6702-0894 X
EXISTING NBI NO. 13925 ; NEW NBI NO. 31870



FOR INDEX OF SHEETS SEE SHEET NO. 0002.

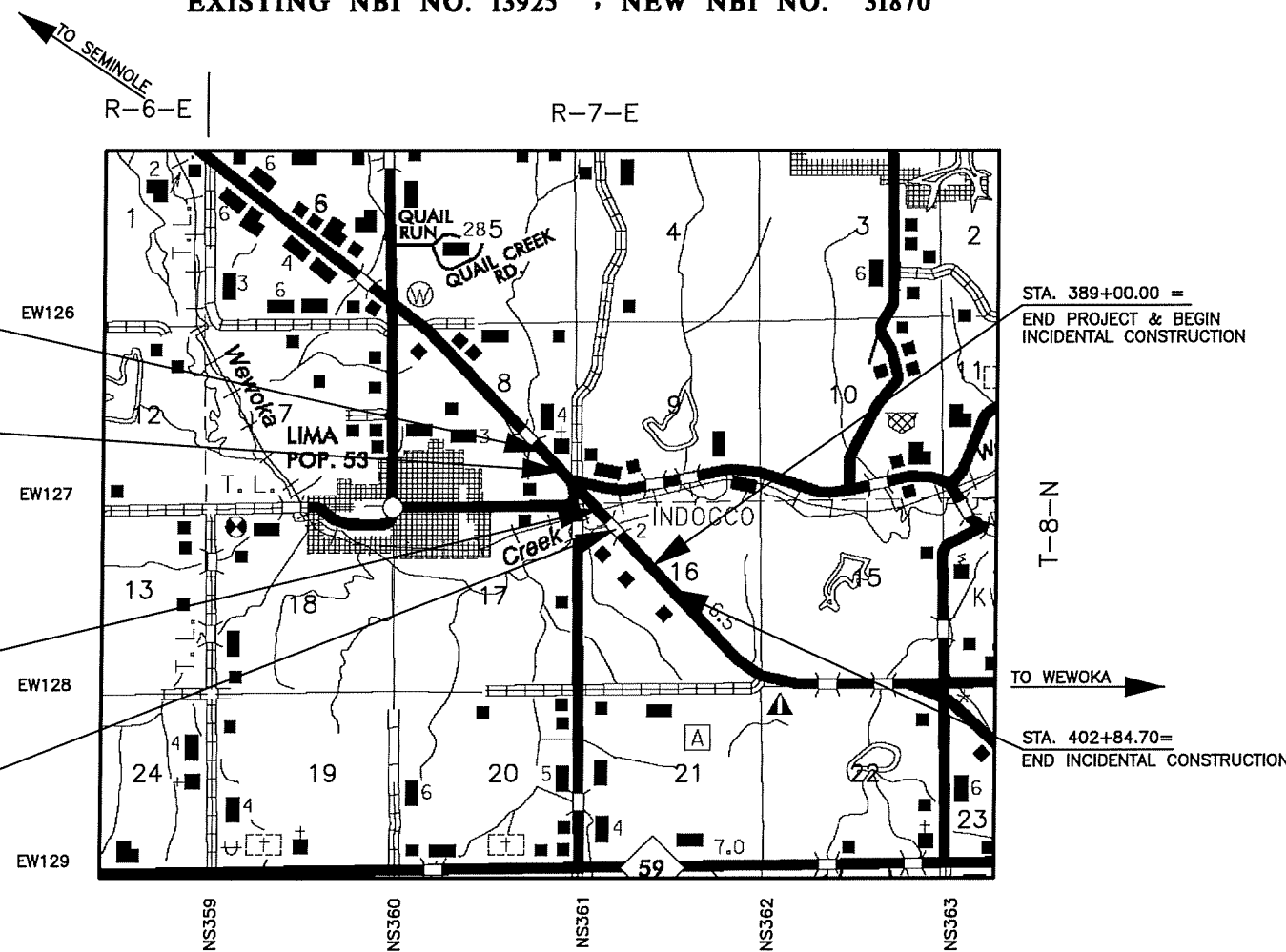
FOR SURVEY CONTROL DATA,
SEE SURVEY DATA SHEETS.

DESIGN DATA	
ADT 2018	= 6767
ADT 2038	= 9292
DHV (2-WAY)	= 530
K (DHV/ADT)	= 10%
D	= 55%
T (% DHV)	= 08%
T (% ADT)	= 12%
T3 (% ADT)	= 5%
V	= 65 MPH
20yr FLEX ESALS	= 3.07M

* SCALES
PLAN 1" = 50'
PROFILE HOR. 1" = 50'
VER. 1" = 5'
LAYOUT MAP 1" = 2640'
* UNLESS OTHERWISE NOTED

CONVENTIONAL SYMBOLS

- PROPOSED ROAD
- RAILROADS
- RANGE & TOWNSHIP
- SECTION LINES
- QUARTER SECTION LINES
- FENCES
- GROUND LINE
- EXISTING ROADS
- BASE LINE
- GRADE LINES
- TELEPHONE & TELEGRAPH
- POWER LINES
- BUILDINGS
- OILWELL
- DRAINAGE STRUCTURES - IN PLACE
- DRAINAGE STRUCTURES - NEW
- RIGHT-OF-WAY LINES - EXISTING
- RIGHT-OF-WAY LINES - NEW
- CONTROLLED ACCESS
- RIGHT-OF-WAY FENCE



STA. 345+05.10 =
BEGIN INCIDENTAL CONSTRUCTION

STA. 358+00.00 =
END INCIDENTAL CONSTRUCTION & BEGIN PROJECT
CONTROL SUB SECTION NO. 8.60

BEGIN BRIDGE CRL STA 368+78.43
BRIDGE LENGTH 242.84'
END BRIDGE CRL STA 371+21.27

BEGIN BRIDGE CRL STA 374+68.00
BRIDGE LENGTH 464.00'
END BRIDGE CRL STA 379+32.00

PROJECT LENGTH BASED ON C.R.L. STATIONING

ROADWAY LENGTH	2,393.16 FT.	0.453 MI.
BRIDGE LENGTH	706.84 FT.	0.133 MI.
PROJECT LENGTH		0.586 MI.

EQUATIONS : NONE
EXCEPTIONS : NONE

2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION-ENGLISH GOVERN, APPROVED BY THE U.S. DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, JANUARY 4, 2010.

 OKLA. REG. NO. 4006	PREPARED BY: CP&Y, INC. 2000 N. CLASSEN BLVD., SUITE 1410 OKLAHOMA CITY, OK 73106 405-848-2346
	DATE: 6.14.2018 DAVID M. NEUHAUSER, P.E. OKLA. REG. NO. 19980
THE FOLLOWING DRAWINGS ARE INTENDED TO BE AUTHENTICATED BY MY SEAL:	
DATE: 6-14-18 DON STEEL, P.E. OKLA. REG. NO. 22570	
THE FOLLOWING DRAWINGS ARE INTENDED TO BE AUTHENTICATED BY MY SEAL:	
OKLAHOMA DEPARTMENT OF TRANSPORTATION	DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION
DATE APPROVED: _____ BY: _____ CHIEF ENGINEER	DATE APPROVED: _____ BY: _____ DIVISION ADMINISTRATOR
SWO 4879(1)	PROJECT NO. 21006(07) SHEET NO. 0001

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	0002	44
REVISIONS					
NO.	DATE	DESCRIPTION			


INDEX OF SHEETS	
SHEET NO.	DESCRIPTION
0001	TITLE SHEET
0002	INDEX OF SHEETS AND ODOT STANDARD DRAWINGS
0003-0005	TYPICAL SECTIONS
AB01	GENERAL NOTES (BRIDGE)
AB02	GENERAL NOTES & SUMMARY OF PAY QUANTITIES (BRIDGE)
AR01	GENERAL NOTES & SUMMARY OF PAY QUANTITIES (ROADWAY)
AR02	GENERAL NOTES (ROADWAY)
AT01	GENERAL NOTES & SUMMARY OF PAY QUANTITIES (TRAFFIC)
AX01-AX03	SUMMARY SHEET
<u>BRIDGE A</u>	
B001	GENERAL PLAN AND ELEVATION
B002	SUBSTRUCTURE STAKING DIAGRAM
B003-B004	SUBSURFACE PROFILE
B005	BRIDGE CONSTRUCTION SEQUENCE
B006	THROW FENCE DETAILS
B007	SUBSTRUCTURE EXCAVATION
B008-B009	ABUTMENT NO. 1 DETAILS
B010-B011	ABUTMENT NO. 2 DETAILS
B012	ABUTMENT DETAILS
B013-B014	PIER DETAILS
B015-B019	SUPERSTRUCTURE DETAILS
B020-B022	P.C. BEAM DETAILS
B023	BEARING DETAILS
B024	APPROACH SLAB NO. 1 DETAILS
B025	APPROACH SLAB NO. 2 DETAILS
B026	APPROACH SLAB DETAILS
B027	SLOPE WALL DETAILS
B028	DRAINS AT END OF BRIDGE
<u>BRIDGE B</u>	
B029-B030	GENERAL PLAN AND ELEVATION
B031	SUBSTRUCTURE STAKING DIAGRAM
B032-B034	SUBSURFACE PROFILE
B035	BRIDGE CONSTRUCTION SEQUENCE
B036	SUBSTRUCTURE EXCAVATION
B037-B038	ABUTMENT NO. 1 DETAILS
B039-B040	ABUTMENT NO. 2 DETAILS
B041	ABUTMENT DETAILS
B042-B043	PIER DETAILS
B044-B048	SUPERSTRUCTURE DETAILS
B049-B051	P.C. BEAM DETAILS
B052	BEARING DETAILS
B053	APPROACH SLAB NO. 1 DETAILS
B054	APPROACH SLAB NO. 2 DETAILS
B055	APPROACH SLAB DETAILS
B056	DRAINS AT END OF BRIDGE
E001	SECTION 404 PERMIT COMPLIANCE
R001	DRAINAGE AREA MAP
R002	STORM WATER MANAGEMENT PLAN
R003-R005	GEOMETRIC LAYOUT
R006-R008	EROSION CONTROL
R009-R010	MASS DIAGRAMS
R011-R017	PLAN AND PROFILE
R018	WICK DRAIN PLAN
R019	WICK DRAIN DETAIL
S001-S047	SURVEY DATA SHEET
T001-T002	TCP TYPICAL SECTIONS
T003	TCP - ADVANCE WARNING SIGN
T004-T006	TRAFFIC CONTROL PLAN - PHASE 1
T007-T009	TRAFFIC CONTROL PLAN - PHASE 2
T010-T012	SIGNING AND STRIPING
X001-X030	CROSS SECTIONS

THE FOLLOWING ODOT STANDARDS SHALL BE REQUIRED:

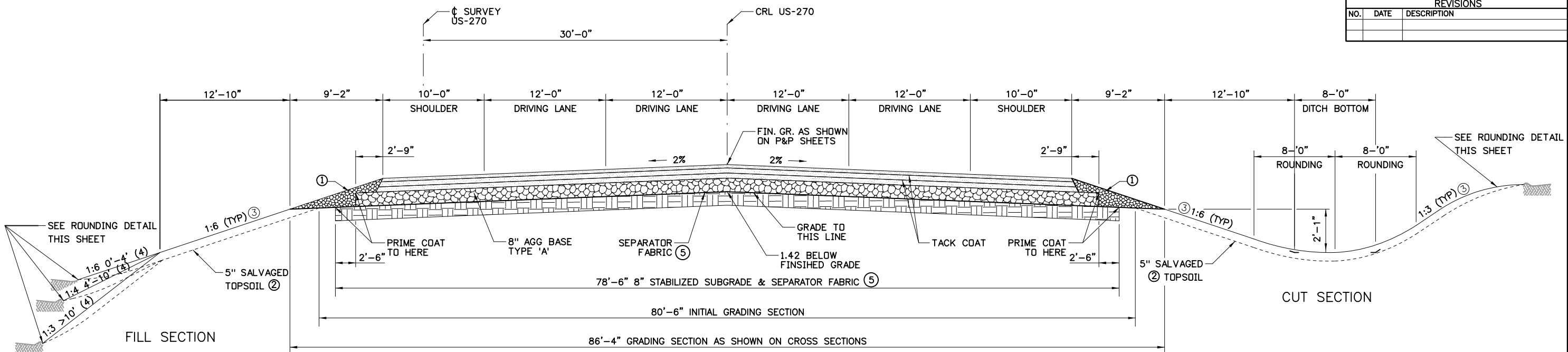
ROADWAY:	BRIDGE:	TRAFFIC CONTROL:	TRAFFIC SIGNING:	TRAFFIC SAFETY:
SSS-1-1	EJ-DTL-02E	TCS1-1-01	PM1-1-02	THRI-1-02
TSC2-3-2	EJ-SK-04E	TCS2-1-00	PM2-1-01	SKT-1-00
TFL-1-1	FSHP-42-2-00E	TCS3-1-01	PM3-1-02	GHW1-1-00
PSE-1-0	HP1-2-01E	TCS4-1-01	DU2-1-00	GHW2-1-00
PCES-4-1		TCS5-1-00	RSD1-1-00	
SMD-3-1		TCS6-1-02	WSD1-1-00	
SPI-4-1		TCS7-1-02	WSD2-1-00	
SPB-1-4		TCS8-1-00	SBS1-1-00	
FHTMPP-1-0		TCS9-1-01	SBS2-1-00	
FHTCP-3-1		TCS11-1-01	GMS1-1-00	
SBI-4-2		TCS13-1-00	SSP1-1-02	
RDI-3-1		TCS14-1-00	SSA1-1-00	
PDT-1-3		TCS19-1-01		
RWF2-2-1		TCS20-1-00		
RWF3-2-2		TCS21-1-02		
		TCS24-1-02		

11:46:41 AM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.01 General\21006INDEX&STANDARDS.dgn
 6/7/2018

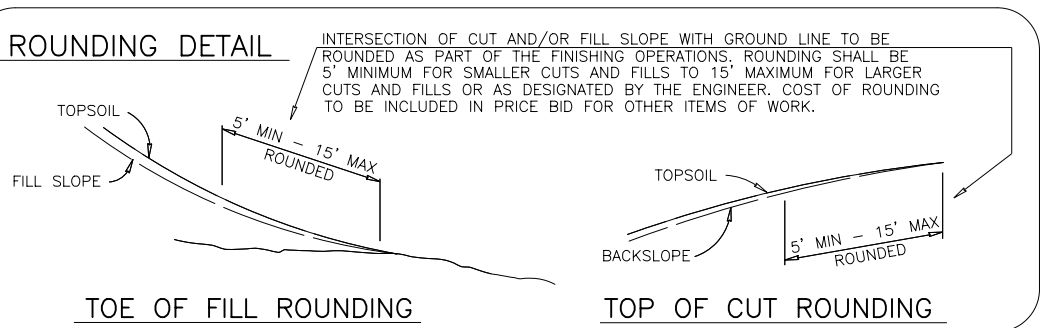
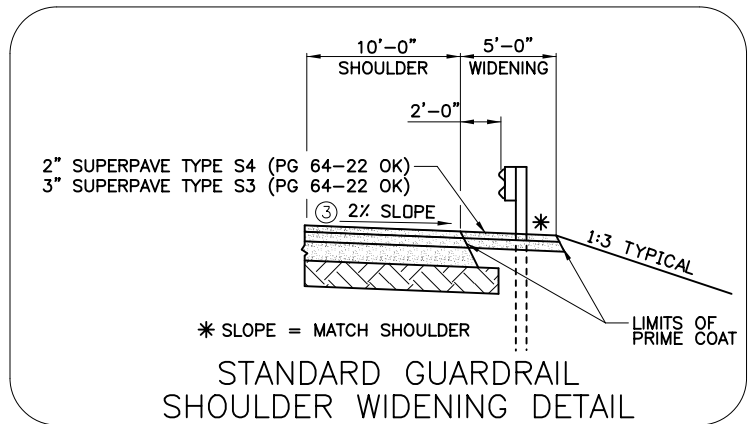
DESIGN: CPY	06/18	US 270 INDEX OF SHEETS AND ODOT STANDARD DRAWINGS SHEET 1 OF 1 SHEET NO. 0002	SEMINOLE COUNTY STATE JOB PIECE NO: 21006(07)
DRAWN: CPY	06/18		
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	0003	44
REVISIONS					
NO.	DATE	DESCRIPTION			

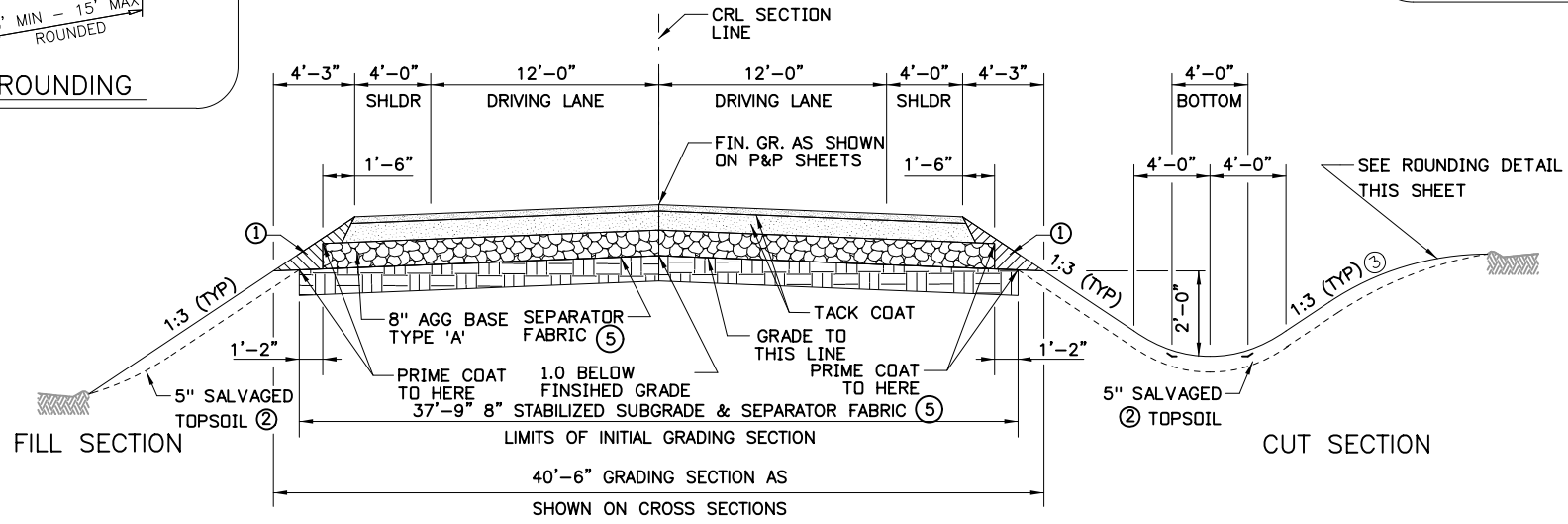


PAVEMENT REQUIREMENT		
17" PAVT. STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 PG (70-28 OK)	2" SUPERPAVE TYPE S4 PG (64-22 OK)
BASE COURSE	3" SUPERPAVE TYPE S3 PG (70-28 OK)	3" SUPERPAVE TYPE S3 PG (64-22 OK)
	4" SUPERPAVE TYPE S3 PG (64-22 OK)	4" SUPERPAVE TYPE S3 PG (64-22 OK)
	8" AGGREGATE BASE TYPE 'A'	8" AGGREGATE BASE TYPE 'A'



TYPICAL SECTION NO. 1
US 270
FULL DEPTH CONSTRUCTION
 US 270 CRL STA 358+00.00 TO STA 368+34.83
 US 270 CRL STA 371+64.87 TO STA 374+14.02
 US 270 CRL STA 379+85.98 TO STA 389+00.00

STA. 363+26.70 TO 368+38.00 RT.
 STA. 371+68.20 TO 374+18.26 RT.
 STA. 379+81.74 TO 383+52.39 RT.
 STA. 364+26.70 TO 368+38.00 LT.
 STA. 371+68.20 TO 374+18.26 LT.
 STA. 379+81.74 TO 384+77.39 LT.



PAVEMENT REQUIREMENT		
12" PAVT. STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 PG (64-22 OK)	2" SUPERPAVE TYPE S4 PG (64-22 OK)
BASE COURSE	4" SUPERPAVE TYPE S3 PG (64-22 OK)	4" SUPERPAVE TYPE S3 PG (64-22 OK)
	6" AGGREGATE BASE TYPE 'A'	6" AGGREGATE BASE TYPE 'A'

TYPICAL SECTION NO. 2
SECTION LINE 3610
 STA. 10+00.00 TO 16+20.04
 STA. 16+88.04 TO 18+99.62

DESIGN: CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY	06/18		
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		

TYPICAL SECTIONS

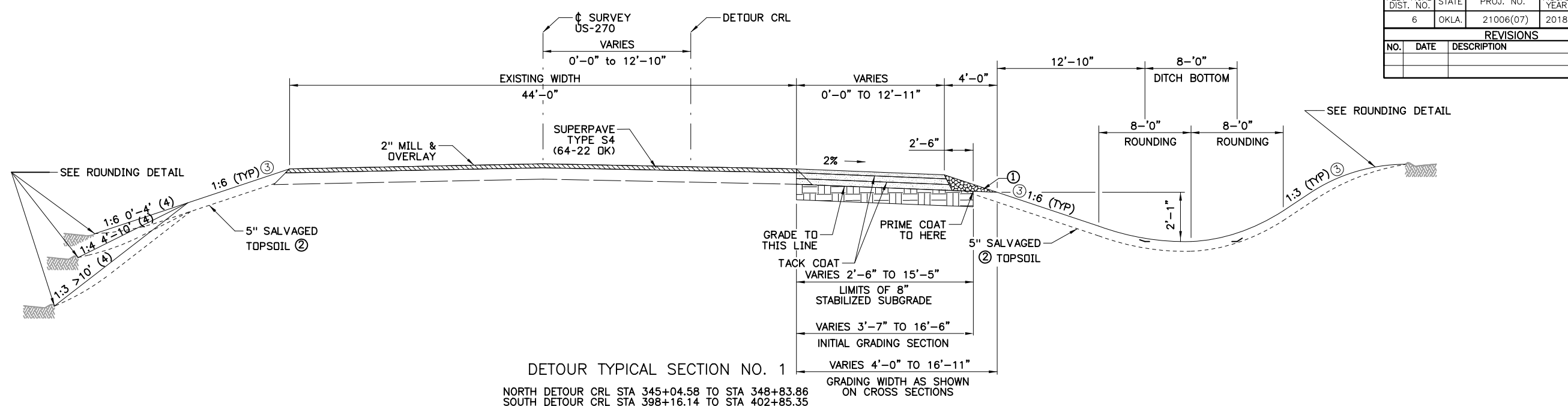
STATE JOB PIECE NO: 21006(07) SHEET 1 OF 3 SHEET NO. 0003

- ① BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN TBSC TYPE 'E'.
- ② TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREA LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.
- ③ REFER TO CROSS SECTION SHEETS FOR ADDITIONAL INFORMATION.
- ④ DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER
- ⑤ SEPARATOR FABRIC SHALL BE MIRAFI RS3801 OR APPROVED EQUIVALENT

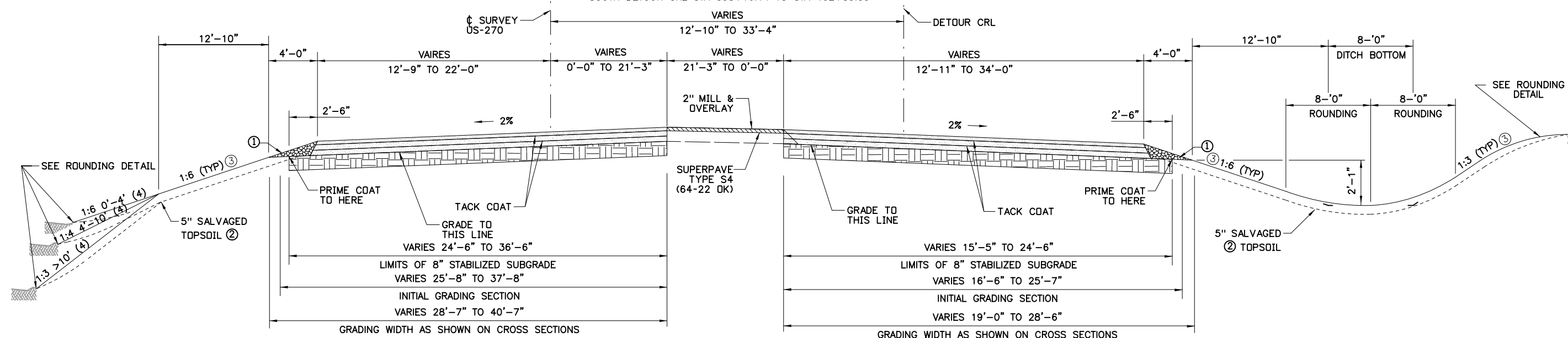
2:39:23 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 cpybdf_ANSIB.plt
 Plans and Drawings/8.30 Cut Sheets/8.3.01 General/21006C0RDTY01.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	0004	44
REVISIONS					
NO.	DATE	DESCRIPTION			



DETOUR TYPICAL SECTION NO. 1

NORTH DETOUR CRL STA 345+04.58 TO STA 348+83.86
SOUTH DETOUR CRL STA 398+16.14 TO STA 402+85.35



DETOUR TYPICAL SECTION NO. 2

NORTH DETOUR CRL STA 348+83.86 TO STA 353+26.42
SOUTH DETOUR CRL STA 393+69.98 TO STA 398+16.14

① BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS.
QUANTITY IS MEASURED IN TBSO TYPE 'E'.

② TOPSOIL NOTE:
THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT,
AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF
THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST
ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON
COMPLETED FILL SLOPES OR OTHER PRIORITY AREA LOCATED BY THE ENGINEER.
ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE
PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE
TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE
AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.

③ REFER TO CROSS SECTION SHEETS FOR ADDITIONAL INFORMATION.

④ DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER

DETOUR TYPICAL NO. 1 & 2 PAVEMENT REQUIREMENT		
7" PAVT. STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 PG (64-22 OK)	2" SUPERPAVE TYPE S4 PG (64-22 OK)
BASE COURSE	2.5" SUPERPAVE TYPE S3 PG (64-22 OK)	2.5" SUPERPAVE TYPE S3 PG (64-22 OK)
	2.5" SUPERPAVE TYPE S3 PG (64-22 OK)	2.5" SUPERPAVE TYPE S3 PG (64-22 OK)

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		

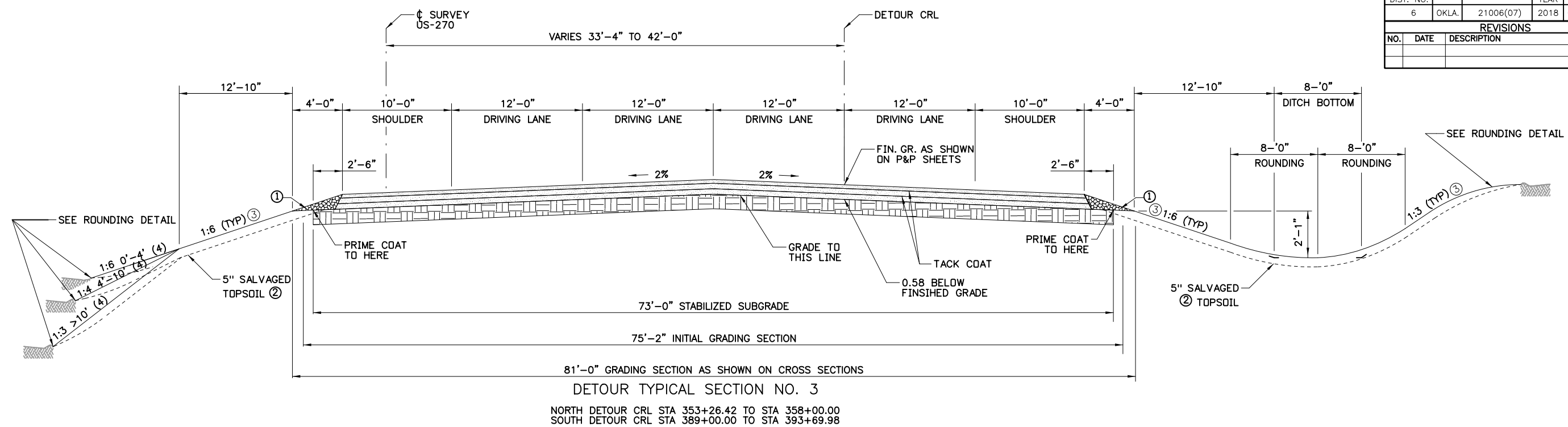
TYPICAL SECTIONS

SHEET 2 OF 3
SHEET NO. 0004

STATE JOB PIECE NO: 21006(07)

2:39:29 PM
 cpyb&w_AINSIB.tbl
 6/5/2018
 cpyb&w_AINSIB.tbl
 pw:/Active Projects/ODOT113446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.01 General/21006CORFDY02.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	0005	44
REVISIONS					
NO.	DATE	DESCRIPTION			



PAVEMENT REQUIREMENT		
7" PAVT. STRUCTURE	DRIVING LANES	SHOULDERS
SURFACE COURSE	2" SUPERPAVE TYPE S4 PG (64-22 OK)	2" SUPERPAVE TYPE S4 PG (64-22 OK)
BASE COURSE	2.5" SUPERPAVE TYPE S3 PG (64-22 OK)	2.5" SUPERPAVE TYPE S3 PG (64-22 OK)
	2.5" SUPERPAVE TYPE S3 PG (64-22 OK)	2.5" SUPERPAVE TYPE S3 PG (64-22 OK)

① BACKFILL NOTE:
 TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS.
 QUANTITY IS MEASURED IN TBSC TYPE 'E'.

② TOPSOIL NOTE:
 THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT,
 AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF
 THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST
 ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON
 COMPLETED FILL SLOPES OR OTHER PRIORITY AREA LOCATED BY THE ENGINEER.
 ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE
 PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE
 TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE
 AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.

③ REFER TO CROSS SECTION SHEETS FOR ADDITIONAL INFORMATION.

④ DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		

TYPICAL SECTIONS

SHEET 3 OF 3
 SHEET NO. 0005

STATE JOB PIECE NO: 21006(07)

6/5/2018 2:39:34 PM c:\p\w\ANSIB.tbl c:\p\w\ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.01 General\21006CORTY03.dgn

GENERAL NOTES

SPECIFICATIONS:

COMPLY WITH THE REQUIREMENTS OF THE 2009 OKLAHOMA STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION, EXCEPT AS MODIFIED BY THE PLANS AND SPECIAL PROVISIONS.

UTILITIES:

CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF ALL UTILITIES PRIOR TO CONSTRUCTION.

REMOVAL OF EXISTING STRUCTURES:

ITEM "REMOVAL OF EXISTING BRIDGE STRUCTURE" FOR BRIDGE "A" CONSISTS OF REMOVAL OF 45'-50'-45' I-BEAM SPANS, 28'-0" CLR ROADWAY AT CRL STA. 370+03.35. BRIDGE "B" CONSISTS OF REMOVAL OF 4-100' CONT. I-BEAM SPANS, 28'-0" CLR ROADWAY AT CRL STA. 377+19.91. ALL REMOVAL SHALL INCLUDE THE SUBSTRUCTURE REMOVAL TO A MINIMUM DEPTH OF 1'-0" BELOW FINAL GRADE IN ACCORDANCE WITH SECTION 619.04.B AND SECTION 512.01(B) CODE OF FEDERAL REGULATIONS 29 CFR-1926.62 OF THE SPECIFICATIONS, AND IN A MANNER APPROVED BY THE ENGINEER. ALL STEEL I-BEAMS IN BOTH STRUCTURES SHALL BECOME THE PROPERTY OF SEMINOLE COUNTY AND PLACED IN THE RIGHT-OF-WAY FOR PICK UP. ALL OTHER MEMBERS AND MATERIAL OF THE STRUCTURE SHALL BECOME PROPERTY OF THE CONTRACTOR AND DISPOSED OF PROPERLY UPON REMOVAL. ALL COSTS OF REMOVAL INCLUDING MATERIALS, LABOR, AND EQUIPMENT SHALL BE INCLUDED IN THE PRICE BID PER LUMP SUM OF "REMOVAL OF EXISTING BRIDGE STRUCTURE".

THE ORIGINAL CONSTRUCTION PLANS FOR THE EXISTING BRIDGE STRUCTURES MAY BE OBTAINED FROM THE REPRODUCTION BRANCH OF THE OKLAHOMA DEPARTMENT OF TRANSPORTATION. THE EXISTING BRIDGES WERE CONSTRUCTED UNDER FEDERAL AID PROJECT NO. F-222(9).

PILE DRIVING EQUIPMENT:

USE A PILE DRIVING HAMMER OF THE SIZE AND TYPE CAPABLE OF CONSISTENTLY DELIVERING THE EFFECTIVE DYNAMIC ENERGY SUFFICIENT TO DRIVE THE PILES TO THE REQUIRED TIP ELEVATION AND TO ACHIEVE THE REQUIRED ULTIMATE PILE CAPACITY WITHOUT EXCEEDING THE LIMITATIONS SET ON THE ALLOWABLE DRIVING STRESSES IN ACCORDANCE WITH SECTION 514.03.A(2).

ABUTMENT PILING CAPACITY:

THE FACTORED REACTION FOR EACH HP 12X53 PILE IS 77.1 TONS FOR BRIDGE "A" AND 81.4 FOR BRIDGE "B".

THE FOLLOWING FORMULA (GATES EQUATION) SHALL BE USED TO DETERMINE THE AXIAL LOAD RESISTANCE OF THE DRIVEN FOUNDATION PILES:

$$\text{AXIAL LOAD RESISTANCE} = \phi [(0.875 \sqrt{E} \text{ LOG}_{10}(10N)) - 50] \quad (\text{TONS})$$

WHERE:

- ϕ = RESISTANCE FACTOR OF 0.4
- E = ENERGY PRODUCED BY THE HAMMER BLOW IN FOOT-POUNDS. FOR GRAVITY AND SINGLE ACTING DIESEL HAMMERS THE VALUE IS BASED ON ACTUAL RAM STROKE OBSERVED IN THE FIELD AND MEASURED IN FEET MULTIPLIED BY THE RAM WEIGHT IN POUNDS.
- N = AVERAGE NUMBER OF HAMMER BLOWS PER INCH OF PILE PENETRATION FOR THE LAST 10 TO 20 BLOWS DELIVERED TO THE PILE HEAD.

THE ABOVE FORMULA IS ONLY APPLICABLE WHEN:

- (1) THE PILE DRIVING HAMMER HAS A FREE FALL (GRAVITY AND SINGLE ACTING HAMMERS ONLY.)
- (2) THE HEAD OF THE PILE IS NOT BROOMED, CRUSHED OR OTHERWISE DAMAGED.
- (3) THE PENETRATION IS QUICK AND UNIFORM.
- (4) THERE IS NO APPRECIABLE REBOUND ON THE HAMMER.
- (5) A FOLLOWER IS NOT USED.

THE NUMBER OF BLOWS PER INCH OF PILE PENETRATION MAY BE MEASURED EITHER DURING INITIAL DRIVING OR BY RE-DRIVING WITH A WARM HAMMER OPERATED AT FULL ENERGY AFTER A PILE SET PERIOD, AS DETERMINED BY THE ENGINEER.

IF WATER JETS ARE USED IN CONNECTION WITH THE DRIVING, DETERMINE THE AXIAL LOAD RESISTANCE BY THE FORMULA SHOWN ABOVE ONLY AFTER THE JETS HAVE BEEN WITHDRAWN.

STEEL PILES:

STRUCTURAL STEEL FOR THE HP 12x53 AND HP 10x42 SHALL CONFORM TO AASHTO M270 (ASTM A572), GRADE 50.

PROVIDE AND INSTALL STRUCTURAL STEEL FOR PILING AND STEEL PLATE REINFORCING TIPS IN ACCORDANCE WITH STANDARD HP1-2.

ALL PILING SHALL BE DRIVEN THROUGH THE COMPACTED FILL. PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN ON THE PLANS. IF THE AXIAL LOAD RESISTANCE IS OBTAINED. THE LENGTH OF STEEL PILING IS AN ESTIMATE.

BEARING ASSEMBLIES:

PROVIDE AND INSTALL FIXED AND EXPANSION BEARING ASSEMBLIES OF THE SIZE, SHAPE, AND LOCATION AS SPECIFIED AND SHOWN IN THE PLANS. THERE IS AN ESTIMATED TOTAL OF 2,750 POUNDS (BRIDGE A) AND 2,950 POUNDS (BRIDGE B) OF STAINLESS STEEL REQUIRED TO FABRICATE ALL FIXED BEARING ASSEMBLIES. THERE IS AN ESTIMATED TOTAL OF 5,500 POUNDS (BRIDGE A) AND 8,840 POUNDS (BRIDGE B) OF STAINLESS STEEL REQUIRED TO FABRICATE ALL EXPANSION BEARING ASSEMBLIES.

ALL COSTS ASSOCIATED WITH PROVIDING AND INSTALLING THE BEARING ASSEMBLIES AS SPECIFIED OR SHOWN IN THE PLANS INCLUDING ALL MATERIAL, LABOR, EQUIPMENT, AND INCIDENTALS SHALL BE INCLUDED IN THE UNIT BID PRICE PER EACH OF "STAINLESS STEEL FIXED BEARING ASSEMBLY" OR "STAINLESS STEEL EXPANSION BEARING ASSEMBLY" AS APPLICABLE.

CONCRETE INTERMEDIATE DIAPHRAGMS:

ONCE THE CONCRETE HAS BEEN PLACED FOR THE CONCRETE INTERMEDIATE DIAPHRAGMS, WAIT A MINIMUM OF 24 HOURS BEFORE REMOVING THE SIDE FORMS. DO NOT REMOVE THE BOTTOM FORM FOR A MINIMUM OF 3 DAYS OR AT THE DISCRETION OF THE ENGINEER. THIS TIME MAY BE SHORTENED IF THE CONCRETE HAS ATTAINED 80% OF THE SPECIFIED COMPRESSIVE STRENGTH. DO NOT PLACE THE CONCRETE FOR THE DECK SLAB OR APPLY OTHER SIGNIFICANT LOADS TO THE BEAMS OR DIAPHRAGMS UNTIL THE CONCRETE IN THE DIAPHRAGMS HAS BEEN IN PLACE FOR A MINIMUM OF 10 DAYS, OR AT THE DISCRETION OF THE ENGINEER. THIS TIME MAY BE SHORTENED IF THE CONCRETE HAS ATTAINED 80% OF THE SPECIFIED COMPRESSIVE STRENGTH.

BRIDGE DECK CONSTRUCTION METHODS:

ANY STEEL USED BY THE CONTRACTOR TO FACILITATE DECK CONSTRUCTION SUCH AS INSERT WELD ANCHORS, TY-BAR CLIPS, FORM HANGERS, OR OTHER APPURTENANCES THAT REMAIN IN PLACE IN THE BRIDGE DECK MUST BE EPOXY COATED OR GALVANIZED.

EPOXY COAT IN ACCORDANCE WITH AASHTO M 284 AND/OR GALVANIZE IN ACCORDANCE WITH AASHTO M 111.

DECK HAUNCHES:

PLAN QUANTITY FOR CLASS AA CONCRETE INCLUDES HAUNCHES OVER PRESTRESSED BEAMS, THE PAY ITEM INCLUDES APPROXIMATELY 12.5 CUBIC YARDS FOR BRIDGE "A" AND 67.8 CUBIC YARDS FOR BRIDGE "B".

TRANSVERSE CONSTRUCTION JOINTS:

TRANSVERSE CONSTRUCTION JOINTS IN THE BRIDGE DECK SHALL BE SEALED IN ACCORDANCE WITH SECTION 523 "CONCRETE SURFACE REPAIR BY SEALING" OF THE STANDARD SPECIFICATIONS AND SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

PENETRATING WATER REPELLENT SURFACE TREATMENT:

A PENETRATING WATER REPELLENT SURFACE TREATMENT SHALL BE APPLIED TO THE FOLLOWING CONCRETE SURFACES OF THE BRIDGE:

- (A) EDGES AND UNDERSIDE CANTILEVER PORTION OF THE BRIDGE DECK.
- (B) THE OUTER FACE AND BOTTOM OF THE EXTERIOR P.C. BEAM.
- (C) THE ROADWAY FACE, TOP, AND INSIDE OF THE POST OPENINGS OF THE CONCRETE TRAFFIC RAILS.
- (D) THE EXPOSED TOPS, SIDES, AND ENDS OF PIER CAPS.
- (E) FRONT FACE OF BACKWALL, TOP OF BRIDGE SEAT INCLUDING ALL SURFACES OF PEDESTALS, THE FRONT FACE OF BRIDGE SEAT, AND THE EXPOSED SURFACES OF THE WINGS.

THE APPLICATION OF PENETRATING WATER REPELLENT SURFACE TREATMENT SHALL BE IN ACCORDANCE WITH SECTION 515 OF THE STANDARD SPECIFICATIONS.

ALL COSTS ASSOCIATED WITH THE USE OF PENETRATING WATER REPELLENT SURFACE TREATMENT INCLUDING MATERIAL, LABOR, EQUIPMENT AND INCIDENTALS SHALL BE INCLUDED IN THE PRICE BID PER SQUARE YARD OF "WATER REPELLENT (VISUALLY INSPECTED)".

STAY-IN-PLACE FORMS:

STAY-IN-PLACE STEEL DECK FORMS MAY BE USED IF THE MINIMUM DECK SLAB THICKNESS SHOWN IN THE PLANS IS OBTAINED BY MEASURING FROM THE TOP OF THE DECK SLAB TO THE TOP PORTION OF THE STEEL CORRUGATION. PREFORMED FILLER SUCH AS POLYSTYRENE OR ANY OTHER FILLER MATERIAL USED IN THE STEEL CORRUGATIONS MUST BE BONDED TO THE STAY-IN-PLACE FORMS, AND NO ADDITIONAL CONCRETE WEIGHT OF THE DECK SLAB IS PERMITTED. PREFORMED STYROFOAM OR ANY OTHER FILLER MATERIAL MUST BE BONDED TO THE STEEL STAY-IN-PLACE FORMS. ADDITIONAL WEIGHT OF THE STEEL DECK FORMS AND FILLER MATERIAL SHALL NOT EXCEED 5 PSF.

STAY-IN-PLACE PRESTRESSED CONCRETE DECK FORMS MAY BE USED IF THE FOLLOWING CONDITIONS ARE MET:

- (1) SHOP DRAWINGS AND STRUCTURAL CALCULATIONS FOR THE FORMS ARE SUBMITTED TO THE BRIDGE ENGINEER FOR APPROVAL.
- (2) A NEW STRUCTURAL DESIGN, STRUCTURAL CALCULATIONS, AND A NEW REINFORCING SCHEDULE FOR THE DECK SLAB ARE SUBMITTED TO THE BRIDGE ENGINEER FOR APPROVAL.
- (3) SHOP DRAWINGS, NEW DECK SLAB SLAB REINFORCING SCHEDULE, STRUCTURAL DESIGNS, AND CALCULATIONS SHALL BE PREPARED BY AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN THE STATE OF OKLAHOMA.

ALL COST ASSOCIATED WITH THE USE OF STAY-IN-PLACE FORMS, INCLUDING ALL MATERIAL, LABOR, EQUIPMENT, INCIDENTALS AND PROFESSIONAL SERVICES SHALL BE AT THE CONTRACTOR'S EXPENSE. FOR ADDITIONAL INFORMATION CONCERNING THE USE OF STAY-IN-PLACE FORMS, SEE SECTION 502 OF THE SPECIFICATIONS.

RIPRAP:

A 1'-6" THICK LAYER OF TYPE 1-A RIPRAP WITH A 6" THICK LAYER OF TYPE 1-A FILTER BLANKET SHALL BE PLACED ON THE BRIDGE HEADERS AT THE BRIDGE "B" ABUTMENTS AS SHOWN ON THE PLANS. THE FILTER BLANKET SHALL BE PLACED IN ONE LAYER.

PERFORATED PIPE UNDERDRAIN:

THE ITEM "6" PERFORATED PIPE UNDERDRAIN ROUND" INCLUDES 154' (BRIDGE A) AND 164' (BRIDGE B) OF PERFORATED PIPE. THE INSTALLATION OF THE PERFORATED PIPE AND PIPE UNDERDRAIN COVER MATERIAL SHALL BE AS SHOWN ON STANDARDS B40-C-ABUT-MISC AND PUD-3.

ALL COSTS ASSOCIATED WITH PERFORATED PIPE UNDERDRAIN INSTALLATION INCLUDING MATERIAL, LABOR, EQUIPMENT, AND INCIDENTALS SHALL BE INCLUDED IN THE BID PRICE PER LINEAR FOOT OF "6" PERF. PIPE UNDERDRAIN RND."

NON-PERFORATED PIPE UNDERDRAIN:

THE ITEM "6" NON-PERF. PIPE UNDERDRAIN RND" INCLUDES 60' (BRIDGE A) AND 60' (BRIDGE B) OF NON-PERFORATED PIPE. THE INSTALLATION OF THE NON-PERFORATED PIPE AND PIPE UNDERDRAIN COVER MATERIAL SHALL BE AS SHOWN ON STANDARDS B40-C-ABUT-MISC AND PUD-3.

IF THE GRADING AT THE ABUTMENTS IS SUCH THAT THE NON-PERFORATED PIPE WILL NOT REACH THE GROUND LINE IN A REASONABLE DISTANCE, THE PIPE SHALL BE REDIRECTED TO DRAIN DIRECTLY INTO THE CHANNEL.

ALL COSTS ASSOCIATED WITH NON-PERFORATED PIPE UNDERDRAIN INSTALLATION INCLUDING MATERIAL, LABOR, EQUIPMENT, AND INCIDENTALS SHALL BE INCLUDED IN THE BID PRICE PER LINEAR FOOT OF "6" NON-PERF. PIPE UNDERDRAIN RND."

APPROACH SLAB:

CLASS AA CONCRETE SHALL BE USED IN THE APPROACH SLABS. THE QUANTITY GIVEN IS BASED ON THE ACTUAL SQUARE YARDS OF THE APPROACH SLABS. THE APPROACH SLAB/DECK SLAB CONSTRUCTION JOINTS SHALL BE SEALED AND SEALED WITH RAPID CURE JOINT SEALANT. ALL COSTS OF CONCRETE, REINFORCING STEEL, RAPID CURE JOINT SEALANT, EXCAVATION, LABOR, EQUIPMENT AND OTHER INCIDENTALS NECESSARY TO COMPLETE THE WORK AS SPECIFIED SHALL BE INCLUDED IN THE PRICE BID PER SQUARE YARD OF "APPROACH SLAB."

UNION PACIFIC RAILROAD COMPANY NOTES

NOTIFICATION OF WORK:

THE CONTRACTOR IS REQUIRED TO GIVE THE UNION PACIFIC RAILROAD COMPANY AT LEAST 10 WORKING DAYS ADVANCE NOTICE, IN WRITING, BEFORE ANY WORK IS STARTED ON THE SITE. TO AVOID HAZARDS, THE UNION PACIFIC RAILROAD COMPANY MAY HAVE A REPRESENTATIVE PRESENT, IF DEEMED NECESSARY, FOR THE PURPOSE OF INSPECTION AND THE ISSUANCE OF ANY APPROPRIATE INSTRUCTIONS FOR RAILROAD OPERATIONS DURING THE BRIDGE REPLACEMENT AND APPROACH WORK IN SEMINOLE COUNTY AS IT RELATES TO THE UNION PACIFIC RAILROAD COMPANY'S PROPERTY. (AARDOT 600 148N, MILEPOST 424.10)

THE CONTRACTOR SHALL NOTIFY:

HANS WAMMEL MANAGER OF TRACK UNION PACIFIC RAILROAD COMPANY 220 S. MILES EL RENO, OK 73036 PHONE: 405-274-4426 EMAIL: HCWAMMEL@UP.COM	MR. CLAY A. MCMANAMAN MAINTENANCE MANAGER PUBLIC PROJECTS UNION PACIFIC RAILROAD COMPANY P.O. BOX 1337 EL RENO, OKLAHOMA 73036 PHONE: 501-373-2927 CAMCMANA@UP.COM
---	--

FLAGGING AND INSURANCE:

FLAGGING AND INSURANCE SHALL BE PROVIDED AS SPECIFIED IN SECTION 107 OF THE STANDARD SPECIFICATIONS AND IN THE SPECIAL PROVISIONS FOR RAILROAD FLAGGING (SEE PROPOSAL FOR SPECIAL PROVISIONS) AND WHAT IS STATED IN THE UNION PACIFIC RAILROAD COMPANY'S RIGHT OF ENTRY AGREEMENT. UNION PACIFIC RAILROAD COMPANY, AT THEIR DISCRETION, SHALL PROVIDE FLAGGING FOR THE RAILROAD DURING CONSTRUCTION OPERATIONS.

THE CONTRACTOR IS REQUIRED TO REIMBURSE UNION PACIFIC RAILROAD COMPANY FOR FLAGGING SERVICES PROVIDED.

THE CONTRACTOR SHALL ALSO FURNISH SATISFACTORY EVIDENCE TO THE STATE OF OKLAHOMA THAT THEY HAVE PROVIDED INSURANCE OF THE KINDS AND AMOUNTS AS SPECIFIED IN THE SPECIAL PROVISIONS FOR RAILROAD INSURANCE AND IN THE UNION PACIFIC COMPANY'S RIGHT OF ENTRY AGREEMENT.

THE CONTRACTOR WILL BE REQUIRED TO ENTER INTO A RIGHT OF ENTRY AGREEMENT WITH THE UNION PACIFIC RAILROAD COMPANY BEFORE THEY WILL BE ALLOWED ON THE RAILROAD'S RIGHT-OF-WAY.

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	AB01	44
REVISIONS					
NO.	DATE	DESCRIPTION			

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
GENERAL NOTES (BRIDGE)		SHEET 1 OF 2
		SHEET NO. AB01
STATE JOB PIECE NO: 21006(07)		

2:39:40 PM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.01 General\21006BR001.dgn
 6/5/2018
 bmcanelly

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	AB02	44

REVISIONS		
NO.	DATE	DESCRIPTION

UNION PACIFIC RAILROAD COMPANY NOTES (CONT.)

PRE-WORK MEETING:

PRIOR TO WORKING ON THE UNION PACIFIC RAILROAD COMPANY'S RIGHT-OF-WAY OR IN THE VICINITY OF THEIR TRACKS, YOU MUST CONTACT THE LOCAL MANAGER OF TRACK MAINTENANCE FOR THE UNION PACIFIC RAILROAD COMPANY TO COORDINATE YOUR WORK. IT IS VITAL THAT YOU HAVE CONTACT WITH THE UNION PACIFIC RAILROAD COMPANY MANAGER OF TRACK MAINTENANCE PRIOR TO GETTING ON THE RAILROAD'S PROPERTY.

COORDINATION WITH RAILROAD:

THE CONTRACTOR SHALL CONDUCT CONSTRUCTION OPERATIONS IN A MANNER WHICH WILL NOT DELAY OR INTERFERE WITH TRAIN OPERATIONS. CONSTRUCTION ACTIVITY WITHIN 25 (TWENTY-FIVE) FEET OF ACTIVE TRACKS WILL REQUIRE A FLAGMAN TO BE PROVIDED BY THE UNION PACIFIC RAILROAD COMPANY AT THE CONTRACTOR'S EXPENSE.

THE CONTRACTOR SHALL GIVE WRITTEN NOTICE TO THE UNION PACIFIC RAILROAD COMPANY MANAGER OF TRACK MAINTENANCE, A MINIMUM OF 30 (THIRTY) CALENDAR DAYS IN ADVANCE OF WHEN FLAGGING IS REQUIRED.

SPECIAL PERMISSION MUST BE OBTAINED FROM THE UNION PACIFIC RAILROAD COMPANY BEFORE MOVING ANY EQUIPMENT OR OTHER OBJECT WHICH COULD MAKE THE TRACK IMPASSABLE IF IT FELL WITHIN THE AREA SHOWN ON THE CONSTRUCTION CLEARANCE DIAGRAM.

RAILROAD FLAGGERS, PROTECTIVE SERVICES, AND PROTECTIVE DEVICES WILL BE REQUIRED, BUT NOT LIMITED TO, EVENTS WHEN:

- o THE CONTRACTOR WORK ACTIVITIES ARE WITHIN 25 (TWENTY-FIVE) FEET OF THE TRACK, MEASURED FROM THE TRACK CENTERLINE.
- o ACTIVITIES ARE OVER OR UNDER THE TRACK.
- o CRANES OR SIMILAR EQUIPMENT WILL NOT BE POSITIONED WHERE THEY COULD FOUL THE TRACK IF THEY TIPPED OVER OR EXPERIENCED SOME OTHER CATASTROPHIC EVENT.
- o IN THE OPINION OF THE UNION PACIFIC RAILROAD COMPANY REPRESENTATIVE:
- o IT IS NECESSARY TO SAFEGUARD THE UNION PACIFIC RAILROAD COMPANY PROPERTY, EMPLOYEES, TRAINS, ENGINES, AND FACILITIES.
- o WHEN ANY EXCAVATION IS PERFORMED BELOW THE BOTTOM OF THE ELEVATIONS AND TRACK OR OTHER UNION PACIFIC RAILROAD COMPANY FACILITIES MAY BE SUBJECT TO MOVEMENT OR SETTLEMENT.
- o WHEN WORK IN ANY WAY INTERFERES WITH SAFE OPERATION OF TRAINS AND TIMETABLE SPEEDS.
- o WHEN ANY HAZARD IS PRESENTED TO RAILROAD TRACK, SIGNALS, COMMUNICATIONS, ELECTRICAL, OR OTHER FACILITIES EITHER DUE TO PERSON, MATERIAL, EQUIPMENT, OR BLASTING IN THE AREA.

PROTECTION OF RAILROAD UNDER BRIDGE:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROTECTING THE RAILROAD TRACK BED DURING ALL CONSTRUCTION OPERATIONS. PRIOR TO ANY WORK BEING STARTED, A PROPOSED METHOD OF PREVENTING DEBRIS FROM FALLING ON THE RAILROAD TRACK BED SHALL BE SUBMITTED TO THE RAILROAD REPRESENTATIVE FOR HIS APPROVAL.

THE CONTRACTOR SHALL NOT BE PERMITTED TO LEAVE ANY WORKER SCAFFOLDING IN PLACE IN WORKING POSITION. AT THE END OF EACH WORKDAY, THE SCAFFOLDING SHALL BE REMOVED AND SET A SAFE DISTANCE FROM ANY OPERATING RAILROAD LINE. SCAFFOLDING SHALL AT ALL TIMES MAINTAIN THE MINIMUM CLEARANCE AS SHOWN ON THE "FALSEWORK DIAGRAM" ON THE PLANS (SHEET NO. B001).

DEMOLITION OF STRUCTURES OVER RAILROAD:

ALL DEMOLITION PLANS FOR REMOVAL OF STRUCTURES OVER RAILROAD LINES SHALL BE REVIEWED AND APPROVED BY THE UNION PACIFIC RAILROAD COMPANY BEFORE ANY REMOVAL MAY BEGIN.

DEMOLITION OF STRUCTURES WILL BE PERFORMED IN ACCORDANCE WITH THE RAILROAD'S "INSTRUCTIONS FOR PREPARATION OF DEMOLITION PLANS FOR STRUCTURES OVER THE UNION PACIFIC RAILROAD."

UNION PACIFIC RAILROAD COMPANY STANDARD REQUIREMENTS:

- 1) THE PROPOSED GRADE SEPARATION PROJECT SHALL NOT INCREASE THE QUANTITY AND/OR CHARACTERISTICS OF THE FLOW IN THE RAILROAD'S DITCHES AND/OR DRAINAGE STRUCTURES.
- 2) THE ELEVATION OF THE EXISTING TOP-OF RAIL SHALL BE VERIFIED BEFORE BEGINNING CONSTRUCTION. ALL DISCREPANCIES SHALL BE BROUGHT TO THE ATTENTION OF THE RAILROAD PRIOR TO CONSTRUCTION.
- 3) THE CONTRACTOR MUST SUBMIT A PROPOSED METHOD OF EROSION AND SEDIMENT CONTROL AND HAVE THE METHOD APPROVED BY THE RAILROAD.
- 4) ALL SHORING SYSTEMS THAT IMPACT THE RAILROAD'S OPERATIONS AND/OR SUPPORTS THE RAILROAD'S EMBANKMENT SHALL BE DESIGNED AND CONSTRUCTED PER CURRENT RAILROAD GUIDELINES FOR TEMPORARY SHORING.
- 5) ALL DEMOLITIONS WITHIN THE RAILROAD'S RIGHT-OF-WAY AND/OR DEMOLITION THAT MAY IMPACT THE RAILROAD'S TRACKS OR OPERATIONS SHALL BE IN COMPLIANCE WITH THE RAILROAD'S DEMOLITION GUIDELINES.
- 6) ERECTION OVER THE RAILROAD'S RIGHT-OF-WAY SHALL BE DESIGNED TO CAUSE NO INTERRUPTION TO THE RAILROAD'S OPERATION, ENABLING THE TRACK(S) TO REMAIN OPEN TO TRAFFIC PER THE RAILROAD'S REQUIREMENTS.
- 7) RAILROAD REQUIREMENTS DO NOT ALLOW WORK WITHIN 50 FEET OF TRACK CENTERLINE WHEN A TRAIN PASSES THE WORK SITE AND ALL PERSONNEL MUST CLEAR THE AREA WITHIN 25 FEET OF THE TRACK CENTERLINE AND SECURE ALL EQUIPMENT.
- 8) FALSE-WORK CLEARANCES SHALL COMPLY WITH MINIMUM CONSTRUCTION CLEARANCES.
- 9) ALL PERMANENT CLEARANCES SHALL BE VERIFIED BEFORE PROJECT CLOSING.

EROSION CONTROL AND DRAINAGE:

THE CONTRACTOR MUST SUBMIT A PROPOSED METHOD OF EROSION AND SEDIMENT CONTROL AND HAVE THE METHOD APPROVED BY THE RAILROAD. THE CONTRACTOR WILL INSTALL, MAINTAIN, AND REMOVE ALL EROSION CONTROL MEASURES DEEMED NECESSARY WITHIN THE RAILROAD RIGHT OF WAY.

THE PROPOSED GRADE SEPARATION PROJECT SHALL NOT INCREASE THE QUANTITY AND/OR CHARACTERISTICS OF THE FLOW IN THE RAILROAD'S DITCHES AND/OR DRAINAGE STRUCTURES. THE CONTRACTOR WILL MAINTAIN THE RAILROAD DRAINAGE AT ALL TIMES WHEN WORKING WITHIN THE RAILROAD RIGHT OF WAY.

RAIL TRAFFIC:


THE UNION PACIFIC RAILROAD COMPANY DOES NOT CURRENTLY HAVE THIS RAIL-LINE ACTIVE. RAIL TRAFFIC IS FOR INFORMATION PURPOSES ONLY. ACTUAL RAIL TRAFFIC MAY VARY.

21006(07)		PAY QUANTITIES		
0200 BRIDGE "A" 85-70'-85' PC BEAM SPANS X 68'-0" CLR RDY SKEWED 22.50' LF				
ITEM CODE	DESCRIPTION	UNIT	TOTAL	
501(B)	1307 SUBSTRUCTURE EXCAVATION COMMON (BR-1)	CY	244.00	
501(G)	6309 CLSM BACKFILL (BR-1)	CY	424.00	
503(A)	1313 PRESTRESSED CONCRETE BEAMS (TYPE IV) (BR-1)	LF	1,673.00	
504(A)	1304 APPROACH SLAB (BR-1)	SY	688.20	
504(B)	1305 SAW-CUT GROOVING (BR-1)	SY	1,995.00	
504(C)	6250 SEALED EXPANSION JOINT (BR-1)	LF	74.50	
504(E)	6190 42" F-SHAPED PARAPET (BR-1)	LF	659.80	
506(A)	1322 STRUCTURAL STEEL (BR-1)	LB	2,760.00	
507(A)	6170 STAINLESS STEEL FIXED BEARING ASSEMBLY (BR-1)	EA	14.00	
507(B)	6174 STAINLESS STEEL EXPANSION BEARING ASSEMBLY (BR-1)	EA	28.00	
509(A)	1326 CLASS AA CONCRETE (BR-1)	CY	478.00	
509(B)	1328 CLASS A CONCRETE (BR-1)	CY	370.10	
509(D)	1331 CLASS C CONCRETE (1)	CY	39.50	
510(C)	6138 SLOPE WALL (5") (BR-1)	SY	1,468.00	
511(A)	1332 REINFORCING STEEL (BR-1)	LB	2,440.00	
511(B)	6010 EPOXY COATED REINFORCING STEEL (BR-1)	LB	206,130.00	
514(A)	6010 PILES, FURNISHED (HP10x42)	LF	440.00	
514(A)	6011 PILES, FURNISHED (HP12x53)	LF	2,784.00	
514(B)	6292 PILES, DRIVEN (HP10x42)	LF	440.00	
514(B)	6294 PILES, DRIVEN (HP12x53)	LF	2,784.00	
514(L)	6220 PILE SPLICE, H-PILE (NON-BIDDABLE)	EA	1.00	
515(A)	6013 WATER REPELLENT (VISUALLY INSPECTED) (BR-1)	SY	1,389.20	
516(A)	6096 DRILLED SHAFTS 60" DIAMETER	LF	740.00	
516(C)	6200 CROSSHOLE SONIC LOGGING	EA	8.00	
523(A)	6550 SEALER CRACK PREPARATION (BR-1)	LF	316.30	
523(B)	6560 SEALER RESIN (BR-1)	GAL	3.60	
525(C)	1000 (SP) NEST PREVENTION	LSUM	1.00	
613(H)	6204 6" PERFORATED PIPE UNDERDRAIN ROUND (BR-1)	LF	154.00	
613(I)	6207 6" NON-PERF. PIPE UNDERDRAIN RND.	LF	60.00	
619(D)	1397 REMOVAL OF EXISTING BRIDGE STRUCTURE	LSUM	1.00	
624(E)	4294 FENCE-STYLE CLF (8' HIGH, CLASS A) (BR-1)	LF	140.00	

21006(07)		PAY QUANTITIES		
0210 BRIDGE "B" 105'-125'-125'-105' PC BEAM SPANS X 68'-0" CLR RDY SKEWED 30' LF				
ITEM CODE	DESCRIPTION	UNIT	TOTAL	
501(B)	1307 SUBSTRUCTURE EXCAVATION COMMON (BR-1)	CY	301.00	
501(G)	6309 CLSM BACKFILL (BR-1)	CY	655.30	
503(A)	6290 PRESTRESSED CONCRETE BEAMS (TYPE J BT) (BR-1)	LF	3,210.67	
504(A)	1304 APPROACH SLAB (BR-1)	SY	889.20	
504(B)	1305 SAW-CUT GROOVING (BR-1)	SY	4,357.50	
504(C)	6250 SEALED EXPANSION JOINT (BR-1)	LF	79.40	
504(E)	6190 42" F-SHAPED PARAPET (BR-1)	LF	1,153.20	
506(A)	1322 STRUCTURAL STEEL (BR-1)	LB	5,040.00	
507(A)	6170 STAINLESS STEEL FIXED BEARING ASSEMBLY (BR-1)	EA	42.00	
507(B)	6174 STAINLESS STEEL EXPANSION BEARING ASSEMBLY (BR-1)	EA	14.00	
509(A)	1326 CLASS AA CONCRETE (BR-1)	CY	1,026.20	
509(B)	1328 CLASS A CONCRETE (BR-1)	CY	538.90	
509(D)	1331 CLASS C CONCRETE (1)	CY	28.50	
511(A)	1332 REINFORCING STEEL (BR-1)	LB	3,240.00	
511(B)	6010 EPOXY COATED REINFORCING STEEL (BR-1)	LB	369,870.00	
514(A)	6010 PILES, FURNISHED (HP10x42)	LF	734.00	
514(A)	6011 PILES, FURNISHED (HP12x53)	LF	2,951.00	
514(B)	6292 PILES, DRIVEN (HP10x42)	LF	734.00	
514(B)	6294 PILES, DRIVEN (HP12x53)	LF	2,951.00	
514(L)	6220 PILE SPLICE, H-PILE (NON-BIDDABLE)	EA	1.00	
515(A)	6013 WATER REPELLENT (VISUALLY INSPECTED) (BR-1)	SY	2,635.40	
516(A)	6096 DRILLED SHAFTS 60" DIAMETER	LF	1,044.00	
516(C)	6200 CROSSHOLE SONIC LOGGING	EA	12.00	
523(A)	6550 SEALER CRACK PREPARATION (BR-1)	LF	1,084.80	
523(B)	6560 SEALER RESIN (BR-1)	GAL	12.20	
525(C)	1000 (SP) NEST PREVENTION	LSUM	1.00	
601(B)	1353 TYPE I-A PLAIN RIPRAP	TON	3,900.00	
601(C)	1355 TYPE I-A FILTER BLANKET	TON	1,130.00	
613(H)	6204 6" PERFORATED PIPE UNDERDRAIN ROUND (BR-1)	LF	164.00	
613(I)	6207 6" NON-PERF. PIPE UNDERDRAIN RND.	LF	60.00	
619(D)	1397 REMOVAL OF EXISTING BRIDGE STRUCTURE (BR-1)	LSUM	1.00	

(BR-1) PAYMENT FOR THIS ITEM WILL BE BASED ON THE PLAN QUANTITIES. SEE SUBSECTION 109.01(B) OF THE STANDARD SPECIFICATIONS FOR HIGHWAY CONSTRUCTION.

(1) ACCOUNTS FOR CLASS C CONCRETE QUANTITY USED FOR SLOPE DRAINS. QUANTITY ALSO INCLUDES AN ADDITIONAL 15.0 CY TO BE USED AT THE DISCRETION OF THE ENGINEER THROUGHOUT THE PROJECT.

DESIGN: CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY	06/18	GENERAL NOTES & SUMMARY OF PAY QUANTITIES (BRIDGE)	
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		
		STATE JOB PIECE NO: 21006(07)	
		SHEET 2 OF 2 SHEET NO. AB02	

2:39:43 PM c:\p\df_ANSIB\p\ Projects\ODOT113446\00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.01 General\21006BRQY02.dgn
 6/5/2018
 bmcanelly cpyb&w_ANSIB.tbl
 p\:/Active

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	AR01	44
REVISIONS					
NO.	DATE	DESCRIPTION			

ROADWAY PAY QUANTITY NOTES

- (R-1) PAYMENT FOR THIS ITEM WILL BE BASED ON PLAN QUANTITY ONLY. SEE SECTION 109.01B OF THE STANDARD SPECIFICATIONS.
- (R-4) INCLUDES 1000 CU. YDS. FOR DRIVEWAYS, RETURNS, DIKES, AND MISCELLANEOUS.
- (R-5) AN ESTIMATED QUANTITY OF 8,487 C.Y. TOPSOIL TO BE RESERVED FOR REPLACEMENT OF APPROXIMATELY 5' ON COMPLETED FORESLOPES, DITCHES, AND BACKSLOPES. THIS QUANTITY IS INCLUDED IN THE EARTHWORK BALANCE. ANY ADDITIONAL EXCAVATION REQUIRED IN CUT SECTIONS TO ALLOW FOR PLACEMENT OF TOPSOIL TO FINAL GRADE, SHALL BE INCLUDED IN THE PRICE BID.
- (R-7) FOR TYPE A - SALVAGED TOPSOIL PRICE BID TO INCLUDE COST OF 18-46-0 FERTILIZER, ESTIMATED AT 150 POUNDS PER ACRE.
FOR SOLID SLAB SODDING PRICE BID TO INCLUDE COST OF 10-20-10 FERTILIZER, ESTIMATED AT 200 POUNDS PER 1,000 SQ. YDS.
- (R-8) FOR SOLID SLAB SODDING PRICE TO INCLUDE THE COST OF WATERING, ESTIMATED AT 80 GALLONS PER S.Y.
- (R-11) THE QUANTITIES ESTIMATED FOR TEMPORARY EROSION AND SEDIMENT CONTROL IS 13 ACRES.
- (R-16) QUANTITY BASED ON TWO APPLICATIONS.
- (R-25) ESTIMATED AT 150 LBS. PER CU. FT.
- (R-28) PRIME COAT SHALL BE APPLIED AT AN ESTIMATED RATE OF 0.35 GAL. PER SQ. YD. WHEN APPLIED TO SUBGRADE AND 0.25 GAL. PER SQ. YD. WHEN APPLIED TO AGGREGATE BASE. THE ACTUAL CUTBACK PRIME COAT REQUIRED FOR PLACEMENT OPERATIONS WILL BE DETERMINED BY THE CONTRACTOR, AND SHALL CONSIDER THE RESIDUE FROM DISTILLATION PERCENTAGE SHOWN IN SECTION 708.03 OF THE STANDARD SPECIFICATIONS.
- (R-32) ESTIMATED AT 112 LBS. PER SQ. YD. PER 1" THICK.
- (R-41) QUANTITY INCLUDES AN ESTIMATED 20 C.Y. TO BE USED AS DIRECTED BY THE ENGINEER.
- (R-48) INCLUDES REMOVAL OF ALL EXISTING ROADWAY DRAINAGE STRUCTURES, HEADWALLS (UNLESS OTHERWISE SPECIFIED), INLETS, FENCES, AND OTHER STRUCTURES WITHIN THE RIGHT-OF-WAY.
- (R-49) TO BECOME THE PROPERTY OF AND BE DISPOSED OF BY THE CONTRACTOR IN A MANNER APPROVED BY THE ENGINEER.
- (R-50) MATERIALS REMOVED SHALL NOT BE MEASURED FOR PAYMENT UNDER SECTION 202.06 UNCLASSIFIED EXCAVATION.
- (R-52) INCLUDES 2% FOR GROUND MEASUREMENT.
- (R-53) ALL GATES AND GATE END POSTS FOR STRANDED WIRE FENCE (SWF) SHALL BE CONSTRUCTED AT THE SAME WIDTH AS THE EXISTING, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

PAY ITEM NOTES

- (1) INCLUDES 1,095 CY OF EXCAVATION FOR ROADWAY DRAINAGE STRUCTURES. SEE SUMMARY OF DRAINAGE STRUCTURES FOR DETAILS.
- (2) PRICE BID FOR THIS ITEM TO INCLUDE THE COST OF SEDIMENT REMOVAL AS REQUIRED DURING CONSTRUCTION AS DIRECTED BY THE ENGINEER.
- (3) INCLUDES 100 TONS ADDITIONAL QUANTITY TO BE USED AS DIRECTED BY THE ENGINEER.
- (4) ESTIMATED AT 0.075 GAL PER S.Y. PRIOR TO DILUTION.
- (5) PRICE BID INCLUDES 15 CY AS SHOWN TO PLUG TEMPORARY STRUCTURES AND 35 CY TO PLUG AND ABANDON EXISTING STRUCTURES AS SHOWN ON PLAN AND PROFILE SHEETS.
- (6) ESTIMATED AT 150 LB PER CU. FT.
- (7) PRICE BID INCLUDES TRENCH EXCAVATION AND STANDARD BEDDING MATERIAL AS SHOWN ON STD SPI-4, SPB-1 (LATEST VERSION) AND ON THE SUMMARY OF DRAINAGE STRUCTURES.
- (8) PRICE SHALL INCLUDE, BUT NOT LIMITED TO THE REMOVAL OF CONCRETE DITCH LINER, CONCRETE FLUMES, FOOTINGS, STRUCTURES, STORM SEWERS, PIPELINES, LIGHT POLES, FENCES, GUARDRAIL, METER BOXES, POLES, SHEET METAL SIGNS, DELINEATORS, ETC. AND ANY OTHER NON-ORGANIC ITEM NOT SPECIFICALLY LISTED AS REMOVAL PAY ITEM. ITEMS TO BE REMOVED MAY OR MAY NOT BE PRESENT IN ANY SPECIFIED CONDITION.
- (9) PRICE BID INCLUDES SAW CUTTING AS NECESSARY TO REMOVE EXISTING PAVEMENT.
- (10) FOR SEPARATOR FABRIC USE MIRAFI RS380I OE APPROVED EQUIVALENT
- (11) IN ADDITION TO SELECTION 642.04(B) THE CONTRACTOR IS RESPONSIBLE FOR THE FOLLOWING: SURVEY CONTROL POINTS, REFERENCE POINTS AND BENCH MARKS NOTED ON THE PLANS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND REFERENCING THE CENTERLINE POINTS REQUIRED FOR CONSTRUCTION TO INCLUDE BRIDGE, CURVES, CONSTRUCTION REFERENCE LINES (CRL), AND RIGHT-OF-WAY. THE SURVEYOR WILL PROVIDE THE RESIDENT ENGINEER WITH A COMPUTERIZED DISK OF SURVEY DATA.

THE SURVEYOR WILL IDENTIFY AND VERIFY BENCH MARKS SET AND MAINTAIN ADDITIONAL BENCH MARKS WITHIN THE PROJECT LIMITS AT A MINIMUM OF 500' AS REQUIRED TO ENSURE CONSTRUCTION OF A SMOOTH PROFILE OF MAINLINE AND TO ENSURE SMOOTH TRANSITIONS AT THE BOP, EOP, AND BRIDGES AS REQUIRED IN SECTION 642.04(C). THE SURVEYOR WILL SET TWO BENCHMARKS ON EACH BRIDGE, ONE ON EACH ABUTMENT. THE SURVEYOR WILL PROVIDE A COPY OF CHECKED BENCH MARKS TO THE RESIDENT ENGINEER FOR REVIEW AND ACCEPTANCE PRIOR TO BEGINNING ANY EARTHWORK PAY ITEMS.
- (12) COLD MILLING PAVEMENT TO BECOME PROPERTY OF THE STATE AND BE DELIVERED TO A LOCATION WITHIN 10 MILES OF THE PROJECT SITE.
- (13) TOP 6 INCHES OF ASPHALT PAVEMENT TO BE REMOVED BY COLD MILLING AND MILLINGS SHALL BECOME PROPERTY OF THE STATE.
- (14) INCLUDES COST OF CURBING AS SHOWN ON THE PLAN SHEETS "DRAINS AT END OF BRIDGE."
- (15) INCLUDES 2325 CY FOR THE CONSTRUCTION OF WICK DRAINS.

21006(07) SUMMARY OF PAY QUANTITIES 0100 ROADWAY

ITEM NO.	SPEC CODE	DESCRIPTION	UNIT	QUANTITY
201(A)	0102	CLEARING AND GRUBBING	LSUM	1
202(A)	0183	UNCLASSIFIED EXCAVATION	(R-1)(1) CY	65,864
202(D)	0184	UNCLASSIFIED BORROW	(R-4) CY	83,082
205(A)	4229	TYPE A - SALVAGED TOPSOIL	(R-5)(R-7) LSUM	1
221(C)	2801	TEMPORARY SILT FENCE	(2) LF	4,800
221(K)	0600	TEMPORARY FIBER LOG	(2) LF	419
230(A)	2806	SOLID SLAB SODDING	(R-7)(R-8) SY	61,107
233(A)	2817	VEGETATIVE MULCHING	(R-11) AC	13
241	2832	MOWING	(R-16) AC	26
303(A)	2100	AGGREGATE BASE TYPE A	(15) CY	6,861
307(K)	4300	STABILIZED SUBGRADE	SY	36,398
325	5271	SEPARATOR FABRIC	(10) SY	22,662
402(E)	0225	TRAFFIC BOUND SURFACE COURSE TYPE E	(3)(R-25) TON	2,388
407(B)	0250	TACK COAT	(4) GAL	4,793
408	5774	PRIME COAT	(R-28) GAL	18,776
411(B)	5940	SUPERPAVE, TYPE S3(PG 70-28 OK)	(R-32) TON	1,971
411(B)	5945	SUPERPAVE, TYPE S3(PG 64-22 OK)	(R-32) TON	9,213
411(C)	5955	SUPERPAVE, TYPE S4(PG 70-28 OK)	(R-32) TON	1,315
411(C)	5960	SUPERPAVE, TYPE S4(PG 64-22 OK)	(R-32) TON	3,108
412	5267	COLD MILLING PAVEMENT	(12) SY	5,204
501(A)	0313	STRUCTURAL EXCAVATION UNCLASSIFIED	(R-1) CY	81
501(G)	6315	CLSM BACKFILL	(5) CY	50
509(A)	0319	CLASS AA CONCRETE	(R-1) CY	228
509(D)	0325	CLASS C CONCRETE	(R-4) CY	20
511(A)	0332	REINFORCING STEEL	(R-1) LB	31,495
601(A)	0297	TYPE I PLAIN RIPRAP	(6) TON	140
611(G)	6002	INLET (SMD-TYPE 2)	EA	1
613(A)	0491	18" R.C. PIPE, CLASS III	(7) LF	292
613(B)	0689	18" CORR. GALV. STEEL PIPE	LF	124
613(B)	0690	24" CORR. GALV. STEEL PIPE	LF	38
613(L)	5726	18" PREFAB. CULVERT END SECTION, ROUND	EA	7
613(L)	5730	24" PREFAB. CULVERT END SECTION, ROUND	EA	2
614(A)	7049	PREFABRICATED WICK DRAIN	LF	1,079,600
619(A)	0920	REMOVAL OF STRUCTURES & OBSTRUCTIONS	(8)(R-48)(R-49)(R-50) LSUM	1
619(B)	4728	REMOVAL OF ASPHALT PAVEMENT	(9)(13)(R-49)(R-50) SY	25,512
623	0100	(PL) GUARDRAIL CURBING	(14) EA	8
623(A)	0932	BEAM GUARDRAIL W-BEAM SINGLE	LF	1,600
623(G)	8590	GUARDRAIL END TREATMENT (31")	EA	4
623(I)	8700	GUARDRAIL BRIDGE CONN-THRIE BEAM (31")	EA	8
624(C)	4458	FENCE - STYLE SWF (4 BARBED WIRE)	(R-52)(R-53) LF	2,912
624(C)	4459	FENCE - STYLE SWF (5 BARBED WIRE)	(R-52)(R-53) LF	2,425
624(E)	4288	FENCE - STYLE CLF (4' HIGH, CLASS A)	(R-52)(R-53) LF	167

21006(07) SUMMARY OF PAY QUANTITIES 0600 STAKING

ITEM NO.	SPEC CODE	DESCRIPTION	UNIT	QUANTITY
642(B)	0096	CONSTRUCTION STAKING LEVEL I I	(11) LSUM	1

21006(07) SUMMARY OF PAY QUANTITIES 0640 CONSTRUCTION

ITEM NO.	SPEC CODE	DESCRIPTION	UNIT	QUANTITY
220	2800	SWPPP DOCUMENTATION AND MANAGEMENT	LSUM	1
640(A)	1426	FIELD OFFICE	EA	1
641	1552	MOBILIZATION	LSUM	1

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18	GENERAL NOTES & SUMMARY OF PAY QUANTITIES (ROADWAY)	
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
		SHEET 1 OF 3
STATE JOB PIECE NO: 21006(07)		SHEET NO. AR01

9:54:30 AM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.01 General\21006GNO1.dgn
 6/7/2018

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	AR02	44
REVISIONS					
NO.	DATE	DESCRIPTION			

GENERAL CONSTRUCTION NOTES

THE PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING THE EXISTING ROAD TO LOCAL AND THROUGH TRAFFIC. SEE STANDARD SPECIFICATIONS FOR MAINTENANCE OF LOCAL AND THROUGH TRAFFIC.

MAINTENANCE OF THROUGH TRAFFIC INCLUDES THE MAINTENANCE OF THE EXISTING ROAD IN CLOSE PROXIMITY TO THE NEW CONSTRUCTION AS SHOWN ON THE PLANS.

ALL TREES, BRUSH, AND OTHER DEBRIS THAT MIGHT INTERFERE WITH THE FLOW OF WATER SHALL BE CLEANED OUT TO THE RIGHT-OF-WAY LINE, AT EACH STRUCTURE AND BRIDGE, IN A MANNER APPROVED BY THE ENGINEER. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

THE CONTRACTOR SHALL PROVIDE ALL TEMPORARY RIGHT-OF-WAY FENCE AS REQUIRED. WHEN THE PORTION OF THE PROJECT THAT REQUIRED THIS FENCE IS COMPLETED, THE TEMPORARY FENCE SHALL BE REMOVED, AND PERMANENT RIGHT-OF-WAY FENCING SHALL BE RESTORED OR INSTALLED IN A MANNER APPROVED BY THE ENGINEER. ALL COST OF TEMPORARY FENCING SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

ALL FLOWLINES THAT ARE TO BE FILLED SHALL BE THOROUGHLY TAMPED BEFORE CONSTRUCTION OR EXTENSION OF DRAINAGE STRUCTURES. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

IN ORDER TO ALLEVIATE DUST CONDITIONS DURING GRADING OPERATIONS AND BEFORE PAVEMENT WORK IS COMPLETED, THE CONTRACTOR SHALL SPRINKLE GRADING AT INTERVALS APPROVED BY THE ENGINEER. ALL COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

THE CONTRACTOR SHALL NOT WASTE ANY EXCESS EXCAVATION UNTIL ALL PLANNED EMBANKMENTS AND BACKFILLS ARE COMPLETED. EXCESS UNCLASSIFIED EXCAVATION MATERIAL DETERMINED BY THE ENGINEER TO BE SUITABLE FOR BACKFILL SHALL BE USED TO REDUCE ANY UNCLASSIFIED BORROW NEEDED. COST OF SECOND HANDLING SHALL BE INCLUDED IN OTHER ITEMS OF WORK. ANY REMAINING EXCESS EXCAVATION SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND BE DISPOSED OF IN A MANNER APPROVED BY THE ENGINEER.

PRIME COAT SHALL BE APPLIED TO THE SUBGRADE IMMEDIATELY AFTER FINAL COMPACTION AND SHAPING TO RETAIN MOISTURE FOR PROPER CHEMICAL REACTION OF THE SOIL ADDITIVE.

THE CONTRACTOR SHALL KEEP THE OPEN TRENCH DRAINED. COST TO BE INCLUDED IN OTHER ITEMS OF WORK.

VEGATIVE MULCHING: THE VEGATIVE MULCH SHALL BE ANCHORED IN ACCORDANCE WITH THE "MULCHING-TILLER METHOD", AS SPECIFIED IN 233.04B(2) OF THE STANDARD SPECIFICATIONS.

AREAS ON WHICH SALVAGED TOPSOIL IS TO BE REPLACED SHALL HAVE 18-46-0 FERTILIZER APPLIED, AT THE RATE OF 150 POUNDS PER ACRE, JUST PRIOR TO THE REPLACEMENT OF SALVAGED TOPSOIL.

AT THE BEGINNING OF TURFING OPERATIONS, ANY AREAS INCLUDED IN PLANNED QUANTITIES THAT HAVE GROWN A SATISFACTORY VOLUNTEER TURF OF PERENNIAL GRASS, AS DETERMINED BY THE ENGINEER, SHALL BE FERTILIZED AND WATERED AS CALLED FOR ON THE PLANS, BUT SHALL NOT BE SEEDED, SODDED, OR SPRIGGED.

SURFACING OF RETURNS, UNLESS OTHERWISE SHOWN ON THE PLANS, SHALL BE OF THE SAME MATERIAL (BASE AND SURFACE) AS THAT OF THE ABUTTING SHOULDER OF THE MAINLINE. BASE AND SURFACE THICKNESS SHALL BE THE THICKNESS SHOWN ON PLANS.

T.B.S.C. SURFACES SHALL BE SPRINKLED WITH WATER AND ROLLED WITH A PNEUMATIC ROLLER IN A MANNER APPROVED BY THE ENGINEER.

EXCESS ASPHALT AT JOINTS AND CRACKS IN EXISTING PAVEMENT SHALL BE REMOVED FLUSH TO TOP OF PAVING IN A MANNER APPROVED BY THE ENGINEER.

IN ACCORDANCE WITH THE OKLAHOMA UNDERGROUND FACILITIES DAMAGE PREVENTION ACT THE CONTRACTOR SHALL NOTIFY THE OKLAHOMA ONE-CALL SYSTEM, INC. 48 HOURS PRIOR TO BEGINNING EXCAVATION. OKLAHOMA ONE-CALL SYSTEM, INC. "CALL OKIE" 1-800-522-6543 OR 811

ENVIRONMENTAL MITIGATION NOTES

CULTURAL RESOURCES

LOCATIONS OUTSIDE THE PROJECT AREA IN THE FOLLOWING AREA MUST NOT BE UTILIZED FOR BORROW, EQUIPMENT STAGING, HAUL ROADS, SPOIL DUMPS OR ANY OFF-SITE PROJECT-RELATED ACTIVITY.

T&E R6E:
 Section 35:
 Derelict RR R/W: NW ¼, NE ¼, NE ¼
 SW ¼, NE ¼, NE ¼
 SE ¼, NE ¼, NE ¼
 NE ¼, NE ¼, NW ¼, NE ¼

AMERICAN BURYING BEETLE (ABB)
 THE AMERICAN BURYING BEETLE IS A LARGE CARRION BURYING BEETLE THAT OCCURS IN THE ACTION AREA. NO ARTIFICIAL LIGHTING SHALL BE USED DURING CONSTRUCTION. CARCASSES AND ALL FOOD TRASH SHALL BE REMOVED FROM THE PERMANENT AND TEMPORARY ROW THROUGHOUT PROJECT ACTIVITIES.

MIGRATORY BIRDS

MIGRATORY BIRDS ARE PROTECTED BY THE FEDERAL MIGRATORY BIRD TREATY ACT. MANY BIRDS COMMONLY USE BRIDGES AND CULVERTS FOR NESTING. THE NESTING SEASON FOR MOST MIGRATORY BIRD SPECIES EXTENDS FROM APRIL 1 TO AUGUST 31. MIGRATORY BIRD NESTING USE OF BRIDGES NBI#S . 10053; 12977; 12934; 12935; 12980; 01807; 13783; 13925; AND 13757; AND CULVERTS AT STATIONS 132+71; 209+13; 240+87; 350+84; 383+72; 417+05; AND 495+43 WAS OBSERVED. PAINTING, REPAIR, RETROFIT, REHABILITATION OR DEMOLITION OF THE EXISTING BRIDGE/STRUCTURES SHALL BE CONDUCTED BETWEEN SEPTEMBER 1, AND MARCH 31, WHEN MIGRATORY BIRD NESTS ARE NOT OCCUPIED. IF PAINTING, REPAIR, RETROFIT, REHABILITATION OR DEMOLITION CANNOT BE COMPLETED BETWEEN SEPTEMBER 1 AND MARCH 31, THE BRIDGE SHALL BE PROTECTED FROM NEW NEST ESTABLISHMENTS PRIOR TO APRIL 1, BY MEANS THAT DO NOT RESULT IN BIRD DEATH OR INJURY. OPTIONS INCLUDE THE EXCLUSION OF ADULT BIRDS FROM SUITABLE NEST SITES ON OR WITHIN A STRUCTURE BY THE PLACEMENT OF WEATHER-RESISTANT POLYPROPYLENE NETTING WITH 0.25-INCH OR SMALLER OPENINGS, PRIOR TO APRIL 1. METHODS OTHER THAN NETTING MUST BE PRE-APPROVED BY THE ODOT BIOLOGIST.

HAZARDOUS MATERIALS

STATION	OCC FAC./CASE NO.	FACILITY
APP. 104+75 TO 107+50 RT 25 FT.	6702166/064-0191	DOWELL SCHLUMBERGER
APP. 107+00 TO 109+00 LT 50 FT.	6719421/064-2803	SEMINOLE BATCH PLANT
APP. 213+50 TO 216+50 RT 25 FT.	6702350/064-BI	HALLIBURTON SERVICES

PETROLEUM CONTAMINATION MAY EXIST AT OR NEAR THE REFERENCED LEAKING UNDERGROUND STORAGE TANK (LUST) SITES. BASED ON THE AVAILABLE INFORMATION, CONTAMINATION IS NOT EXPECTED TO AFFECT CONSTRUCTION ACTIVITIES, BUT IS STILL POSSIBLE. IN THE EVENT CONTAMINATED SOIL OR GROUNDWATER IS ENCOUNTERED, THE CONTRACTOR SHALL ADHERE TO ODOT'S HAZARDOUS MATERIALS SPECIFICATION 107.15 AND NOTIFY THE RESIDENT ENGINEER, WHO MAY THEN CONTACT THE ENVIRONMENTAL PROGRAMS DIVISION AT (405) 521-3050 FOR ASSISTANCE.

AN OIL/GAS WELL WAS OBSERVED DURING SITE RECONNAISSANCE LOCATED WITHIN THE PROPOSED RIGHT-OF-WAY APPROXIMATELY AT STATION 122+00 RT 10 FT. AS A RESULT, THERE IS A POTENTIAL TO ENCOUNTER CRUDE OIL PRODUCTS AND RELATED WASTES. IF SUCH MATERIALS ARE FOUND, THE RESIDENT ENGINEER SHOULD BE NOTIFIED IMMEDIATELY.

IN ADDITION, ANY OIL/ GAS WELLS ENCOUNTERED DURING CONSTRUCTION ACTIVITIES MUST BE PLUGGED BY PROPERLY LICENSED PERSONNEL, IN ACCORDANCE WITH ALL APPLICABLE OKLAHOMA CORPORATION COMMISSION RULES AND REGULATIONS.

AN ILLICIT DUMP SITE EXISTS AT THE PROPERTY LOCATED APPROXIMATELY AT STATION 289+50 RT TO 296+25 RT. THIS SITE IS THE SUBJECT OF AN ACTIVE ODEQ INVESTIGATION (ODEQ CASE NUMBER 15-021). AS SUCH, THE EXTENT OF GROUND DISTURBANCE NEEDED FOR THE PROJECT HAS BEEN MINIMIZED, BUT NOT ELIMINATED. IF EVIDENCE OF DUMPED WASTE IS ENCOUNTERED, THE CONTRACTOR SHALL IMMEDIATELY CEASE WORK IN THE AREA AND NOTIFY THE RESIDENT ENGINEER, WHO MAY THEN CONTACT THE ENVIRONMENTAL PROGRAMS DIVISION AT (405) 521-3050 FOR ASSISTANCE.

THE CONTRACTOR'S OPERATION MUST PROCEED ON ITEMS OF WORK NOTE RELATED TO, OR IN THE VICINITY OF THE POTENTIALLY HAZARDOUS OR CONTAMINATED MATERIALS. THE CONTRACTOR'S OPERATIONS IN THE VICINITY OF THE POTENTIALLY HAZARDOUS OR CONTAMINATED MATERIALS MUST NOT RESUME UNTIL SO DIRECTED BY ODOT.

SUGGESTED SEQUENCE OF CONSTRUCTION

PHASE 1

- (1) INSTALL ROAD TRAFFIC CONTROL DEVICES BASED ON PHASE 1 TRAFFIC CONTROL.
- (2) EXTEND STR'S 2, 4 AND 5 ON SOUTH SIDE.
- (3) CONSTRUCT GRADING AND SURFACING ON THE SOUTH HALF OF PROPOSED ROADWAY. CONSTRUCT SOUTH PORTION OF NS 3610 ROAD. SEE PHASE 1 OF TCP TYPICAL SECTIONS.
- (4) CONSTRUCT SOUTH 37'-6" OF PROPOSED BRIDGE STRUCTURES. SEE PHASE 1 OF BRIDGE CONSTRUCTION SEQUENCE.

PHASE 2

- (1) INSTALL ROAD TRAFFIC CONTROL DEVICES AND PROTABLE LONGITUDINAL BARRIER BASED ON PHASE 2 TRAFFIC CONTROL. SHIFT TRAFFIC TO PHASE 1 PERMANENT PAVING AND BRIDGE.
- (2) CONSTRUCT GRADING AND SURFACING ON THE NORTH HALF OF PROPOSED ROADWAY. SEE PHASE 2 OF TCP TYPICAL SECTIONS.
- (4) CONSTRUCT NORTH 33'-6" OF PROPOSED BRIDGE STRUCTURES. SEE PHASE 2 OF BRIDGE CONSTRUCTION SEQUENCE.

PHASE 3

- (1) COMPLETE FINAL TRAFFIC STRIPING AND MISCELLANEOUS CONSTRUCTION OPERATIONS.
- (2) OPEN ALL LANES FOR TRAFFIC.

****NOTE**** PRIOR TO CONSTRUCTION, THE CONTRACTOR MAY SUBMIT AN ALTERNATIVE WRITTEN SEQUENCE TO THE ENGINEER FOR APPROVAL.

2:39:49 PM
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.01 General/21006GND02.dgn
 6/5/2018

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18	GENERAL NOTES (ROADWAY)	
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
		SHEET 2 OF 3
STATE JOB PIECE NO: 21006(07)		SHEET NO. AR02

TRAFFIC CONSTRUCTION
PAY QUANTITY NOTES

- (TC-1) THE CONTRACTOR SHALL FURNISH AND INSTALL SUCH LIGHTS, SIGNS, BARRICADES, AND PROVIDE FLAGGERS NECESSARY FOR THE CONTROL, SAFETY, AND MAINTENANCE OF TRAFFIC WHEN INSTALLING, RELOCATING OR DELIVERING PORTABLE LONGITUDINAL BARRIER.
- (TC-2) QUANTITY INCLUDES SUFFICIENT LENGTH OF PORTABLE LONGITUDINAL BARRIER TO PROVIDE FOR THE LONGEST SECTION SHOWN ON THE PLANS. THE SAME BARRIER WILL BE USED ON OTHER DETOUR PHASES.
- (TC-13) A PART, OR ALL, OF THIS ITEM IS INTENDED FOR REPLACEMENT OF REMOVED CONFLICTING STRIPING.
- (TC-14) SEE STANDARD DRAWING PM1-1, PM2-1, PM3-1, PM4-1, PM5-1, PM6-1, PM7-1, PM8-1 (LATEST REVISION). A PART, OR ALL, OF THE QUANTITY SHOWN IS TO BE USED AS FINAL PAVEMENT MARKING.
- (TC-17) INCLUDES AN ESTIMATED 12,400 L.F. (PAINT)(4" WIDE) WHITE AND 12,400 L.F. (PAINT)(4" WIDE) YELLOW STRIPE.
- (TC-20) ALL STRIPING TO BE PLACED ON TEMPORARY SURFACES OR ON SURFACES SCHEDULED TO BE REMOVED SHALL BE DONE WITH PAINT UNLESS OTHERWISE NOTED ON THE PLANS OR STANDARD DRAWINGS. TEMPORARY PAVEMENT MARKINGS PLACED ON FINISHED PAVEMENT OR EXISTING PAVEMENT TO REMAIN IN PLACE SHALL USE ONE OF THE FOLLOWING METHODS:
REMOVABLE PAVEMENT MARKING TAPE
CLASS A PAVEMENT MARKERS
- (TC-21) INCLUDED IN THE COST OF THIS ITEM SHALL BE THE INSTALLATION, MAINTENANCE, AND REMOVAL. THIS ITEM SHALL BE BID ACCORDINGLY.
- (TC-22) AMOUNT SHOWN IS AN APPROXIMATION AND THE ACTUAL AMOUNT OF REMOVAL, IF NECESSARY, SHALL BE DETERMINED BY THE ENGINEER. PRICE BID FOR PAVEMENT MARKING REMOVAL SHALL INCLUDE THE COST OF REMOVING STRIPE, ARROWS, WORDS AND SYMBOLS, AS SHOWN IN PLANS. THESE ITEMS MAY CONSIST OF PLASTIC, PAINT OR NON-REMOVABLE MARKING TAPE.
- (TC-26) CONSTRUCTION TRAFFIC CONTROL WILL BE IMPLEMENTED ACCORDING TO CONSTRUCTION PLANS, AND INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (CURRENT EDITION), AND COMPLIANT WITH APPLICABLE O.D.O.T. STANDARD DRAWINGS. PRICE BID FOR THIS ITEM SHALL BE PAYMENT IN FULL FOR THE INSTALLATION, MAINTENANCE AND SUBSEQUENT REMOVAL OF ALL NECESSARY CONSTRUCTION TRAFFIC CONTROL DEVICES REQUIRED FOR COMPLETION OF THE PROJECT.
- ALL SIGNS AND BARRICADES WHICH ARE SHOWN WITH TYPE 'A' LIGHTS IN THE STANDARD DRAWINGS SHALL HAVE THE CORRESPONDING LIGHT ATTACHED DURING NON-DAYLIGHT HOURS.
- (TC-33) ALL CONSTRUCTION WORK ZONE SIGNS SHALL HAVE FLUORESCENT SHEETING. THE FLUORESCENT SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956 (LATEST REVISION).
- THE MANUFACTURER SHALL FURNISH A TYPE 'D' CERTIFICATION IN ACCORDANCE WITH O.D.O.T STANDARD SPECIFICATIONS (CURRENT EDITION) SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON MATERIAL SUBMITTED FOR APPROVAL.
- (TC-52) ANY USED CHANGEABLE MESSAGE SIGN OR CONSTRUCTION ZONE IMPACT ATTENUATOR TO BE PLACED ON THIS PROJECT SHALL BE SUBJECT TO INSPECTION AND APPROVAL, BY THE OKLAHOMA DEPARTMENT OF TRANSPORTATION, TO ASSURE THAT THEY ARE IN GOOD WORKING CONDITION, PRIOR TO PLACEMENT ON THE PROJECT.
- (TC-61) ANY DAMAGE TO A FINISHED OR EXISTING SURFACE RESULTING FROM THE CONTRACTOR'S NEGLIGENCE IN THE REMOVAL OF CONSTRUCTION ZONE PAVEMENT MARKERS OR CHANNELIZING DEVICES AND THE BITUMINOUS ADHESIVE USED IN THEIR INSTALLATION, SHALL BE REPAIRED AT THE CONTRACTOR'S EXPENSE AND TO THE SATISFACTION OF THE ENGINEER.
- (TC-70) THIS IS AN ESTIMATED QUANTITY TO BE USED AS DEEMED NECESSARY BY THE ENGINEER.
- (TC-73) QUANTITY SHOWN INCLUDES 750 EA. (WHITE) AND 750 EA. (YELLOW) CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TABS). THESE CONSTRUCTION ZONE PAVEMENT MARKERS SHALL BE EITHER "DAVIDSON PLASTICS: MODEL TOM", OR AN APPROVED EQUAL. PRICE BID FOR THIS ITEM SHALL INCLUDE THE INITIAL PLACEMENT MARKERS (FLEX TABS) SHALL BE INSTALLED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS AND AS SHOWN ON STANDARD DRAWING TCS21-1-1(LATEST REVISION).
- (TC-75) TEMPORARY PAVEMENT MARKINGS SHALL BE IN PLACE THE SAME DAY THAT EXISTING PAVEMENT MARKERS ARE REMOVED FROM ANY ROADWAY OPEN TO TRAFFIC. ALSO, ALL TEMPORARY PAVEMENT MARKINGS SHALL BE REMOVED PRIOR TO THE INSTALLATION OF FINAL STRIPING.
- (TC-80) INCLUDED IN THIS ITEM SHALL BE ONE (1) ADDITIONAL UNIT TO BE USED AS A STAND-BY OR REPLACEMENT. THIS STAND-BY UNIT SHALL BE IMMEDIATELY ACCESSIBLE TO REPLACE A DAMAGED, STOLEN OR MALFUNCTIONING UNIT. THE AMOUNT OF TIME BETWEEN THE REMOVAL OF THE DAMAGED UNIT AND THE INSTALLATION OF THE STAND-BY UNIT SHALL BE NO MORE THAN TWENTY-FOUR (24) HOURS.
- (TC-84) 360 CONSTRUCTION CALENDAR DAYS WERE USED TO COMPUTE THE SIGN DAY PAY ITEMS. THE AMOUNT OF CALENDAR DAYS USED TO COMPUTE THE SIGN DAY PAY ITEMS IS AN ESTIMATED QUANTITY ONLY, BASED ON THE CURRENT O.D.O.T. STANDARDS AND SUGGESTED CONSTRUCTION SEQUENCE FOR THIS PROJECT. THESE ESTIMATED SIGN DAY QUANTITIES MAY CHANGE AS THE PROJECT'S CONSTRUCTION TRAFFIC CONTROL IS MODIFIED DURING CONSTRUCTION.
- (TC-85) THESE SIGNS MUST BE ON THE OKLAHOMA DEPARTMENT OF TRANSPORTATION LIST OF APPROVED CHANGEABLE MESSAGE SIGNS. FOR A LIST OF THE APPROVED SIGNS GO TO THE OKLAHOMA DEPARTMENT OF TRANSPORTATION WEBSITE AT: <http://www.okladot.state.ok.us/traffic/qpl/index.php>

21006(07) SUMMARY OF PAY QUANTITIES					
0310 TRAFFIC SIGNING AND STRIPING					
ITEM NO.	SPEC CODE	DESCRIPTION	UNIT	QUANTITY	
850(A)	8110	SHEET ALUMINUM SIGNS (TS-34)	SF	184	
851(C)	8324	2" SQUARE TUBE POST (TS-33)	LF	208	
853	9069	GUARDRAIL DELINEATORS (TYPE 2 CODE 1)	EA	36	
855(A)	8812	TRAFFIC STRIPE (PLASTIC)(4" WIDE) (TC-13, 14)(TS-19)	LF	37,277	
855(A)	8818	TRAFFIC STRIPE (PLASTIC)(12" WIDE) (TC-14)(TS-22)	LF	3,965	
855(A)	8825	TRAFFIC STRIPE (PLASTIC)(24" WIDE) (TC-14)(TS-23)	LF	24	
855(B)	8818	TRAFFIC STRIPE (PLASTIC) (ARROWS) (TC-14)	EA	3	
855(B)	8821	TRAFFIC STRIPE (PLASTIC)(WORDS) (TC-14)	EA	2	

21006(07) SUMMARY OF PAY QUANTITIES					
0340 TRAFFIC CONTROL					
ITEM NO.	SPEC CODE	DESCRIPTION	UNIT	QUANTITY	
857(A)	8839	CONSTRUCTION TRAFFIC STRIPE (PAINT)(4" WIDE) (TC-17, 20, 70, 75)	LF	24,800	
857(E)	8890	(PL) CONSTRUCTION ZONE PAVEMENT MARKERS (FLEX TABS) TYPE 2-2 (TC-21, 61, 70, 73, 75)	EA	1,500	
857(F)	8006	PAVEMENT MARKING REMOVAL (TRAFFIC STRIPE) (TC-22, 70, 75)	LF	29,600	
871(B)	8705	(SP) CONST. ZONE IMPACT ATTN. (TC-52, 80, 84)	SD	360	
877(B)	8484	DELIVER PORTABLE LONGITUDINAL BARRIER (TC-1, 2)	LF	1,763	
880(B)	8818	CONSTRUCTION SIGNS 0 TO 6.25 SF (TC-26, 33, 84)	SD	4,680	
880(B)	8821	CONSTRUCTION SIGNS 6.26 SF TO 15.99 SF (TC-26, 33, 84)	SD	7,020	
880(B)	8824	CONSTRUCTION SIGNS 16.0 SF TO 32.99 SF (TC-26, 33, 84)	SD	360	
880(C)	8842	CONSTRUCTION BARRICADES (TYPE III) (TC-26, 84)	SD	4,680	
880(C)	8848	WING BARRICADES (TC-26, 84)	SD	1,440	
880(E)	8860	WARNING LIGHTS (TYPE A) (TC-26, 84)	SD	11,520	
880(F)	8878	DRUMS (TC-26, 84)(SP-2)	SD	21,240	
880(G)	8890	CHANNELIZER CONES (TC-26, 84)	SD	9,720	
882(A)	8306	PORT. CHANGEABLE MESSAGE SIGN (TC-52, 70, 85)(SP-1)	SD	120	

SPECIAL PROVISION NOTES

- (SP-1) CHANGEABLE MESSAGE BOARDS SHALL BE PLACED TWO WEEKS PRIOR TO ROAD WORK. LOCATIONS FOR THE CHANGEABLE MESSAGE BOARDS SHALL BE DETERMINED BY THE ENGINEER.
- (SP-2) WARNING LIGHTS TYPE 'C' ARE NOT REQUIRED.

TRAFFIC SIGNING
GENERAL CONSTRUCTION NOTES

EXISTING ROADWAY SHALL REMAIN OPEN DURING CONSTRUCTION. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROPER BARRICADES, LIGHTS, AND SIGNING WITHIN THE LIMITS OF CONSTRUCTION. ALL CONSTRUCTION SIGNING WILL BE IMPLEMENTED ACCORDING TO CONSTRUCTION PLANS. CONSTRUCTION TRAFFIC CONTROL WILL BE INSTALLED IN A MANNER APPROVED BY THE ENGINEER, IN ACCORDANCE WITH CHAPTER VI OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES, (CURRENT EDITION), AND COMPLIANT WITH APPLICABLE O.D.O.T. STANDARD DRAWINGS.

THIS PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING TRAFFIC ON CROSS STREETS. A MINIMUM OF ONE LANE IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES.

THIS PROJECT SHALL BE CONSTRUCTED WITHOUT CLOSING THE EXISTING ROAD TO LOCAL AND THROUGH TRAFFIC. SEE O.D.O.T. STANDARDS AND DETAIL DRAWINGS FOR MAINTENANCE OF LOCAL AND THROUGH TRAFFIC.

ANY DAMAGE CAUSED BY THE CONTRACTOR TO ANY STRUCTURES, ROADWAY SURFACES, STRIPING, RAISED PAVEMENT MARKERS, GUARDRAIL, SLOPES, AND SIGNS SHALL BE REPAIRED AT CONTRACTOR'S EXPENSE TO THE SATISFACTION OF THE ENGINEER.

ALL REGULATORY SIGNS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE III SHEETING.

ALL WARNING SIGNS SHALL HAVE FLUORESCENT YELLOW SHEETING. THE FLUORESCENT YELLOW SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) REQUIREMENTS FOR TYPE VIII SHEETING.

ALL GREEN AND BLUE SIGNS ON CONVENTIONAL HIGHWAYS SHALL HAVE HIGH INTENSITY SHEETING. THE HIGH INTENSITY SHEETING SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST REVISION) FOR TYPE III SHEETING.

ALL PANEL AND OVERHEAD SIGNS SHALL HAVE TYPE III HIGH INTENSITY BACKGROUND WITH TYPE VIII LEGENDS AND BORDERS. THE TYPE III BACKGROUND AND THE TYPE VIII LEGENDS AND BORDER SHALL MEET THE REQUIREMENTS OF ASTM D4956-(LATEST VERSION).

THE MANUFACTURER SHALL FURNISH A TYPE 'A' CERTIFICATION IN ACCORDANCE WITH ODOT STANDARD SPECIFICATIONS, LATEST EDITION, AND SUBSECTION 106.04. THE CERTIFICATION SHALL INCLUDE TEST RESULTS ON THE MATERIAL SUBMITTED FOR APPROVAL.

ALL BROKEN CONCRETE INCLUDING OLD SIGN FOOTINGS WITH STUBS, WASTE MATERIAL AND DEBRIS SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED FROM THE LIMITS OF THE PROJECT AND DISPOSED OF IN AN AREA APPROVED BY THE ENGINEER. NO PAYMENT SHALL BE MADE FOR THE DISPOSAL OF THIS MATERIAL. ANY PIPE POST OR WIDE FLANGE POST ABOVE THE OLD SIGN FOOTINGS SHALL BE CUT AND HANDLED AS PROPERTY OF THE STATE AND SHALL BE NEATLY STACKED ON THE JOB SITE, AS DESIGNATED BY THE ENGINEER UNTIL SUCH TIME AS DIVISION PERSONNEL CAN REMOVE THE MATERIAL FROM THE JOB SITE.

THE STATIONS AND LOCATIONS OF THE SIGN PLACEMENT, AS SHOWN ON THE PLAN SHEETS, ARE APPROXIMATE. EXACT STATIONS AND LOCATIONS SHALL BE DETERMINED BY THE CONTRACTOR SO THAT THE SIGN IS INSTALLED IN ACCORDANCE WITH DEPARTMENT STANDARDS AND THE MUTCD IN ORDER TO PROVIDE OPTIMUM VISIBILITY TO THE ONCOMING/APPROACHING MOTORIST. IF A PROPOSED LOCATION CONFLICTS WITH OTHER SIGNS, UTILITIES OR OTHER ROADWAY FEATURES, THE ENGINEER SHALL BE NOTIFIED.

POST LENGTHS SHOWN ON SIGN SUMMARY ARE APPROXIMATE, EXACT LENGTH SHALL BE DETERMINED BY FIELD SURVEY BY THE CONTRACTOR.

ALL REMOVED SIGNS, SIGN POSTS, BOLTS, MISCELLANEOUS HARDWARE, AND DELINEATORS SHALL REMAIN THE PROPERTY OF THE STATE. THE CONTRACTOR SHALL NEATLY STACK SUCH REMOVED MATERIAL AT A LOCATION ON THE JOB SITE AS DESIGNATED BY THE ENGINEER UNTIL SUCH TIME AS DIVISION PERSONNEL CAN REMOVE THE MATERIAL FROM THE JOB SITE.

AFTER REMOVAL OF ANY SIGN FOOTINGS, THE HOLES SHALL BE FILLED WITH SOIL AND TAMPED AND SHAPED IN A MANNER APPROVED BY THE ENGINEER.

FOR NEW OR EXISTING GROUND MOUNTED SIGNS, MAXIMUM STUB POST PROJECTION ABOVE FOOTING/GROUND LINE SHALL BE 1-3/4" +/- 1/4". MAXIMUM FOOTINGS PROJECTION ABOVE GROUND LINE SHALL BE NO MORE THAN 2". SHOULD ADDITIONAL SOIL BE REQUIRED, THE ENGINEER WILL DESIGNATE AN AREA TO OBTAIN ADDITIONAL SOIL. ALL ASSOCIATED COSTS SHALL BE INCLUDED IN OTHER ITEMS OF WORK.

ALL TEMPORARY TRAFFIC CONTROL DEVICES SHALL MEET ODOT'S, "QUALITY STANDARD FOR TEMPORARY TRAFFIC CONTROL DEVICES".

THE CONTRACTOR SHALL PROVIDE A PERSON TO BE ON 24 HOUR CALL AS NEEDED AS DETERMINED BY THE ENGINEER. THIS PERSON SHALL HOLD A CURRENT CERTIFICATION FROM THE AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA) OR THE OKLAHOMA TRAFFIC ENGINEERING ASSOCIATION (OTEA) AS A TRAFFIC CONTROL TECHNICIAN OR TRAFFIC CONTROL SUPERVISOR.

TRAFFIC SIGNING
PAY QUANTITY NOTES

- (TS-19) QUANTITY SHOWN INCLUDES 13,867 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND 23,060 L.F. TRAFFIC STRIPE (PLASTIC)(YELLOW) AND WILL BE MEASURED BY THE LINEAR FOOT OF FOUR INCH (4") WIDE TRAFFIC STRIPE.
- (TS-22) QUANTITY SHOWN INCLUDES 0 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND 3,695 (PLASTIC)(YELLOW) WILL BE MEASURED BY THE LINEAR FOOT OF TWELVE INCH (12") WIDE TRAFFIC STRIPE.
- (TS-23) QUANTITY SHOWN INCLUDES 32 L.F. TRAFFIC STRIPE (PLASTIC)(WHITE) AND WILL BE MEASURED BY THE LINEAR FOOT OF TWENTY-FOUR INCH (24") WIDE TRAFFIC STRIPE.
- (TS-33) INCLUDED IN THIS PAY ITEM IS ALL HARDWARE ASSOCIATED WITH PROPERLY ANCHORING AND MOUNTING THE HIGHWAY SIGN IN ACCORDANCE WITH O.D.O.T. PLANS AND STANDARD DRAWINGS SSA1-1 AND SSP1-1-(LATEST REVISION).
- (TS-34) INCLUDED IN THIS PAY ITEM IS THE REMOVAL OF ANY EXISTING SIGNS TO BE REPLACED BY NEW ASSEMBLIES AND THE REMOVAL OF ANY EXISTING SIGNS THAT WILL BE IN CONFLICT WITH THE NEW ROADWAY OR NEW SIGNAGE.

DESIGN:	CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN:	CPY	06/18		
CHECKED:	CPY	06/18		
APPRVD:	CPY	06/18		
			GENERAL NOTES & SUMMARY OF PAY QUANTITIES (TRAFFIC)	
			SHEET 3 OF 3	
			STATE JOB PIECE NO: 21006(07)	SHEET NO. AT01

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	AT01	44
REVISIONS					
NO.	DATE	DESCRIPTION			

SUMMARY OF SURFACING

PLAN SHEET	STATION TO STATION CRL	AGGREGATE BASE TYPE A	COLDMILLING	8" STABILIZED SUBGRADE	SEPARATOR FABRIC	T.B.S.C. TYPE E	TACK COAT	PRIME COAT	SUPERPAVE TYPE S3 (PG 70-28 OK)	SUPERPAVE TYPE S3 (PG 64-22 OK)	SUPERPAVE TYPE S4 (PG 70-28 OK)	SUPERPAVE TYPE S4 (PG 64-22 OK)
		303(A)	412	307(K)	325	402(E)	407(B)	408	411(B)	411(B)	411(C)	411(C)
		CY	SY	SY	SY	TON	GAL	GAL	TON	TON	TON	TON
MAINLINE												
2	BOP TO 357+00		2379	5951		150	819	2083		1535		873
3	357+00 TO 372+00	1942		10145	9333	738	1338	5736	959	2468	640	354
4	372+00 TO 387+00	1685		8095	8095	627	1061	4728	832	1955	555	233
5	387+00 TO 402+00	363	2407	8718	1745	300	1194	3461	180	2229	120	1036
6	402+00 TO EOP		418									47
7	NS3610	546		3489	3489	63	225	2042		677		333
	TOTALS	4,536	5,204	36,398	22,662	1,878	4,637	18,050	1,971	8,864	1,315	2,876

SUMMARY OF TEMPORARY SEDIMENT CONTROLS

STATION TO STATION CRL	TEMPORARY SILT FENCE	TEMPORARY FIBER LOG
	221(C) LF	221(K) LF
MAINLINE		
345+00 TO 372+00	1600.00	168.00
372+00 TO 402+00	2000.00	189.00
NS 361 RD.	1200.00	62.00
TOTALS	4,800	419

SUMMARY OF DRAINAGE STRUCTURES

STRUCTURE NO.	STATION	DESCRIPTION	DESIGN	FILL HEIGHT	UNCLASSIFIED EXCAVATION	STRUCTURAL EXCAVATION UNCLASSIFIED	CLASS "AA" CONCRETE	REINFORCING STEEL	INLET SMD-TYPE	18" RCP	18" CGSP	24" CGSP	18" P.C.E.S., RD.	24" P.C.E.S., RD.	CLSM CURB BACKFILL	(*) TRENCH EXCAVATION	(*) STANDARD BEDDING MATERIAL (CLASS C)
					202(A)	501(A)	509(A)	511(A)	611(G)	613(A)	613(B)	613(B)	613(L)	613(L)	501(G)	613(V)	613(R)
					FT.	CY	CY	LB	EA	LF	LF	LF	EA	EA	CY	CY	CY
MAINLINE																	
1	352+65.00	CONST. 18" CGSP X 38.0' LG. SD 44.7' LT. W/ PCES	FHTMPP-1-0, SPI-4-1, SPB-1-4, PCES-4-1	3.3							38.00		2.00				
2	354+20.23	EXTEND EXIST 6'x3' RCB, SK. σ, 48.0 RT. W/ STD. HD. WALLS & 4' CURT. WALLS	SBI-4-2, RCB-C1-6(2-14)-01E, RCB-E1-H3-0-1-01E, RCB-E1-H3-0-2-01E, RCB-CW1-D4-0-01E		435.00	20.00	39.20	5445.00									
3	355+51.78	CONST. 18" CGSP X 52.0' LG. SD 35.7' LT.	FHTMPP-1-0, SPI-4-1, SPB-1-4, PCES-4-1	3.7							52.00		2.00				
T1	355+51.78	CONST. TEMP. 18" x 60.0' LG. RCP 15.0' LG. LT. & 45.0' LG. RT. W/ SMD	FHTCP-3-1, SPI-4-1, SPB-1-4, SMD-3-1						1.00	60.00					4.00	7.00	16.00
4	360+02.78	CONST. 18" x 74.0' LG. RCP 74.0' LG. RT. W/ PCES	FHTCP-3-1, SPI-4-1, SPB-1-4							74.00			1.00			9.00	20.00
T2	363+00.00	CONST. TEMP. 18" x 86.0' LG. RCP 9.0' LG. LT. & 77.0' LG. RT.	FHTCP-3-1, SPI-4-1, SPB-1-4							86.00					6.00	0.00	24.00
T3	381+00.00	CONST. TEMP. 18" x 72.0' LG. RCP 11.0' LG. LT. & 61.0' LG. RT.	FHTCP-3-1, SPI-4-1, SPB-1-4							72.00					5.00	6.00	20.00
5	383+81.35	EXTEND EXIST 5'x7' x 67' LG. RCB 2.0' LT. & 65.0' RT W/ STD. HD. WALLS & 4' CURT. WALLS	SBI-4-2, RCB-C1-3&4&5(2-20)-01E, RCB-E1-H7-0-1-01E, RCB-E1-H7-0-2-01E, RCB-CW1-D4-0-01E		660.00	25.00	94.70	13220.00									
6	394+83.81	CONST. 24" CGSP X 38.0' LG. SD 82.6' RT.	FHTMPP-1-0, SPI-4-1, SPB-1-4, PCES-4-1	3.1								38.00		2.00			
7	400+20.75	CONST. 18" CGSP X 34.0' LG. SD 82.0' RT.	FHTMPP-1-0, SPI-4-1, SPB-1-4, PCES-4-1	2.3							34.00		2.00				
8	13+00.00 NS 3160 RD.	CONST. 8'x4' RCB, SK. σ, 58.0' LG, 29.0' LT. & 29.0' RT. W/ STD. HD. WALLS & 4' CURT. WALLS	SBI-4-2, RCB-C1-8(2-14)-01E, RCB-E1-H4-0-1-01E, RCB-E1-H4-0-2-01E, RCB-CW1-D4-0-01E		0.00	36.00	93.80	12830.00									
	TOTALS				1,095.00	81.00	227.70	31,495.00	1.00	292.00	124.00	38.00	7.00	2.00	15.00	22.00	80.00

*SUBSIDIARY TO THE PERTINENT DRAINAGE STRUCTURE AND NOT INCLUDED AS A SEPARATE PAY ITEM.

SUMMARY OF DRIVEWAYS

P&P SHEET	STATION CRL	SIDE	TYPE	WIDTH	LENGTH	RADIUS	TBSC TYPE E (6")	TACK COAT	PRIME COAT	SUPERPAVE TYPE S3 (PG 64-22 OK) (3")	SUPERPAVE TYPE S4 (PG 64-22 OK) (2")
				FT	FT	FT	402(E)	407(B)	408	411(B)	411(C)
				TON	GAL	GAL	TON	TON	TON	TON	TON
1	351+09.66	LT	20' ASPH. DRIVE	20	81	15		14	66	32	21
1	351+35.00	LT	20' TEMP TBSC DRIVE	20	57	15	47				
1	352+65.00	LT	12' ASPH. DRIVE	12	84	15		9	43	21	14
1	353+00.00	LT	12' TEMP TBSC DRIVE	12	54	15	27				
1	355+50.00	LT	22' TEMP TBSC DRIVE	22	68	15	62				
1	355+51.78	LT	22' ASPH. DRIVE	22	64	15		13	59	28	19
2	361+00.00	LT	24' TEMP TBSC DRIVE	24	96	25	100				
2	18+31.93 - NS	LT	20' ASPH. DRIVE	20	26	15		6	29	14	9
2	361+73.00	RT	20' TEMP TBSC DRIVE	20	141	25	119				
4	394+83.81	RT	12' ASPH. DRIVE	12	120	15		13	59	29	19
4	395+47.00	RT	12' TEMP TBSC DRIVE	12	111	15	55				
4	400+20.75	RT	12' ASPH. DRIVE	12	108	15		12	54	26	17
	TOTALS						410	67	310	150	99

SUMMARY OF FENCING

STATION TO STATION CRL	OFFSET		FENCE-STYLE SWF (4BW) +2%	FENCE-STYLE SWF (5BW) +2%	FENCE-STYLE CLF (4' HIGH) +2%
	LT	RT	624(C)	624(C)	624(E)
	LF	LF	LF	LF	LF
MAINLINE					
STA. 338+51.66 TO 357+03.61	X		1912		
STA. 345+00.00 TO 353+58.77		X	1000		
STA. 379+39.02 TO 393+31.70		X		1447	
STA. 393+31.70 TO 394+94.63		X			167
STA. 393+31.70 TO 394+94.63		X		978	
TOTALS			2912	2425	167

DESIGN:	CPY	06/18	US 270
DRAWN:	CPY	06/18	SEMINOLE COUNTY
CHECKED:	CPY	06/18	
APPRVD:	CPY	06/18	



SUMMARY SHEET

STATE JOB PIECE NO: 21006(07)

SHEET 1 OF 3
SHEET NO. AX01

SUMMARY OF SIGNS									
S&S LAYOUT	SIGN NUMBER	STATION	OFFSET		SIGN TYPE	DESCRIPTION	NOTES	SHEET ALUMINUM SIGNS	
			LT	RT				850(A)	851(C)
			MAINLINE					SF	LF
1	1	354+50.00		28'	W2-1E	CROSSING AHEAD	REPLACE	9.00	13
1	2	356+50.00		39'	SPECIAL SIGN NO. 1	NEW LIMA SIGN	REPLACE	5.58	13
1	3	360+00.00		40'	R2-1E(65)	SPEED LIMIT 65	INSERT	12.00	13
1	4	360+50.00	40'		W4-2(L)	LANE ENDS	INSERT	16.00	13
1	5	363+00.00	40'		R2-1E(65)	SPEED LIMIT 65	INSERT	12.00	13
1	6	363+50.00	40'		SPECIAL SIGN NO. 2	NEW LIMA SIGN	REPLACE	5.58	13
1	7	364+00.00		45'	W8-13E	BRIDGE ICES BEFORE ROAD	REPLACE	16.00	13
1	8	364+25.00	40'		R3-7(R)	RIGHT LANE MUST TURN RIGHT	INSERT	16.00	13
1	9	367+00.00	45'		W2-1E	CROSSING AHEAD	REPLACE	9.00	13
1	10	372+00.00	45'		W11-10E	TRUCK	REPLACE	16.00	13
1	11	372+00.00	45'		SPECIAL SIGN NO. 3	TRUCK ENTRANCE 1000 FT	REPLACE	13.71	
2	12	373+50.00		45'	W4-2L	MERGE RIGHT	INSERT	9.00	13
2	13	374+00.00		45'	SPECIAL SIGN NO. 4	WEWOKA CREEK	REPLACE	6.33	13
2	14	380+10.00	45'		SPECIAL SIGN NO. 4	WEWOKA CREEK	REPLACE	6.33	13
2	15	387+00.00	40'		W8-13E	BRIDGE ICES BEFORE ROAD	REPLACE	16.00	13
3	16	NS361 16+10.00		18'	R1-1E	STOP	REPLACE	7.46	13
3	17	NS361 17+00.00	18'		R1-1E	STOP	REPLACE	7.46	13
TOTALS								183.5	208

SUMMARY OF STRIPING						
STATION TO STATION CRL	TRAFFIC STRIPE (PLASTIC) (4" WIDE) (YELLOW)	TRAFFIC STRIPE (PLASTIC) (4" WIDE) (WHITE)	TRAFFIC STRIPE (PLASTIC) (12" WIDE) (YELLOW)	TRAFFIC STRIPE (PLASTIC) (24" WIDE) (WHITE)	TRAFFIC STRIPE (PLASTIC) (WORDS)	TRAFFIC STRIPE (PLASTIC) (ARROWS)
	855(A)	855(A)	855(A)	855(A)	855(B)	855(B)
	LF	LF	LF	LF	EA	EA
MAINLINE						
BOP TO STA 357+00	5200	2600	1700			
STA 357+00 TO STA 372+00	6000	3983	540		2	3
STA 372+00 TO STA 387+00	4000	3767	175			
STA 387+00 TO STA 402+00	6000	3067	1550			
STA 402+00 TO EOP	200	800	0			
NS3610	1660	0	0	24		
TOTALS	23,060	14,217	3,965	24	2	3

SUMMARY OF TRAFFIC CONTROL																									
SHEET NO.	CONST. TRAFFIC STRIPE (PAINT) (4" WIDE) YELLOW	CONST. TRAFFIC STRIPE (PAINT) (4" WIDE) WHITE	PAVEMENT MARKING REMOVAL (TRAFFIC STRIPE)	(SP) CONST. ZONE IMPACT ATTEN.		DELIVER PORTABLE LONGITUDINAL BARRIER	CONSTRUCTION SIGNS						CONSTRUCTION BARRICADES (TYPE III)	WING BARRICADES	WARNING LIGHTS (TYPE A)	DRUMS	CHANNELIZER CONES								
				EA	SD		0.00 - 6.25 S.F.		6.26 - 15.99 S.F.		16.0 - 32.99 S.F.						EA	SD	EA	SD	EA	SD	EA	SD	
							EA	SD	EA	SD	EA	SD													EA
PHASE 1	LF	LF	LF			LF	EA	SD	EA	SD	EA	SD	EA	SD	EA	SD	EA	SD	EA	SD					
1	0	0	0		0				10	1800	7	1260	1	180			0	2	360	4	720	26	4680	2	360
2					0					0	2	360		0	7	1260		0	14	2520		0	24	4320	
3					0				3	540	3	540	0	0	2	360	2	360	4	720	32	5760	4	720	
PHASE 1 TOTALS:	0	0	0	0	0	0			13	2340	12	2160	1	180	9	1620	4	720	22	3960	58	10440	30	5400	
PHASE 2																									
4	2800	2800	7700		0				10	1800	17	3060	1	180	8	1440	2	360	20	3600	28	5040	2	360	
5	6000	6000	12000	2	360	1763				0	2	360		0	7	1260		0	14	2520		0	20	3600	
6	3600	3600	9900		0				3	540	8	1440	0	0	2	360	2	360	8	1440	32	5760	2	360	
PHASE 2 TOTALS:	12400	12400	29600	2	360	1763			13	2340	27	4860	1	180	17	3060	4	720	42	7560	60	10800	24	4320	
TOTALS:	12400	12400	29600	2	360	1763			26	4680	39	7020	2	360	26	4680	8	1440	64	11520	118	21240	54	9720	

SUMMARY OF GUARDRAIL										
STATION TO STATION CRL	OFFSET		ANCHOR UNITS		BEAM GUARDRAIL W-BEAM SINGLE	TACK COAT	PRIME COAT	SUPERPAVE TYPE S3 (PG 64-22) (OK) (3")	SUPERPAVE TYPE S4 (PG 64-22) (OK) (2")	GUARDRAIL DELINEATORS (TYPE 1 CODE 1)
	LEFT	RIGHT	GUARDRAIL BRIDGE CONNECTION THRIE BEAM (31")	GUARDRAIL END TREATMENT (31")						
			623(I) EA	623(G) EA						
MAINLINE										
STA. 364+05.30 TO 368+35.43		X	1	1	362.5	20	92	44	29	8
STA. 365+05.30 TO 368+35.43	X		1	1	262.5	16	73	35	23	6
STA. 371+64.27 TO 374+14.62		X	2		212.5	10	49	23	16	5
STA. 371+64.27 TO 374+14.62	X		2		212.5	10	49	23	16	5
STA. 379+85.38 TO 382+65.51		X	1	1	212.5	14	64	31	21	5
STA. 379+85.38 TO 383+90.51	X		1	1	337.5	19	89	43	28	7
			8	4	1600	89	416	199	133	36

SUMMARY OF RIP RAP							
STR	STATION OR LOCATION	LOCATION		BASIN DIMENSIONS			TYPE 1 PLAIN RIP RAP 601(A) TON
		LT.	RT.	LENGTH	WIDTH	RIP RAP THICKNESS	
				FT	FT	IN	
2	354+20.23		X	24	22	18	59
8	13+00.00 NS 3610		X	24	30	18	81
TOTALS							140

9:54:38 AM 6/7/2018 bmcanelly cpyb&w_ANSIB.tbl p:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.02 Summaries/21006C0RDSUMMARY02.dgn

SUMMARY OF EARTHWORK				
STATION TO STATION CRL	UNCLASSIFIED EXCAVATION	EMBANKMENT +15% CY	(**) EXCESS EXCAVATION CY	UNCLASSIFIED BORROW
	202(A) CY			202(D) CY
PHASE 1 ESTIMATE 1				
STA. 345+05.08 TO 368+51.36	2556	54875		52319
PHASE 1 ESTIMATE 2				
STA. 371+39.35 TO 374+40.67	0	30813		30813
PHASE 1 ESTIMATE 3				
STA. 379+55.89 TO 402+84.71	37109	11307	25802 *	
PHASE 1 ESTIMATE 4				
STA. 10+00 TO 18+99.62 NS 3610 ROAD	594	12544		11950
PHASE 1 ESTIMATE 5				
BRIDGE HEADERS	2541	15343		12802
PHASE 2 ESTIMATE 1				
STA. 345+05.08 TO 368+51.36	9577	4706	4871 **	
PHASE 2 ESTIMATE 2				
STA. 371+39.35 TO 374+40.67	0	1525		1525
PHASE 2 ESTIMATE 3				
STA. 379+55.89 TO 402+84.71	5391	2461	2930 **	
PHASE 2 ESTIMATE 4				
BRIDGE HEADERS	7001	1550	5451 **	
PROJECT TOTAL				
	64769	135124	27327	82082

SUMMARY OF PERMANENT EROSION CONTROL			
STATION TO STATION CRL	DESCRIPTION	SOLID SLAB SODDING	VEGETATIVE MULCHING
		230(A) SY	233(A) AC
MAINLINE			
345+00 TO 361+00	LT. SIDE OF ROADWAY	11033.00	2.28
345+00 TO 361+00	RT. SIDE OF ROADWAY	11760.00	2.43
361+00 TO 368+35	LT. SIDE OF ROADWAY	1699.00	0.35
361+00 TO 368+35	RT. SIDE OF ROADWAY	7570.00	1.56
371+65 TO 374+14	LT. SIDE OF ROADWAY	548.00	0.11
371+65 TO 374+14	RT. SIDE OF ROADWAY	3423.00	0.71
379+86 TO 398+00	LT. SIDE OF ROADWAY	8100.00	1.67
379+86 TO 402+00	RT. SIDE OF ROADWAY	16974.00	3.51
TOTALS		61,107	13

(*) EXCESS EXCAVATION 25802 CY TO BE USED TO REDUCE UNCL. BORROW FOR PHASE 1
(**) EXCESS EXCAVATION 13252 CY FOR PHASE 2 - 1525 CY WILL BE USED TO REDUCE UNCL. BORROW THE REMAINING WILL BE WASTE

SUMMARY OF REMOVALS					
STATION TO STATION CRL	REMOVAL OF ASPHALT PAVEMENT	(*) REMOVAL OF GUARDRAIL	(*) REMOVAL OF FENCE	(*) REMOVAL OF HEADWALL	(*) REMOVAL OF EXISTING PIPE
	619(B) SY	619(B) LF	619(B) LF	619(B) EA	619(B) LF
MAINLINE					
BOP TO STA. 357+00	5844	0	2825	5	40
STA. 357+00 TO STA. 372+00	5250	1836	0	1	0
STA. 372+00 TO STA. 387+00	4144	974	1138	1	0
STA. 387+00 TO STA. 402+00	7166	0	2376	6	120
STA. 402+00 TO EOP	420	0	522	0	0
NS 3610	2688	224	0	0	0
TOTALS					
	25,512	3,034	6,861	13	160

(*) SUBSIDIARY TO PAY ITEM "REMOVAL OF STRUCTURES AND OBSTRUCTIONS"
(FOR CONTRACTOR'S INFORMATION ONLY)

bmcanelly 6/5/2018 2:40:02 PM
c:\p\w\ANSIB.tbl
p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.02 Summaries\21006CORRDSUMMARY03.dgn



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B001	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

INDEX OF BRIDGE SHEETS

B001	GENERAL PLAN AND ELEVATION
B002	SUBSTRUCTURE STAKING DIAGRAM
B003-B004	SUBSURFACE PROFILE
B005	BRIDGE CONSTRUCTION SEQUENCE
B006	THROW FENCE DETAILS
B007	SUBSTRUCTURE EXCAVATION
B008-B009	ABUTMENT NO. 1 DETAILS
B010-B011	ABUTMENT NO. 2 DETAILS
B012	ABUTMENT DETAILS
B013-B014	PIER DETAILS
B015-B019	SUPERSTRUCTURE DETAILS
B020-B022	P.C. BEAM DETAILS
B023	BEARING DETAILS
B024	APPROACH SLAB NO. 1 DETAILS
B025	APPROACH SLAB NO. 2 DETAILS
B026	APPROACH SLAB DETAILS
B027	SLOPE WALL DETAILS
B028	DRAINS AT END OF BRIDGE

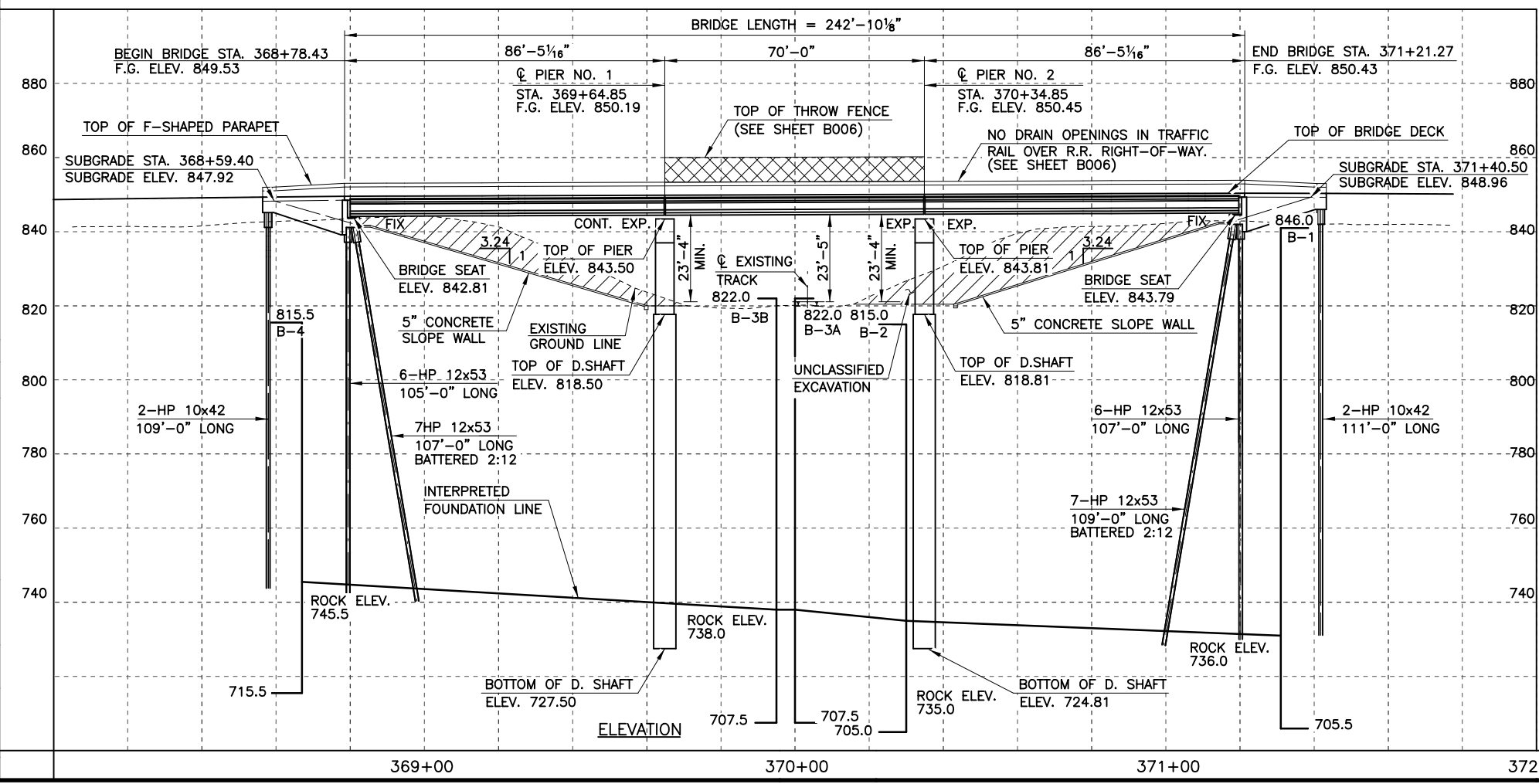
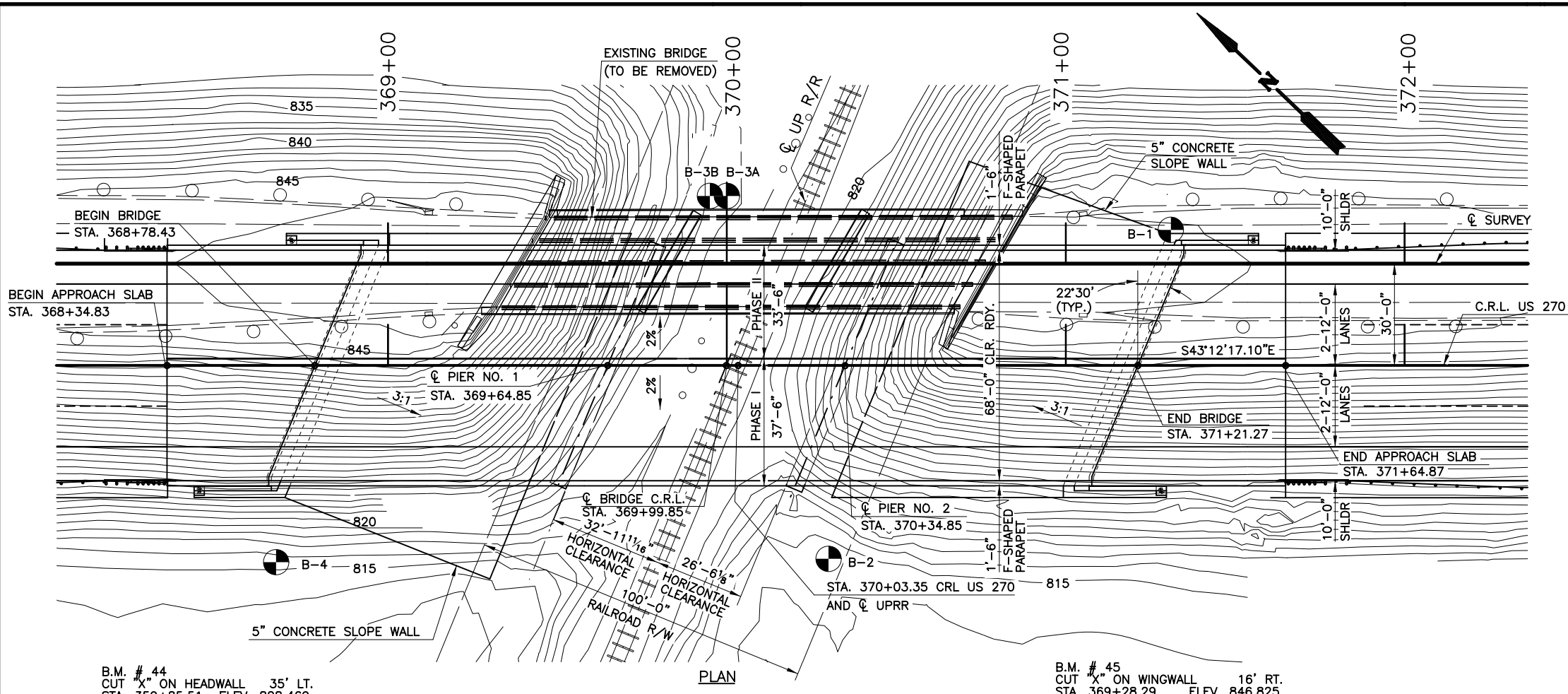
BRIDGE STANDARDS:
 EJ-SK-04E
 EJ-DTL-02E
 FSHP-42-2-00E
 HP1-2-01E

DESIGN DATA:
 MATERIAL:
 CLASS A CONCRETE f'c=3 ksi
 CLASS AA CONCRETE f'c=4 ksi
 REINFORCING STEEL (GRADE 60) Fy=60 ksi
 STRUCTURAL STEEL M270 (GRADE 50W) Fy=50 ksi
 STAINLESS STEEL A240 (TYPE 316) Fy=30 ksi

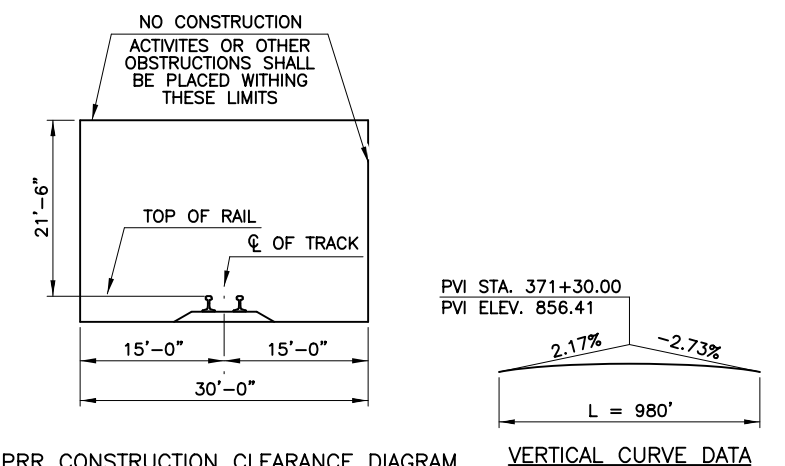
LOADING:
 HL-93 OR OKLAHOMA OVERLOAD TRUCK
 20 PSF FUTURE WEARING SURFACE
 5 PSF STAY-IN-PLACE FORMS

DESIGN:
 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 7TH EDITION

LRFR OPERATING RATING FACTOR = 2.40



EXISTING GROUND TO BE EXCAVATED



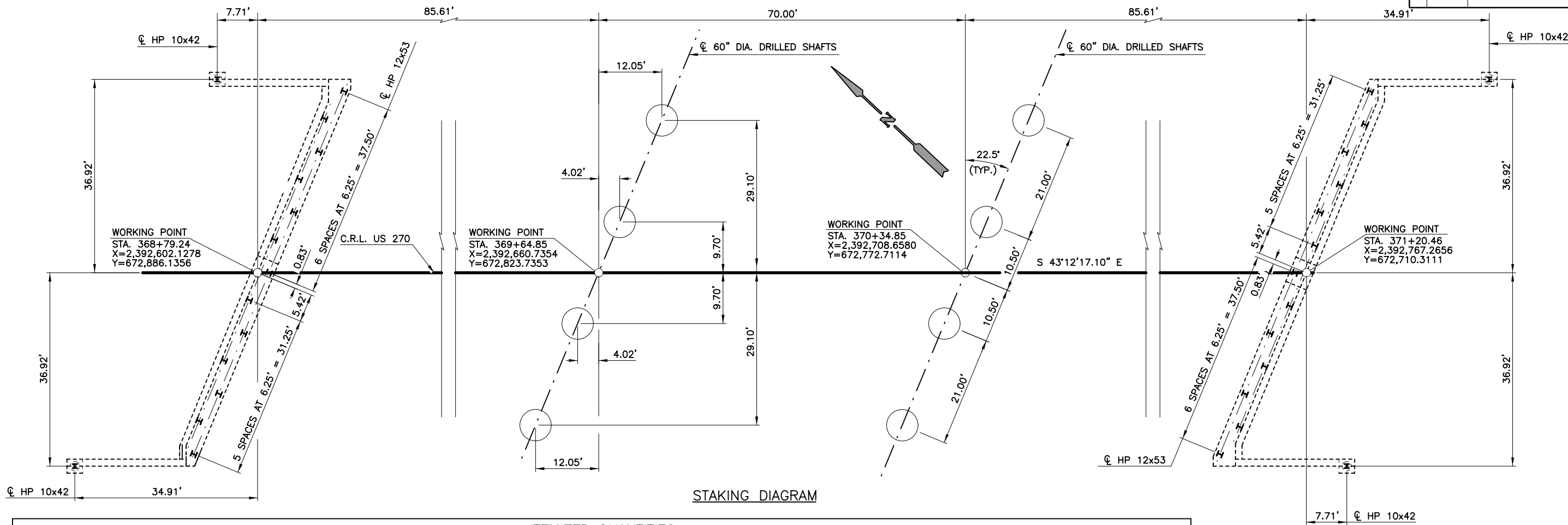
BRIDGE "A"
 US 270 SEMINOLE COUNTY

DESIGN:	DKC	06/18
DRAWN:	LRJ	06/18
CHECKED:	DMN	06/18
APPRVD:	DMN	06/18

GENERAL PLAN AND ELEVATION
 CONSTRUCT 85'-70'-85' TYPE IV PC BEAM SPANS
 SKEWED 22°30' LF WITH 68'-0" CLR RDY AND
 F-SHAPED RAILS C STA. 369+99.85

SHEET 1 OF 1
 STATE JOB PIECE NO: 21006(07) SHEET NO. B001

2:40:08 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 p:\Active Projects\ODOT113446\00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006\A01.dgn



STAKING DIAGRAM

ITEMIZED QUANTITIES

ITEM CODE	ITEM	UNIT	PHASE I					PHASE II					TOTAL
			ABUTMENT	PIERS	SUPER-STRUCTURE	APPROACH SLAB	SUBTOTAL	ABUTMENT	PIERS	SUPER-STRUCTURE	APPROACH SLAB	SUBTOTAL	
501(B)	1307	CY	144.00				144.00	100.00				100.00	244.00
501(G)	6309	CY	268.00				268.00	156.00				156.00	424.00
503(A)	1313	LF			956.00		956.00			717.00		717.00	1,673.00
504(A)	1304	SY				363.50	363.50				324.70	324.70	688.20
504(B)	1305	SY			972.00	348.90	1,320.90			364.00	310.10	674.10	1,995.00
504(C)	6250	LF			39.40		39.40			35.10		35.10	74.50
504(E)	6190	LF			242.70	87.20	329.90			242.70	87.20	329.90	659.80
506(A)	1322	LB			1,460.00		1,460.00			1,300.00		1,300.00	2,760.00
507(A)	6170	EA			8.00		8.00			6.00		6.00	14.00
507(B)	6174	EA			16.00		16.00			12.00		12.00	28.00
509(A)	1326	CY			252.00		252.00			226.00		226.00	478.00
509(B)	1328	CY	88.40	115.90			204.30	61.40	104.40			165.80	370.10
509(D)	1331	CY					13.50					11.00	24.50
510(C)	6138	SY										1,468.00	1,468.00
511(A)	1332	LB		1,220.00			1,220.00		1,220.00			1,220.00	2,440.00
511(B)	6010	LB	10,440.00	17,660.00	84,680.00		112,780.00	7,340.00	15,880.00	70,130.00		93,350.00	206,130.00
514(A)	6010	LF	220.00				220.00	220.00				220.00	440.00
514(A)	6011	LF	1,712.00				1,712.00	1,072.00				1,072.00	2,784.00
514(B)	6292	LF	220.00				220.00	220.00				220.00	440.00
514(B)	6294	LF	1,712.00				1,712.00	1,072.00				1,072.00	2,784.00
514(L)	6220	EA											1.00
515(A)	6013	SY	119.90	150.00	414.00	43.00	726.90	76.30	129.00	414.00	43.00	662.30	1,389.20
516(A)	6096	LF		370.00			370.00		370.00			370.00	740.00
516(C)	6200	EA		4.00			4.00		4.00			4.00	8.00
523(A)	6550	LF			38.90		38.90			277.40		277.40	316.30
523(B)	6560	GAL			0.50		0.50			3.10		3.10	3.60
525(C)	1000	LSUM											1.00
613(H)	6204	LF	95.00				95.00	59.00				59.00	154.00
613(I)	6207	LF						60.00				60.00	60.00
619(B)	2500	LSUM											1.00
624(E)	4294	LF			70.00		70.00			70.00		70.00	140.00

FOUNDATION DATA

ABUTMENTS (HP 12x53 PILING)

FACTORED PILE REACTION = 77.1 TONS/PILE

PIERS (60" DIAMETER DRILLED SHAFTS)

PIER NO. 1 FACTORED REACTION = 544.0 TONS/SHAFT
PIER NO. 2 FACTORED REACTION = 550.0 TONS/SHAFT

NOMINAL UNIT FRICTION RESISTANCE = 9.00 T.S.F.
FRICTION RESISTANCE FACTOR = 0.45
FACTORED FRICTION RESISTANCE = 317.9 TONS/SHAFT

NOMINAL UNIT BEARING RESISTANCE = 60 T.S.F.
BEARING RESISTANCE FACTOR = 0.70
FACTORED BEARING RESISTANCE = 824.4 TONS/SHAFT

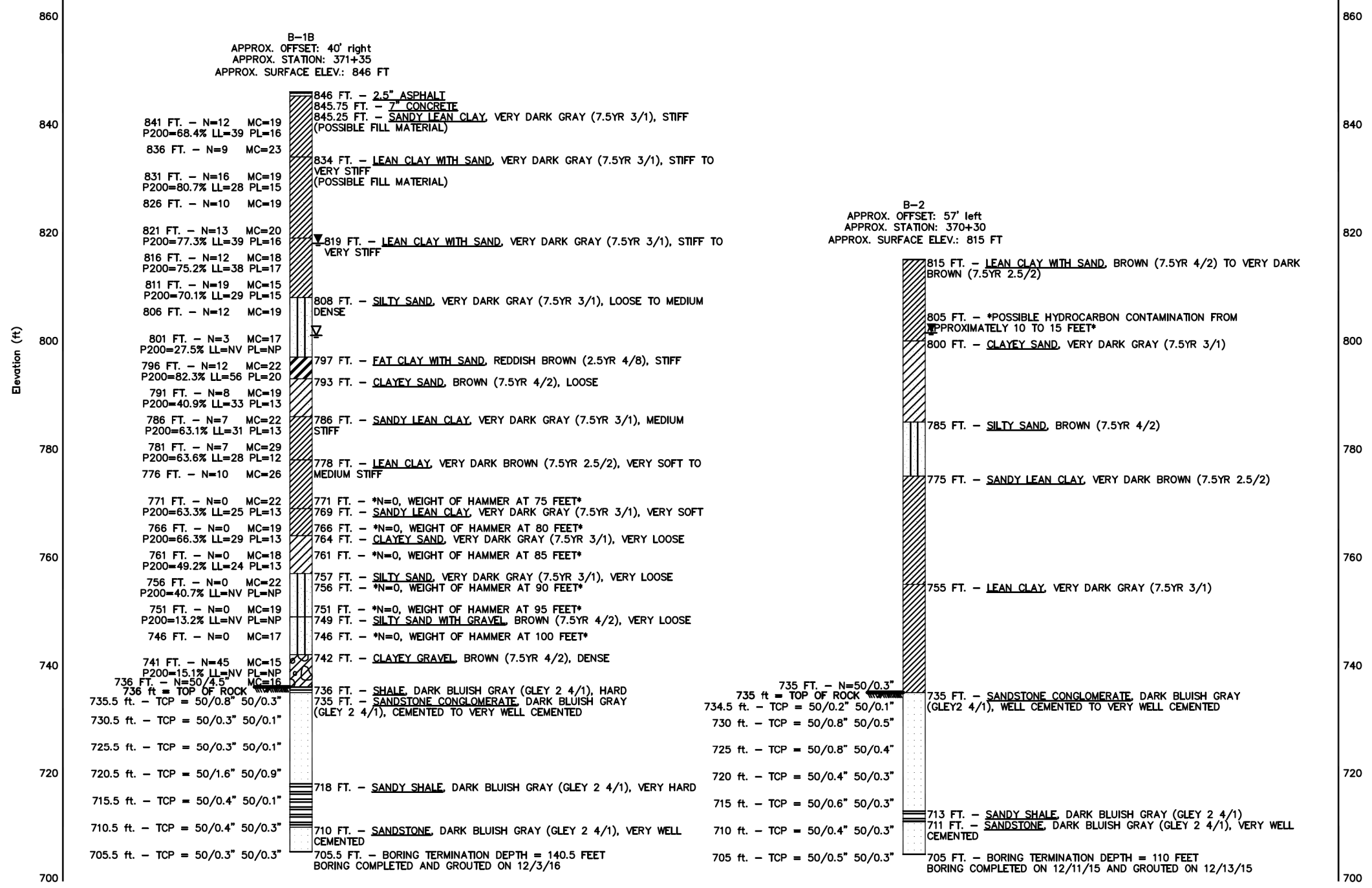
TOTAL FACTORED RESISTANCE = 1,142.3 TONS/SHAFT

MINIMUM DEPTH OF PENETRATION INTO ROCK IS 10'-0".
THE DEPTH OF ROCK NEGLECTED FOR FRICTION IS 5'-0".

DESIGN:	DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN:	LRJ	06/18	BRIDGE "A"	
CHECKED:	DMN	06/18	SUBSTRUCTURE STAKING DIAGRAM	
APPRVD:	DMN	06/18		
			STATE JOB PIECE NO: 21006(07)	SHEET 1 OF 1 SHEET NO. B002

6/5/2018 2:40:11 PM
c:\p\w\ANSIB.tbl
p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAFD01.dgn

REVISIONS		
REV. NO.	DESCRIPTION	DATE



SITE GEOLOGY

Division Three of the "Engineering Classification of Geological Materials", published by the Oklahoma Department of Transportation (ODOT) indicates that the project site is located over Alluvium (Qas) underlain by the Vanoss Unit (Pv).

Alluvium consists of sand, silt, clay, gravel, and/or combinations of materials. Alluvium is found along the flood plains (bottom land) of streams and is normally present at places along all streams.

The Vanoss Unit consists of alternating moderately soft to moderately hard sandstones, conglomerates, shales, and a few thin limestones. The shales are multicolored and resemble those of the underlying Ada Unit. In the outcrop area adjacent to the Arbuckle Mountains and northward to about the middle of Seminole County, the sandstones and conglomerates are thicker and locally are so arkosic that at first glance a few of them might be mistaken for true granites. Commonly, the base unit is referred to as the lowest of the arkosic beds, but this is only true in the southern part of the unit's outcrop area. As far north as Little River, Seminole County, the base of the Vanoss Unit is the first, persistent, non-limestone, conglomerate bed above the base of the Ada Unit. North of Little River, a continuous sandstone horizon marks the base.

A basal limestone conglomerate member is prominent in Murray County adjacent to the Arbuckle Mountains. It is mapped and described separately from the Vanoss Unit and the Vanoss Conglomerate subunit (Pvc). Near the Arbuckle Mountains the total thickness of the Vanoss Unit is 1,550 feet with 650 feet assigned to the conglomerate subunit. Northward, the unit thins from 650 feet in the southern Pontotoc County to 250 feet near Konawa, Seminole County. The thickness of the unit is irregular in Seminole County and varies from 140 to 500 feet, thickening southward.

The Vanoss Unit outcrops in a two to ten mile wide band around the northern and western limits of the Arbuckle Mountains in Murray and Pontotoc Counties. From here, the unit outcrops in a two to seven mile wide, north-south, strip across western Pontotoc and Seminole Counties and the eastern edge of Pottawatomie County. North of the North Canadian River, in Okfuskee and northeastern Pottawatomie Counties, the strata of the Vanoss Unit are inseparable from the strata of the underlying Ada unit and consequently the two are mapped together as the Vanoss-Ada Unit (Pva).

Topographically, the unit is gently rolling to rolling with more rolling topography prominent where the sandstones and conglomerates are thicker.

According to the Geologic Map of the "Hydrologic Atlas 4 of Oklahoma," Reconnaissance of the Water Resources of the Oklahoma City Quadrangle, central Oklahoma, by Roy H. Blingham and Robert L. Moore, Oklahoma Geological Survey, 1975, the project consists of Alluvium (Qal) underlain by the Vanoss Group (IPva). The geological deposit and unit is described therein as follows.

Alluvium consists of sand, silt, clay and lenticular beds of gravel. Thickness ranges from about 30 to 100 feet and probably averages about 50 feet along major streams. Along minor streams, thickness ranges from a few feet to about 50 feet and probably averages about 25 feet. Alluvium is a major aquifer in parts of the quadrangle.

The Vanoss Group consists of orange-brown fine-grained, crossbedded sandstone; grades southward into arkosic sandstone and conglomerate. Includes many thin limestone beds and shale units north of North Canadian River (descending): Roca Shale (75 feet thick), Red Eagle Limestone (3 to 8 feet thick), Johnson Shale (60 feet thick), Long Creek Limestone (9 to 12 feet thick), Hughes Creek Shale (52 feet thick), Americus Limestone (1 to 2.5 feet thick), Admire Shale (70 feet thick), Brownville Limestone (1 to 3 feet thick), Pony Creek Shale (75 feet thick), Grayhorse Limestone (1 foot thick), unnamed shale (70 feet thick), Elmont Limestone (1.2 to 7.8 feet thick), Stonebreaker Shale (60 feet thick), and Reading Limestone (1.5 feet thick), at base. Total thickness of group ranges from 250 feet in south to 490 feet in north.

LEGEND

SPT	DENOTES STANDARD PENETRATION TEST, ASTM D1586
N	DENOTES NUMBER OF BLOW COUNTS PER 12 INCHES
REC	DENOTES RECOVERY IN ROCK CORING
ROD	DENOTES ROCK QUALITY DESIGNATION
MC	DENOTES MOISTURE CONTENT TESTS
P200	DENOTES PERCENT PASSING NO 200 SIEVE
LL	DENOTES LIQUID LIMIT TESTS (NV=NO VALUE)
PL	DENOTES PLASTIC LIMIT TESTS (NP=NO PLASTICITY)
▽	DENOTES WATER ELEVATION DURING DRILLING
▽	DENOTES WATER ELEVATION IMMEDIATELY AFTER DRILLING
▽	DENOTES WATER ELEVATION HOURS AFTER DRILLING
▽	DENOTES TOP OF ROCK

■ NOTE: WATER ELEVATIONS SHOWN WERE OBTAINED AT THE TIME BORINGS WERE DRILLED AND MAY FLUCTUATE THROUGHOUT THE YEAR.

■ NOTE: TOP OF ROCK LINE SHOWN FOR ESTIMATING PURPOSE ONLY

■ NOTE: ROCK CLASSIFICATION IS BASED ON DRILLING CHARACTERISTICS AND VISUAL OBSERVATION. PTEROGRAPHIC ANALYSIS OF THIN SECTIONS OF THE ROCK CORE SAMPLES MAY REVEAL OTHER TYPES.

GEOLOGIC REPORT

ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER AT (405) 521-2625. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

US 270 OVER UP RAILROAD SEMINOLE COUNTY

SUBSURFACE PROFILE SHEET 1 OF 3

STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION

JOB PIECE NO. 21006(07) SHEET NO. B003

Design	JTU	4/19
Detail	JTU	4/19
Check	RAG	4/19
Squad:		
Engr.:		

REV. NO.	DESCRIPTION	DATE

Elevation (ft)

Elevation (ft)

B-3a
 APPROX. OFFSET: 50' right
 APPROX. STATION: 369+95
 APPROX. SURFACE ELEV.: 822 FT

B-3b
 APPROX. OFFSET: 50' right
 APPROX. STATION: 370+00
 APPROX. SURFACE ELEV.: 822 FT

B-4B
 APPROX. OFFSET: 58' left
 APPROX. STATION: 368+67
 APPROX. SURFACE ELEV.: 815.5 FT

822 FT. - N=7 MC=14
 817 FT. - N=5 MC=21
 P200=75.3% LL=32 PL=13
 812 FT. - N=4 MC=22
 P200=62.3% LL=25 PL=14
 807 FT. - N=5 MC=23
 P200=77.8% LL=47 PL=15
 802 FT. - N=10
 797 FT. - N=8 MC=25
 P200=93.0% LL=56 PL=19
 792 FT. - N=5 MC=19
 P200=29.5% LL=NV PL=NP
 787 FT. - N=0 MC=17
 782 FT. - N=10 MC=26
 P200=92.8% LL=54 PL=20
 777 FT. - N=5 MC=28
 772 FT. - N=0 MC=24
 P200=50.2% LL=25 PL=13
 767 FT. - N=4 MC=18
 P200=38.8% LL=NV PL=NP
 762 FT. - N=6 MC=19
 P200=43.5% LL=24 PL=12
 757 FT. - N=5 MC=20
 P200=60.4% LL=24 PL=12
 752 FT. - N=9 MC=17
 747 FT. - N=0 MC=18
 P200=50.2% LL=25 PL=13
 742 FT. - N=13 MC=17
 P200=8.0% LL=NV PL=NP
 738 FT. - N=50/0.6
 738 ft = TOP OF ROCK
 RC 1 - Total = 26" Rec = 43.3% RQD = 9.1%
 RC 2 - Total = 8.5" Rec = 14.2% RQD = 0%
 RC 3 - Total = 19" Rec = 31.6% RQD = 0%
 RC 4 - Total = 33" Rec = 55% RQD = 0%
 RC 5 - Total = 57.5" Rec = 95.8% RQD = 0%
 RC 6 - Total = 41.5" Rec = 69.1% RQD = 6.7%

822 FT. - LEAN CLAY WITH SAND, REDDISH BROWN (7.5YR 4/8) TO DARK BROWN (7.5YR 3/2), MEDIUM STIFF
 814 FT. - LEAN CLAY, DARK BROWN (7.5YR 3/2), SOFT
 810 FT. - LEAN CLAY WITH SAND, DARK BROWN (7.5YR 3/2) AND REDDISH BROWN (7.5YR 4/8), MEDIUM STIFF TO STIFF
 798 FT. - FAT CLAY, DARK BROWN (7.5YR 3/2) AND BLACK (10YR 2/1), MEDIUM STIFF
 794 FT. - SILTY SAND, BROWN (7.5YR 4/2), VERY LOOSE TO LOOSE
 787 FT. - *N=0, WEIGHT OF HAMMER AT 35 FEET*
 783 FT. - FAT CLAY, BROWN (7.5YR 4/2), MEDIUM STIFF TO STIFF
 772 FT. - SANDY LEAN CLAY, DARK BROWN (7.5YR 3/2), VERY SOFT
 771.5 FT. - *N=0, WEIGHT OF HAMMER AT 50 FEET*
 768 FT. - SILTY SAND, DARK BROWN (7.5YR 3/2), LOOSE
 761 FT. - CLAYEY SAND, BROWN (7.5YR 4/2), LOOSE
 758 FT. - SANDY LEAN CLAY, BROWN (7.5YR 4/2), VERY SOFT TO STIFF
 747 FT. - *N=0, WEIGHT OF HAMMER AT 75 FEET*
 744 FT. - WELL GRADED SAND WITH SILT, BROWN (7.5YR 4/2), VERY LOOSE TO MEDIUM DENSE
 738 FT. - SANDSTONE CONGLOMERATE, BLUISH GRAY (GLEY 2 4/1), COARSE GRAINED, HOMOGENOUS, THICK BEDDED, VERY HIGH TO HIGH JOINTING, FINE PORES
 721 FT. - SHALE, BLUISH GRAY (GLEY 2 4/1), FINE GRAINED, HOMOGENOUS, FISSILE, VERY HIGH TO HIGH JOINTING, VERY FINE PORES
 719 FT. - SANDSTONE, BLUISH GRAY (GLEY 2 4/1), COARSE GRAINED, HOMOGENOUS, THICK BEDDED, VERY HIGH JOINTING, FINE PORES
 708.25 FT. - SHALE, BLUISH GRAY (GLEY 2 4/1), FINE GRAINED, HOMOGENOUS, HIGH JOINTING, VERY FINE PORES
 707.5 FT. - BORING TERMINATION DEPTH = 114.5 FEET
 BORING COMPLETED ON 12/9/15 AND GROUTED ON 12/12/15

822 FT. - LEAN CLAY WITH SAND, REDDISH BROWN (7.5YR 4/8) TO DARK BROWN (7.5YR 3/2)
 814 FT. - LEAN CLAY, DARK BROWN (7.5YR 3/2)
 810 FT. - LEAN CLAY WITH SAND, DARK BROWN (7.5YR 3/2) AND REDDISH BROWN (7.5YR 4/8)
 798 FT. - FAT CLAY, DARK BROWN (7.5YR 3/2) AND BLACK (10YR 2/1)
 794 FT. - SILTY SAND, BROWN (7.5YR 4/2)
 783 FT. - FAT CLAY, BROWN (7.5YR 4/2)
 772 FT. - SANDY LEAN CLAY, DARK BROWN (7.5YR 3/2)
 768 FT. - SILTY SAND, DARK BROWN (7.5YR 3/2)
 761 FT. - CLAYEY SAND, BROWN (7.5YR 4/2)
 758 FT. - SANDY LEAN CLAY, BROWN (7.5YR 4/2)
 744 FT. - WELL GRADED SAND WITH SILT, BROWN (7.5YR 4/2)
 738 FT. - SANDSTONE CONGLOMERATE, BLUISH GRAY (GLEY 2 4/1), WELL CEMENTED TO VERY WELL CEMENTED
 737.5 ft. - TCP = 50/0.3" 50/0.1"
 732.5 ft. - TCP = 50/0.8" 50/0.6"
 727.5 ft. - TCP = 50/0.5" 50/0.5"
 722.5 ft. - TCP = 50/1.3" 50/0.6"
 717.5 ft. - TCP = 50/0.8" 50/0.6"
 712.5 ft. - TCP = 50/0.6" 50/0.5"
 707.5 ft. - TCP = 50/1.1" 50/0.9"
 721 FT. - SHALE, BLUISH GRAY (GLEY 2 4/1)
 719 FT. - SANDSTONE, BLUISH GRAY (GLEY 2 4/1), WELL CEMENTED
 708.25 FT. - SHALE, BLUISH GRAY (GLEY 2 4/1), MODERATELY HARD
 707.5 FT. - BORING TERMINATION DEPTH = 114.5 FEET
 BORING COMPLETED ON 12/9/15 AND GROUTED ON 12/12/15


815.5 FT. - LEAN CLAY WITH SAND, BROWN (7.5YR 4/2), MEDIUM STIFF
 807.5 FT. - SANDY LEAN CLAY, BROWN (7.5YR 4/2), STIFF
 802.5 FT. - CLAYEY SAND, BROWN (7.5YR 4/2), LOOSE
 795.5 FT. - FAT CLAY, BROWN (7.5YR 4/2), MEDIUM STIFF
 791.5 FT. - SANDY LEAN CLAY, DARK BROWN (7.5YR 3/2), MEDIUM STIFF
 788.5 FT. - LEAN CLAY, DARK BROWN (7.5YR 3/2), MEDIUM STIFF
 785.5 FT. - N=8
 780.5 FT. - N=6 MC=28
 P200=87.6% LL=47 PL=20
 775.5 FT. - N=8 MC=30
 P200=93.1% LL=43 PL=20
 770.5 FT. - N=0 MC=26
 P200=62.5% LL=27 PL=14
 765.5 FT. - N=0 MC=27
 P200=56.5% LL=24 PL=13
 760.5 FT. - N=9 MC=20
 P200=77.9% LL=33 PL=14
 755.5 FT. - N=9 MC=22
 750.5 FT. - N=13 MC=20
 P200=96.2% LL=34 PL=15
 745.5 FT. - N=50/3.5
 745 ft. - TCP = 50/0.6" 50/0.3"
 740.5 ft. - TCP = 50/3" 50/1"
 735.5 ft. - TCP = 50/0.4" 50/0.3"
 730.5 ft. - TCP = 50/0.3" 50/0.3"
 725.5 ft. - TCP = 50/0.6" 50/0.6"
 720.5 ft. - TCP = 50/0.5" 50/0.3"
 715.5 ft. - TCP = 50/0.4" 50/0.4"
 715.5 FT. - BORING TERMINATION DEPTH = 100 FEET
 BORING COMPLETED ON 1/21/16 AND GROUTED ON 1/23/16

LEGEND

- SPT DENOTES STANDARD PENETRATION TEST, ASTM D1586
 - N DENOTES NUMBER OF BLOW COUNTS PER 12 INCHES
 - REC DENOTES RECOVERY IN ROCK CORING
 - RQD DENOTES ROCK QUALITY DESIGNATION
 - MC DENOTES MOISTURE CONTENT TESTS
 - P200 DENOTES PERCENT PASSING NO 200 SIEVE
 - LL DENOTES LIQUID LIMIT TESTS (NV=NO VALUE)
 - PL DENOTES PLASTIC LIMIT TESTS (NP=NO PLASTICITY)
 - ▽ DENOTES WATER ELEVATION DURING DRILLING
 - ▽ DENOTES WATER ELEVATION IMMEDIATELY AFTER DRILLING
 - ▽ DENOTES WATER ELEVATION HOURS AFTER DRILLING
 - ▽ DENOTES TOP OF ROCK
- NOTE: WATER ELEVATIONS SHOWN WERE OBTAINED AT THE TIME BORINGS WERE DRILLED AND MAY FLUCTUATE THROUGHOUT THE YEAR.
- NOTE: TOP OF ROCK LINE SHOWN FOR ESTIMATING PURPOSE ONLY
- NOTE: ROCK CLASSIFICATION IS BASED ON DRILLING CHARACTERISTICS AND VISUAL OBSERVATION. PTEROGRAPHIC ANALYSIS OF THIN SECTIONS OF THE ROCK CORE SAMPLES MAY REVEAL OTHER TYPES.

GEOLOGIC REPORT

ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER AT (405) 521-2625. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.



US 270 OVER UP RAILROAD SEMINOLE COUNTY

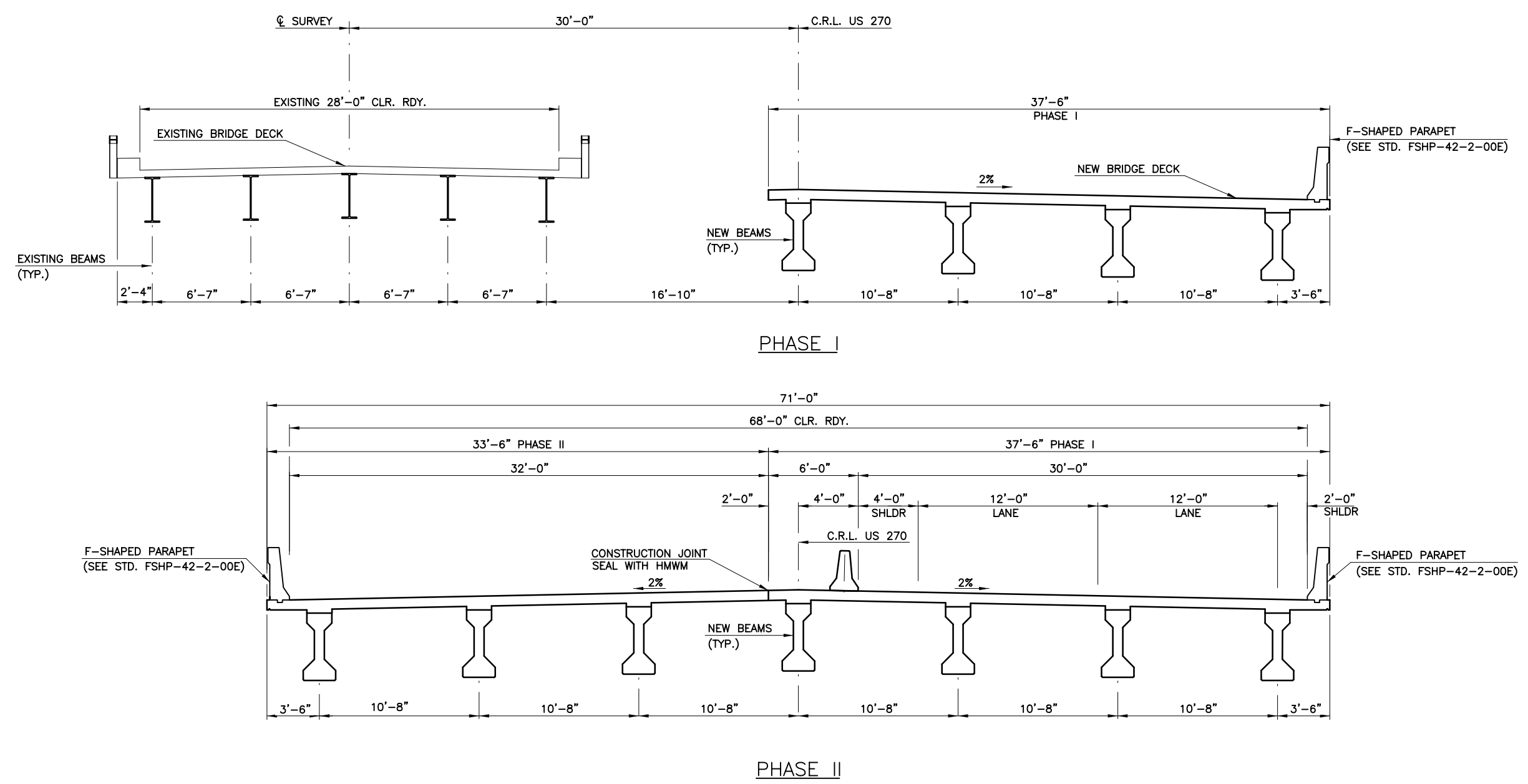
SUBSURFACE PROFILE
SHEET 2 OF 3

STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION

JOB PIECE NO. 21006(07) SHEET NO. B004

Design	JTU	4/19
Detail	JTU	4/19
Check	RAG	4/19
Squad:		
Engr.:		

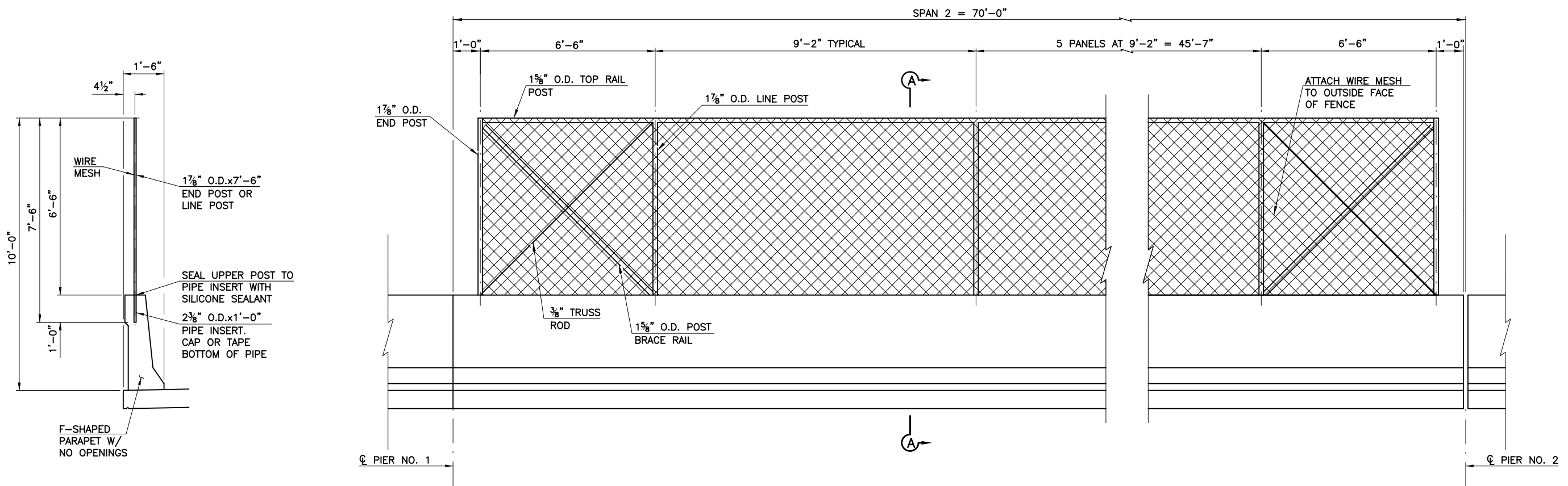
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B005	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



bmcanelly 6/5/2018 2:40:37 PM cpyb&w_ANSIB.tbl cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRTD01.dgn

DESIGN: DKC 06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ 06/18	BRIDGE "A"	
CHECKED: DMN 06/18	BRIDGE CONSTRUCTION SEQUENCE	
APPRVD: DMN 06/18		
CP&Y		SHEET 1 OF 1
STATE JOB PIECE NO: 21006(07)		SHEET NO. B005

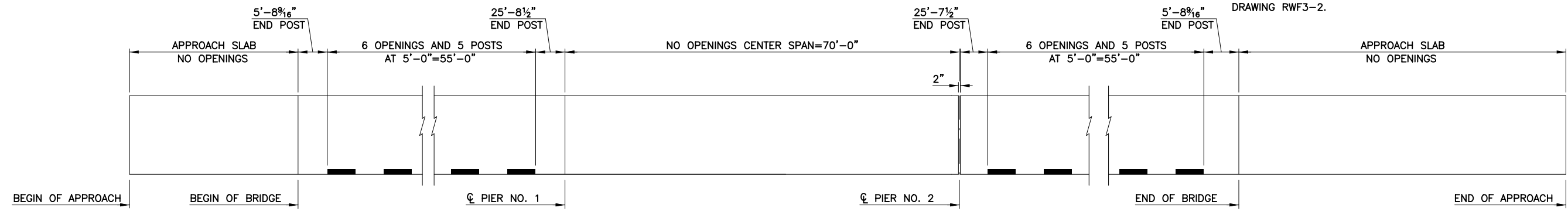
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B006	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



SECTION A-A

ELEVATION

NOTE:
 WIRE MESH SHALL BE FENCE STYLE CLF TYPE IV (SEE SPECIFICATION 732.07(b)).
 PLACE FENCE PER SPECIFICATION 624.04(d).
 FOR DETAILS NOT SHOWN SEE STANDARD DRAWING RWF3-2.



TRAFFIC RAIL LAYOUT

6/5/2018 2:40:41 PM cpyb&w_ANSIB.tbl p:\Active Projects\ODOT13446.00\8.00 Cut Sheets\8.30 Bridge\21006BRFB01.dgn

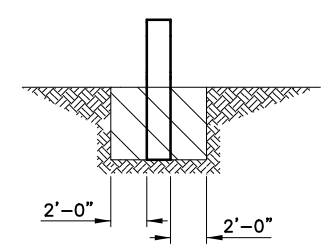
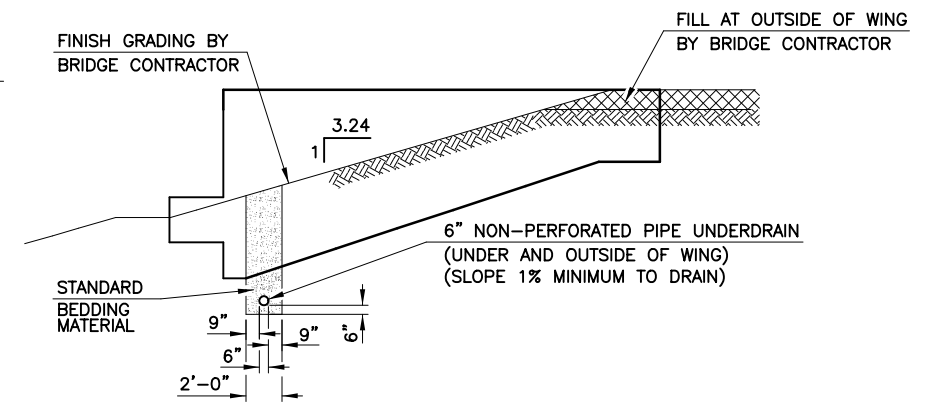
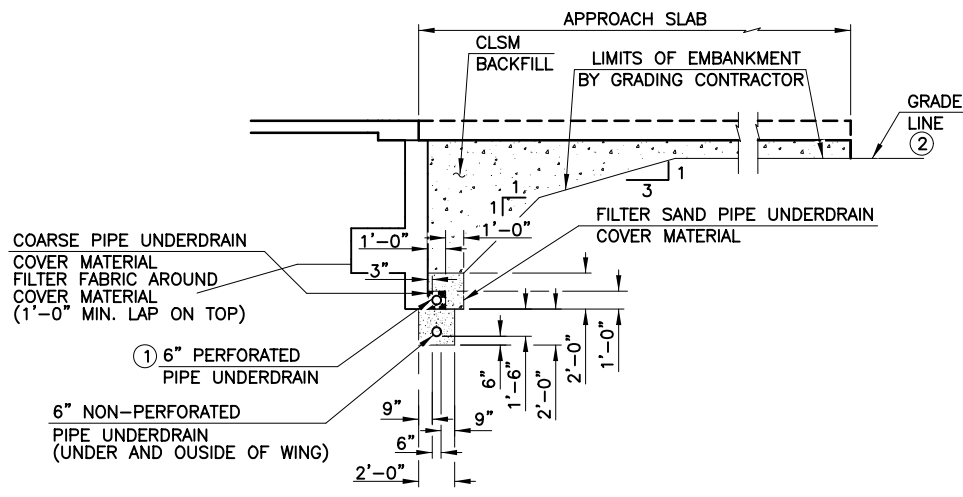
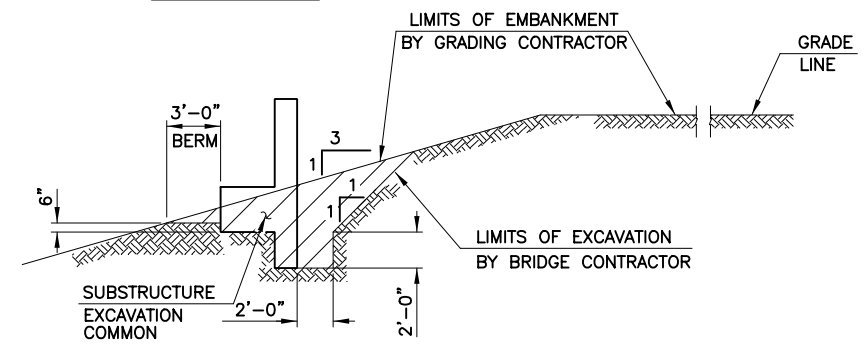
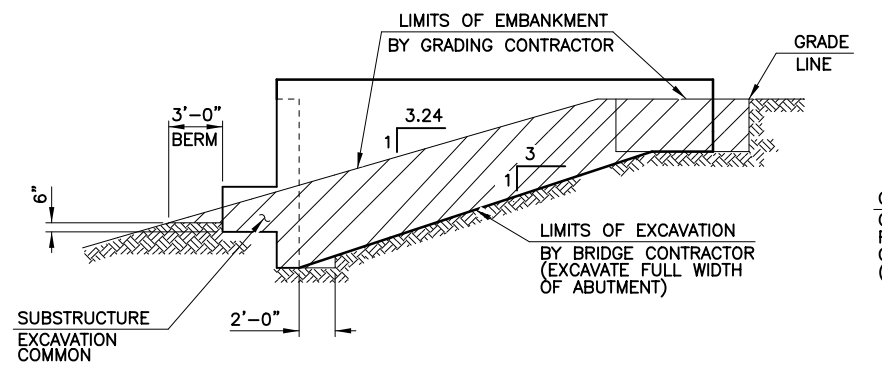
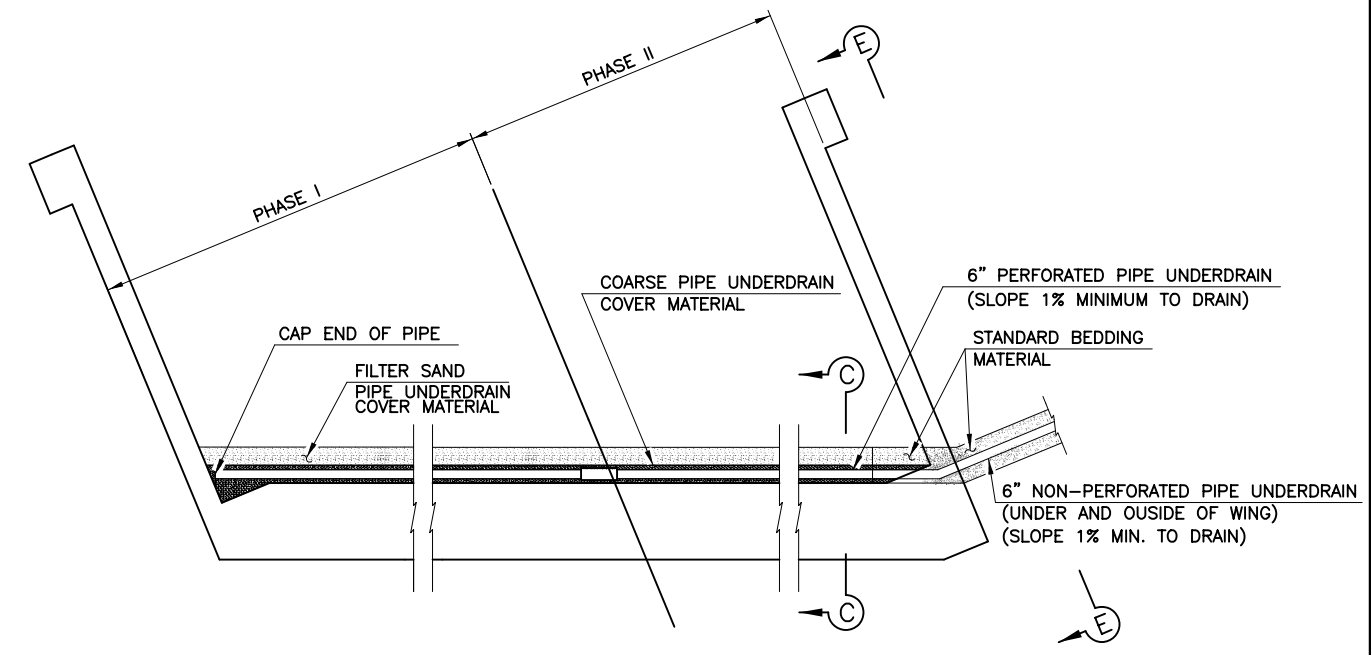
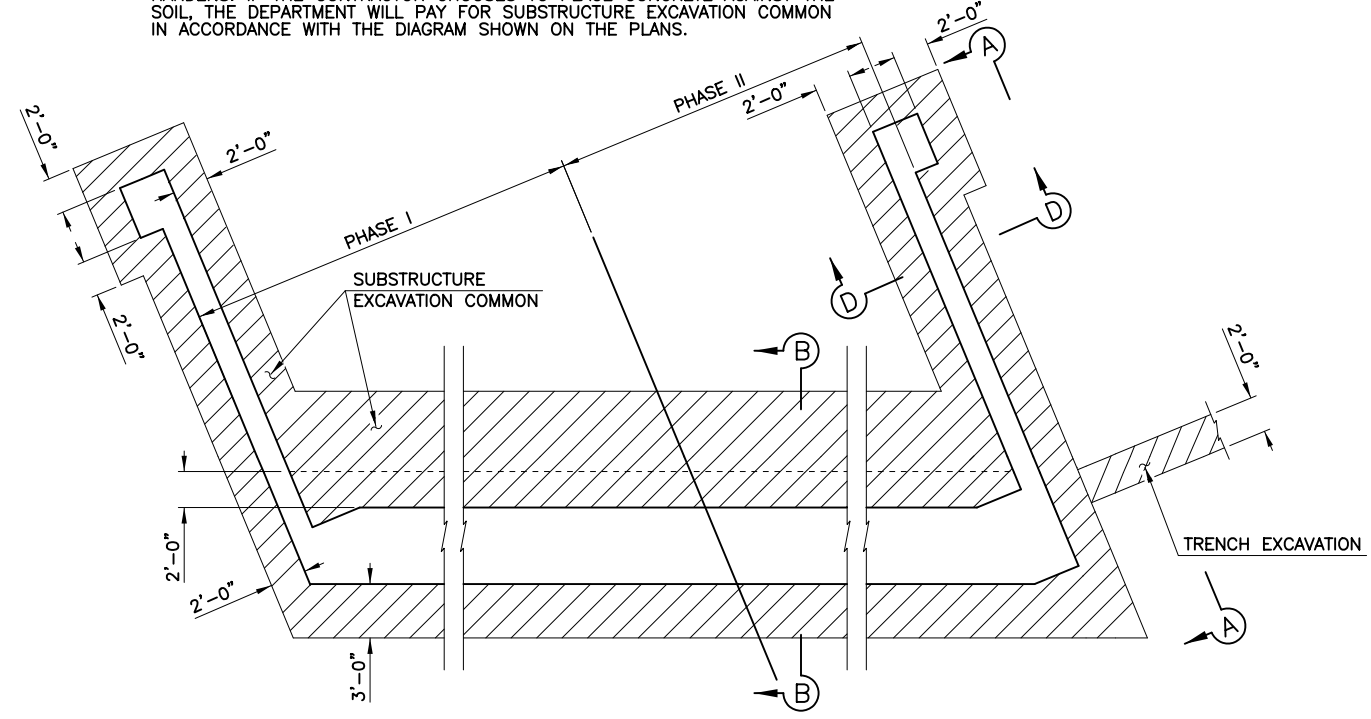
DESIGN: DKC	06/18	US 270 BRIDGE "A" SEMINOLE COUNTY THROW FENCE DETAILS STATE JOB PIECE NO: 21006(07)
DRAWN: LRJ	06/18	
CHECKED: DMN	06/18	
APPRVD: DMN	06/18	

SHEET 1 OF 1
SHEET NO. B006

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B007	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

NOTE:
THE CONTRACTOR MAY PLACE CONCRETE AGAINST THE LIMITS OF EXCAVATION IF THE MATERIAL IS EXCAVATED TO THE MEAT LINES OF THE ABUTMENT AND APPROVED BY THE ENGINEER. IF NECESSARY, USE FORMS ON THE BACK VERTICAL FACE OF THE ABUTMENT AND REMOVE FORMS AFTER CONCRETE HARDENS. IF THE CONTRACTOR CHOOSES TO PLACE CONCRETE AGAINST THE SOIL, THE DEPARTMENT WILL PAY FOR SUBSTRUCTURE EXCAVATION COMMON IN ACCORDANCE WITH THE DIAGRAM SHOWN ON THE PLANS.

NOTE:
THE ENGINEER MAY ADJUST THE EXTENT, LOCATION AND DEPTH OF 6" NON-PERFORATED PIPE UNDERDRAIN DURING CONSTRUCTION. INCLUDE THE COST OF PIPE UNDERDRAIN COVER MATERIAL (BOTH FINE SAND AND COARSE), FILTER FABRIC, TRENCH EXCAVATION, STANDARD BEDDING MATERIAL, AND EQUIPMENT AND LABOR FOR THEIR INSTALLATION IN THE CONTRACT UNIT PRICE OF 6" PERFORATED PIPE UNDERDRAIN ROUND AND 6" NON-PERF. PIPE UNDERDRAIN RND. INSTALL AS SHOWN ON HTE PLANS AND ON STD. PUD-3.



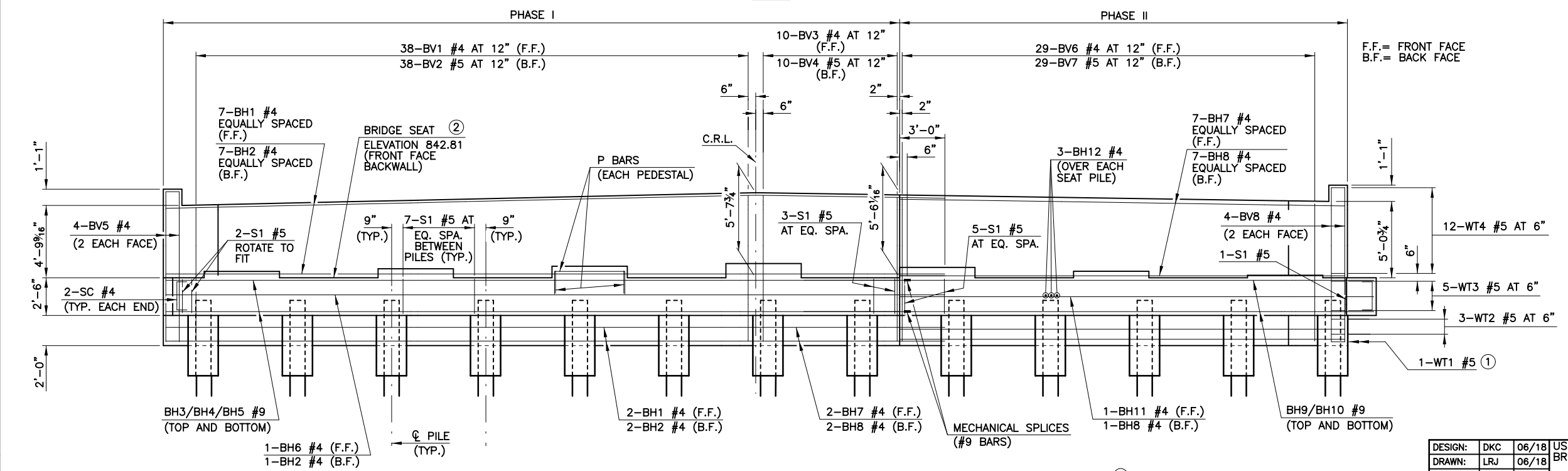
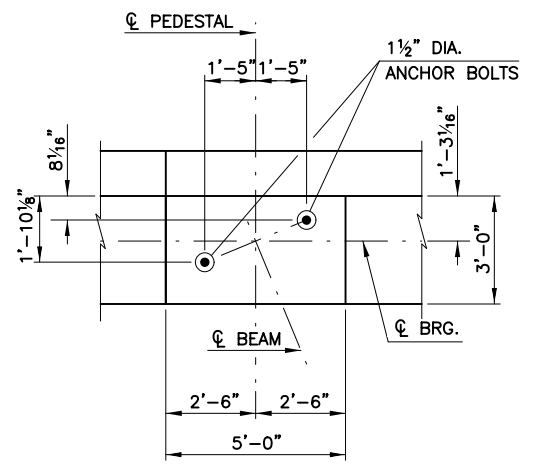
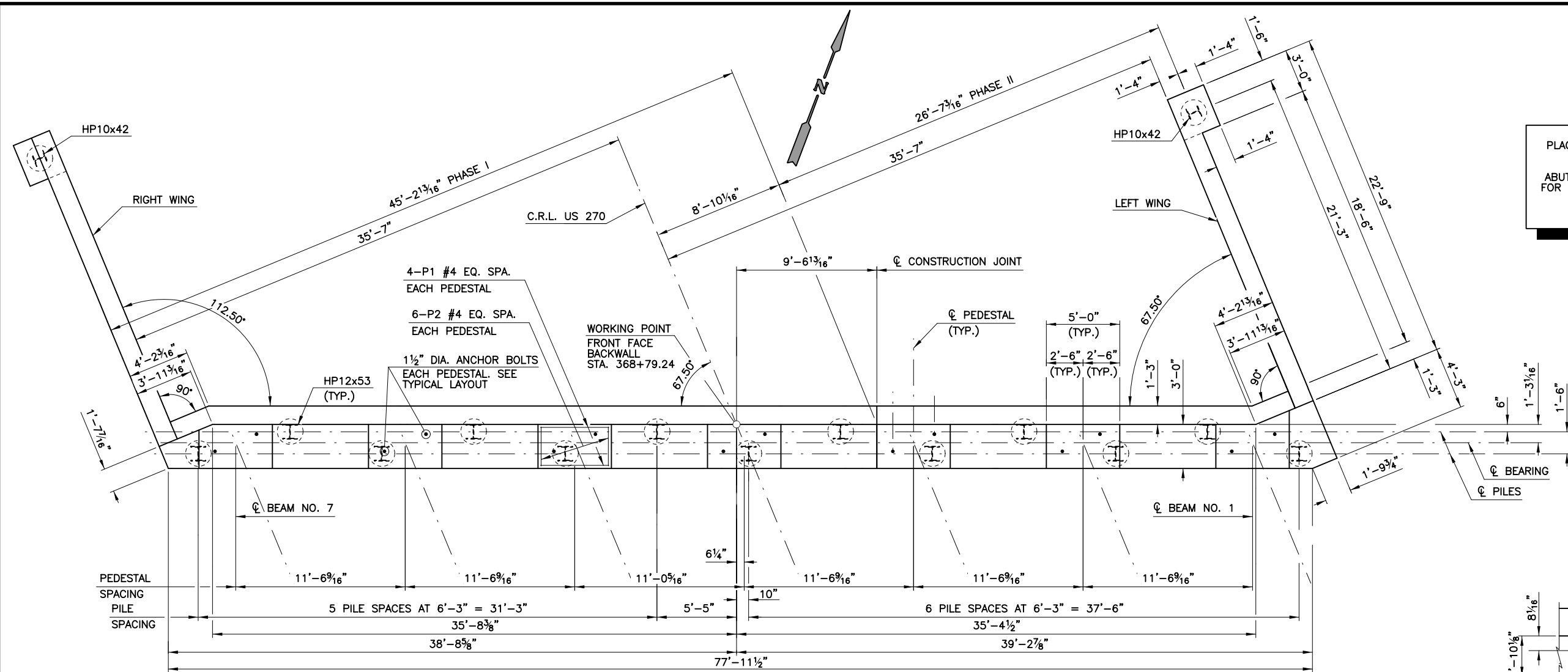
- 1 SET BOTTOM OF PIPE 3" ABOVE THE BOTTOM OF THE ABUTMENT AT THE LOW END.
- 2 GRADE LINE ASSUMED TO BE LOCATED 12" BELOW BOTTOM OF APPROACH SLAB FOR COMPUTING CLSM BACKFILL QUANTITY SHOWN ON PLANS. THE DEPARTMENT WILL PAY FOR CLSM BACKFILL IN ACCORDANCE WITH THE PLAN QUANTITY AND NO ADJUSTMENT WILL BE MADE FOR ACTUAL LOCATION OF GRADE LINE.

DESIGN: DKC	06/18	US 270 BRIDGE "A"	SEMINOLE COUNTY
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
CP&Y		SUBSTRUCTURE EXCAVATION	
		STATE JOB PIECE NO: 21006(07)	SHEET 1 OF 1 SHEET NO. B007

6/5/2018 2:40:46 PM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAFD03.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B008	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

PLACE ALL WT WING REINFORCING TIED TO ABUTMENT SEAT AND BACKWALL REINFORCING BEFORE PLACING ABUTMENT SEAT AND BACKWALL CONCRETE. FOR ADDITIONAL DETAILS AND BAR LIST, SEE ABUTMENT NO. 1 DETAILS (SHEET 2 OF 2).



② PEDESTAL HEIGHTS

BEAM NO.	TOP OF PED. ELEV.	HEIGHT
1	843.23	5 1/16"
2	843.41	7 1/8"
3	843.58	9 3/16"
4	843.75	11 1/4"
5	843.49	8 3/16"
6	843.24	5 5/8"
7	842.98	2"

DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "A"	
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		

ABUTMENT NO. 1 DETAILS

SHEET 1 OF 2
SHEET NO. B008

STATE JOB PIECE NO: 21006(07)

2:40:50 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAAD01.dgn

ABUTMENT NO. 1 BAR LIST-PHASE I

EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
BH1	#4	9	BNT.	52'-1"	
BH2	#4	10	BNT.	52'-5"	
BH3	#9	2	STR.	46'-0"	
BH4	#9	2	STR.	48'-4"	
BH5	#9	6	STR.	49'-1" AVG	48'-8" TO 49'-6"
BH6	#4	1	STR.	51'-2"	
BH12	#4	24	BNT.	5'-1"	
BV1	#4	38	STR.	9'-3" AVG	8'-10" TO 9'-8"
BV2	#5	38	STR.	9'-3" AVG	8'-10" TO 9'-8"
BV3	#4	10	STR.	9'-7½" AVG	9'-7" TO 9'-8"
BV4	#5	10	STR.	9'-7½" AVG	9'-7" TO 9'-8"
BV5	#4	4	STR.	9'-11"	
P1	#4	16	BNT.	8'-8"	
P2	#4	24	BNT.	6'-8"	
S1	#5	54	BNT.	12'-11"	
SC	#4	2	BNT.	3'-5"	
WT1	#5	1	BNT.	10'-2"	
WT2	#5	3	BNT.	9'-0" AVG	6'-0" TO 12'-0"
WT3	#5	5	BNT.	14'-0"	
WT4	#5	12	BNT.	11'-0"	

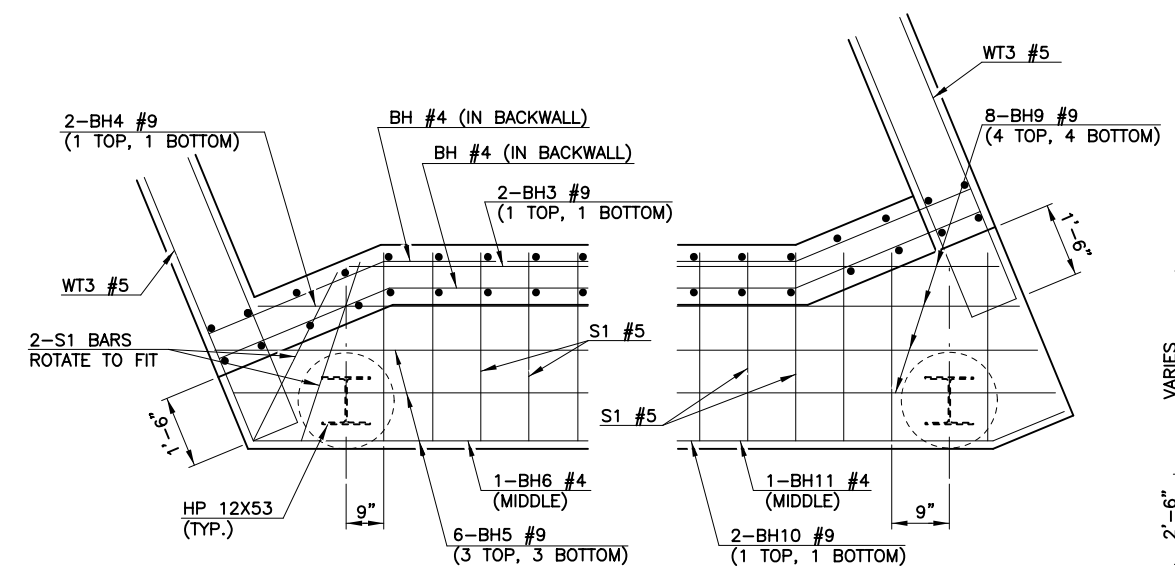
WINGWALL-EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
WH1	#5	2	BNT.	10'-2"	
WH2	#5	18	STR.	11'-9" AVG	5'-9" TO 17'-9"
WH3	#5	16	STR.	21'-2"	
WP1	#4	3	BNT.	8'-8"	
WP2	#4	4	STR.	1'-7"	
WV1	#4	6	STR.	3'-10"	
WV2	#4	38	STR.	7'-0" AVG	3'-11" TO 10'-1"

ABUTMENT NO. 1 BAR LIST-PHASE II

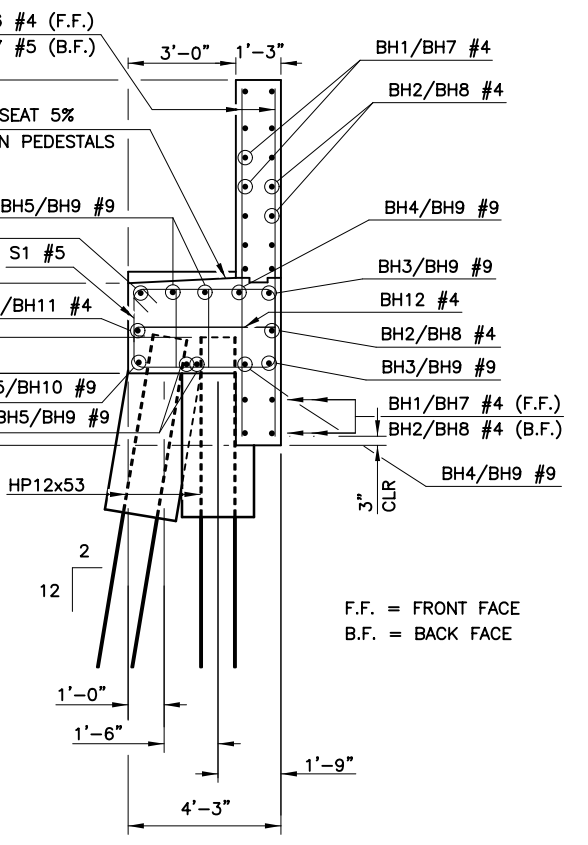
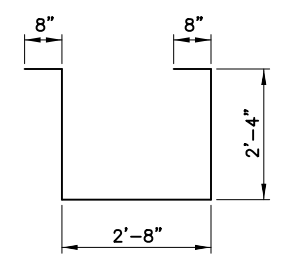
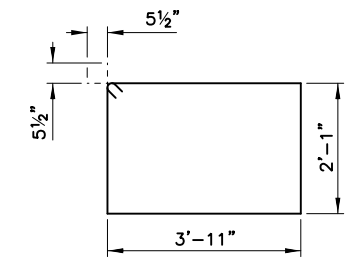
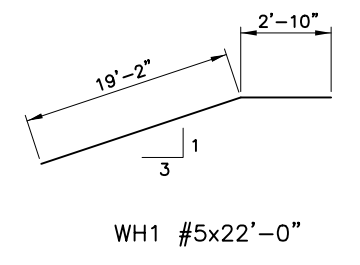
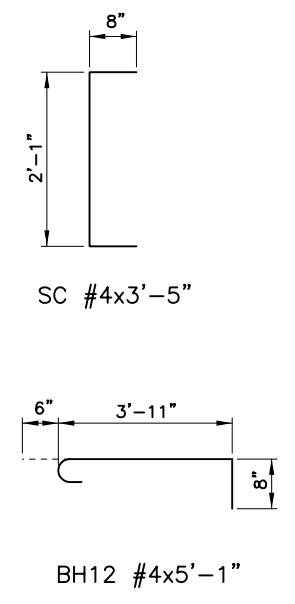
EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
BH7	#4	9	BNT.	29'-9"	
BH8	#4	10	BNT.	29'-5"	
BH9	#9	8	STR.	29'-10" AVG	29'-3" TO 30'-5"
BH10	#9	2	BNT.	30'-6"	
BH11	#4	1	BNT.	31'-0"	
BH12	#4	15	BNT.	5'-1"	
BV6	#4	38	STR.	9'-4" AVG	9'-2" TO 9'-6"
BV7	#5	38	STR.	9'-4" AVG	9'-2" TO 9'-6"
BV8	#4	4	STR.	10'-2"	
P1	#4	12	BNT.	8'-8"	
P2	#4	18	BNT.	6'-8"	
S1	#5	34	BNT.	12'-11"	
SC	#4	2	BNT.	3'-5"	
WT1	#5	1	BNT.	10'-2"	
WT2	#5	3	BNT.	9'-0" AVG	6'-0" TO 12'-0"
WT3	#5	5	BNT.	14'-0"	
WT4	#5	12	BNT.	11'-0"	

WINGWALL-EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
WH1	#5	2	BNT.	10'-2"	
WH2	#5	18	STR.	11'-9" AVG	5'-9" TO 17'-9"
WH3	#5	16	STR.	21'-2"	
WP1	#4	3	BNT.	8'-8"	
WP2	#4	4	STR.	1'-7"	
WV3	#4	6	STR.	3'-6"	
WV4	#4	38	STR.	6'-9" AVG	3'-8" TO 9'-10"

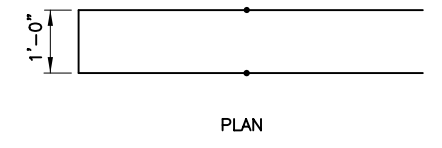
SUMMARY OF QUANTITIES (ABUTMENT NO. 1)				
ITEM	UNIT	PHASE I	PHASE II	TOTAL
SUBSTRUCTURE EXCAVATION COMMON	CY	72.00	50.00	122.00
CSLM BACKFILL	CY	125.00	87.00	212.00
CLASS A CONCRETE	CY	44.10	31.10	75.20
EPOXY COATED REINFORCING STEEL	LB	5,210.00	3,770.00	8,980.00
PILES, FURNISHED (HP10x42)	LF	109.00	109.00	218.00
PILES, FURNISHED (HP12x53)	LF	848.00	531.00	1,379.00
PILES, DRIVEN (HP10x42)	LF	109.00	109.00	218.00
PILES, DRIVEN (HP12x53)	LF	848.00	531.00	1,379.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	59.60	39.30	98.90
6" PERFORATED PIPE UNDERDRAIN ROUND	LF	48.00	29.00	77.00
6" NON-PERF. PIPE UNDERDRAIN ROUND	LF		30.00	30.00



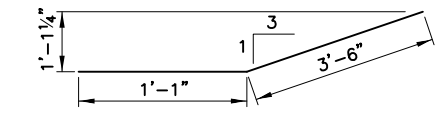
PHASE I
BRIDGE SEAT DETAIL



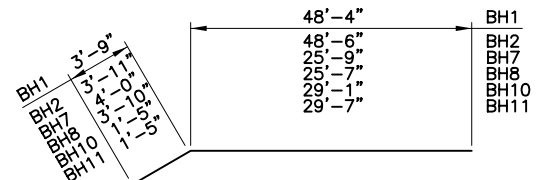
TYPICAL SECTION THRU SEAT



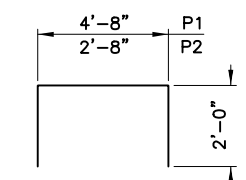
PLAN



ELEVATION



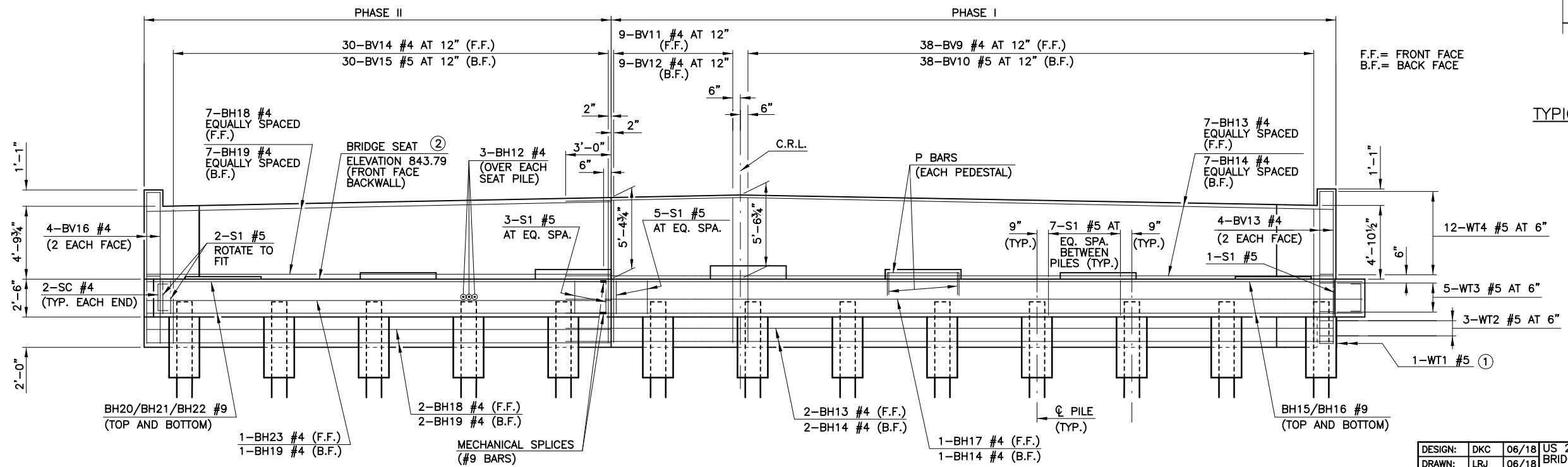
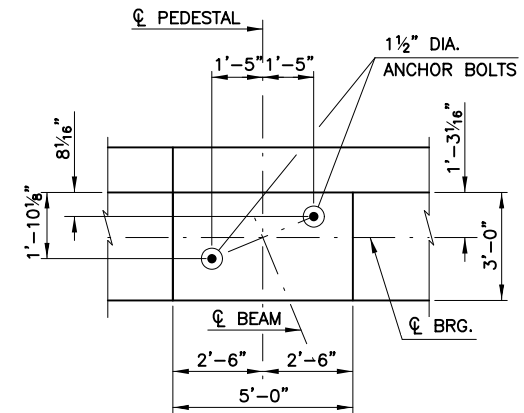
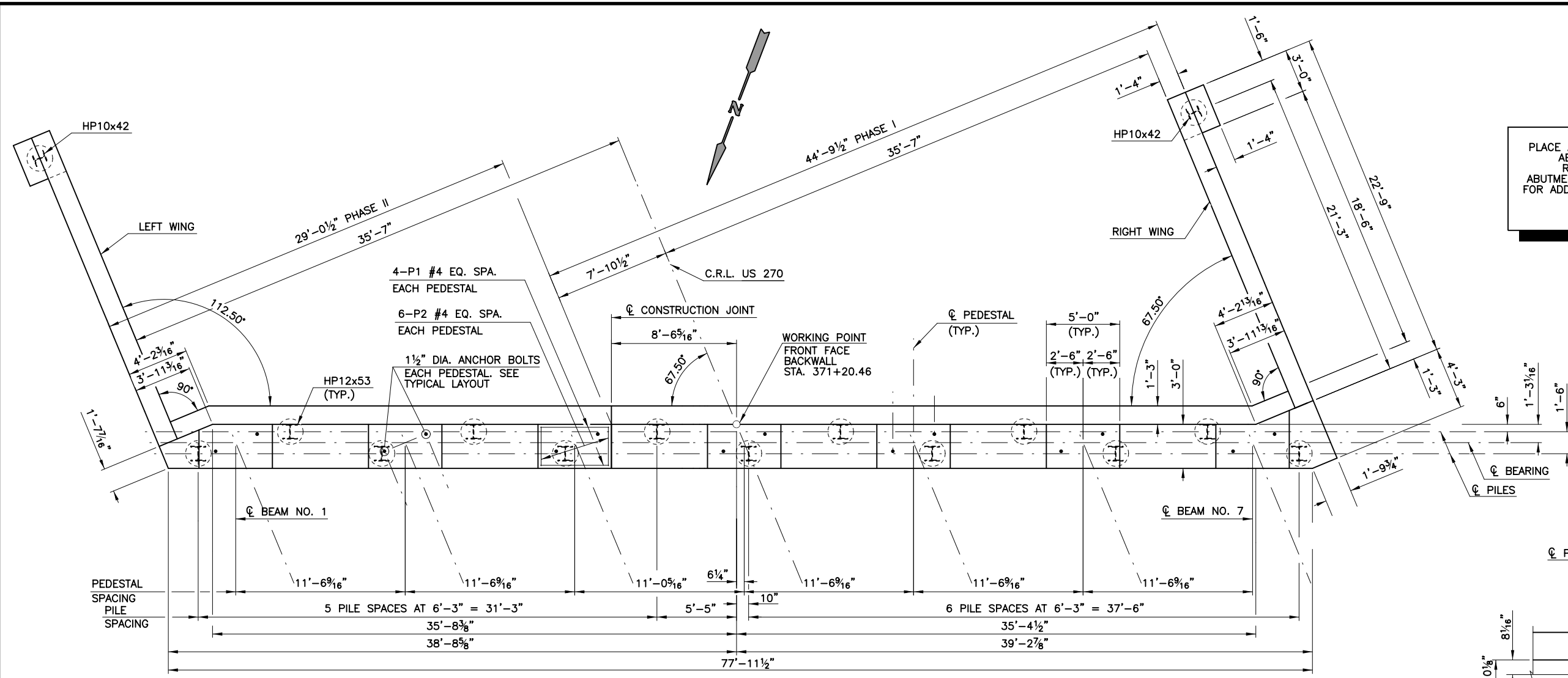
BH1 #4x52'-1"
BH2 #4x52'-5"
BH7 #4x29'-9"
BH8 #4x29'-5"
BH10 #9x30'-6"
BH11 #4x31'-0"



P1 #4x8'-8"
P2 #4x6'-8"

2:40:55 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 cpypdf_ANSIB.plt
 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRAAD04.dgn
 bmcanelly
 p:/Active Projects/ODOT113446.00/8.00

PLACE ALL WT WING REINFORCING TIED TO ABUTMENT SEAT AND BACKWALL REINFORCING BEFORE PLACING ABUTMENT SEAT AND BACKWALL CONCRETE. FOR ADDITIONAL DETAILS AND BAR LIST, SEE ABUTMENT NO. 2 DETAILS (SHEET 2 OF 2).



(2) PEDESTAL HEIGHTS

BEAM NO.	TOP OF PED. ELEV.	HEIGHT
1	843.96	2"
2	844.18	4 1/16"
3	844.41	7 3/8"
4	844.63	10 1/16"
5	844.43	7 3/8"
6	844.22	5 3/16"
7	844.02	2 1/16"

DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "A"	
CHECKED: DMN	06/18	ABUTMENT NO. 2 DETAILS	
APPRVD: DMN	06/18		
STATE JOB PIECE NO: 21006(07)			SHEET 1 OF 2 SHEET NO. B010

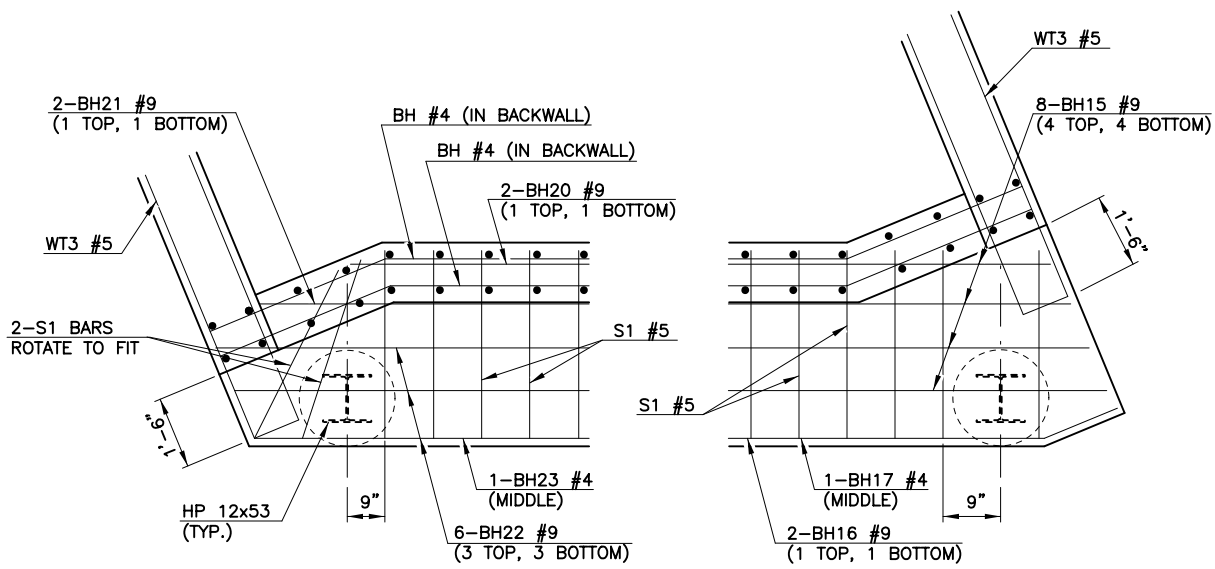
- ① WT BARS ARE TYPICAL FOR BOTH WINGS
- ② BRIDGE SEAT ELEVATION AND PEDESTAL HEIGHTS TAKEN AT FRONT FACE OF BACKWALL

2:40:59 PM 6/5/2018 bmcanelly cpyb&w_ANSIB.tbl p:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRAAD02.dgn

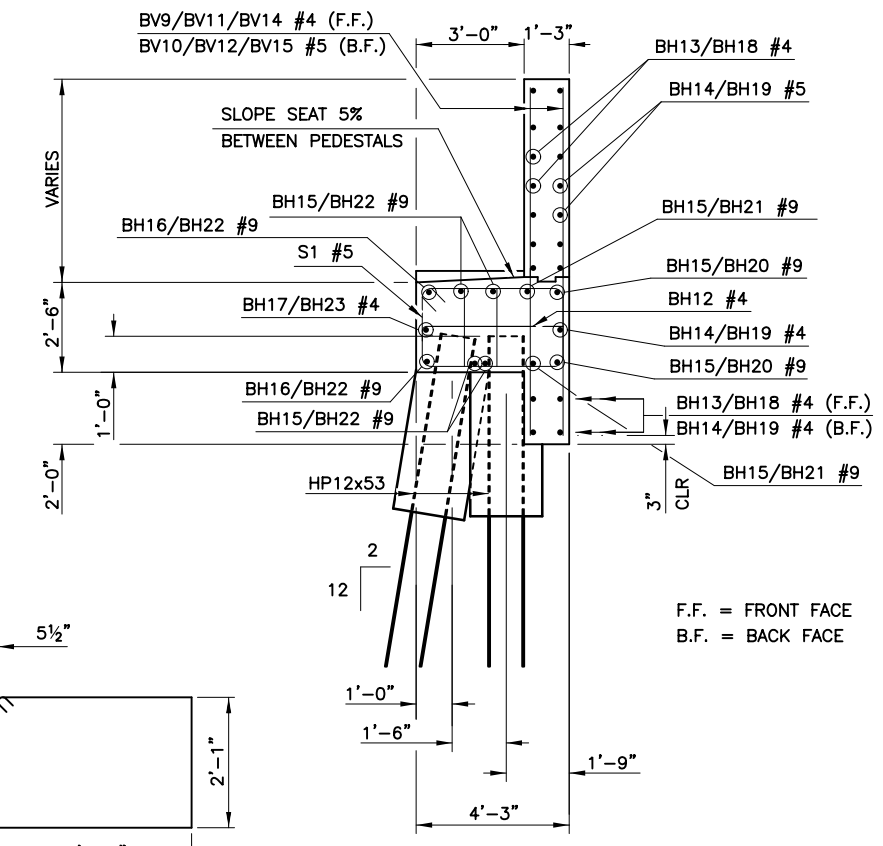
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B011	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

ABUTMENT NO. 2 BAR LIST-PHASE I

EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
BH12	#4	24	BNT.	5'-1"	
BH13	#4	9	BNT.	50'-10"	
BH14	#4	10	BNT.	50'-6"	
BH15	#9	8	STR.	51'-5" AVG	48'-4" TO 49'-6"
BH16	#9	2	BNT.	49'-7"	
BH17	#4	1	BNT.	52'-1"	
BV9	#4	38	STR.	9'-3" AVG	8'-11" TO 9'-7"
BV10	#5	38	STR.	9'-3" AVG	8'-11" TO 9'-7"
BV11	#4	9	STR.	9'-6" AVG	9'-5" TO 9'-7"
BV12	#5	9	STR.	9'-6" AVG	9'-5" TO 9'-7"
BV13	#4	4	STR.	10'-0"	
P1	#4	16	BNT.	8'-8"	
P2	#4	24	BNT.	6'-8"	
S1	#5	55	BNT.	12'-11"	
SC	#4	2	BNT.	3'-5"	
WT1	#5	1	BNT.	10'-2"	
WT2	#5	3	BNT.	9'-0" AVG	6'-0" TO 12'-0"
WT3	#5	5	BNT.	14'-0"	
WT4	#5	12	BNT.	11'-0"	
WINGWALL-EPOXY COATED REINFORCING BARS					
WH1	#5	2	BNT.	10'-2"	
WH2	#5	18	STR.	11'-9" AVG	5'-9" TO 17'-9"
WH3	#5	14	STR.	21'-2"	
WP1	#4	3	BNT.	8'-8"	
WP2	#4	4	STR.	1'-7"	
WV5	#4	6	STR.	3'-9"	
WV6	#4	38	STR.	6'-10" AVG	3'-10" TO 9'-10"

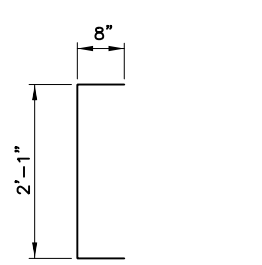


PHASE II
PHASE I
BRIDGE SEAT DETAIL

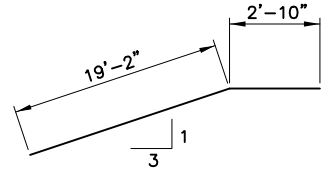


TYPICAL SECTION THRU SEAT

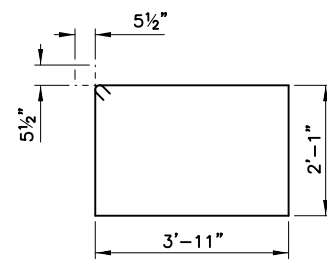
F.F. = FRONT FACE
B.F. = BACK FACE



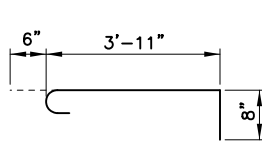
SC #4x3'-5"



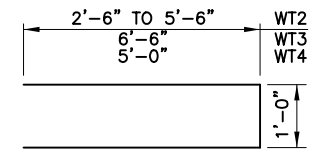
WH1 #5x22'-0"



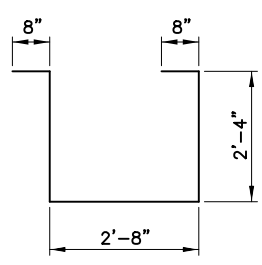
S1 #5x12'-11"



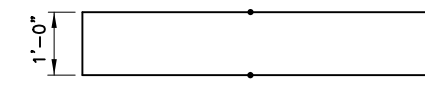
BH12 #4x5'-1"



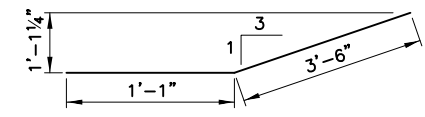
WT2 #5x9'-0" AVG
WT3 #5x14'-0"
WT4 #5x11'-0"



WP1 #4x8'-8"



PLAN



ELEVATION

WT1 #5x10'-2"

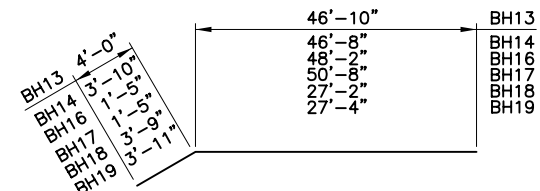
ABUTMENT NO. 2 BAR LIST-PHASE II

EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
BH12	#4	15	BNT.	5'-1"	
BH18	#4	9	BNT.	30'-11"	
BH19	#4	10	BNT.	31'-3"	
BH20	#9	2	STR.	26'-10"	
BH21	#9	2	STR.	29'-3"	
BH22	#9	6	STR.	29'-11" AVG	29'-6" TO 30'-4"
BH23	#4	1	STR.	30'-0"	
BV14	#4	30	STR.	9'-1 1/2" AVG	8'-10" TO 9'-5"
BV15	#5	30	STR.	9'-1 1/2" AVG	8'-10" TO 9'-5"
BV16	#4	4	STR.	9'-11"	
P1	#4	12	BNT.	8'-8"	
P2	#4	18	BNT.	6'-8"	
S1	#5	33	BNT.	12'-11"	
SC	#4	2	BNT.	3'-5"	
WT1	#5	1	BNT.	10'-2"	
WT2	#5	3	BNT.	9'-0" AVG	6'-0" TO 12'-0"
WT3	#5	5	BNT.	14'-0"	
WT4	#5	12	BNT.	11'-0"	
WINGWALL-EPOXY COATED REINFORCING BARS					
WH1	#5	2	BNT.	10'-2"	
WH2	#5	18	STR.	11'-9" AVG	5'-9" TO 17'-9"
WH3	#5	14	STR.	21'-2"	
WP1	#4	3	BNT.	8'-8"	
WP2	#4	4	STR.	1'-7"	
WV5	#4	6	STR.	3'-9"	
WV6	#4	38	STR.	6'-10" AVG	3'-10" TO 9'-10"

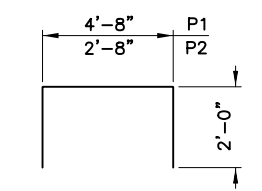
- ① 2 SETS OF 3 BARS
- ② 2 SETS OF 9 BARS
- ③ 2 SETS OF 19 BARS
- ④ 2 SETS OF 4 BARS

SUMMARY OF QUANTITIES (ABUTMENT NO. 2)

ITEM	UNIT	PHASE I	PHASE II	TOTAL
SUBSTRUCTURE EXCAVATION COMMON	CY	72.00	50.00	122.00
CSLM BACKFILL	CY	143.00	69.00	212.00
CLASS A CONCRETE	CY	44.30	30.30	74.60
EPOXY COATED REINFORCING STEEL	LB	5,230.00	3,570.00	8,800.00
PILES, FURNISHED (HP10x42)	LF	111.00	111.00	222.00
PILES, FURNISHED (HP12x53)	LF	864.00	541.00	1,405.00
PILES, DRIVEN (HP10x42)	LF	111.00	111.00	222.00
PILES, DRIVEN (HP12x53)	LF	864.00	541.00	1,405.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	60.30	37.00	97.30
6" PERFORATED PIPE UNDERDRAIN ROUND	LF	47.00	30.00	77.00
6" NON-PERF. PIPE UNDERDRAIN ROUND	LF		30.00	30.00



BH13 #4x50'-10"
BH14 #4x50'-6"
BH16 #9x49'-7"
BH17 #4x52'-1"
BH18 #4x30'-11"
BH19 #4x31'-3"



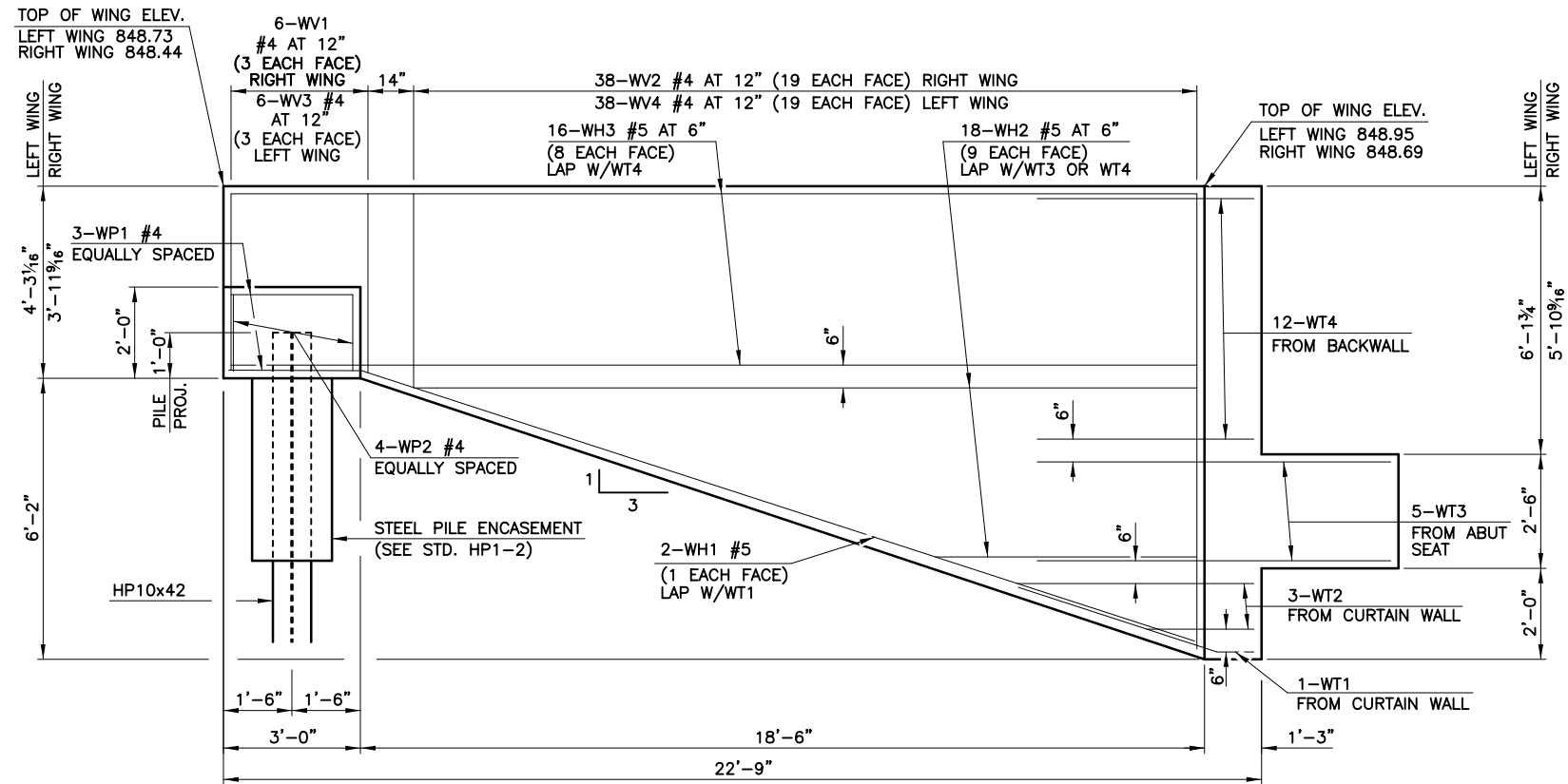
P1 #4x8'-8"
P2 #4x6'-8"

DESIGN: DKC 06/18
DRAWN: LRJ 06/18
CHECKED: DMN 06/18
APPRVD: DMN 06/18

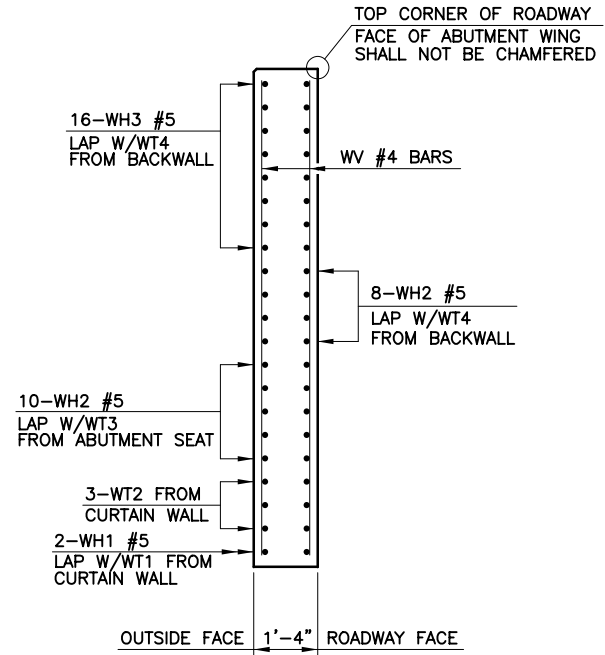
US 270 BRIDGE "A"
SEMINOLE COUNTY
ABUTMENT NO. 2 DETAILS
STATE JOB PIECE NO: 21006(07)
SHEET 2 OF 2
SHEET NO. B011

2:41:04 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRAAD05.dgn

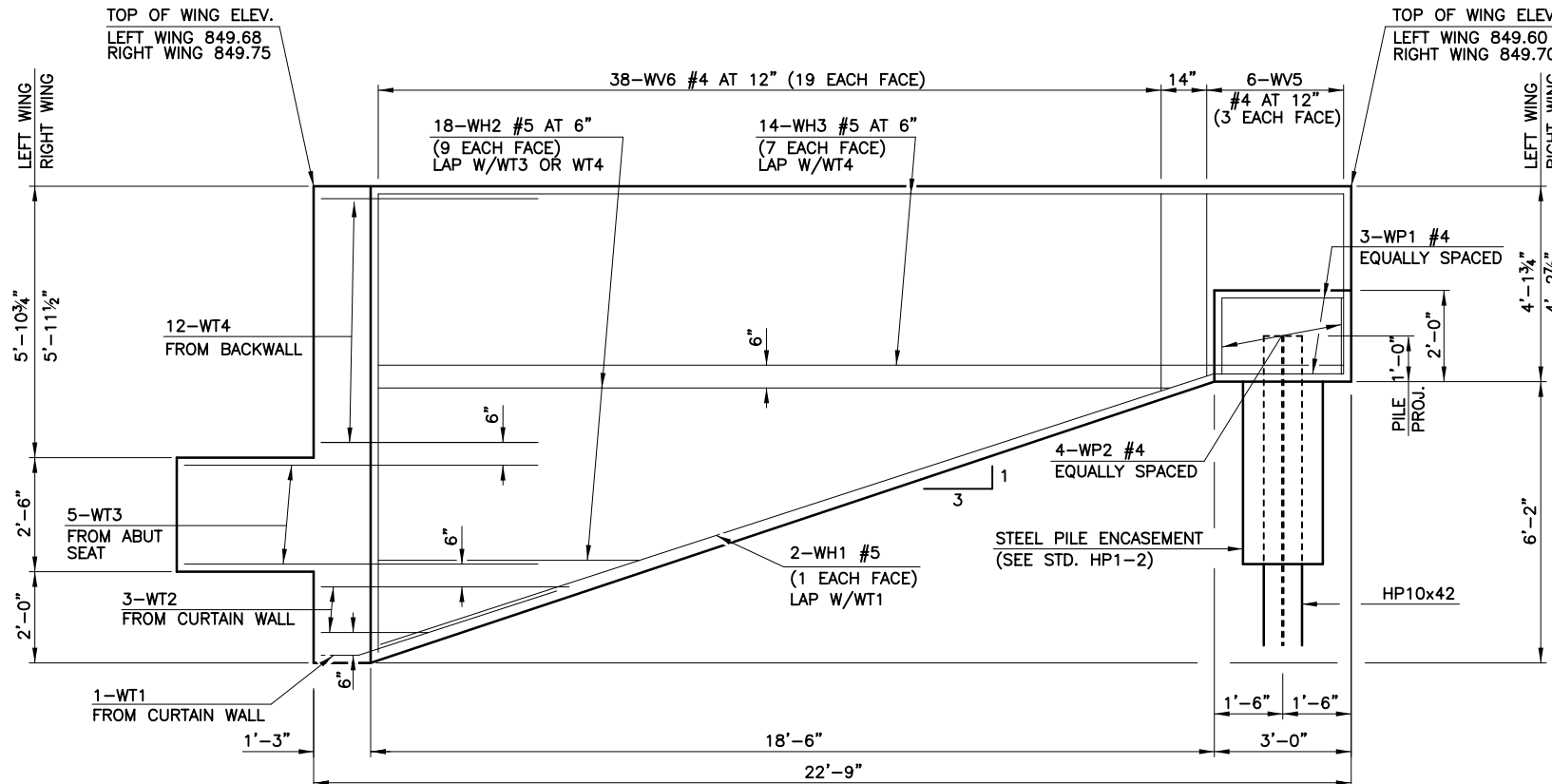
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B012	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



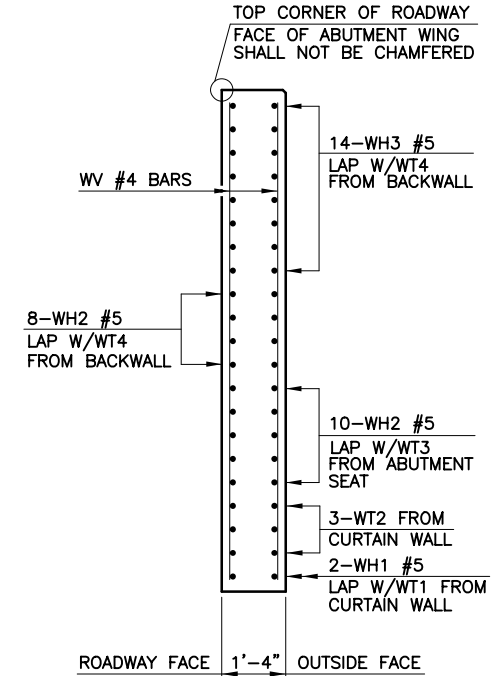
ABUTMENT NO. 1



SECTION THROUGH WING AT BACK FACE OF ABUTMENT SEAT (ABUT. NO. 1)



ABUTMENT NO. 2



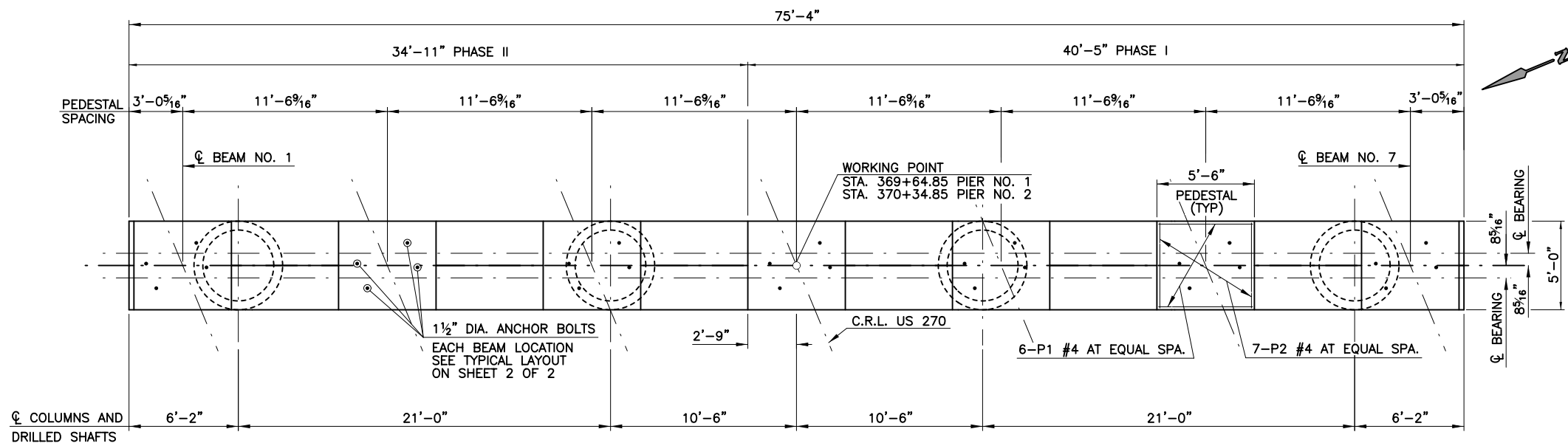
SECTION THROUGH WING AT BACK FACE OF ABUTMENT SEAT (ABUT. NO. 2)

2:41:08 PM 6/5/2018 bmcanelly
 cpy: Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAAD03.dgn
 cpy: Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAAD03.dgn

DESIGN:	DKC	06/18
DRAWN:	LRJ	06/18
CHECKED:	DMN	06/18
APPRVD:	DMN	06/18

US 270 BRIDGE "A"
 SEMINOLE COUNTY
ABUTMENT DETAILS
 STATE JOB PIECE NO: 21006(07)
 SHEET 1 OF 1
 SHEET NO. B012

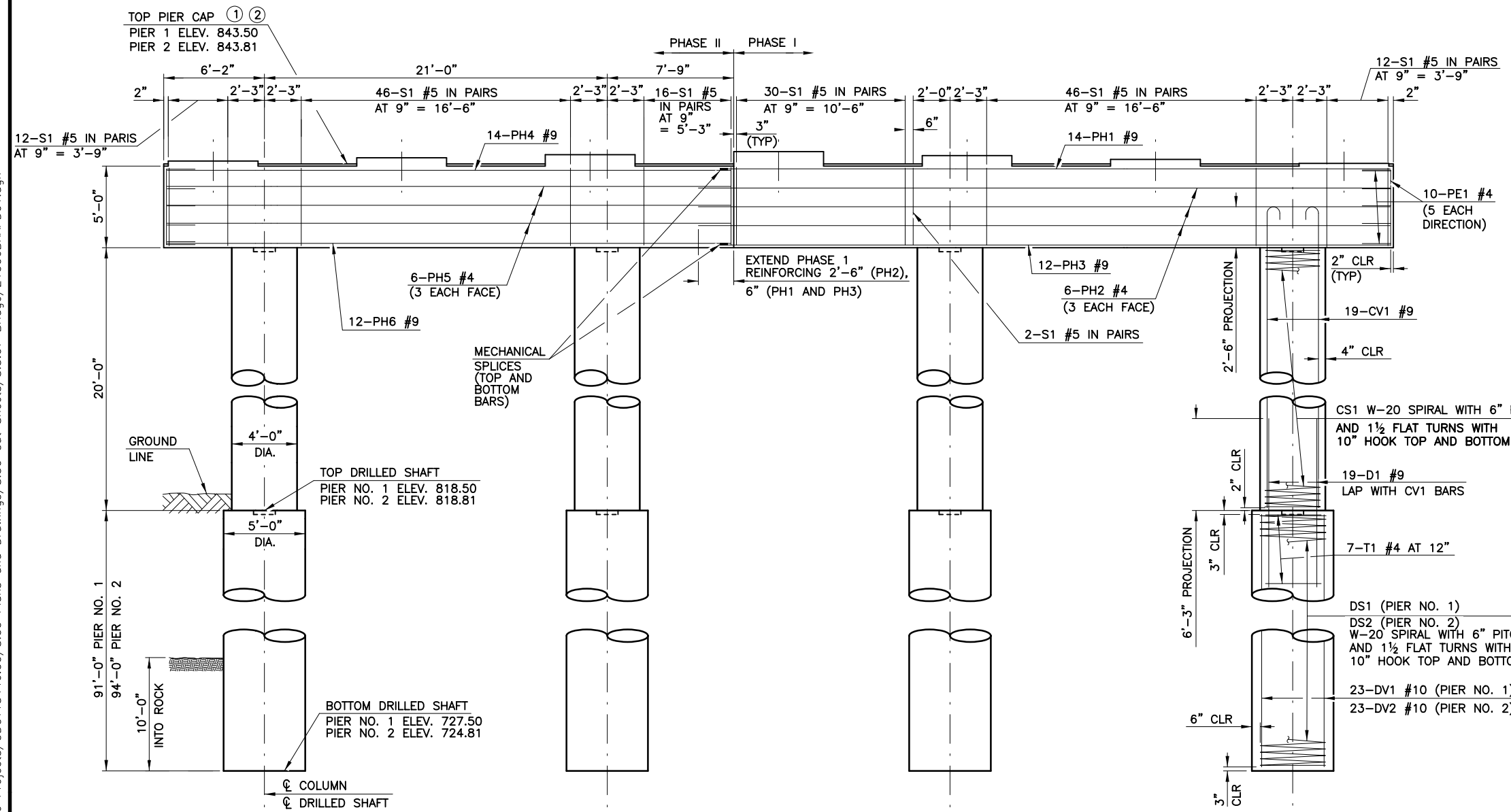




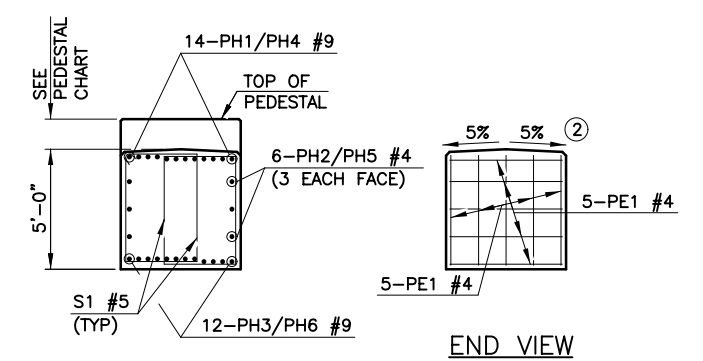
PLAN

① PEDESTAL HEIGHTS				
BEAM NO.	PIER NO. 1		PIER NO. 2	
	ELEV.	HEIGHT	ELEV.	HEIGHT
1	843.81	3 3/4"	844.03	2 5/8"
2	844.01	6 1/16"	844.23	5 1/8"
3	844.20	8 7/16"	844.45	7 9/16"
4	844.39	10 5/8"	844.65	10 1/16"
5	844.15	7 3/4"	844.42	7 3/8"
6	843.91	4 7/8"	844.20	4 1 1/16"
7	843.67	2"	843.98	2"

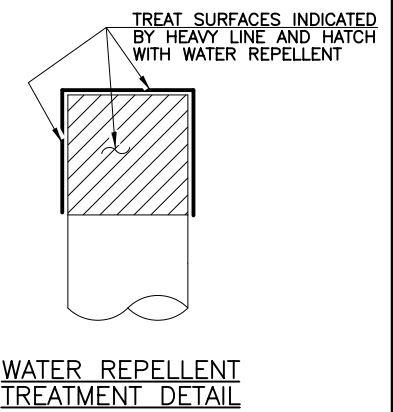
- ① PIER CAP ELEVATION, PEDESTAL ELEVATIONS, AND PEDESTAL HEIGHTS ARE ESTABLISHED AT CENTER OF PIER CAP.
- ② SLOPE PIER CAP 5% EACH DIRECTION FROM CENTER BETWEEN PEDESTALS.



ELEVATION



TYPICAL SECTION THROUGH CAP



WATER REPELLENT TREATMENT DETAIL

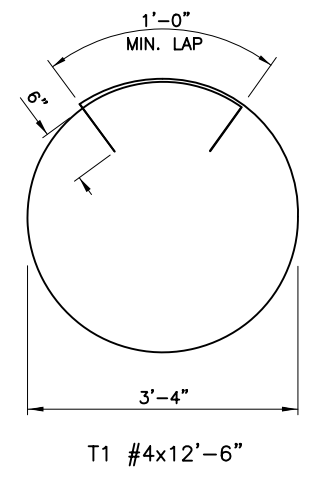
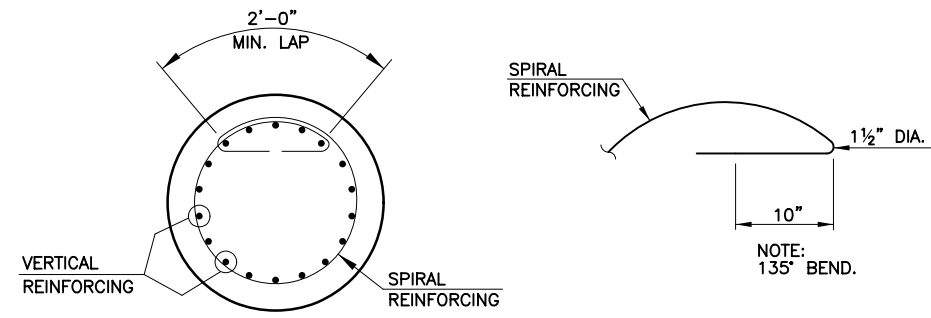
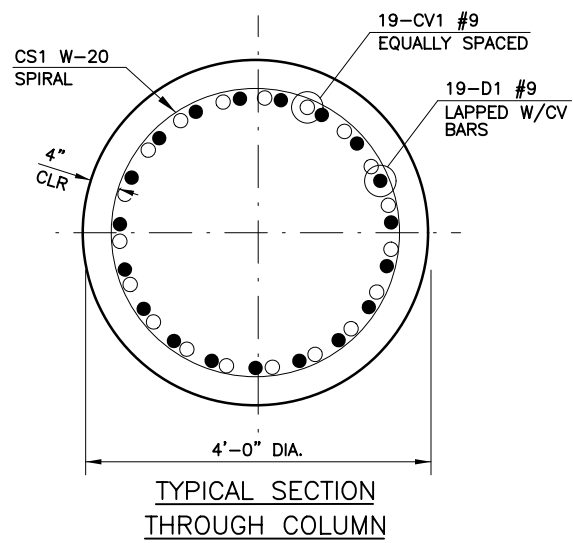
DESIGN: DKC 06/18	US 270 BRIDGE "A"	SEMINOLE COUNTY
DRAWN: LRJ 06/18		
CHECKED: DMN 06/18		
APPRVD: DMN 06/18		

PIER DETAILS

SHEET 1 OF 2
SHEET NO. B013

STATE JOB PIECE NO: 21006(07)

bmcanelly 6/5/2018 2:41:13 PM
 cpyb&w_ANSIB.tbl cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRAPD01.dgn



PIER COLUMN AND CAP BAR LIST (PHASE I) TWO REQUIRED

EPOXY COATED REINFORCING				
MARK	SIZE	NO.	FORM	LENGTH
CV1	#9	38	BNT.	23'-8"
P1	#4	24	BNT.	9'-2"
P2	#4	28	BNT.	8'-8"
PE1	#4	10	BNT.	6'-8"
PH1	#9	14	BNT.	42'-5"
PH2	#4	6	STR.	42'-9"
PH3	#9	12	STR.	40'-9"
S1	#5	90	BNT.	16'-7"
PLAIN REINFORCING BARS				
CS1	W20	2	BNT.	447'-10"

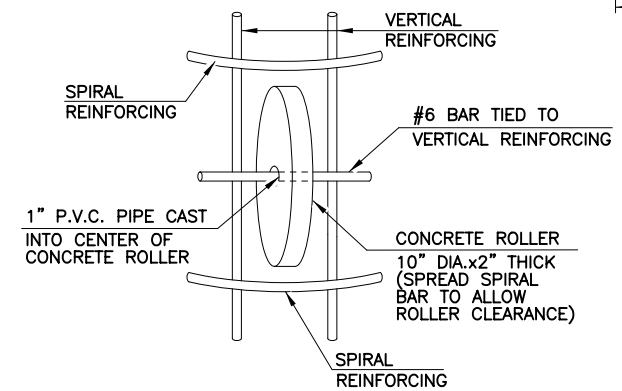
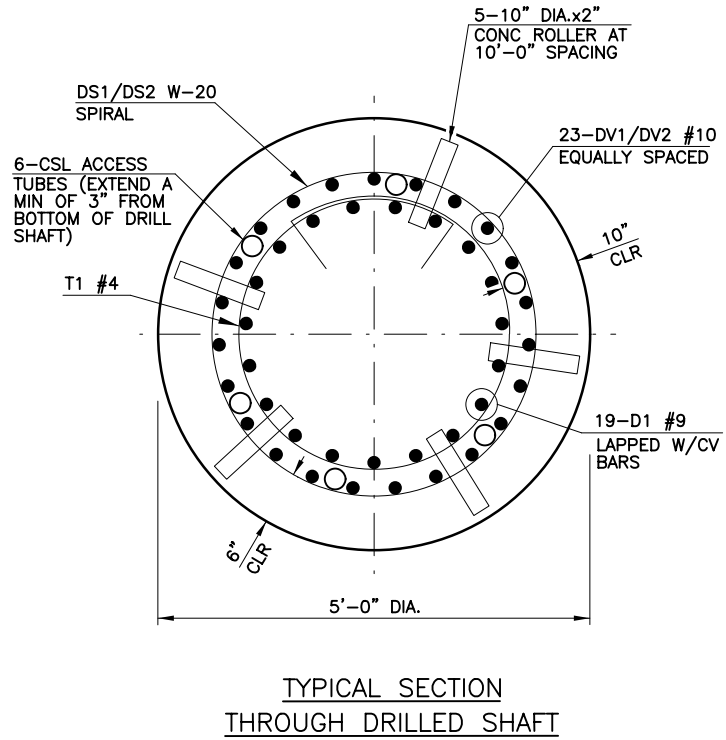
PIER COLUMN AND CAP BAR LIST (PHASE II) TWO REQUIRED

EPOXY COATED REINFORCING				
MARK	SIZE	NO.	FORM	LENGTH
CV1	#9	38	BNT.	23'-8"
P1	#4	18	BNT.	9'-2"
P2	#4	21	BNT.	8'-8"
PE1	#4	10	BNT.	6'-8"
PH4	#9	14	BNT.	35'-11"
PH5	#4	6	STR.	34'-9"
PH6	#9	12	STR.	34'-3"
S1	#5	74	BNT.	16'-7"
PLAIN REINFORCING BARS				
CS1	W20	2	BNT.	447'-10"

DRILLED SHAFT BAR LIST

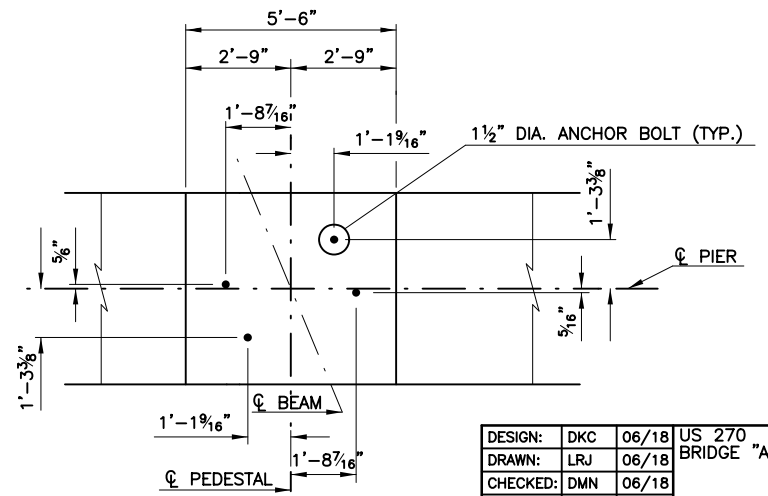
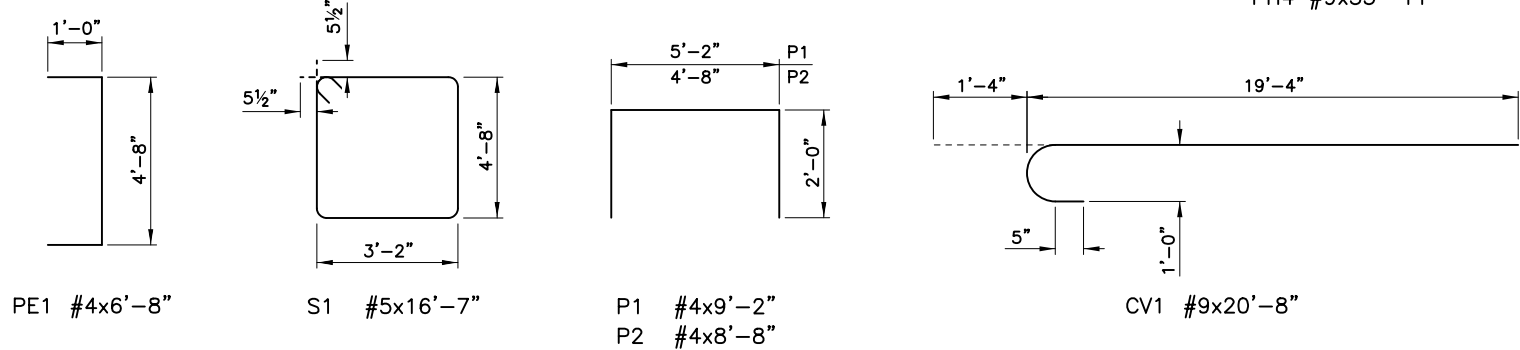
EPOXY COATED REINFORCING					QUANTITY			
MARK	SIZE	FORM	LENGTH	PIER NO. 1		PIER NO. 2		
				PHASE I	PHASE II	PHASE I	PHASE II	
D1	#9	STR.	12'-6"	38	38	38	38	
PLAIN REINFORCING					QUANTITY			
MARK	SIZE	FORM	LENGTH	PIER NO. 1		PIER NO. 2		
				PHASE I	PHASE II	PHASE I	PHASE II	
DS1	W20	BNT.	2,331'-7"	2	2			
DS2	W20	BNT.	2,407'-7"			2	2	
DV1	#10	STR.	96'-6"	46	46			
DV2	#10	STR.	99'-6"			46	46	
T1	#4	BNT.	12'-6"	14	14	14	14	

① INCLUDES ONE 6'-0" LAP



SUMMARY OF PIER QUANTITIES

ITEM	UNIT	PHASE I			PHASE II		
		PIER NO. 1	PIER NO. 2	TOTAL	PIER NO. 1	PIER NO. 2	TOTAL
CLASS A CONCRETE	CY	58.00	57.90	115.90	52.30	52.10	104.40
REINFORCING STEEL	LB	610.00	610.00	1,220.00	610.00	610.00	1,220.00
EPOXY COATED REINFORCING STEEL	LB	8,830.00	8,830.00	17,660.00	7,940.00	7,940.00	15,880.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	75.00	75.00	150.00	65.00	64.00	129.00
DRILLED SHAFTS 60" DIAMETER	LF	182.00	188.00	370.00	182.00	188.00	370.00
CROSSHOLE SONIC LOGGING	EA	2.00	2.00	4.00	2.00	2.00	4.00

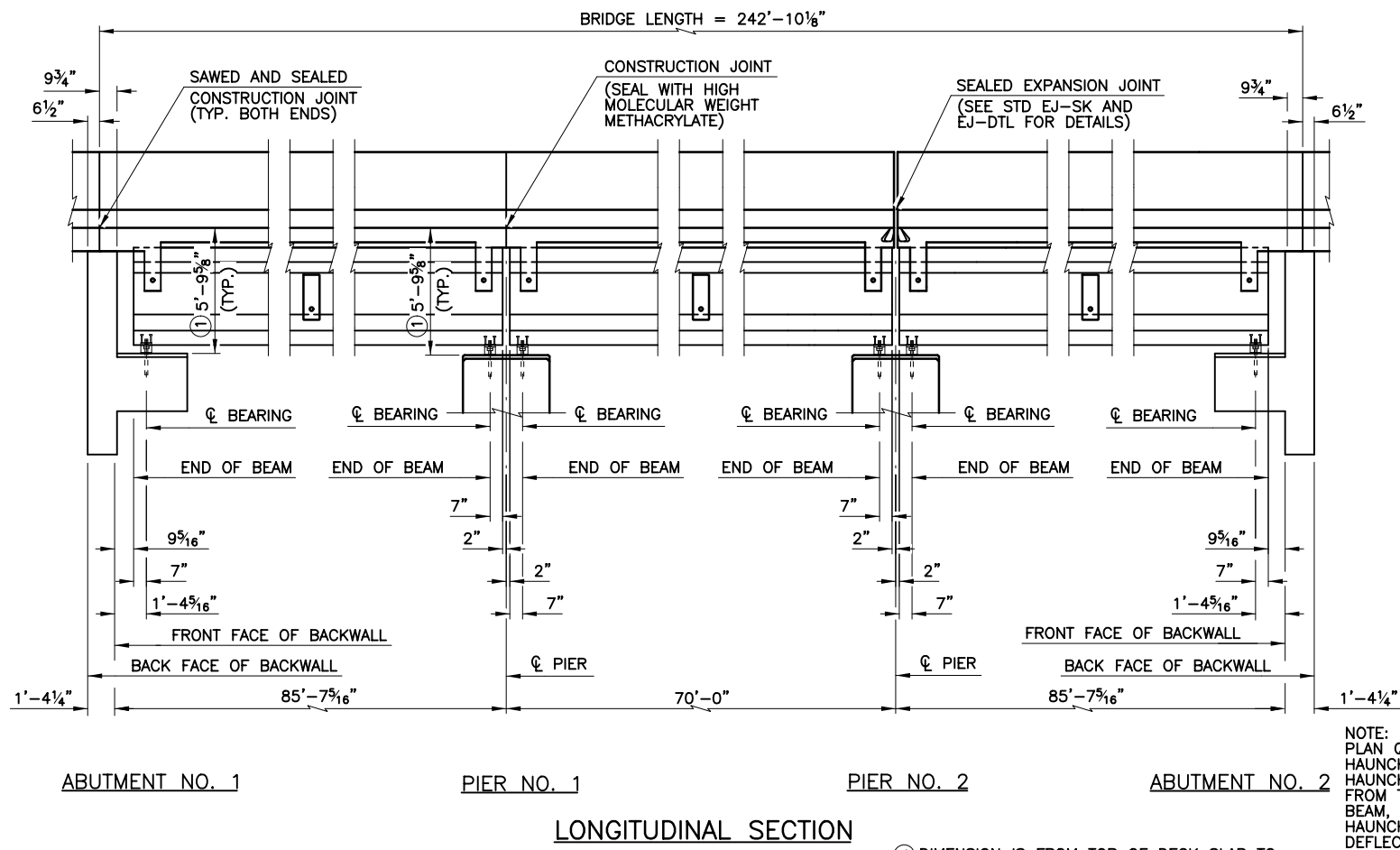
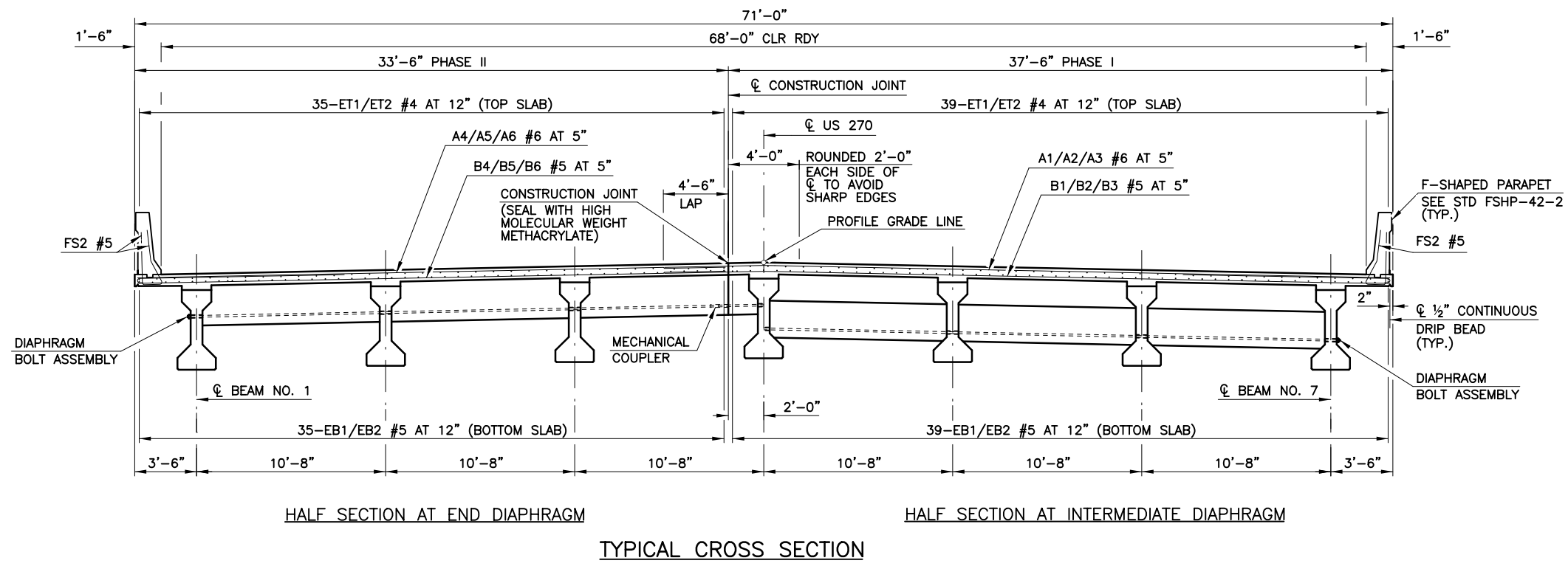


① CSL TUBES SHALL EXTEND TO 3" CLEARANCE FROM BOTTOM OF DRILLED SHAFT.

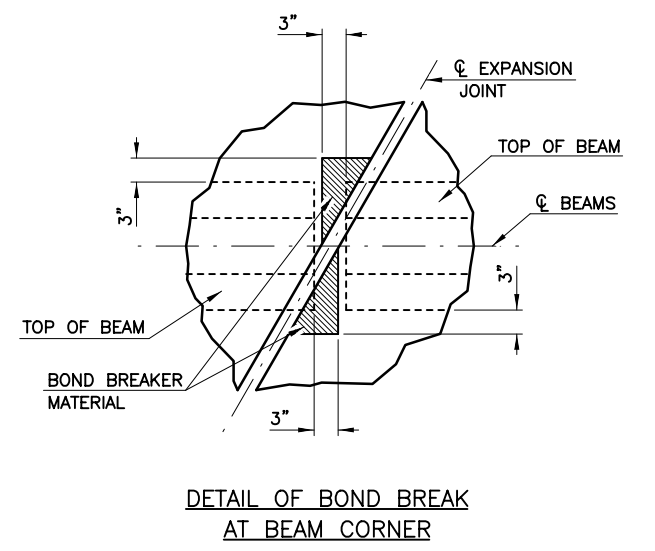
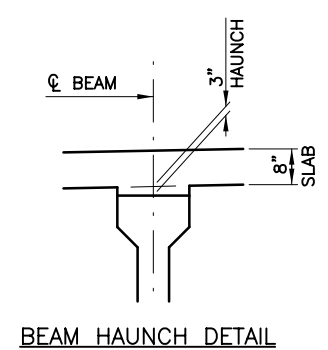
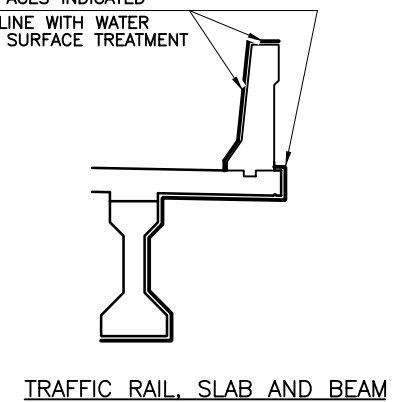
2:41:17 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAPD03.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B015	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

DO NOT PLACE THE CONCRETE FOR THE DECK SLAB OR APPLY OTHER MASSIVE LOADS TO THE BEAMS OR DIAPHRAGMS UNTIL THE CONCRETE IN THE DIAPHRAGMS HAS BEEN IN PLACE A MINIMUM OF 10 DAYS OR AT THE DISCRETION OF THE ENGINEER. THE ENGINEER MAY APPROVE SHORTENED TIME IF THE BEAM AND DIAPHRAGM CONCRETE HAS ATTAINED 80% OF THE SPECIFIED COMPRESSIVE STRENGTH.



TREAT SURFACES INDICATED BY HEAVY LINE WITH WATER REPELLENT SURFACE TREATMENT



NOTE: PLAN QUANTITIES FOR CLASS AA CONCRETE INCLUDE BEAM HAUNCHES. THE HAUNCH HEIGHT SHOWN IS THE THEORETICAL HAUNCH HEIGHT AT THE CENTERLINE BEARING ONLY, MEASURED FROM THE BOTTOM OF THE DECK SLAB TO THE TOP OF THE BEAM, AND VARIES ACROSS THE SPAN. DETERMINE THE ACTUAL HAUNCH HEIGHT (ACCOUNTING FOR BEAM CAMBER, DEAD LOAD DEFLECTION AND ROADWAY GRADE) AFTER ERECTION OF THE BEAMS AND SUBMIT TO THE ENGINEER FOR APPROVAL. THE ENGINEER WILL NOT MEASURE DIFFERENCE BETWEEN THE THEORETICAL AND THE ACTUAL HAUNCH HEIGHTS FOR PAYMENT.

DESIGN:	DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN:	LRJ	06/18	BRIDGE "A"	
CHECKED:	DMN	06/18		
APPRVD:	DMN	06/18		

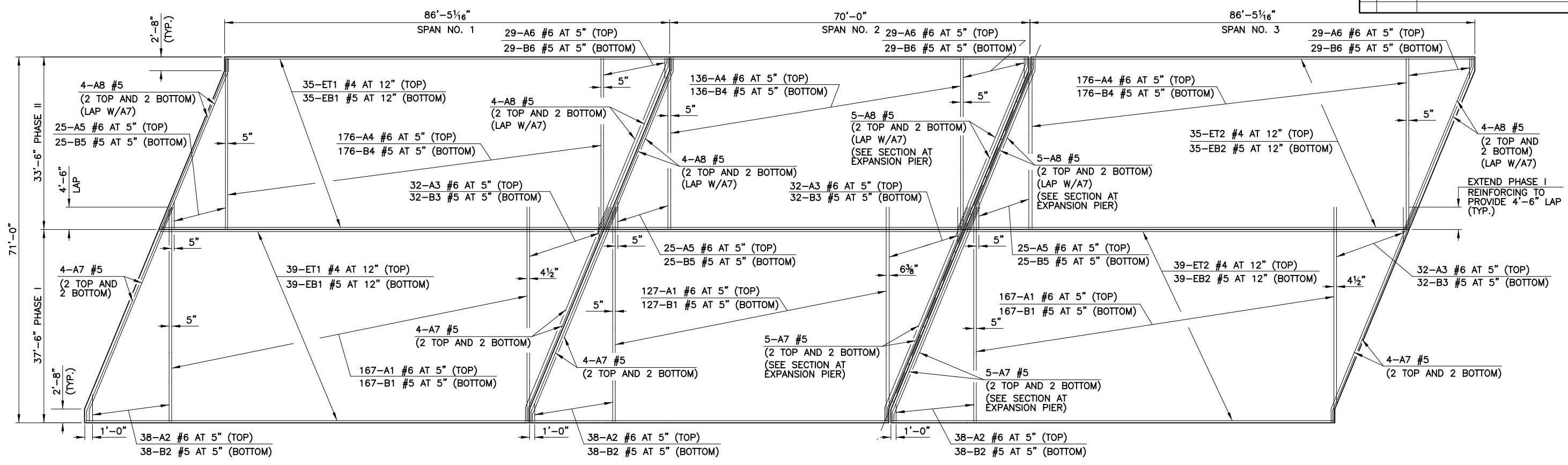
SUPERSTRUCTURE DETAILS

STATE JOB PIECE NO: 21006(07)

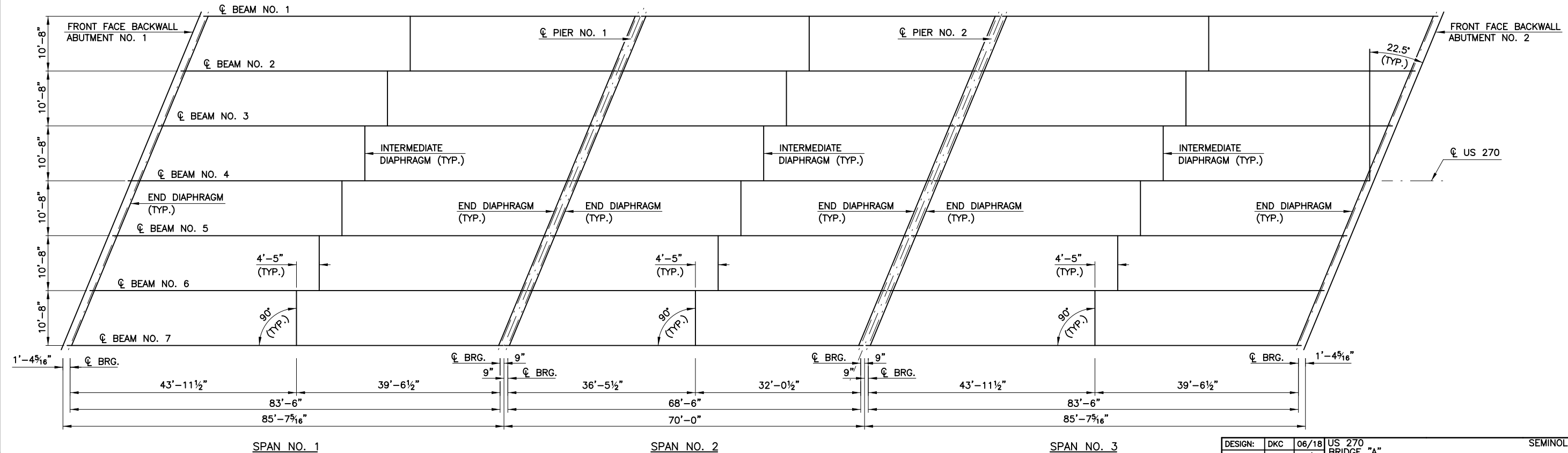
SHEET 1 OF 5
SHEET NO. B015

2:41:22 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAS101.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B016	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



SLAB REINFORCING LAYOUT

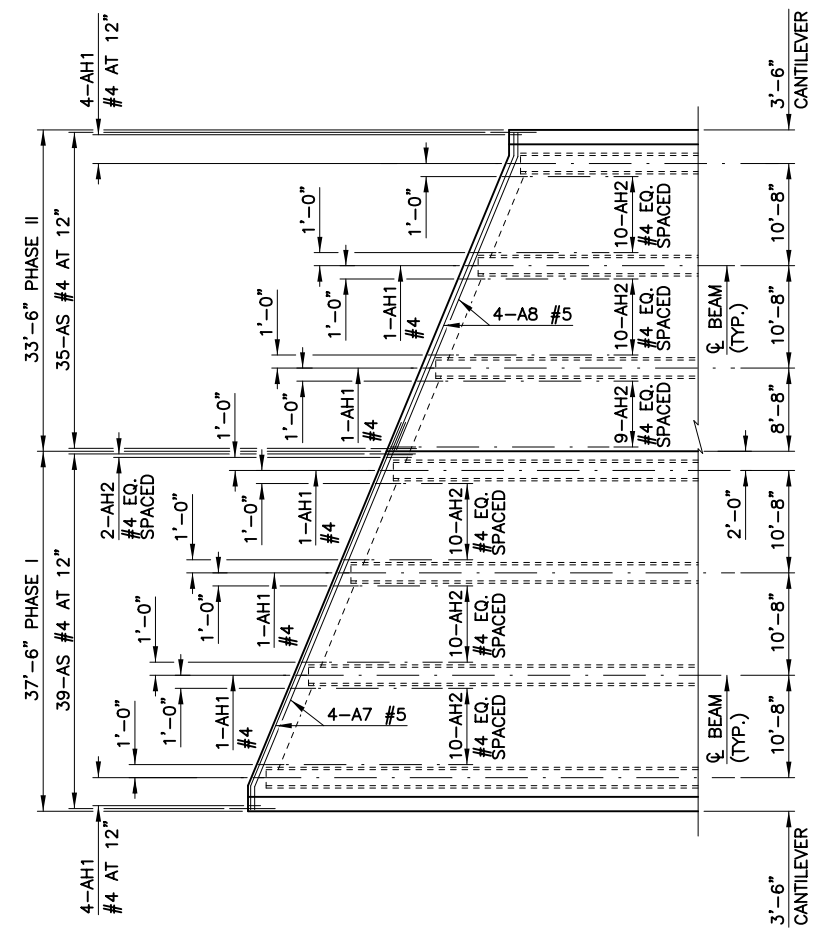


FRAMING PLAN

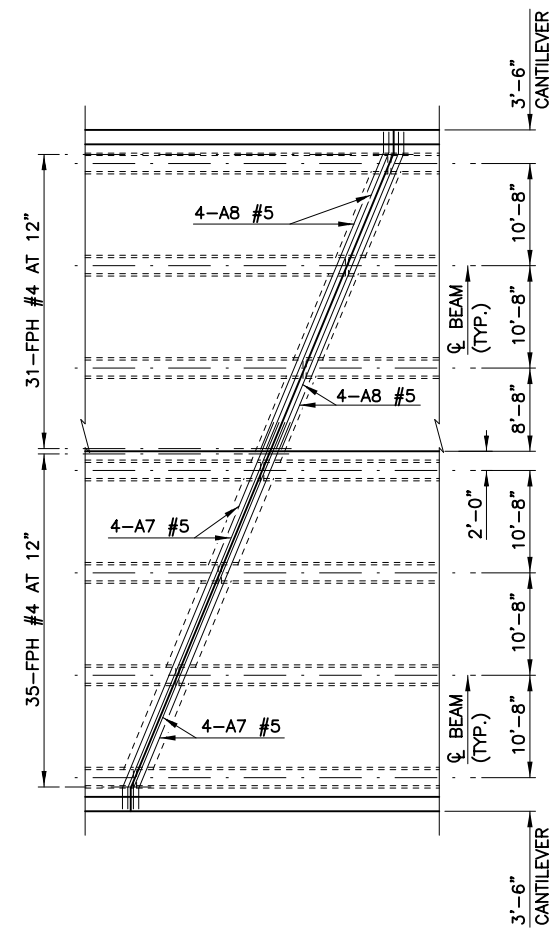
DESIGN:	DKC	06/18	US 270 BRIDGE "A"	SEMINOLE COUNTY
DRAWN:	LRJ	06/18		
CHECKED:	DMN	06/18		
APPRVD:	DMN	06/18		
CP&Y			SUPERSTRUCTURE DETAILS	
			STATE JOB PIECE NO: 21006(07)	
			SHEET 2 OF 5 SHEET NO. B016	

2:41:26 PM
 cpydf_ANSIB.plt
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAS01.dgn

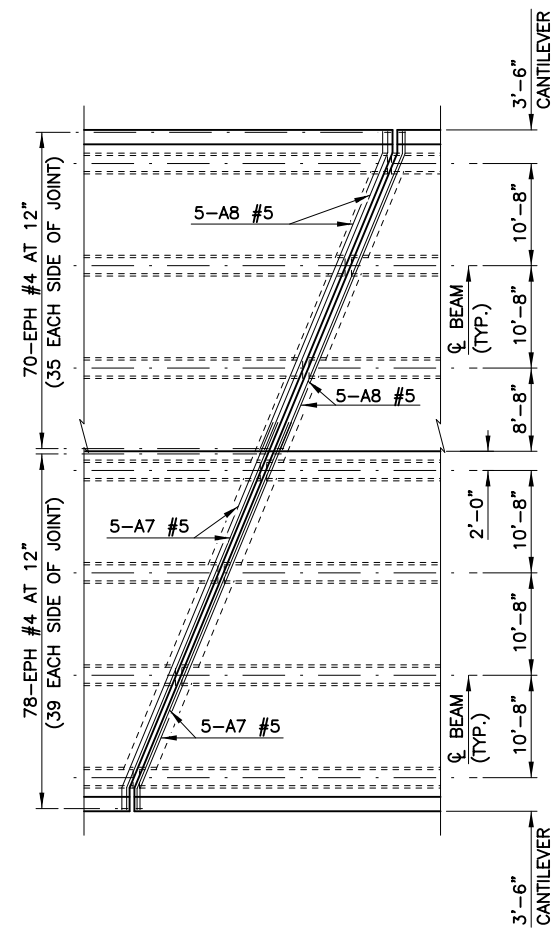
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B017	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



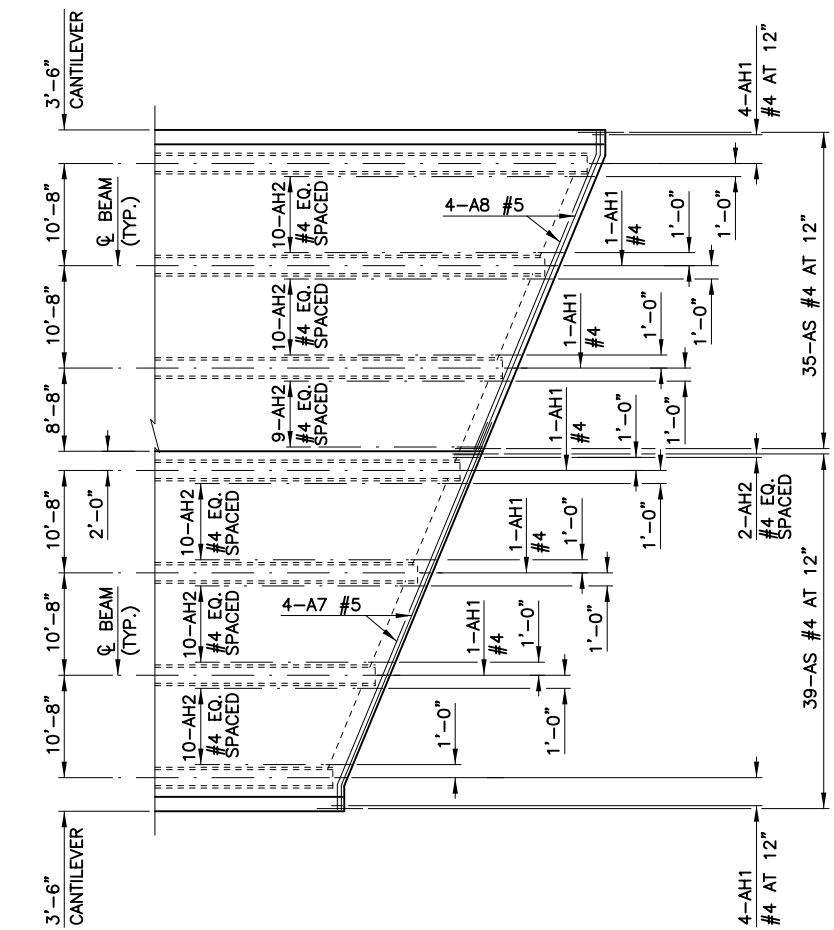
ABUTMENT NO. 1



PIER NO. 1

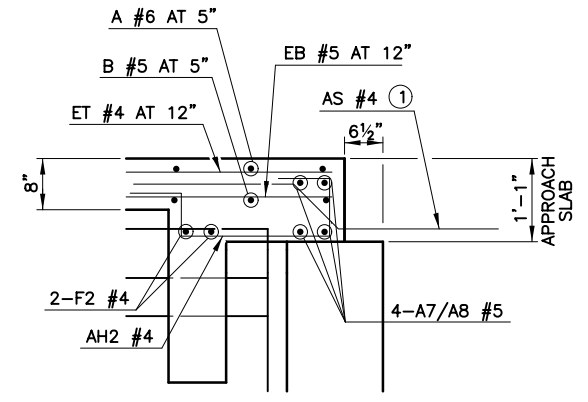


PIER NO. 2

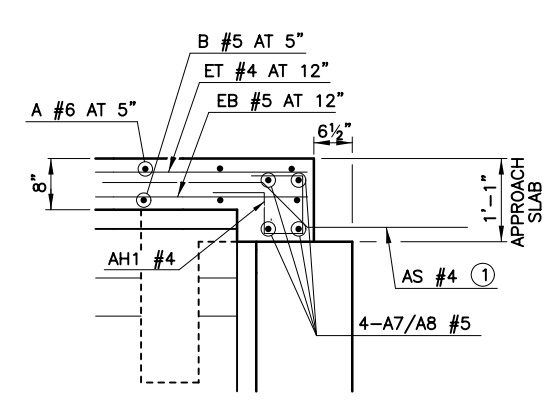


ABUTMENT NO. 2

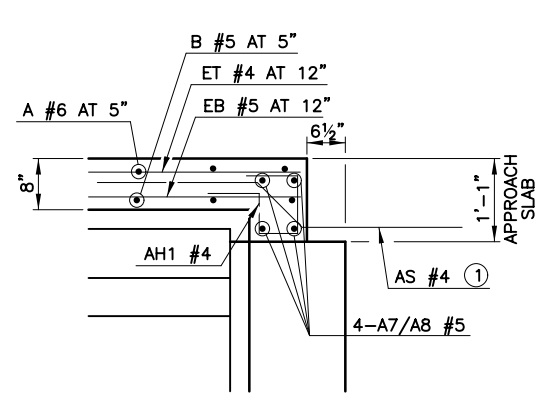
DIAPHRAGM REINFORCING PLAN



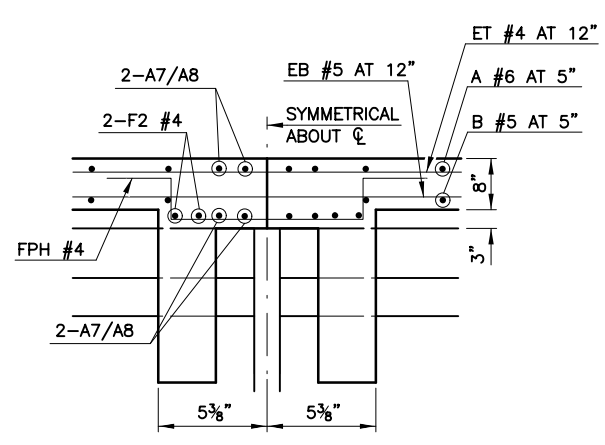
SECTION AT ABUTMENT BETWEEN BEAMS



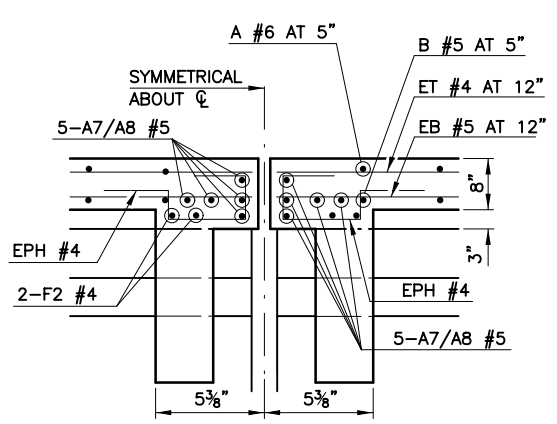
SECTION AT ABUTMENT THROUGH BEAMS



SECTION AT ABUTMENT THROUGH CANTILEVER



SECTION AT FIXED PIER



SECTION AT EXPANSION PIER

① TIE TO TOP REINFORCING OF DECK SLAB AND BOTTOM REINFORCING OF THE APPROACH SLAB. (PLACE BOTTOM LEG OF AS BAR THRU JOINT)

DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "A"	
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		

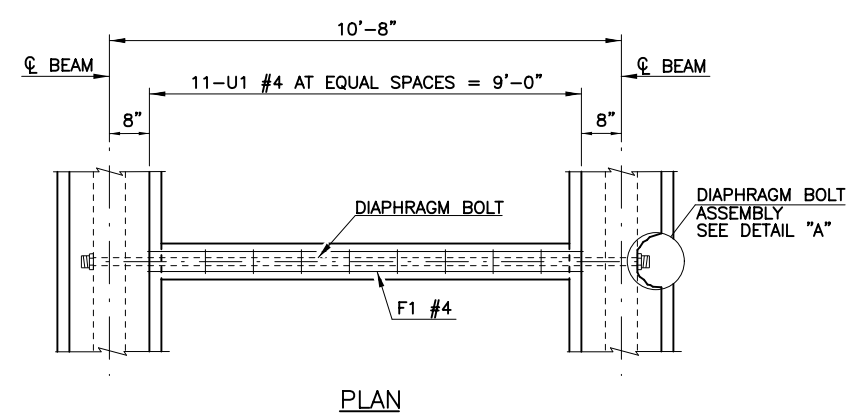
SUPERSTRUCTURE DETAILS

SHEET 3 OF 5
SHEET NO. B017

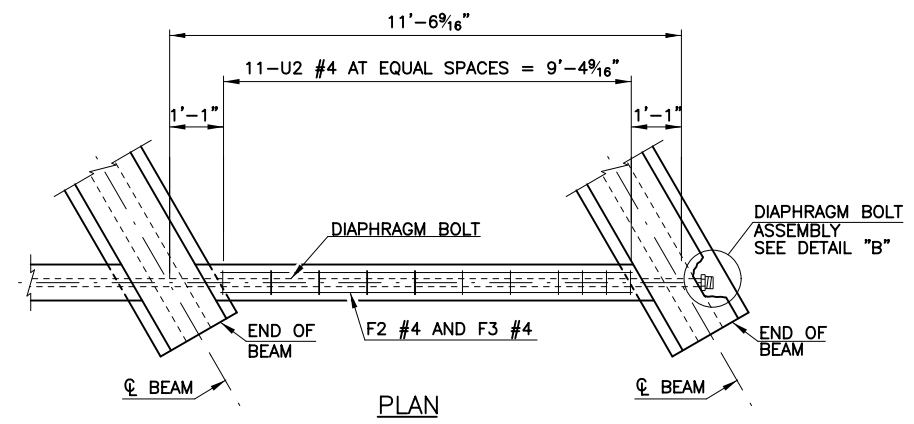
STATE JOB PIECE NO: 21006(07)

2:41:31 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRAS02.dgn

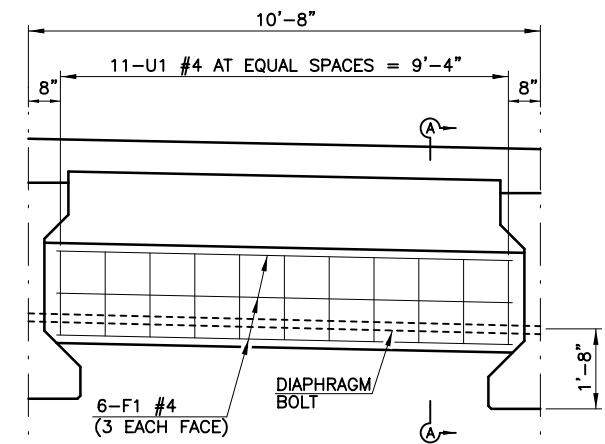
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B018	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



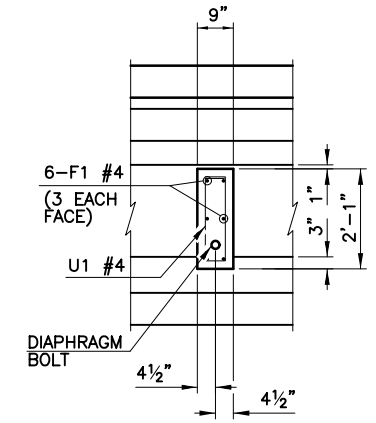
PLAN



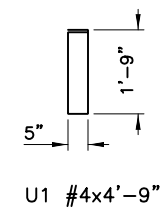
PLAN



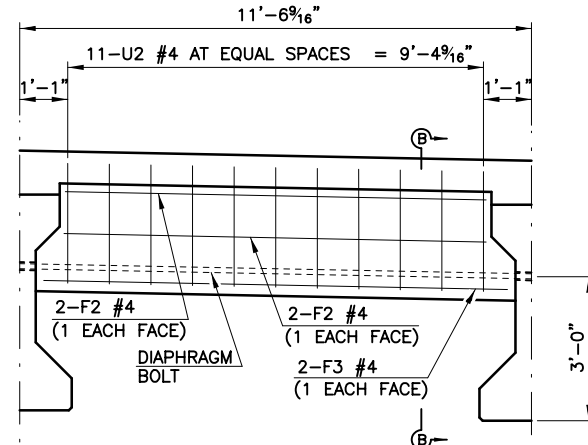
INTERMEDIATE DIAPHRAGM DETAIL



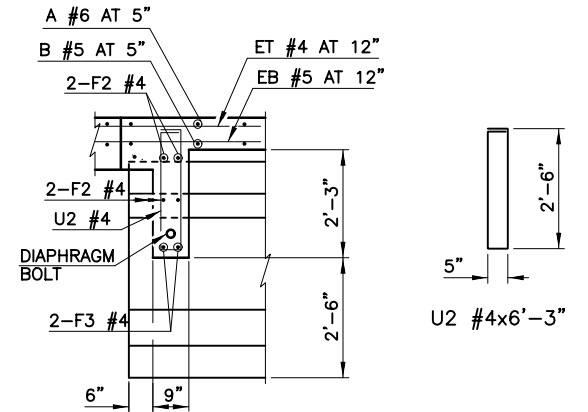
SECTION A-A



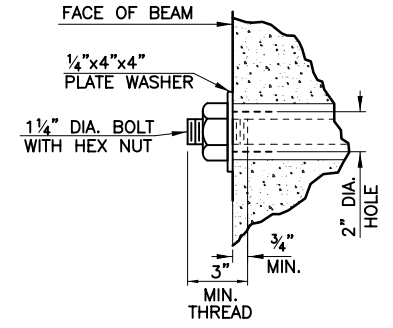
U1 #4x4'-9"



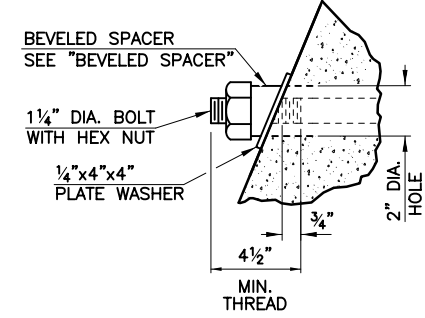
END DIAPHRAGM DETAIL



SECTION B-B



DETAIL "A"

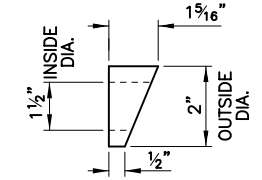


DETAIL "B"

DIAPHRAGM BOLT NOTES

PROVIDE STRUCTURAL STEEL FOR DIAPHRAGM BOLTS AND PLATE WASHERS IN ACCORDANCE WITH AASHTO M270 (ASTM A709), GRADE 50W (WEATHERING STEEL, CHARPY V-NOTCH TESTING NOT REQUIRED). THE CONTRACTOR MAY SUBSTITUTE A #10 REINFORCING BAR IN ACCORDANCE WITH AASHTO M31, GRADE 60, AND THREADED AT THE ENDS AS SHOWN FOR THE DIAPHRAGM BOLT AT NO ADDITIONAL COST TO THE DEPARTMENT. PROVIDE HEX NUTS IN ACCORDANCE WITH AASHTO M291 (ASTM A563).

PAINT EXPOSED DIAPHRAGM BOLT, PLATE WASHER, AND HEX NUT WITH TWO (2) COATS OF ZINC-RICH PAINT (6 MIL MINIMUM THICKNESS) AFTER ASSEMBLY. INCLUDE ALL COST OF DIAPHRAGM BOLT, PLATE WASHER, AND HEX NUT IN THE CONTRACT UNIT PRICE FOR STRUCTUAL STEEL.



BEVELED SPACER

2:41:36 PM 6/5/2018
 cpyb&w_ANSIB.tbi
 cpybdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Cut Sheets/8.30 Bridge/8.3.07 Bridge/21006BRAS03.dgn

DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "A"	
CHECKED: DMN	06/18	SUPERSTRUCTURE DETAILS	
APPRVD: DMN	06/18		
CP&Y		STATE JOB PIECE NO: 21006(07)	SHEET 4 OF 5 SHEET NO. B018

SUPERSTRUCTURE BAR LIST - PHASE 1					
EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	VARIANCE
A1	#6	461	BNT.	42'-8"	
A2	#6	114	BNT.	23'-8½" AVG	5'-1" TO 42'-4"
A3	#6	96	STR.	23'-5" AVG	7'-10" TO 39'-0"
A7	#5	30	BNT.	44'-7"	
AH1	#4	14	BNT.	3'-1"	
AH2	#4	64	BNT.	5'-1"	
AS	#4	78	BNT.	5'-0"	
B1	#5	461	STR.	41'-10"	
B2	#5	114	STR.	23'-0½" AVG	4'-5" TO 41'-8"
B3	#5	96	STR.	23'-5" AVG	7'-10" TO 39'-0"
EB1	#5	39	STR.	162'-2"	
EB2	#5	39	STR.	89'-2"	
ET1	#4	39	STR.	160'-2"	
ET2	#4	39	STR.	88'-2"	
EPH	#4	78	BNT.	3'-3"	
F1	#4	54	STR.	9'-8"	
F2	#4	72	STR.	9'-4"	
F3	#4	36	STR.	10'-5"	
FPH	#4	35	BNT.	4'-10"	
FS2	#5	195	BNT.	7'-4"	
L	#4	64	BNT.	1'-3"	
U1	#4	99	BNT.	4'-9"	
U2	#4	198	BNT.	6'-3"	

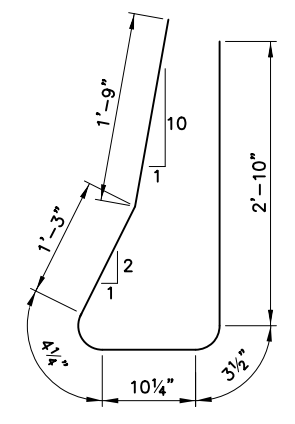
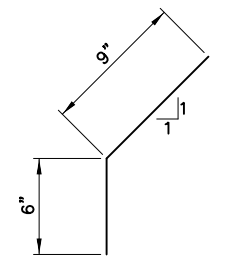
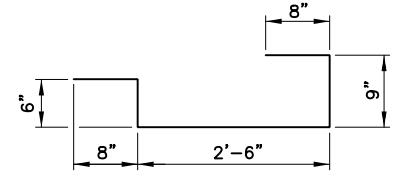
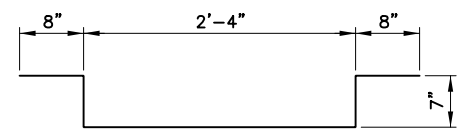
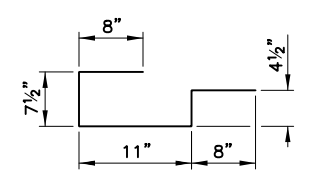
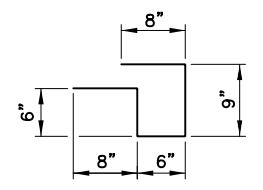
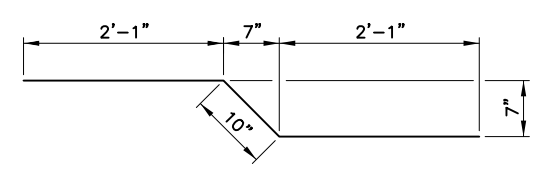
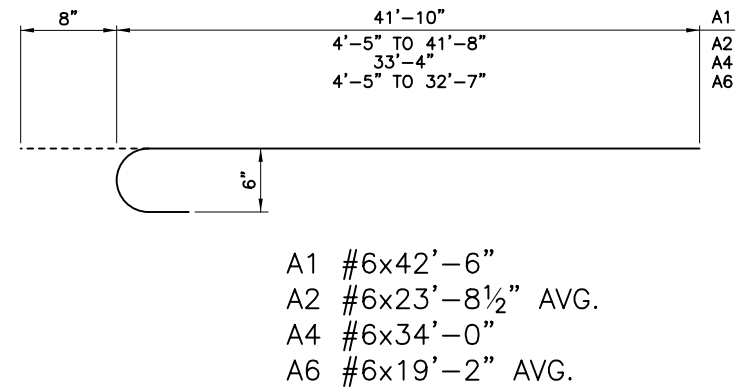
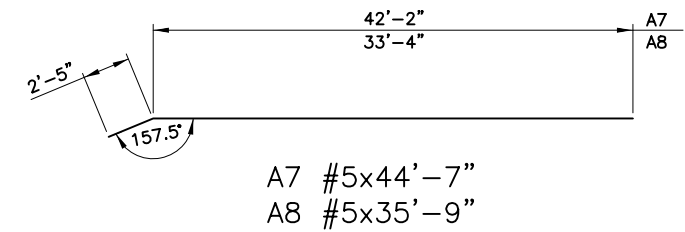
- ①
- ②
- ③
- ④
- ⑤
- ⑥

- ① 3 SETS OF 38 BARS
- ② 3 SETS OF 32 BARS
- ③ INCLUDES TWO 3'-0" LAPS
- ④ INCLUDES ONE 3'-0" LAP
- ⑤ INCLUDES TWO 2'-0" LAPS
- ⑥ INCLUDES ONE 2'-0" LAP
- ⑦ 3 SETS OF 25 BARS
- ⑧ 3 SETS OF 29 BARS

SUPERSTRUCTURE BAR LIST - PHASE 2					
EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	VARIANCE
A4	#6	488	BNT.	34'-0"	
A5	#6	75	STR.	18'-5" AVG	6'-4" TO 30'-6"
A6	#6	87	BNT.	19'-2" AVG	5'-1" TO 33'-3"
A8	#5	30	BNT.	35'-9"	
AH1	#4	12	BNT.	3'-1"	
AH2	#4	58	BNT.	5'-1"	
AS	#4	70	BNT.	5'-0"	
B1	#5	488	STR.	33'-4"	
B2	#5	75	STR.	18'-5" AVG	6'-4" TO 30'-6"
B3	#5	87	STR.	18'-6" AVG	4'-5" TO 32'-7"
EB1	#5	35	STR.	162'-2"	
EB2	#5	35	STR.	89'-2"	
ET1	#4	35	STR.	160'-2"	
ET2	#4	35	STR.	88'-2"	
EPH	#4	70	BNT.	3'-3"	
F1	#4	54	STR.	9'-8"	
F2	#4	72	STR.	9'-4"	
F3	#4	36	STR.	10'-5"	
FPH	#4	31	BNT.	4'-10"	
FS2	#5	195	BNT.	7'-4"	
L	#4	64	BNT.	1'-3"	
U1	#4	99	BNT.	4'-9"	
U2	#4	198	BNT.	6'-3"	

- ⑦
- ⑧
- ③
- ④
- ⑤
- ⑥

SUMMARY OF SUPERSTRUCTURE QUANTITIES				
ITEM	UNIT	PHASE I	PHASE II	TOTAL
PRESTRESSED CONCRETE BEAMS (TYPE IV)	LF	956.00	717.00	1,673.00
SAW-CUT GROOVING	SY	972.00	364.00	1,836.00
SEALED EXPANSION JOINT	LF	39.40	35.10	74.50
42" F-SHAPED PARAPET	LF	242.70	242.70	485.40
STRUCTURAL STEEL	LB	1,460.00	1,300.00	2,760.00
STAINLESS STEEL FIXED BEARING ASSEMBLY	EA	8.00	6.00	14.00
STAINLESS STEEL EXPANSION BEARING ASSEMBLY	EA	16.00	12.00	28.00
CLASS AA CONCRETE	CY	252.00	226.00	478.00
EPOXY COATED REINFORCING STEEL	LB	84,680.00	70,130.00	154,810.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	414.00	414.00	828.00
SEALER CRACK PREPARATION	LF	38.90	277.40	316.30
SEALER RESIN	GAL	0.50	3.10	3.60



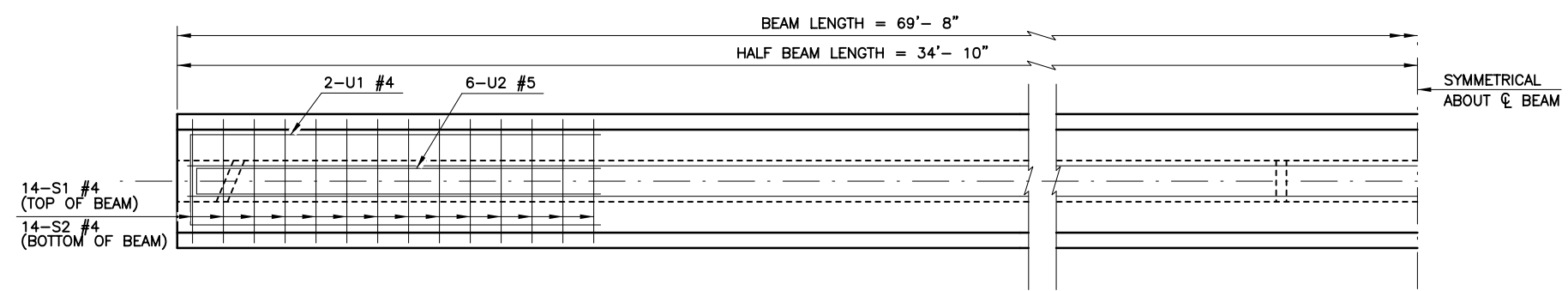
BAR BENDS

DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "A"	
CHECKED: DMN	06/18	SUPERSTRUCTURE DETAILS	
APPRVD: DMN	06/18		

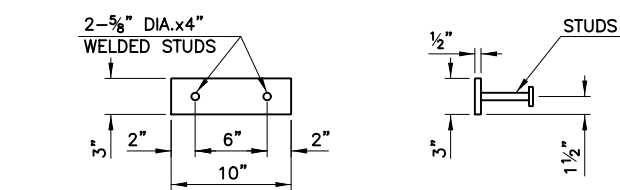
SHEET 5 OF 5
STATE JOB PIECE NO: 21006(07)
SHEET NO. B019

2:41:39 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRAS05.dgn

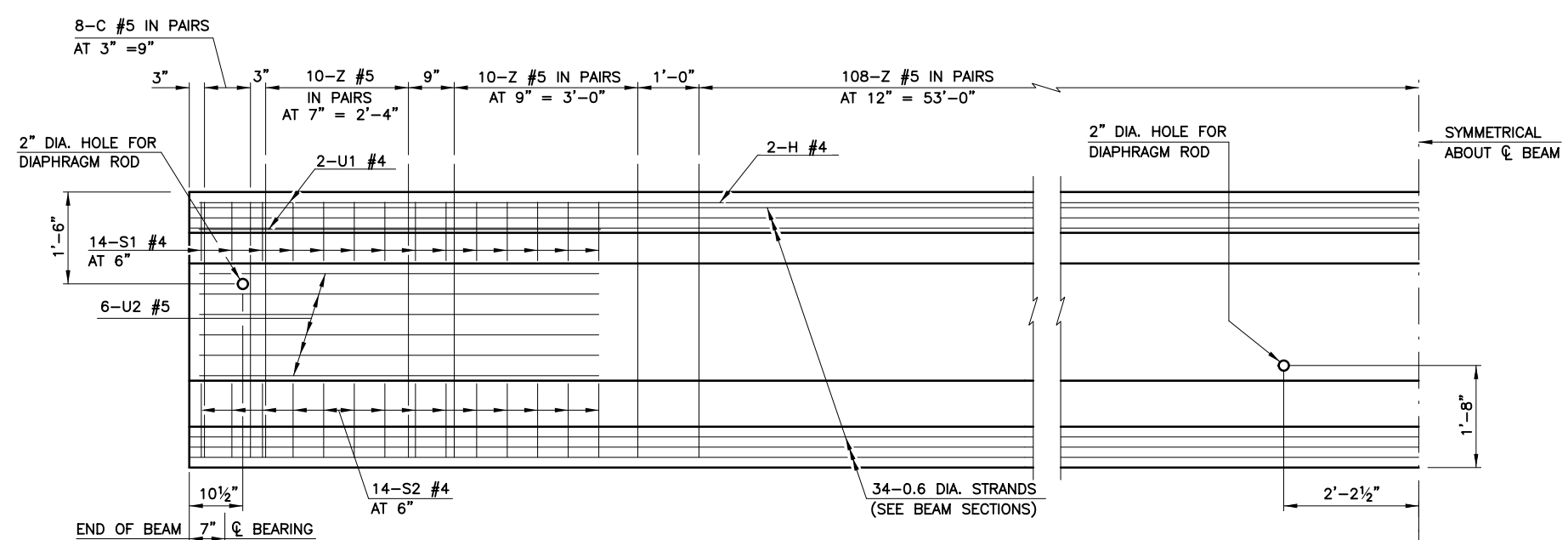
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B020	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



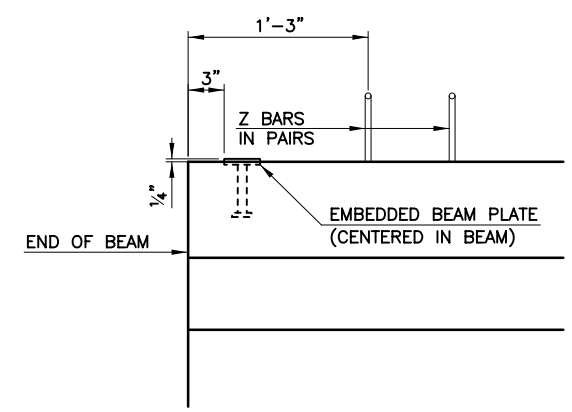
PLAN VIEW



TOP VIEW END VIEW



ELEVATION VIEW



EMBEDDED BEAM PLATE DETAILS

NOTE: PROVIDE AN EMBEDDED BEAM PLATE AT EXPANSION END ONLY.

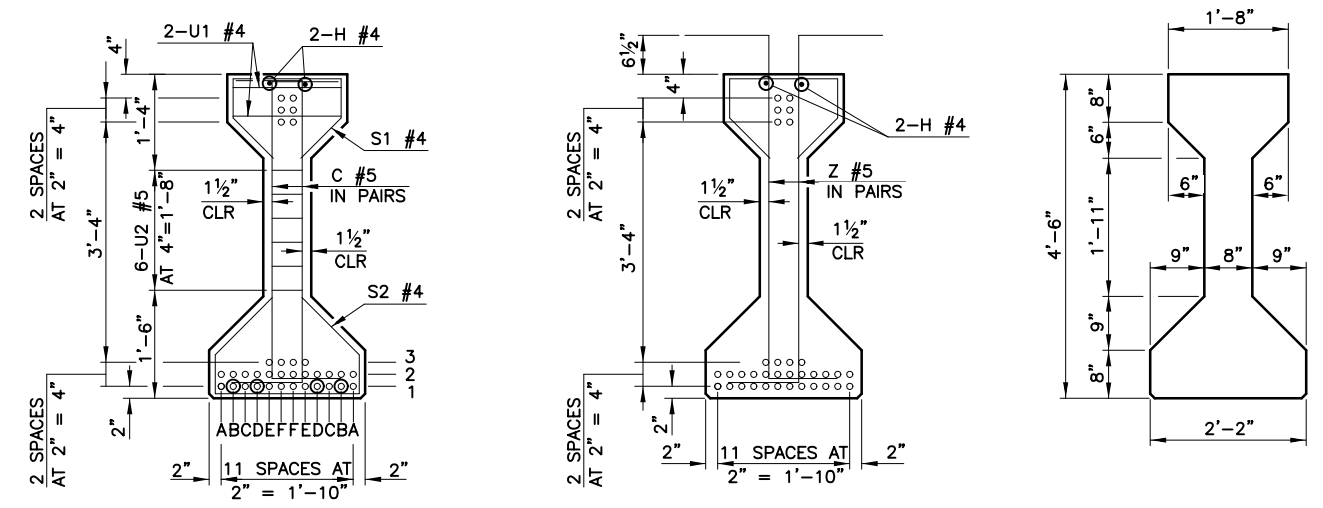
DEBOND SCHEDULE	
DEBOND PAIR	DEBOND LENGTH FROM END OF BEAM
B1	4'-0"
D1	4'-0"

PRESTRESSED CONCRETE BEAM NOTES

COMPRESSIVE STRENGTH
 PROVIDE CONCRETE WITH A COMPRESSIVE STRENGTH OF 6,000 P.S.I. AT TRANSFER OF PRESTRESS AND 8,000 P.S.I. AT 28 DAYS.

STRAND TYPE
 PROVIDE LOW-RELAXATION STRANDS HAVING A NOMINAL DIAMETER OF 0.6" WITH ULTIMATE TENSILE STRENGTH OF 270 K.S.I.

LRFR OPERATING RATING FACTOR = 3.17
 THE OPERATING RATING SHOWN IS BASED ON A NOMINAL STRENGTH USING ONLY STRANDS THAT ARE BONDED FOR THE FULL LENGTH OF THE BEAM. ALL PARTIALLY BONDED STRANDS ARE NEGLECTED IN STRENGTH COMPUTATIONS.

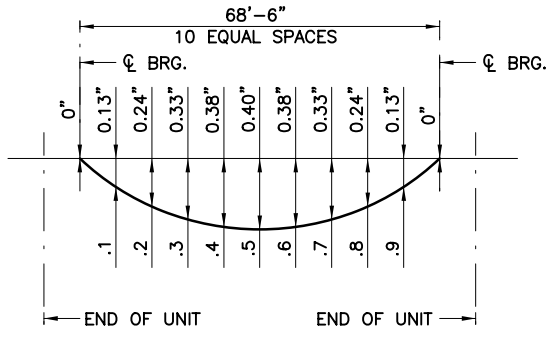


END SECTION

BEAM SECTIONS
(34-0.6" DIA. STRANDS)

CL SECTION

END VIEW



DEAD LOAD DEFLECTION DIAGRAM

NOTE:
 THE DEAD LOAD DEFLECTIONS SHOWN ABOVE AT THE TENTH POINTS ARE THE INITIAL DEFLECTIONS DUE TO DECK SLAB + HAUNCH + S.I.P. STEEL DECK FORM ALLOWANCE + CONCRETE TRAFFIC RAIL. IT DOES NOT INCLUDE THE BEAM WEIGHT OR FUTURE WEARING SURFACE.

DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "A"	
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		

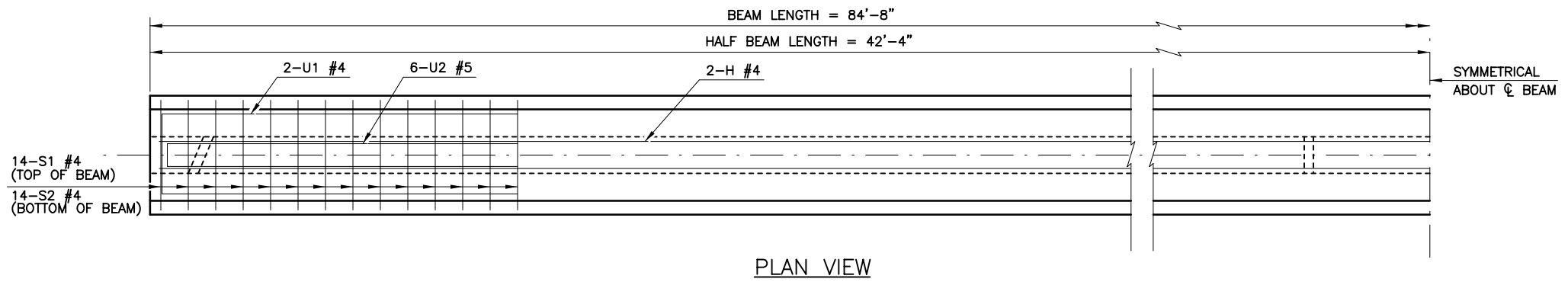
P.C. BEAM DETAILS
(70' SPAN)

SHEET 1 OF 3
SHEET NO. B020

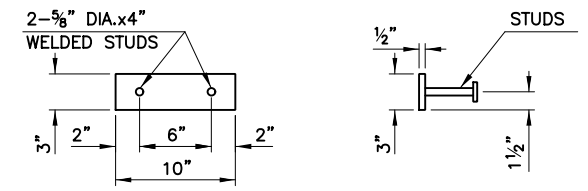
STATE JOB PIECE NO: 21006(07)

2:41:44 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRABD01.dgn
 bmcanelly

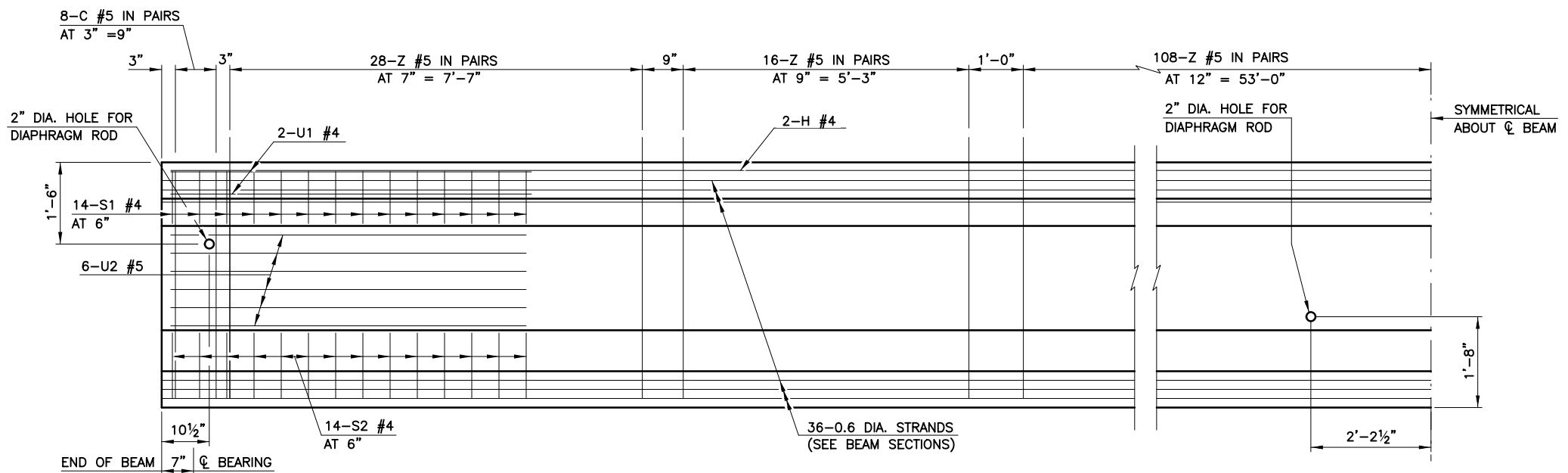
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B021	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



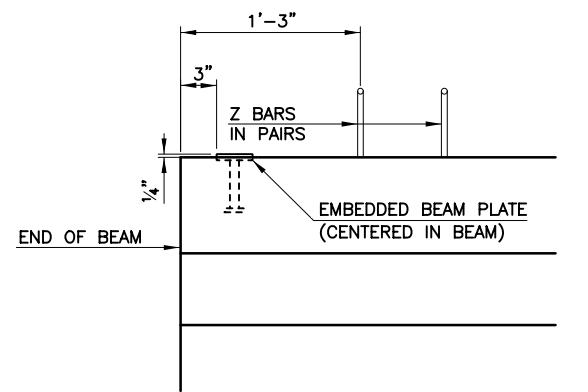
PLAN VIEW



TOP VIEW END VIEW



ELEVATION VIEW



ELEVATION

EMBEDDED BEAM PLATE DETAILS

NOTE: PROVIDE AN EMBEDDED BEAM PLATE AT EXPANSION END ONLY.

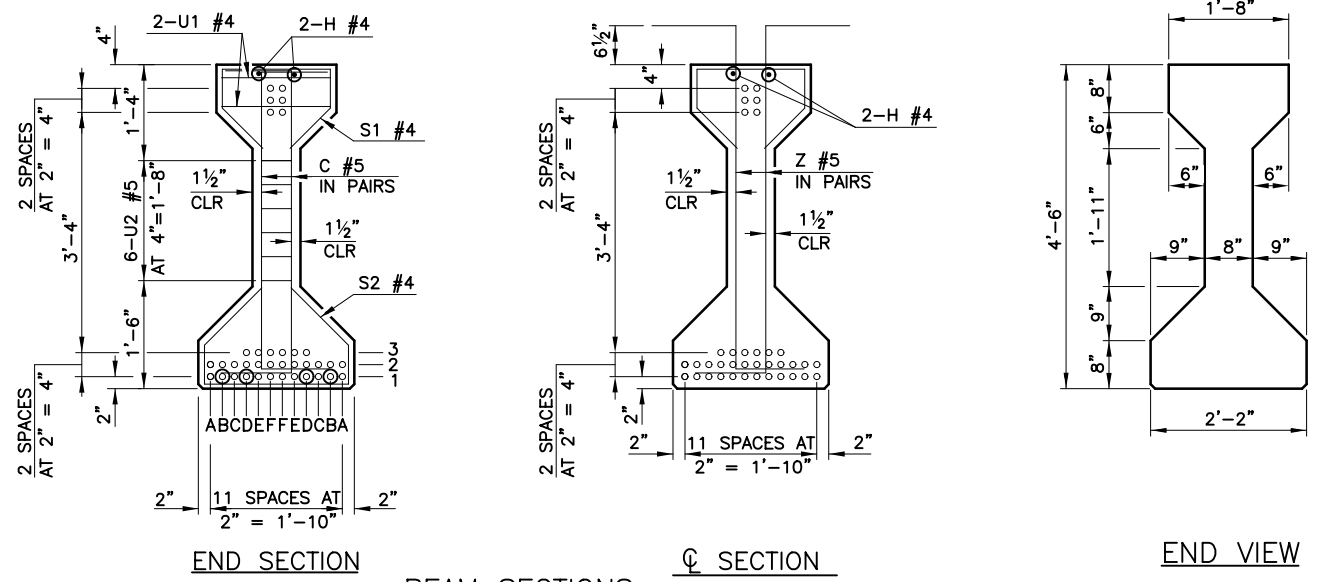
DEBOND SCHEDULE	
DEBOND PAIR	DEBOND LENGTH FROM END OF BEAM
B1	4'-0"
D1	4'-0"

PRESTRESSED CONCRETE BEAM NOTES

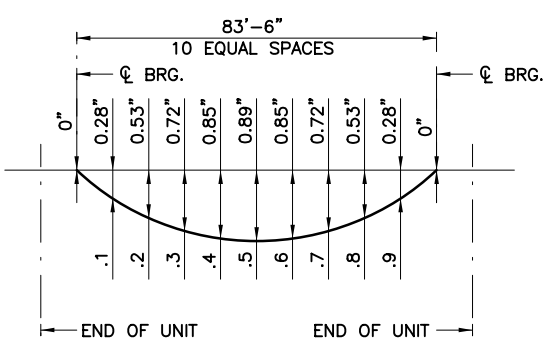
COMPRESSIVE STRENGTH
 PROVIDE CONCRETE WITH A COMPRESSIVE STRENGTH OF 6,000 P.S.I. AT TRANSFER OF PRESTRESS AND 8,000 P.S.I. AT 28 DAYS.

STRAND TYPE
 PROVIDE LOW-RELAXATION STRANDS HAVING A NOMINAL DIAMETER OF 0.6" WITH ULTIMATE TENSILE STRENGTH OF 270 K.S.I.

LRFR OPERATING RATING FACTOR = 2.40
 THE OPERATING RATING SHOWN IS BASED ON A NOMINAL STRENGTH USING ONLY STRANDS THAT ARE BONDED FOR THE FULL LENGTH OF THE BEAM. ALL PARTIALLY BONDED STRANDS ARE NEGLECTED IN STRENGTH COMPUTATIONS.



BEAM SECTIONS
(36-0.6" DIA. STRANDS)



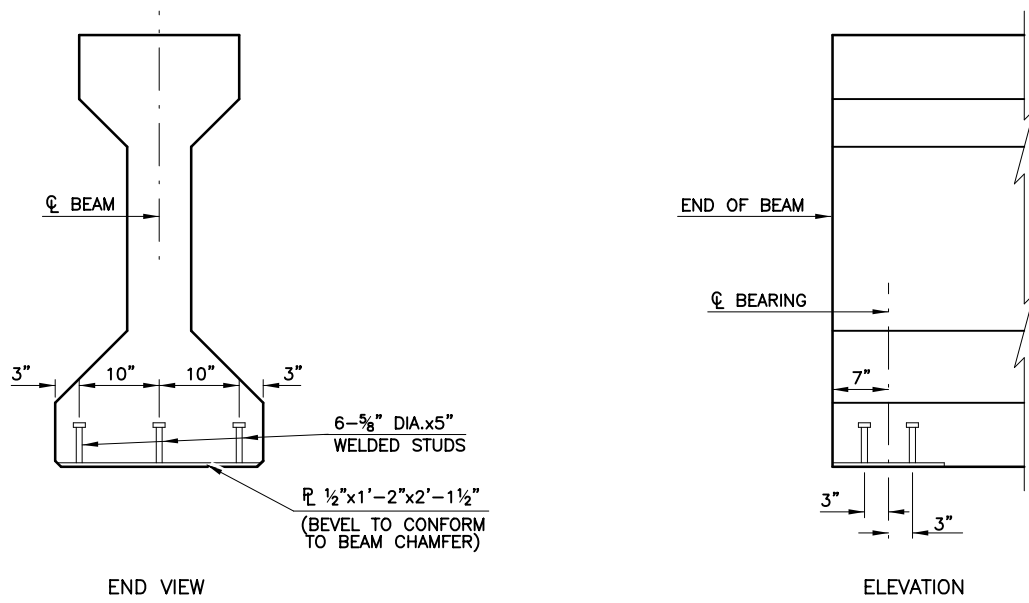
DEAD LOAD DEFLECTION DIAGRAM

NOTE: THE DEAD LOAD DEFLECTIONS SHOWN ABOVE AT THE TENTH POINTS ARE THE INITIAL DEFLECTIONS DUE TO DECK SLAB + HAUNCH + S.I.P. STEEL DECK FORM ALLOWANCE + CONCRETE TRAFFIC RAIL. IT DOES NOT INCLUDE THE BEAM WEIGHT OR FUTURE WEARING SURFACE.

DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "A"	
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
CP&Y		P.C. BEAM DETAILS (85' SPAN)	
		STATE JOB PIECE NO: 21006(07)	SHEET 2 OF 3 SHEET NO. B021

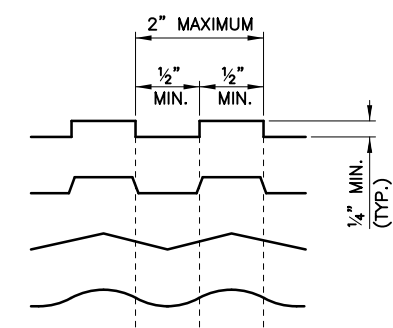
2:41:47 PM
 6/5/2018
 cpyb&w_ANSIB.tbl
 cpydf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRABD02.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B022	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



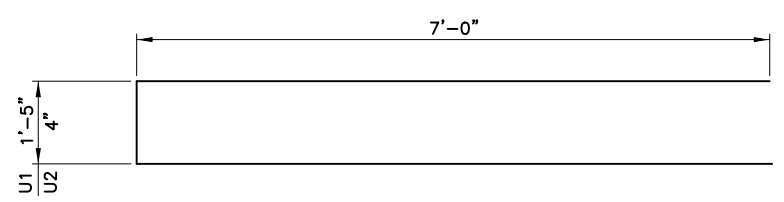
EMBEDDED SOLE PLATE DETAILS

NOTE:
PROVIDE AN EMBEDDED SOLE PLATE AT EACH END OF THE BEAM.

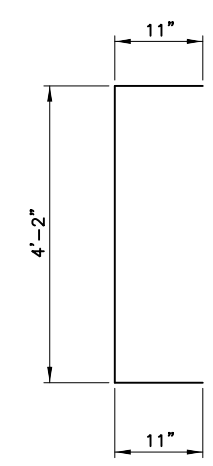


INTENTIONALLY ROUGHENED SURFACE DETAILS

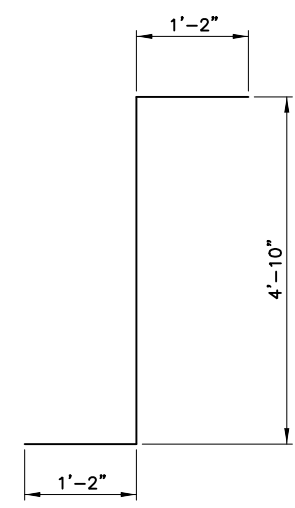
INTENTIONALLY ROUGHEN THE ENTIRE TOP SURFACE OF P.C. BEAM TO A MINIMUM HEIGHT OF 1/4" OVER A MAXIMUM PITCH OF 2" MEASURED LONGITUDINALLY ALONG THE LENGTH OF THE BEAM. PROVIDE A CREST AND TROUGH ASSOCIATED WITH THE HEIGHT OF NOT LESS THAN 1/2". PRODUCE THE ROUGHENED SURFACE BY USING A SPECIAL TROWEL TO FORM ONE OF THE SURFACES SHOWN IN THE DETAILS, BY CLEANING THE CONCRETE SURFACE WITH A STIFF WIRE BRUSH (OR BLASTING) TO EXPOSE THE AGGREGATE TO A HEIGHT OF 1/4", OR BY USING ANOTHER APPROVED METHOD. SUBMIT THE METHOD TO BE USED FOR APPROVAL BY THE ENGINEER. REPAIR ANY DAMAGE TO REINFORCEMENT'S EPOXY COATING BEFORE PLACEMENT OF DECK CONCRETE.



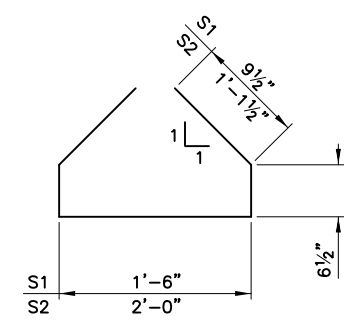
U1 #4x15'-5"
U2 #5x14'-4"



C #5x6'-0"



Z #5x7'-2"
(EPOXY COATED)

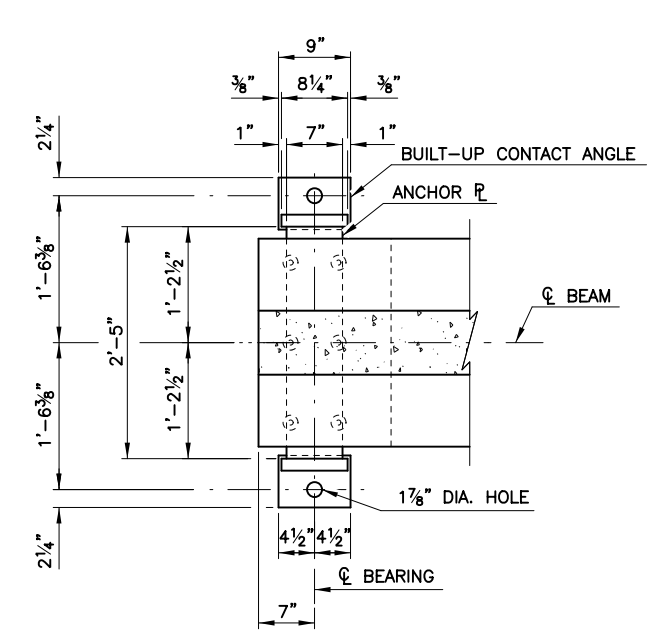


S1 #4x4'-2"
S2 #4x5'-4"

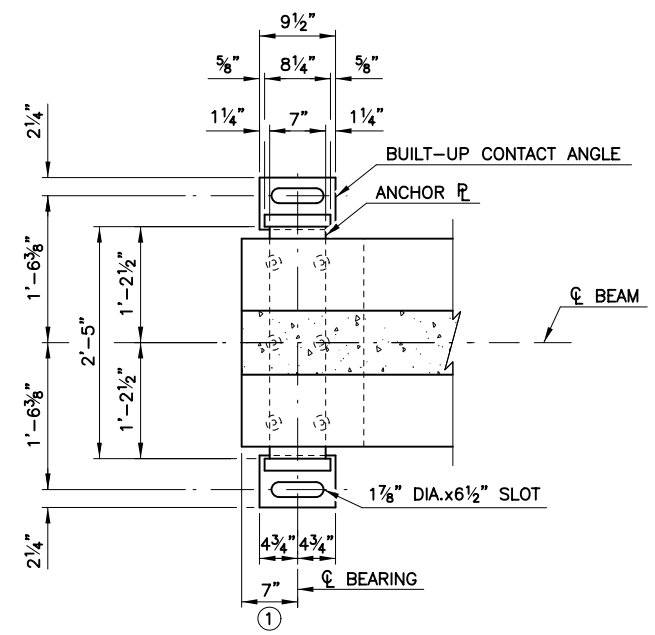
2:41:52 PM
 6/5/2018
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRABD03.dgn

DESIGN: DKC	06/18	US 270 BRIDGE "A"	SEMINOLE COUNTY
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
			P.C. BEAM DETAILS (70' AND 85' SPANS)
STATE JOB PIECE NO: 21006(07)			SHEET 3 OF 3 SHEET NO. B022

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B023	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



PLAN



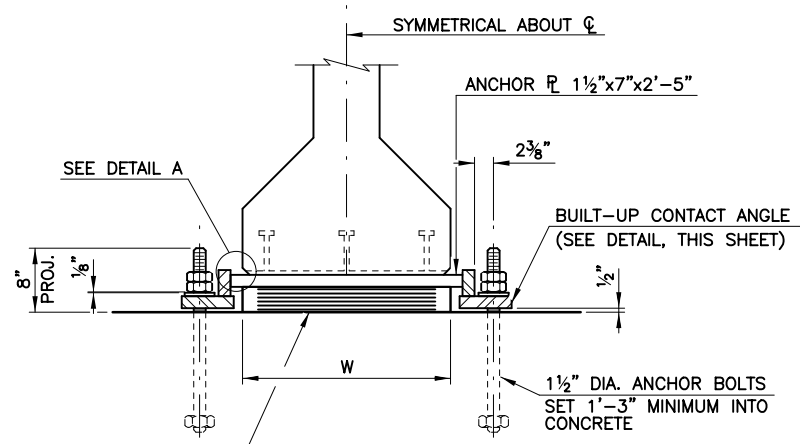
PLAN

BEARING SCHEDULE				
SPAN	60 DUROMETER ELASTOMETER BEARING PAD			
	SIZE (T x L x W)	COVER LAYER	INNER LAYER	LAMINATE PLATE
70'	3 1/8" x 6" x 2'-2"	2 - 1/4"	5 - 3/8"	6 - 1/8"
85'	3 1/8" x 6 1/2" x 2'-2"	2 - 1/4"	5 - 3/8"	6 - 1/8"

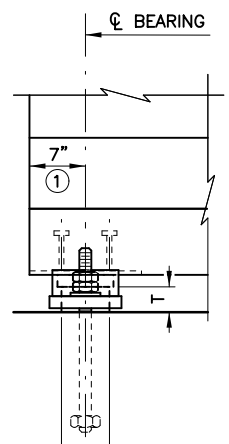
EXPANSION BEARING ASSEMBLY NOTES:
 PROVIDE STRUCTURAL STEEL FOR ANCHOR PLATES AND BUILT-UP CONTACT ANGLES IN ACCORDANCE WITH ASTM A240 (AUSTENITIC STAINLESS STEEL, TYPE 316, CHARPY V-NOTCH TESTING NOT REQUIRED). FOR ANCHOR BOLTS, PROVIDE CONTINUOUSLY THREADED BARS IN ACCORDANCE WITH ASTM A320, CLASS 2, GRADE B8M (AUSTENITIC STAINLESS STEEL, TYPE 316, CHARPY V-NOTCH TESTING NOT REQUIRED). USE AUSTENITIC STAINLESS STEEL NUTS AND WASHERS CONFORMING TO ASTM A194, GRADE 8M AND ASTM A320, RESPECTIVELY. PERFORM ALL WELDING CONSISTENT WITH PROCEDURES FOR STAINLESS STEEL.

FIXED BEARING ASSEMBLY NOTES:
 PROVIDE STRUCTURAL STEEL FOR ANCHOR PLATES IN ACCORDANCE WITH AASHTO M270 (ASTM A709), GRADE 50W (WEATHERING STEEL, CHARPY V-NOTCH TESTING NOT REQUIRED). FOR ANCHOR BARS PROVIDE A REINFORCING BAR IN ACCORDANCE WITH AASHTO M31, GRADE 60.

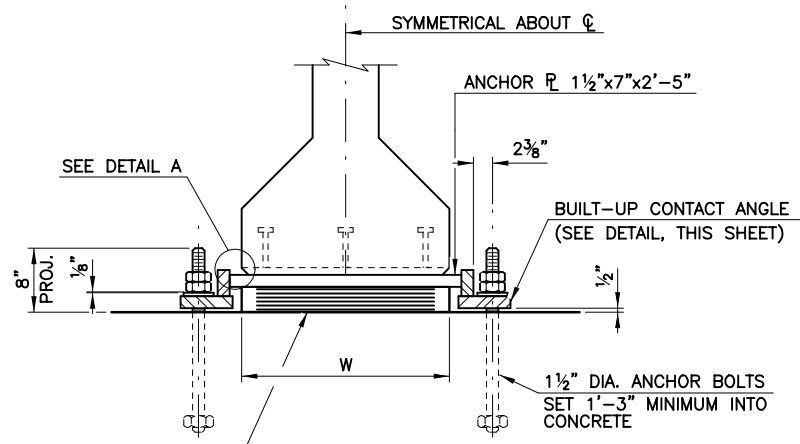
① CENTER ANCHOR BOLTS IN SLOTS DURING SETTING OF BEAMS. DIMENSION MAY VARY DEPENDING ON TEMPERATURE AT THE TIME OF BEAM SETTING.



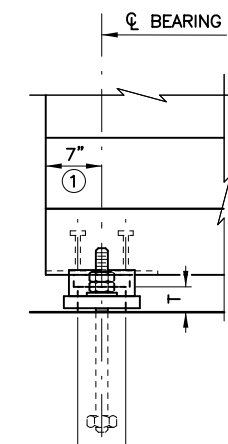
END VIEW



SIDE VIEW



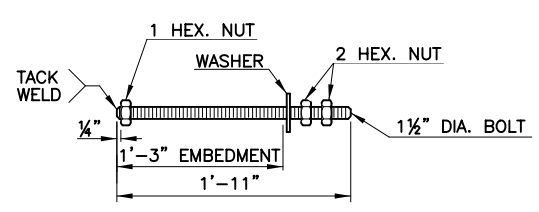
END VIEW



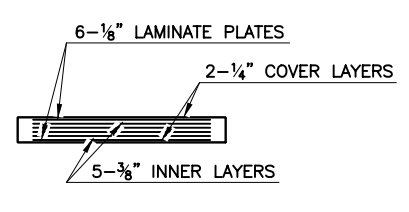
SIDE VIEW

FIXED BEARING DETAILS

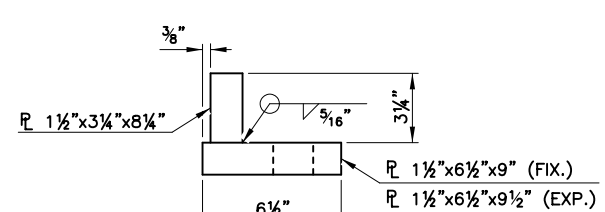
EXPANSION BEARING DETAILS



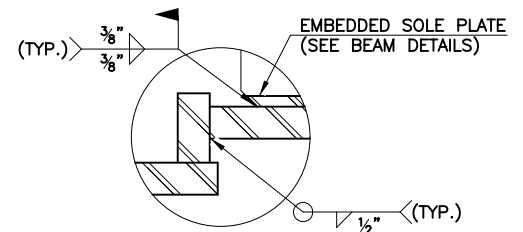
PIER ANCHOR BOLT DETAIL



BEARING PAD DETAIL



BUILT-UP CONTACT ANGLE DETAIL

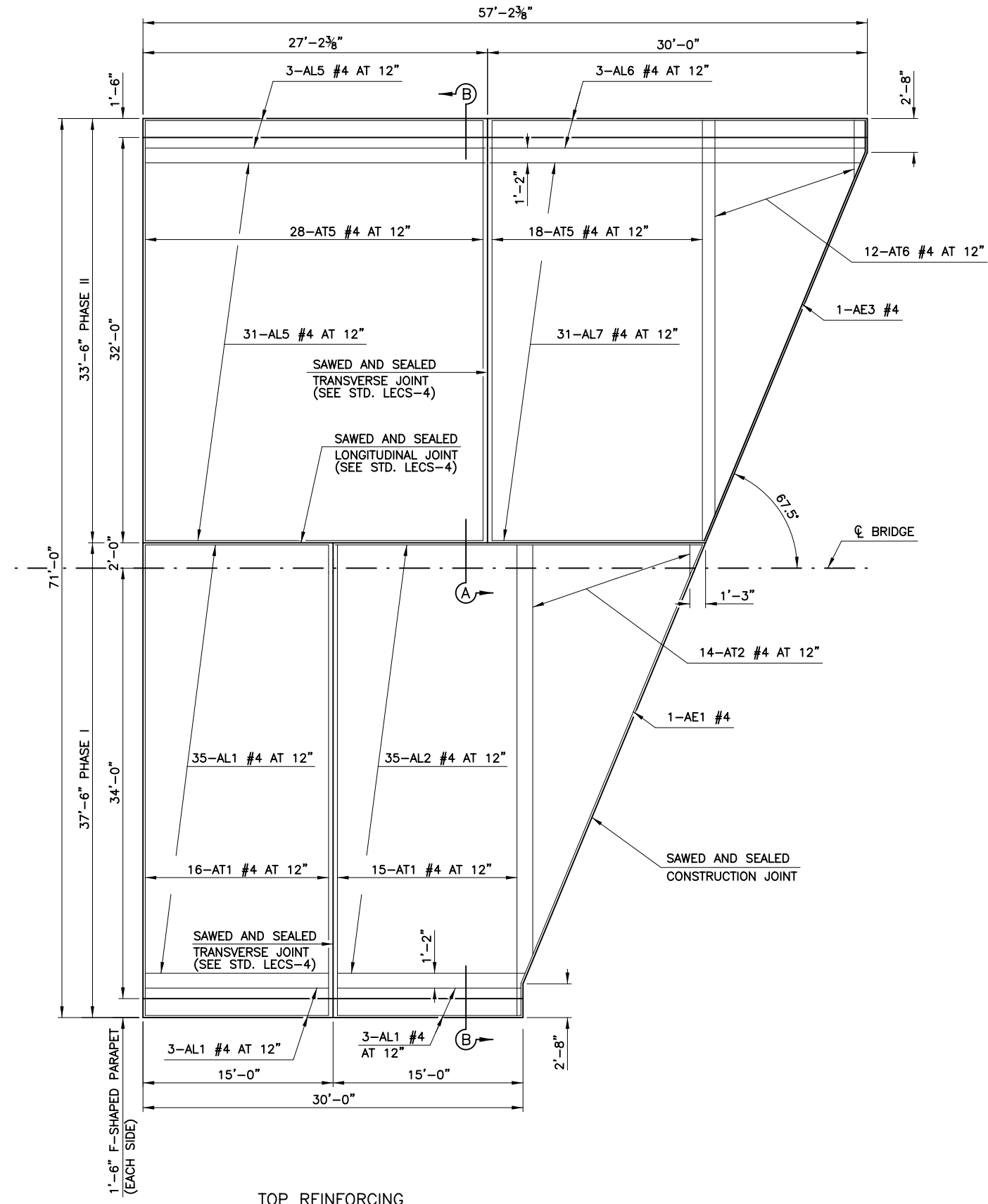


DETAIL A

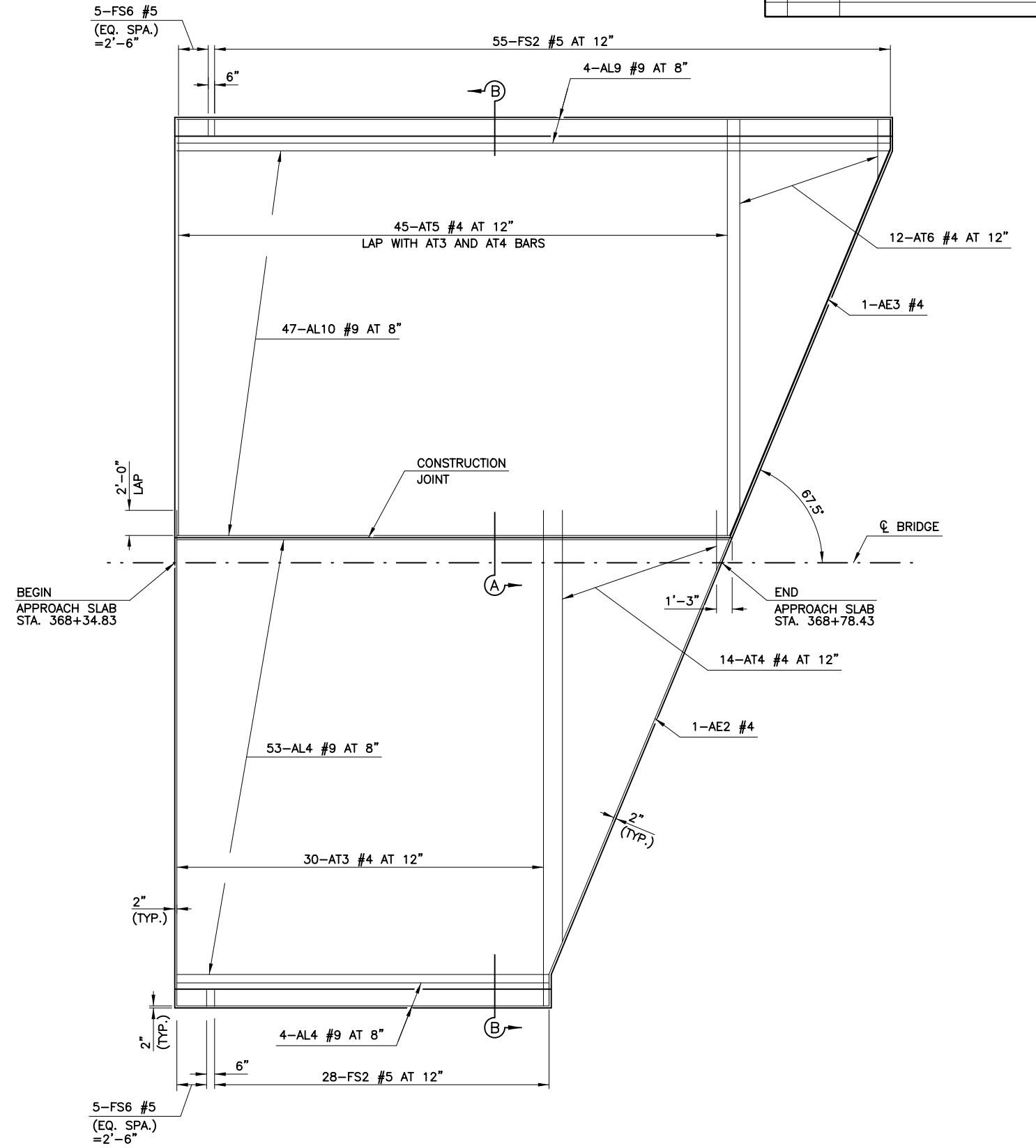
DESIGN: DKC	06/18	US 270 BRIDGE "A"	SEMINOLE COUNTY
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
CP&Y		BEARING DETAILS	
		STATE JOB PIECE NO: 21006(07)	SHEET 1 OF 1 SHEET NO. B023

2:41:57 PM
 6/5/2018
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAS04.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B024	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



TOP REINFORCING MAT DETAIL

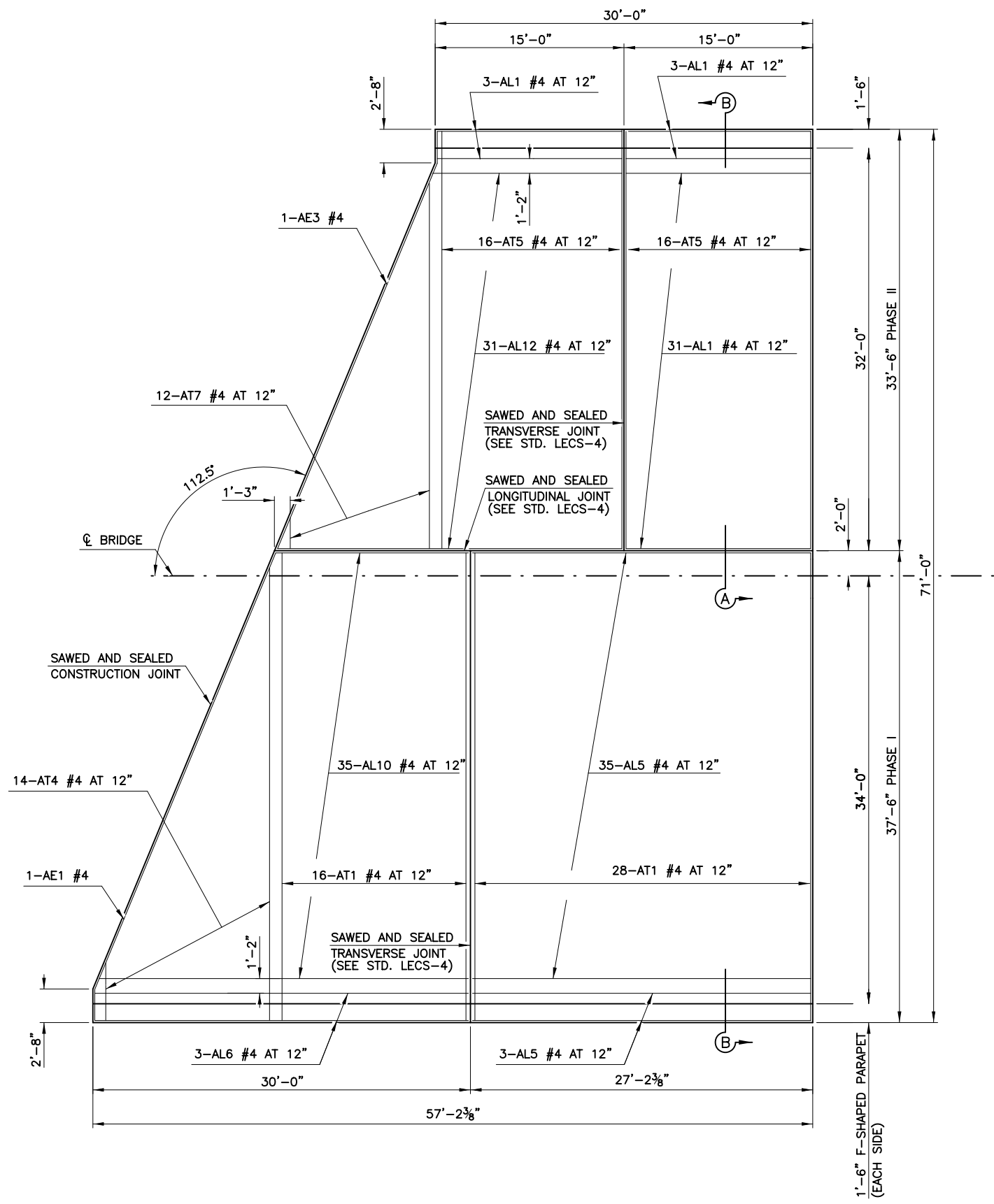


BOTTOM REINFORCING MAT DETAIL

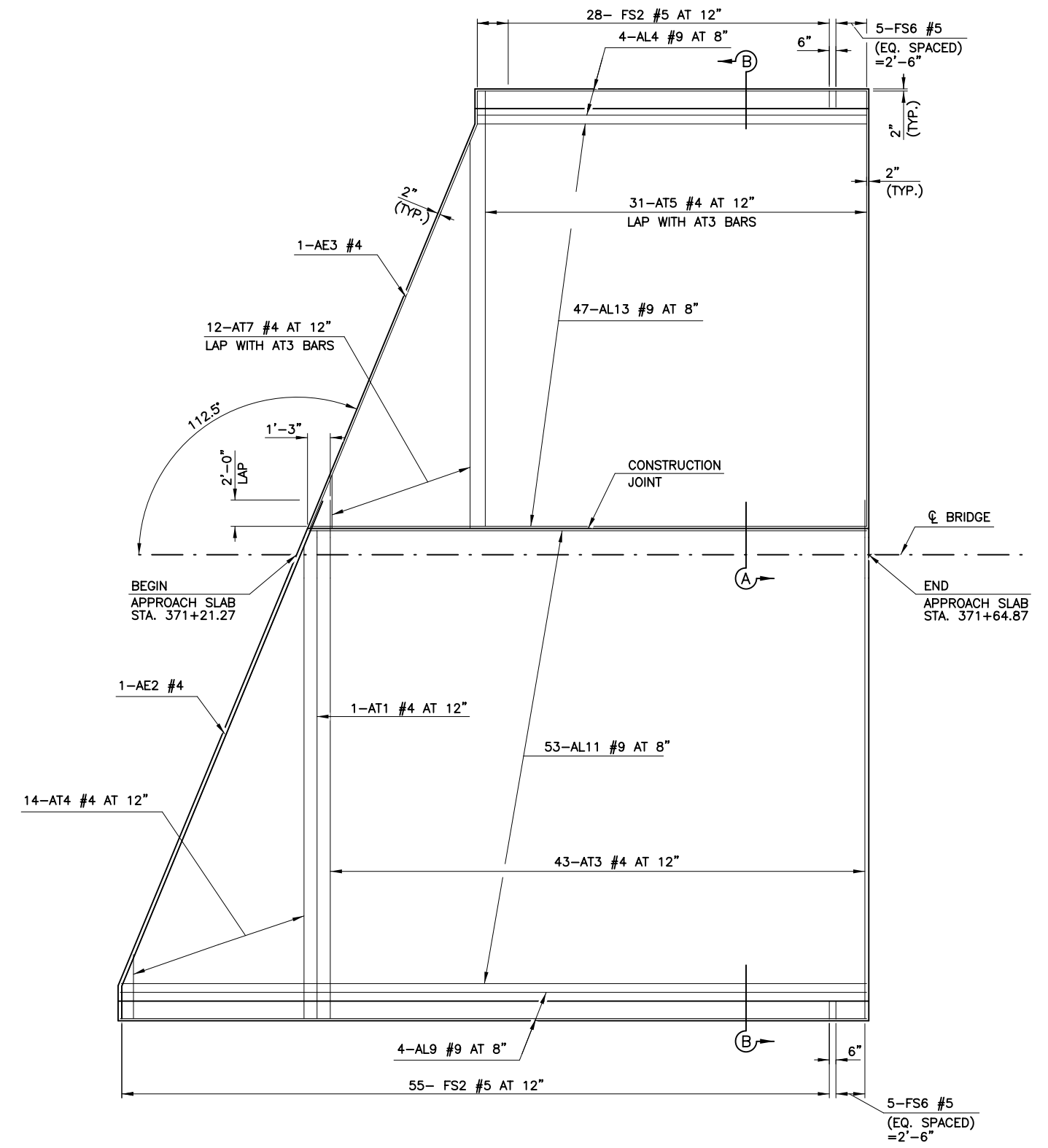
DESIGN: DKC 06/18	US 270 BRIDGE "A"	SEMINOLE COUNTY
DRAWN: LRJ 06/18	APPROACH SLAB NO. 1 DETAILS	
CHECKED: DMN 06/18	SHEET 1 OF 1	
APPRVD: DMN 06/18	SHEET NO. B024	
STATE JOB PIECE NO: 21006(07)		

6/5/2018 2:42:01 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRATD01.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B025	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



TOP REINFORCING MAT DETAIL



BOTTOM REINFORCING MAT DETAIL

DESIGN: DKC	06/18	US 270 BRIDGE "A"	SEMINOLE COUNTY
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
		APPROACH SLAB NO. 2 DETAILS	
		STATE JOB PIECE NO: 21006(07)	
		SHEET 1 OF 1	
		SHEET NO. B025	

6/5/2018 2:42:06 PM
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRATD02.dgn

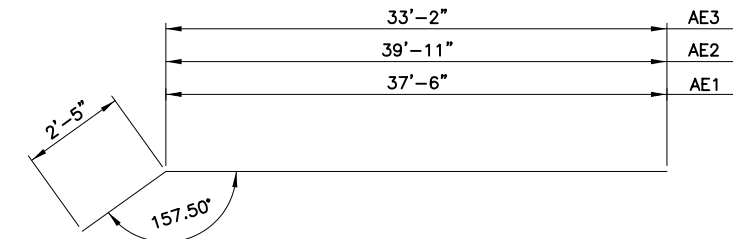
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B026	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

APPROACH SLAB NO. 1 BAR LIST (PHASE I)

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING					
AE1	#4	1	BNT.	39'-11"	
AE2	#4	1	BNT.	42'-4"	
AL1	#4	41	STR.	14'-10"	
AL2	#4	35	STR.	22'-1½" AVG	15'-1" TO 29'-2"
AL3	#9	53	STR.	37'-0" AVG	29'-10" TO 44'-2"
AL4	#9	4	STR.	29'-10"	
AT1	#4	31	STR.	37'-2"	
AT2	#4	14	STR.	18'-1" AVG	2'-5" TO 33'-9"
AT3	#4	30	STR.	39'-6"	
AT4	#4	14	STR.	20'-6" AVG	4'-10" TO 36'-2"
FS2	#5	28	BNT.	7'-4"	
FS6	#5	5	BNT.	7'-6½"	

APPROACH SLAB NO. 2 BAR LIST (PHASE I)

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING					
AE1	#4	1	BNT.	39'-11"	
AE2	#4	1	BNT.	42'-4"	
AL5	#4	38	STR.	27'-0"	
AL6	#4	3	STR.	29'-10"	
AL9	#4	4	STR.	57'-0"	
AL10	#4	35	STR.	22'-5" AVG	15'-5" TO 29'-5"
AL11	#9	53	STR.	49'-9" AVG	42'-7" TO 56'-11"
AT1	#4	45	STR.	37'-2"	
AT3	#4	43	STR.	39'-6"	
AT4	#4	28	STR.	20'-6" AVG	4'-10" TO 36'-2"
FS2	#5	55	BNT.	7'-4"	
FS6	#5	5	BNT.	7'-6½"	



AE1 #4x39'-11"
 AE2 #4x42'-4"
 AE3 #4x35'-7"

APPROACH SLAB NO. 1 BAR LIST (PHASE II)

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING					
AE3	#4	2	BNT.	35'-7"	
AL5	#4	34	STR.	27'-0"	
AL6	#4	3	STR.	29'-10"	
AL7	#4	31	STR.	23'-2½" AVG	17'-0" TO 29'-5"
AL8	#9	47	STR.	50'-8"	
AL9	#9	4	STR.	57'-0"	
AT5	#4	91	STR.	33'-2"	
AT6	#4	24	STR.	18'-1" AVG	4'-10" TO 31'-5"
FS2	#5	55	BNT.	7'-4"	
FS6	#5	5	BNT.	7'-6½"	

APPROACH SLAB NO. 2 BAR LIST (PHASE II)

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING					
AE3	#4	2	BNT.	35'-7"	
AL1	#4	37	STR.	14'-10"	
AL4	#9	4	STR.	29'-10"	
AL12	#4	31	STR.	21'-3½" AVG	15'-1" TO 27'-6"
AL13	#9	47	STR.	36'-1½" AVG	29'-10" TO 42'-5"
AT5	#4	63	STR.	33'-2"	
AT7	#4	24	STR.	15'-8" AVG	2'-5" TO 28'-11"
FS2	#5	28	BNT.	7'-4"	
FS6	#5	5	BNT.	7'-6½"	

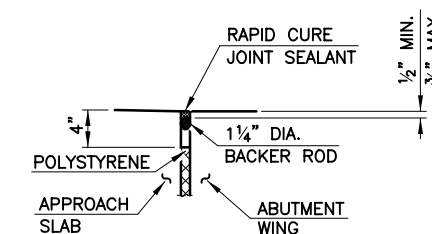
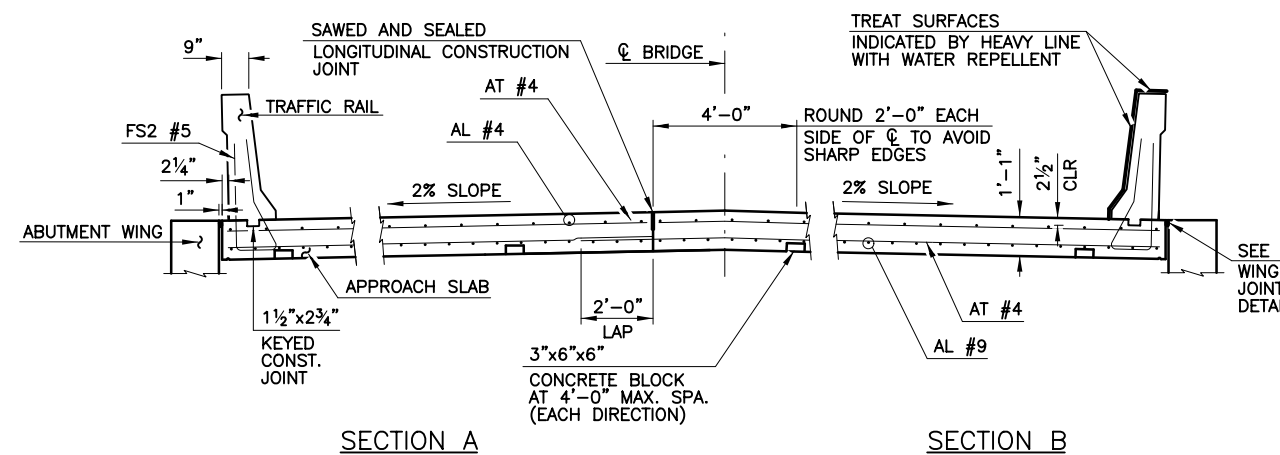
APPROACH SLAB NO. 1 QUANTITIES

ITEM	UNIT	PHASE I	PHASE II	TOTAL
APPROACH SLAB	SY	153.00	191.10	344.10
SAW-CUT GROOVING	SY	148.00	181.50	329.50
42" F-SHAPED PARAPET	LF	30.00	57.20	87.20
WATER REPELLENT (VISUALLY INSPECTED)	SY	15.00	28.00	43.00

APPROACH SLAB NO. 2 QUANTITIES

ITEM	UNIT	PHASE I	PHASE II	TOTAL
APPROACH SLAB	SY	210.50	133.60	344.10
SAW-CUT GROOVING	SY	200.90	128.60	329.50
42" F-SHAPED PARAPET	LF	57.20	30.00	87.20
WATER REPELLENT (VISUALLY INSPECTED)	SY	28.00	15.00	43.00

- ① 2 SETS OF 14 BARS
- ② 2 SETS OF 12 BARS



WING JOINT DETAIL

DESIGN:	DKC	06/18
DRAWN:	LRJ	06/18
CHECKED:	DMN	06/18
APPRVD:	DMN	06/18

US 270
BRIDGE "A"

SEMINOLE COUNTY

APPROACH SLAB DETAILS

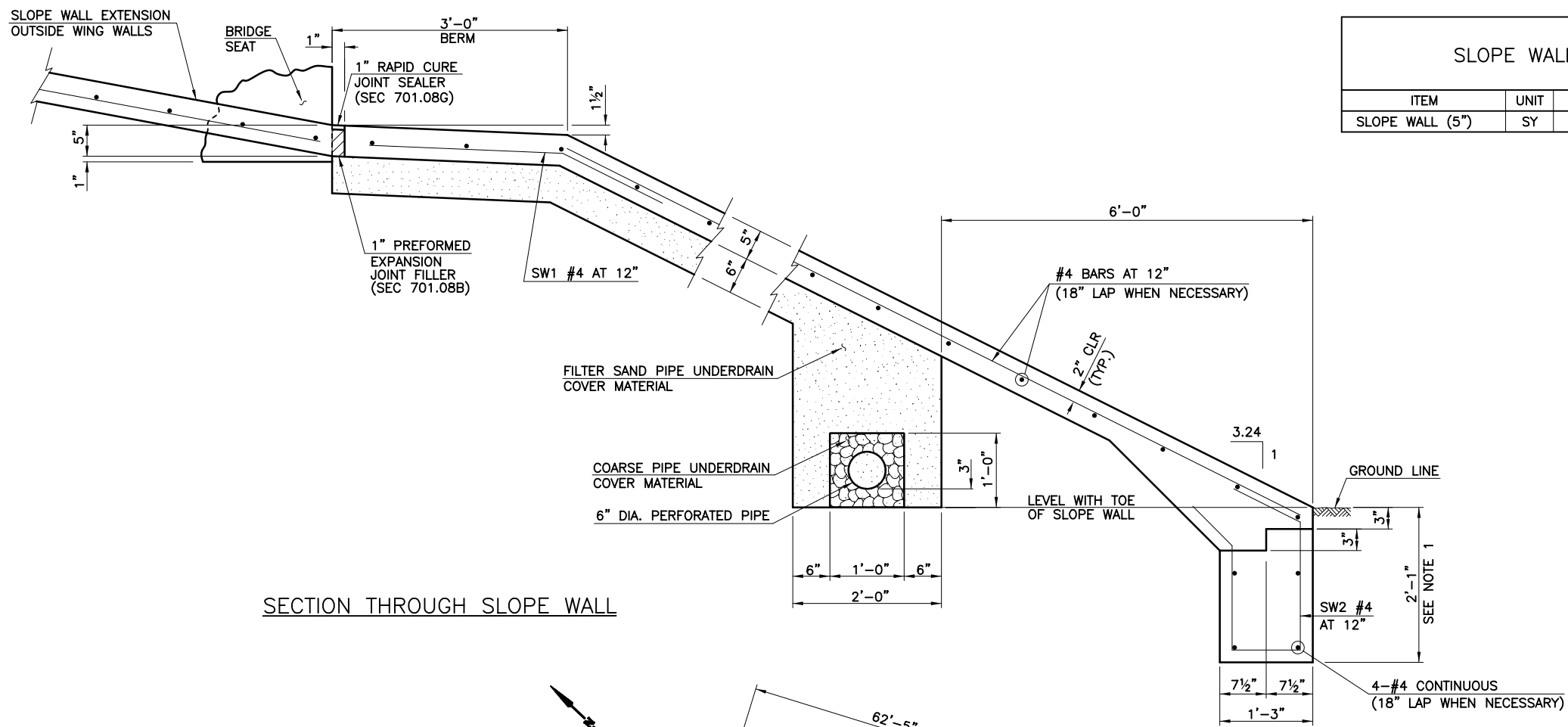


STATE JOB PIECE NO: 21006(07)

SHEET 1 OF 1
SHEET NO. B026

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B027	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

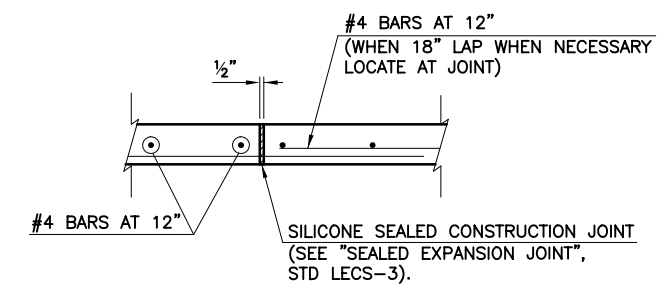
SLOPE WALL QUANTITIES				
ITEM	UNIT	ABUT 1	ABUT 2	TOTAL
SLOPE WALL (5")	SY	732.00	736.00	1,468.00



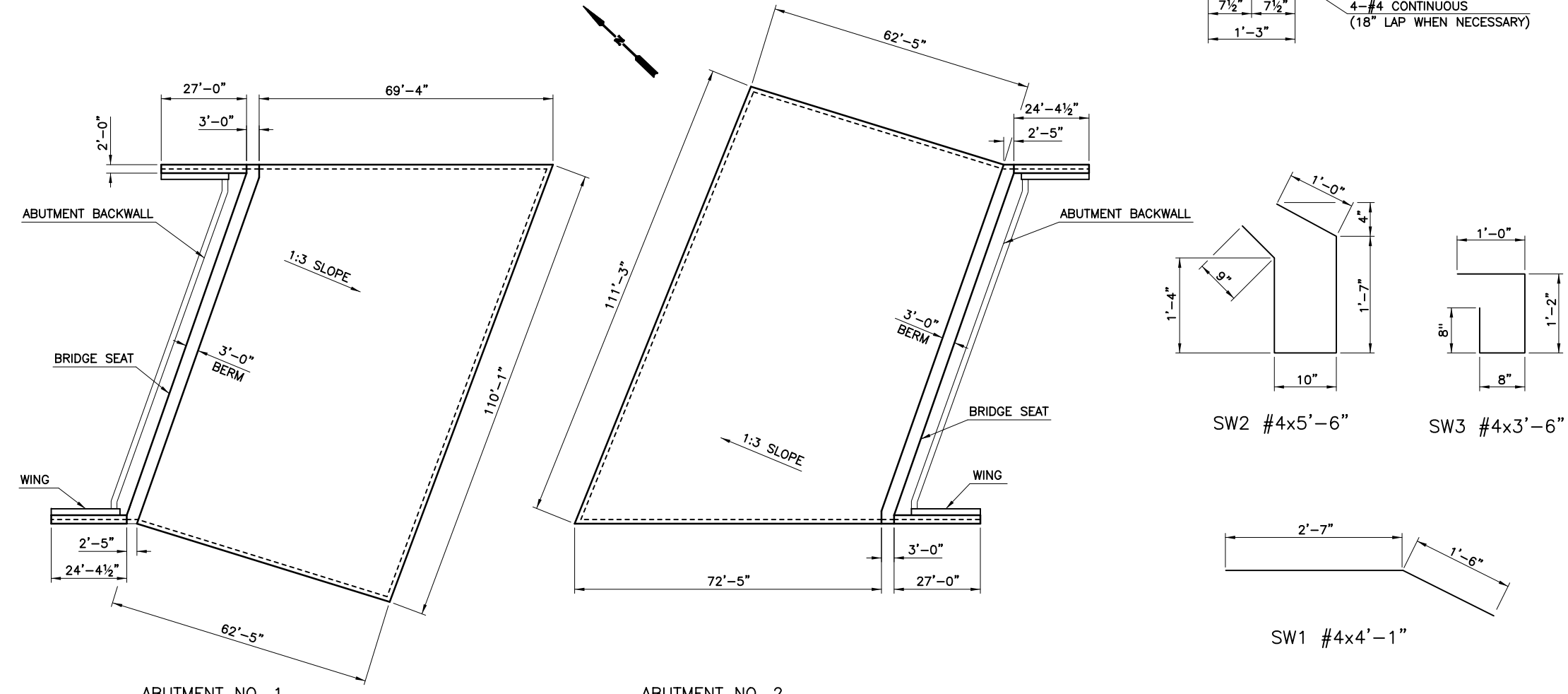
SECTION THROUGH SLOPE WALL

GENERAL NOTES

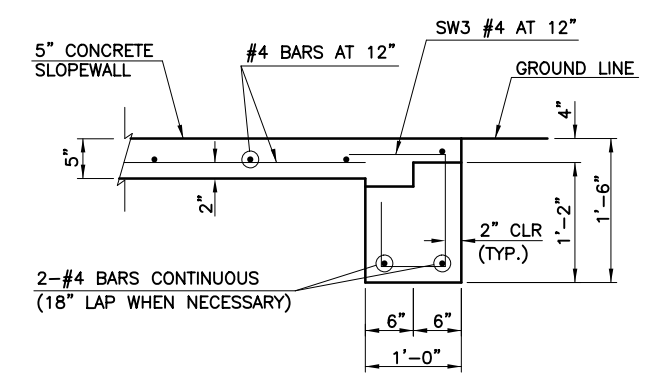
1. THE SURFACE AREA OF THE TOE OF THE SLOPE WALL IS INCLUDED IN THE PAY QUANTITY SHOWN FOR SLOPE WALL.
2. THE 5" CONCRETE SLOPE WALL WILL BE PAID FOR AT THE UNIT PRICE BID PER SQUARE YARD COMPLETE IN PLACE AS SHOWN ON THE PLANS. THE PRICE SHALL INCLUDE ALL COSTS OF JOINT FILLER, REINFORCING STEEL, LABOR, AND ALL OTHER INCIDENTALS NECESSARY TO COMPLETE THE WORK. ALL MATERIALS AND WORK SHALL BE IN ACCORDANCE WITH SECTION 510 OF THE STANDARD SPECIFICATIONS. SLOPE WALLS SHALL BE CONSTRUCTED WITH CLASS A CONCRETE. IN ACCORDANCE WITH SECTION 509, COARSE AGGREGATE FOR THIN SECTION CONCRETE (SEC. 701.06) MAY BE USED.
3. ALL COST OF PIPE UNDERDRAIN COVER MATERIAL (BOTH FINE AND COARSE), TRENCH EXCAVATION, STANDARD BEDDING MATERIAL, AND EQUIPMENT AND LABOR FOR THEIR INSTALLATION SHALL BE INCLUDED IN THE CONTRACT UNIT PRICE OF SLOPE WALL (5").
4. LONGITUDINAL CONSTRUCTION JOINTS SHALL BE SPACED AT 10'-0" MAXIMUM. THE ENGINEER WILL DETERMINE THE FINAL NUMBER AND LOCATION OF THE CONSTRUCTION JOINTS IN THE FIELD.



DETAIL OF VERTICAL CONSTRUCTION JOINT



PLAN OF SLOPE WALLS

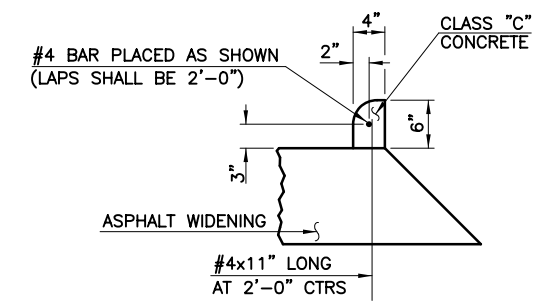
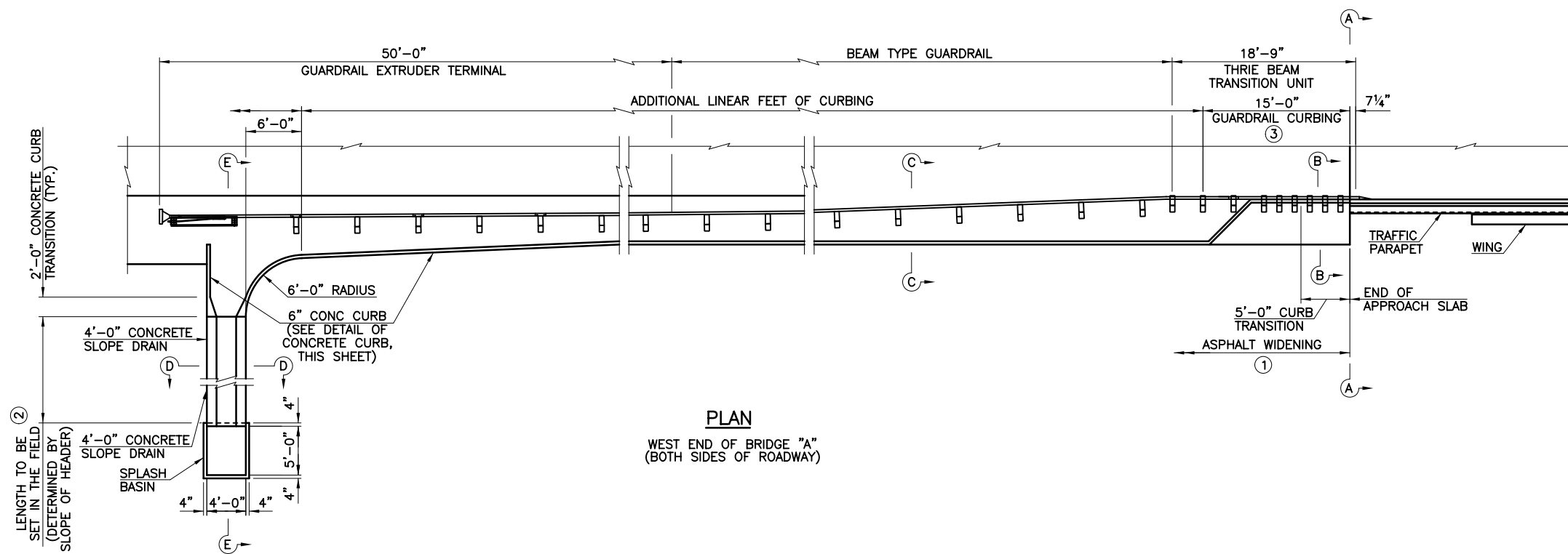


SECTION PERPENDICULAR TO ABUTMENT

DESIGN: DKC 06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ 06/18	BRIDGE "A"	
CHECKED: DMN 06/18		
APPRVD: DMN 06/18		
SLOPE WALL DETAILS		
STATE JOB PIECE NO: 21006(07)		SHEET 1 OF 1
CP&Y		SHEET NO. B027

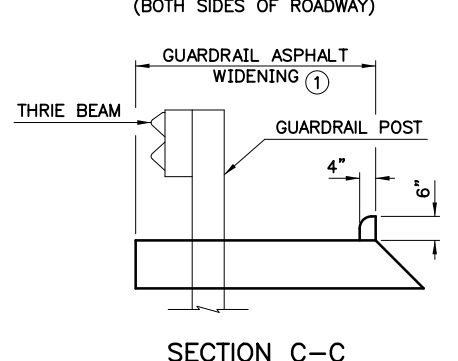
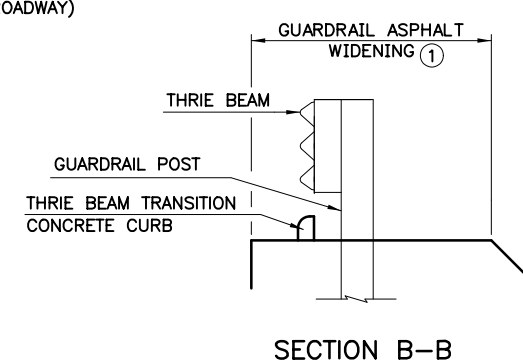
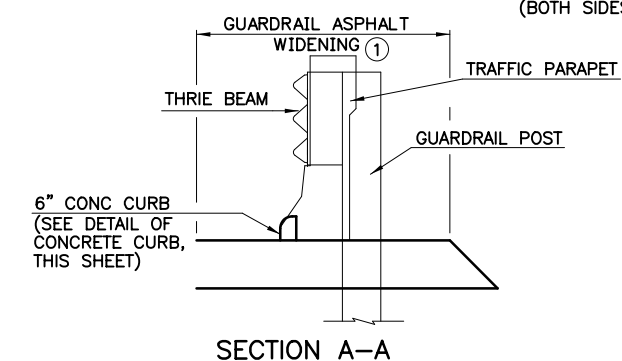
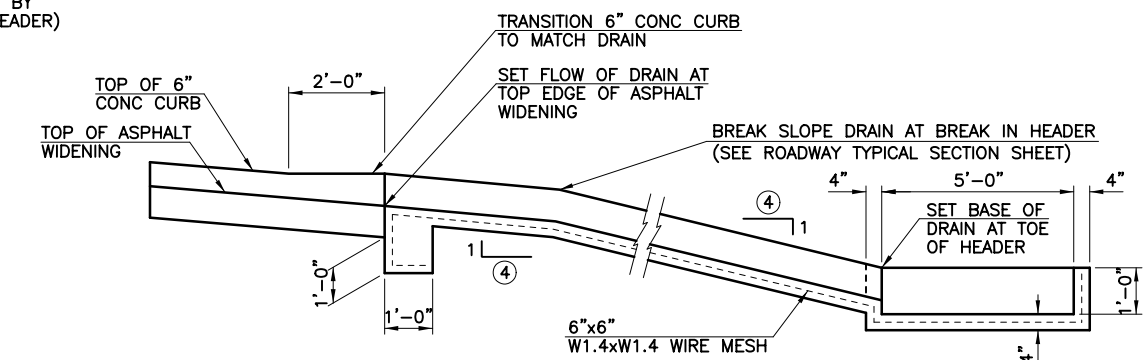
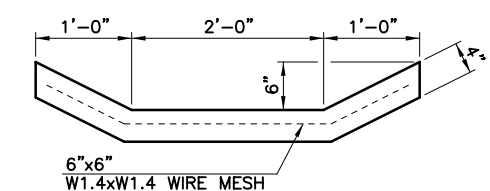
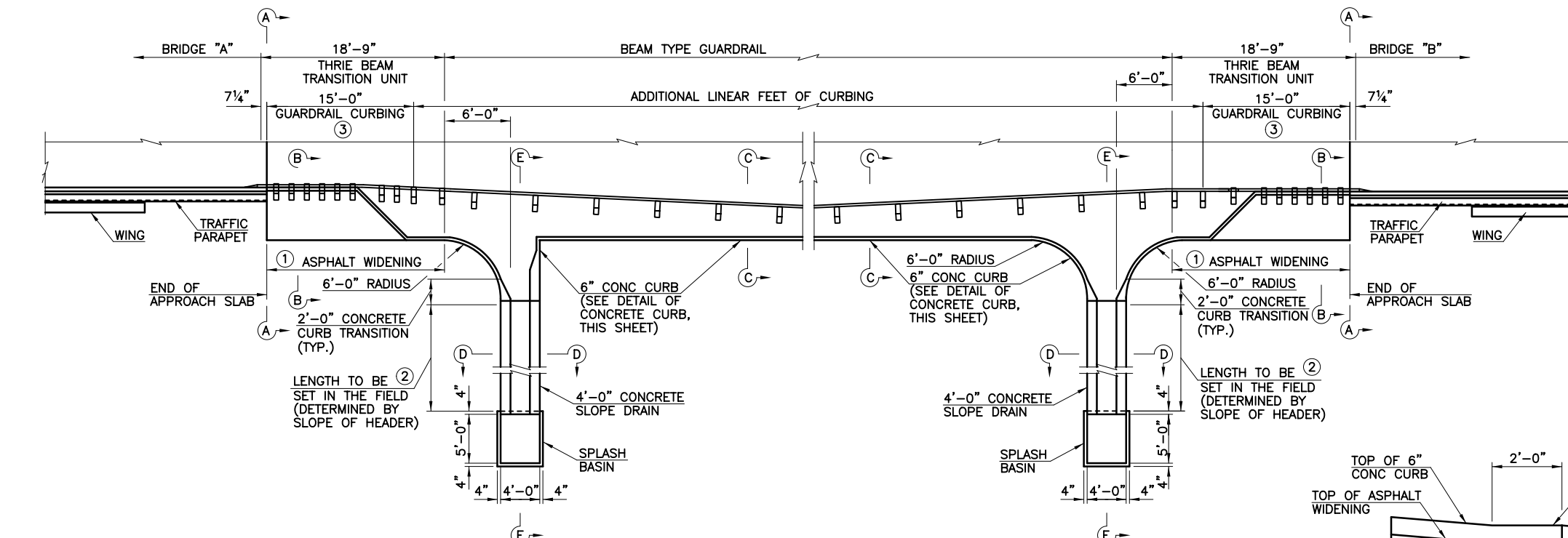
2:42:21 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRSWB01.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B028	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



GENERAL NOTES

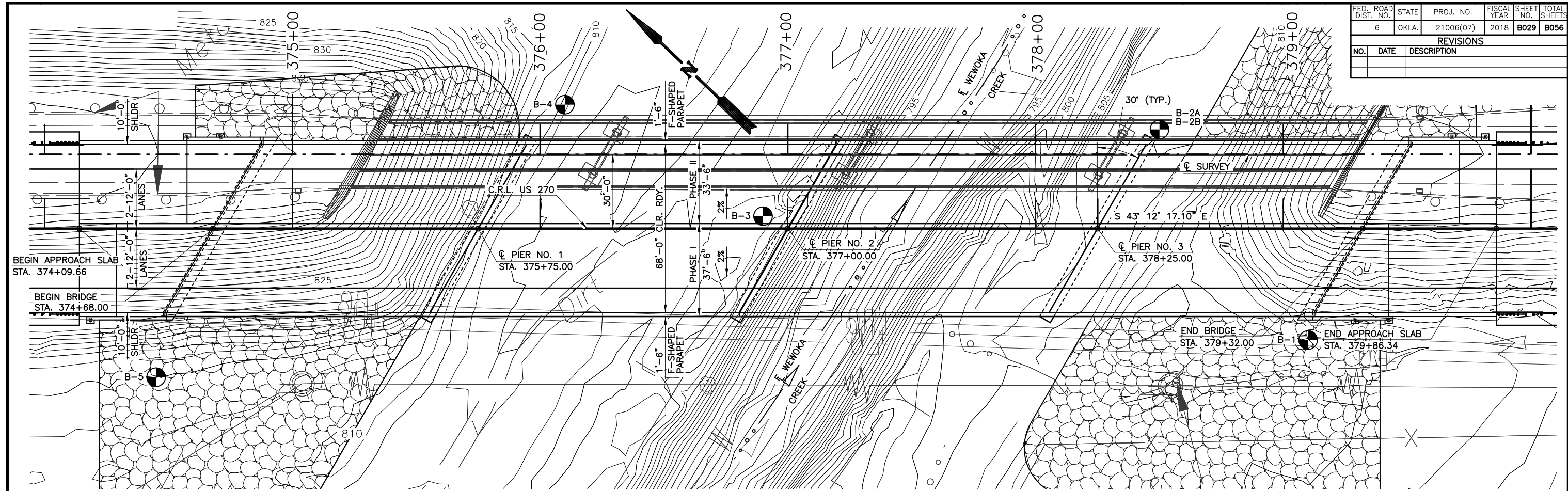
- ASPHALT WIDENING SHALL BE IN ACCORDANCE WITH ODOT STANDARDS GHW1-1-00, GHW2-1-00 AND THRI-1-00 EXCEPT AS SHOWN ON THIS SHEET. ALL COSTS OF ASPHALT WIDENING SHALL BE INCLUDED IN ROADWAY PAY ITEMS.
- SLOPE DRAINS AND SPLASH BASINS SHALL BE CONSTRUCTED USING CLASS "C" CONCRETE AS SHOWN ON THIS SHEET. ALL COSTS OF THE SLOPE DRAINS AND SPLASH BASINS SHALL BE INCLUDED IN THE BRIDGE PAY ITEM FOR "CLASS "C" CONCRETE".
- ADDITIONAL CURBING SHALL BE CONSTRUCTED AS SHOWN IN "DETAIL OF CONCRETE CURB", THIS SHEET. ALL COSTS OF MATERIALS AND INSTALLATION OF CLASS C CONCRETE CURB AND #4 BARS AS SHOWN SHALL BE PAID IN THE PAY ITEM FOR "GUARDRAIL CURBING" AS SHOWN ON STD THRI-1-02.
- SLOPE TO MATCH SLOPE OF HEADER.



DESIGN:	DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN:	LRJ	06/18	BRIDGE "A"	
CHECKED:	DMN	06/18		
APPRVD:	DMN	06/18		
CP&Y			DRAINS AT END OF BRIDGE	
			SHEET 1 OF 1	
			SHEET NO. B028	
STATE JOB PIECE NO: 21006(07)				

2:42:25 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRAM01.dgn

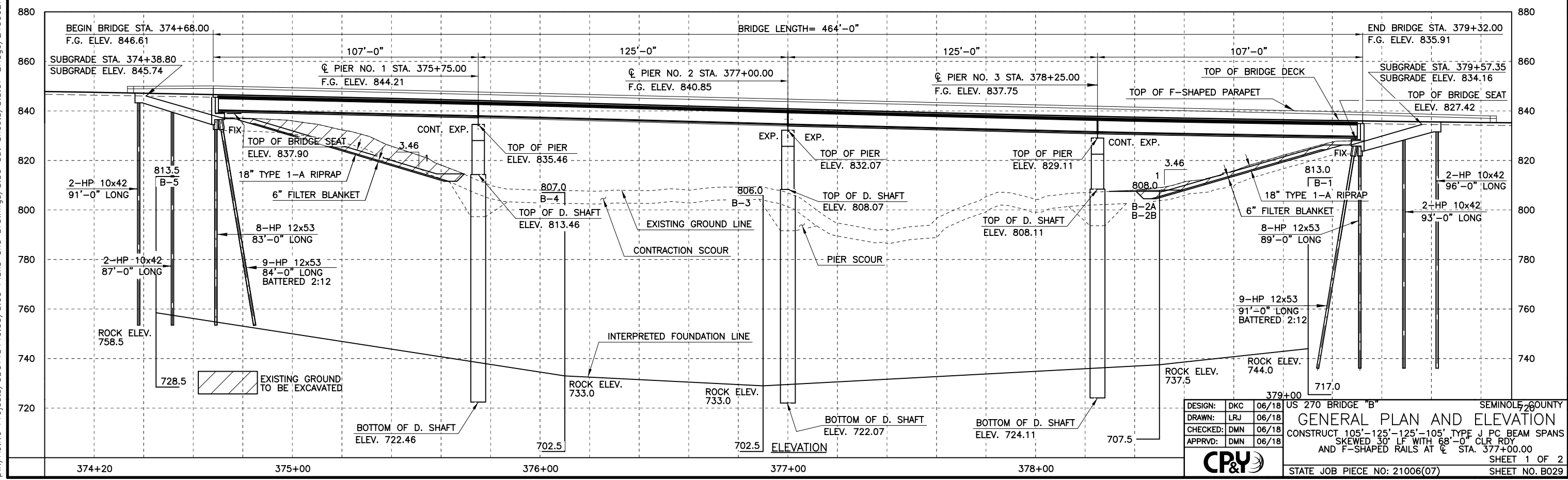
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B029	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



PLAN

B.M. # 46
CUT "X" ON WINGWALL 16' RT.
STA. 375+18.34 ELEV. 841.421

B.M. # 47
CUT "X" ON ROCK OUTCROP 104' RT.
STA. 381+83.04 ELEV. 829.674



ELEVATION

DESIGN:	DKC	06/18	GENERAL PLAN AND ELEVATION CONSTRUCT 105'-125'-125'-105' TYPE J PC BEAM SPANS SKEWED 30° LF WITH 68'-0" CLR RDY AND F-SHAPED RAILS AT STA. 377+00.00
DRAWN:	LRJ	06/18	
CHECKED:	DMN	06/18	
APPRVD:	DMN	06/18	

SEMINOLE COUNTY
STATE JOB PIECE NO: 21006(07)
SHEET 1 OF 2
SHEET NO. B029

6/5/2018 2:42:32 PM
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRLA02.dgn

HYDRAULIC DATA

TOTAL DRAINAGE AREA	= 89.90 SQ MILES	Q2 = 2,680 CFS	Q25 = 11,400 CFS	Q500 = 26,700 CFS
CONTROLLED DRAINAGE AREA	= 34.10 SQ MILES	V2 = 3.80 FPS	V25 = 6.13 FPS	V500 = 8.74 FPS
EFFECTIVE DRAINAGE AREA	= 55.80 SQ MILES	CHW2 = 807.87 FT	CHW25 = 815.48 FT	CHW500 = 820.07 FT
Q5 = 5,200 CFS	Q50 = 14,500 CFS	QOT	>Q500	
V5 = 4.47 FPS	V50 = 6.92 FPS	VOT	>Q500	
CHW5 = 811.71 FT	CHW50 = 816.57 FT	CHWOT	>Q500	
Q10 = 7,540 CFS	Q100 = 17,500 CFS	Q100 SCOUR DEPTH		
V10 = 5.05 FPS	V100 = 7.57 FPS	PIER=9.1 FT, CONTRACTION =5.8 FT		
CHW10 = 813.67 FT	CHW100 = 817.50 FT	Q500 SCOUR DEPTH		
		PIER=9.6 FT, CONTRACTION =9.2 FT		

DESIGN DATA

MATERIAL: 780

CLASS A CONCRETE f'c=3 ksi

CLASS AA CONCRETE f'c=4 ksi

REINFORCING STEEL(GRADE 60) Fy=60 ksi

STRUCTURAL STEEL M270 (GRADE 50W) Fy=50 ksi

STAINLESS STEEL A240 (GRADE 316) Fy=30 ksi

LOADING:
 HL-93 OR OKLAHOMA OVERLOAD TRUCK
 20 PSF FUTURE WEARING SURFACE
 5 PSF STAY-IN-PLACE FORMS

DESIGN:
 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 7TH EDITION

LRFR OPERATING RATING FACTOR = 2.41

INDEX OF SHEETS

B029-B030	GENERAL PLAN AND ELEVATION
B031	SUBSTRUCTURE STAKING DIAGRAM
B032-B034	SUBSURFACE PROFILE
B035	BRIDGE CONSTRUCTION SEQUENCE
B036	SUBSTRUCTURE EXCAVATION
B037-B038	ABUTMENT NO. 1 DETAILS
B039-B040	ABUTMENT NO. 2 DETAILS
B041	ABUTMENT DETAILS
B042-B043	PIER DETAILS
B044-B048	SUPERSTRUCTURE DETAILS
B049-B051	P.C. BEAM DETAILS
B052	BEARING DETAILS
B053	APPROACH SLAB NO. 1 DETAILS
B054	APPROACH SLAB NO. 2 DETAILS
B055	APPROACH SLAB DETAILS
B056	DRAINS AT END OF BRIDGE

BRIDGE STANDARDS:

EJ-SK-04E
 EJ-DTL-02E
 FSHP-42-2-00E
 HP1-2-01E

FOUNDATION DATA

ABUTMENTS (HP 12x53 PILING)

FACTORED PILE REACTION = 81.4 TONS/PILE

PIERS (60" DIAMETER DRILLED SHAFTS)

PIER NO. 1 FACTORED REACTION = 693.0 TONS/SHAFT
 PIER NO. 2 FACTORED REACTION = 719.0 TONS/SHAFT
 PIER NO. 3 FACTORED REACTION = 678.0 TONS/SHAFT

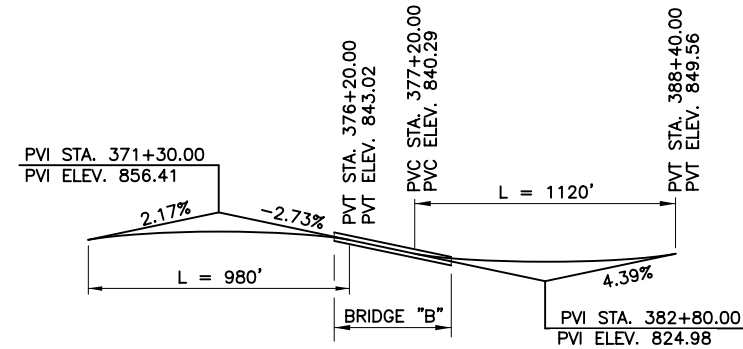
NOMINAL UNIT FRICTION RESISTANCE = 9.00 T.S.F.
 FRICTION RESISTANCE FACTOR = 0.45
 FACTORED FRICTION RESISTANCE = 317.9 TONS/SHAFT

NOMINAL UNIT BEARING RESISTANCE = 60 T.S.F.
 BEARING RESISTANCE FACTOR = 0.70
 FACTORED BEARING RESISTANCE = 824.4 TONS/SHAFT

TOTAL FACTORED RESISTANCE = 1,142.3 TONS/SHAFT

MINIMUM DEPTH OF PENETRATION INTO ROCK IS 10'-0".

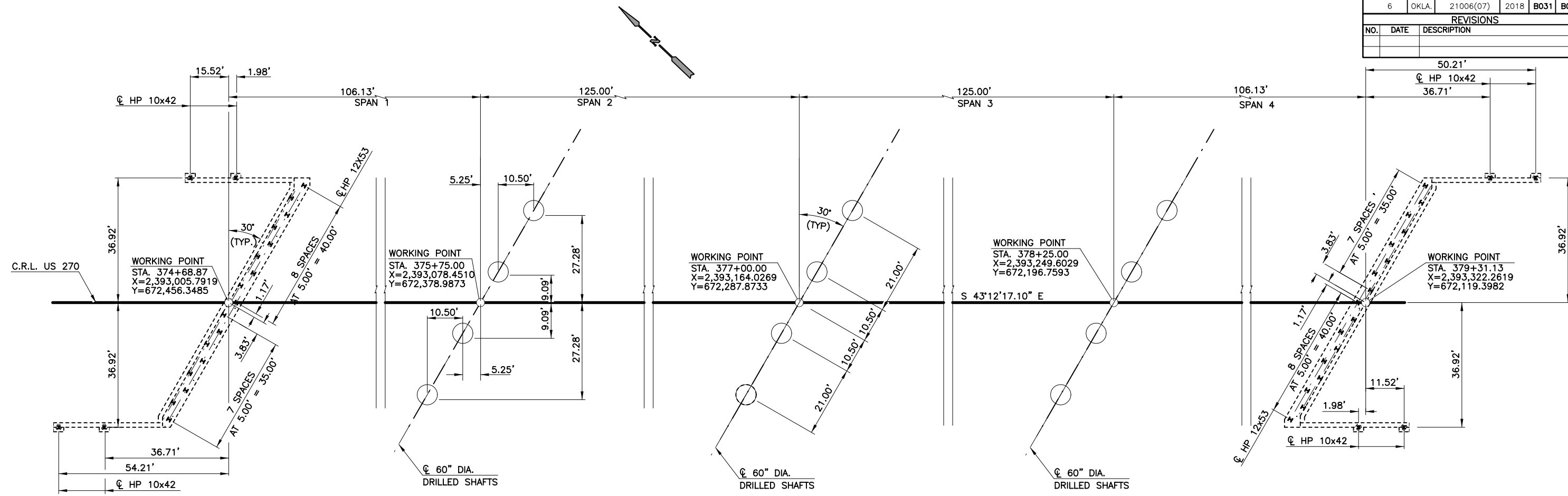
THE DEPTH OF ROCK NEGLECTED FOR FRICTION IS 5'-0".



VERTICAL CURVE DATA

2:42:35 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRLA03.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B031	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



STAKING DIAGRAM

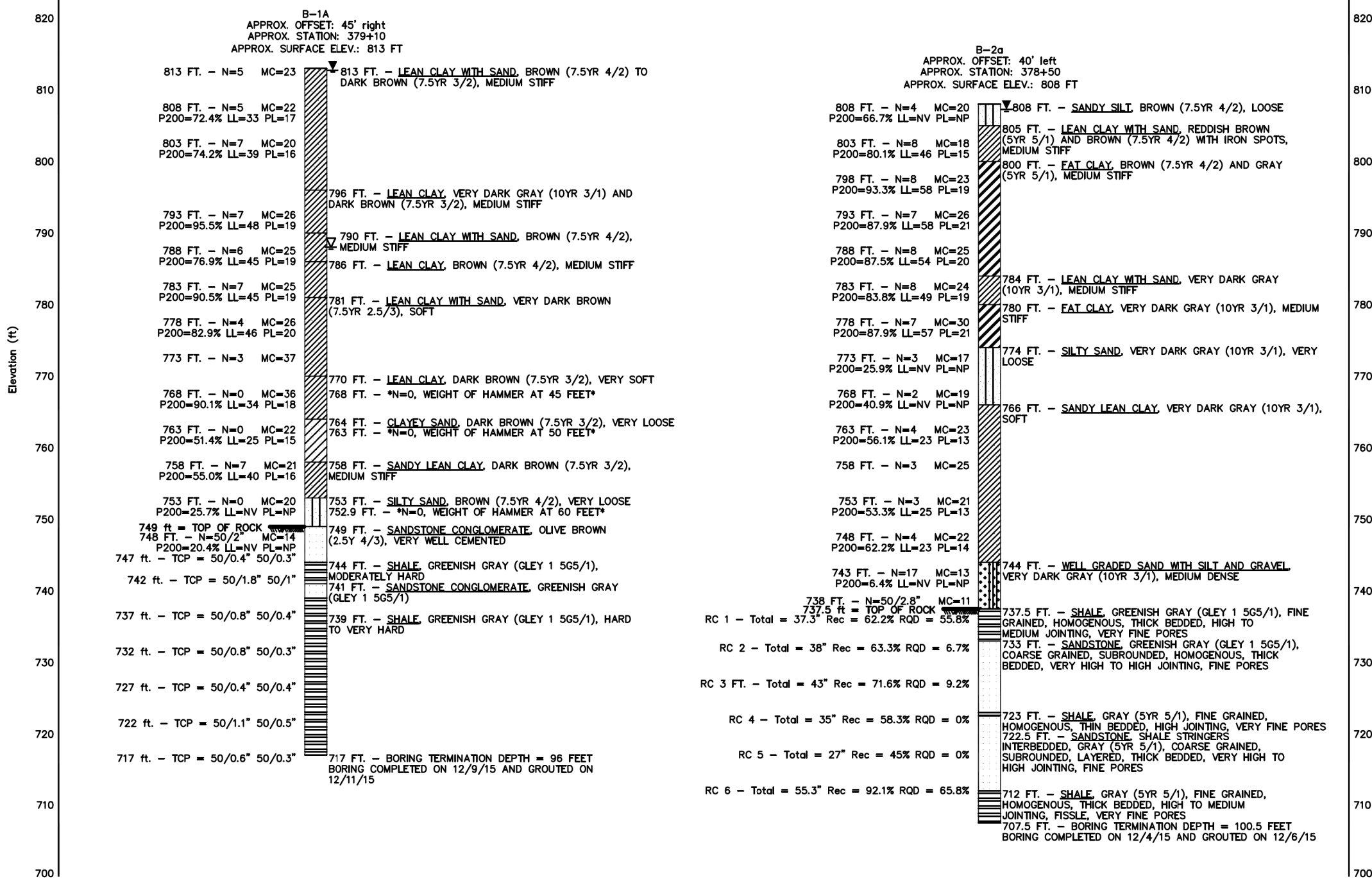
ITEMIZED QUANTITIES

ITEM CODE	ITEM	UNIT	PHASE I					PHASE II					TOTAL
			ABUTMENT	PIERS	SUPER-STRUCTURE	APPROACH SLAB	SUBTOTAL	ABUTMENT	PIERS	SUPER-STRUCTURE	APPROACH SLAB	SUBTOTAL	
501(B)	1307	307	172.00				172.00	129.00				129.00	301.00
501(G)	6309	309	383.80				383.80	271.50				271.50	655.30
503(A)	6290	290			1,834.67		1,834.67			1,376.00		1,376.00	3,210.67
504(A)	1304	304				469.60	469.60				419.60	419.60	889.20
504(B)	1305	305				1,856.00	2,306.80			1,650.00	400.70	2,050.70	4,357.50
504(C)	6250	250				42.00	42.00			37.40		37.40	79.40
504(E)	6190	190				463.90	576.60			463.90	112.70	576.60	1,153.20
506(A)	1322	322				2,640.00	2,640.00			2,400.00		2,400.00	5,040.00
507(A)	6170	170				24.00	24.00			18.00		18.00	42.00
507(B)	6174	174				8.00	8.00			6.00		6.00	14.00
509(A)	1326	326				543.80	543.80			482.40		482.40	1,026.20
509(B)	1328	328	115.90	182.00			297.90	84.60	156.40			241.00	538.90
509(D)	1331	331					7.00					6.50	13.50
511(A)	1332	332			1,620.00		1,620.00		1,620.00			1,620.00	3,240.00
511(B)	6010	010	12,580.00	27,700.00	161,950.00		202,230.00	9,510.00	23,630.00	134,500.00		167,640.00	369,870.00
514(A)	6010	010	367.00				367.00	367.00				367.00	734.00
514(A)	6011	011	1,735.00				1,735.00	1,216.00				1,216.00	2,951.00
514(B)	6292	292	367.00				367.00	367.00				367.00	734.00
514(B)	6294	294	1,735.00				1,735.00	1,216.00				1,216.00	2,951.00
514(L)	6220	220											1.00
515(A)	6013	013	156.60	259.00	899.00	56.00	1,370.60	103.80	206.00	899.00	56.00	1,264.80	2,635.40
516(A)	6096	096			522.00		522.00		522.00			522.00	1,044.00
516(C)	6200	200		6.00			6.00		6.00			6.00	12.00
523(A)	6550	550				83.00	83.00			1,001.80		1,001.80	1,084.80
523(B)	6560	560				1.00	1.00			11.20		11.20	12.20
525(C)	1000	000											1.00
601(B)	1353	353											3,900.00
601(C)	1355	355											1,130.00
613(H)	6204	204	97.00				97.00	67.00				67.00	164.00
613(I)	6207	207						60.00				60.00	60.00
619(B)	2500	500											1.00

DESIGN:	DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN:	LRJ	06/18	BRIDGE "B"	
CHECKED:	DMN	06/18	SUBSTRUCTURE STAKING DIAGRAM	
APPRVD:	DMN	06/18		
CP&Y			STATE JOB PIECE NO: 21006(07)	SHEET 1 OF 1 SHEET NO. B031

6/5/2018 2:42:39 PM
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 p:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRBFD01.dgn

REVISIONS		
REV. NO.	DESCRIPTION	DATE



SITE GEOLOGY

Division Three of the "Engineering Classification of Geological Materials", published by the Oklahoma Department of Transportation (ODOT) indicates that the project site is located over Alluvium (Qas) underlain by the Vanoss Unit (Pv).

Alluvium consists of sand, silt, clay, gravel, and/or combinations of materials. Alluvium is found along the flood plains (bottom land) of streams and is normally present at places along all streams.

The Vanoss Unit consists of alternating moderately soft to moderately hard sandstones, conglomerates, shales, and a few thin limestones. The shales are multicolored and resemble those of the underlying Ada Unit. In the outcrop area adjacent to the Arbuckle Mountains and northward to about the middle of Seminole County, the sandstones and conglomerates are thicker and locally are so arkosic that at first glance a few of them might be mistaken for true granites. Commonly, the base unit is referred to as the lowest of the arkosic beds, but this is only true in the southern part of the unit's outcrop area. As far north as Little River, Seminole County, the base of the Vanoss Unit is the first, persistent, non-limestone, conglomerate bed above the base of the Ada Unit. North of Little River, a continuous sandstone horizon marks the base.

A basal limestone conglomerate member is prominent in Murray County adjacent to the Arbuckle Mountains. It is mapped and described separately from the Vanoss Unit and the Vanoss Conglomerate subunit (Pvc). Near the Arbuckle Mountains the total thickness of the Vanoss Unit is 1,550 feet with 650 feet assigned to the conglomerate subunit. Northward, the unit thins from 650 feet in the southern Pontotoc County to 250 feet near Konawa, Seminole County. The thickness of the unit is irregular in Seminole County and varies from 140 to 500 feet, thickening southward.

The Vanoss Unit outcrops in a two to ten mile wide band around the northern and western limits of the Arbuckle Mountains in Murray and Pontotoc Counties. From here, the unit outcrops in a two to seven mile wide, north-south, strip across western Pontotoc and Seminole Counties and the eastern edge of Pottawatomie County. North of the North Canadian River, in Okfuskee and northeastern Pottawatomie Counties, the strata of the Vanoss Unit are inseparable from the strata of the underlying Ada unit and consequently the two are mapped together as the Vanoss-Ada Unit (Pva).

Topographically, the unit is gently rolling to rolling with more rolling topography prominent where the sandstones and conglomerates are thicker.

According to the Geologic Map of the "Hydrologic Atlas 4 of Oklahoma," Reconnaissance of the Water Resources of the Oklahoma City Quadrangle, central Oklahoma, by Roy H. Blingham and Robert L. Moore, Oklahoma Geological Survey, 1975, the project consists of Alluvium (Qal) underlain by the Vanoss Group (IPva). The geological deposit and unit is described therein as follows.

Alluvium consists of sand, silt, clay and lenticular beds of gravel. Thickness ranges from about 30 to 100 feet and probably averages about 50 feet along major streams. Along minor streams, thickness ranges from a few feet to about 50 feet and probably averages about 25 feet. Alluvium is a major aquifer in parts of the quadrangle.

The Vanoss Group consists of orange-brown fine-grained, crossbedded sandstone; grades southward into arkosic sandstone and conglomerate. Includes many thin limestone beds and shale units north of North Canadian River (descending): *Roca Shale* (75 feet thick), *Red Eagle Limestone* (3 to 8 feet thick), *Johnson Shale* (60 feet thick), *Long Creek Limestone* (9 to 12 feet thick), *Hughes Creek Shale* (52 feet thick), *Americus Limestone* (1 to 2.5 feet thick), *Admire Shale* (70 feet thick), *Brownville Limestone* (1 to 3 feet thick), *Pony Creek Shale* (75 feet thick), *Grayhorse Limestone* (1 foot thick), *unnamed shale* (70 feet thick), *Elmont Limestone* (1.2 to 7.8 feet thick), *Stonebreaker Shale* (60 feet thick), and *Reading Limestone* (1.5 feet thick), at base. Total thickness of group ranges from 250 feet in south to 490 feet in north.

LEGEND

- SPT DENOTES STANDARD PENETRATION TEST, ASTM D1586
- N DENOTES NUMBER OF BLOW COUNTS PER 12 INCHES
- REC DENOTES RECOVERY IN ROCK CORING
- RQD DENOTES ROCK QUALITY DESIGNATION
- MC DENOTES MOISTURE CONTENT TESTS
- P200 DENOTES PERCENT PASSING NO 200 SIEVE
- LL DENOTES LIQUID LIMIT TESTS (NV=NO VALUE)
- PL DENOTES PLASTIC LIMIT TESTS (NP=NO PLASTICITY)
- ▽ DENOTES WATER ELEVATION DURING DRILLING
- ▽ DENOTES WATER ELEVATION IMMEDIATELY AFTER DRILLING
- ▽ DENOTES WATER ELEVATION HOURS AFTER DRILLING
- DENOTES TOP OF ROCK

- NOTE: WATER ELEVATIONS SHOWN WERE OBTAINED AT THE TIME BORINGS WERE DRILLED AND MAY FLUCTUATE THROUGHOUT THE YEAR.
- NOTE: TOP OF ROCK LINE SHOWN FOR ESTIMATING PURPOSE ONLY
- NOTE: ROCK CLASSIFICATION IS BASED ON DRILLING CHARACTERISTICS AND VISUAL OBSERVATION. PTEROGRAPHIC ANALYSIS OF THIN SECTIONS OF THE ROCK CORE SAMPLES MAY REVEAL OTHER TYPES.

GEOLOGIC REPORT

ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER AT (405) 521-2625. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

US 270 OVER WEWOKA CREEK SEMINOLE COUNTY

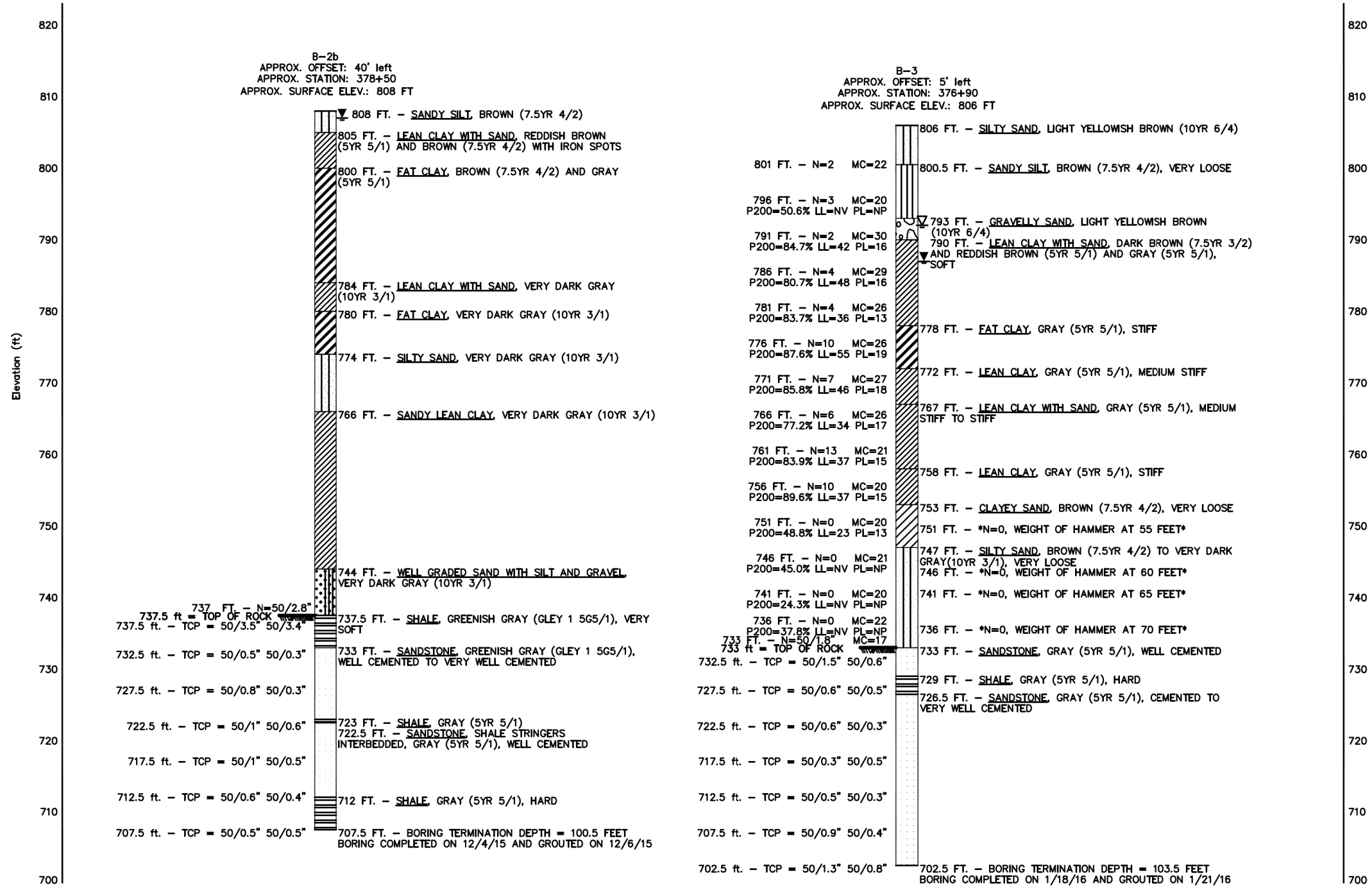
SUBSURFACE PROFILE
SHEET 1 OF 3

STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION

JOB PIECE NO. 21006(07) SHEET NO. B032

Design	JTU	4/19
Detail	JTU	4/19
Check	RAG	4/19
Squad:		
Engr.:		

REVISIONS		
REV. NO.	DESCRIPTION	DATE



LEGEND

- SPT DENOTES STANDARD PENETRATION TEST, ASTM D1586
 - N DENOTES NUMBER OF BLOW COUNTS PER 12 INCHES
 - REC DENOTES RECOVERY IN ROCK CORING
 - ROD DENOTES ROCK QUALITY DESIGNATION
 - MC DENOTES MOISTURE CONTENT TESTS
 - P200 DENOTES PERCENT PASSING NO 200 SIEVE
 - LL DENOTES LIQUID LIMIT TESTS (NV=NO VALUE)
 - PL DENOTES PLASTIC LIMIT TESTS (NP=NO PLASTICITY)
 - ▽ DENOTES WATER ELEVATION DURING DRILLING
 - ▽ DENOTES WATER ELEVATION IMMEDIATELY AFTER DRILLING
 - ▽ DENOTES WATER ELEVATION HOURS AFTER DRILLING
 - ▽ DENOTES TOP OF ROCK
- NOTE: WATER ELEVATIONS SHOWN WERE OBTAINED AT THE TIME BORINGS WERE DRILLED AND MAY FLUCTUATE THROUGHOUT THE YEAR.
- NOTE: TOP OF ROCK LINE SHOWN FOR ESTIMATING PURPOSE ONLY
- NOTE: ROCK CLASSIFICATION IS BASED ON DRILLING CHARACTERISTICS AND VISUAL OBSERVATION. PTEROGRAPHIC ANALYSIS OF THIN SECTIONS OF THE ROCK CORE SAMPLES MAY REVEAL OTHER TYPES.

GEOLOGIC REPORT

ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER AT (405) 521-2625. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

US 270 OVER WEWOKA CREEK SEMINOLE COUNTY

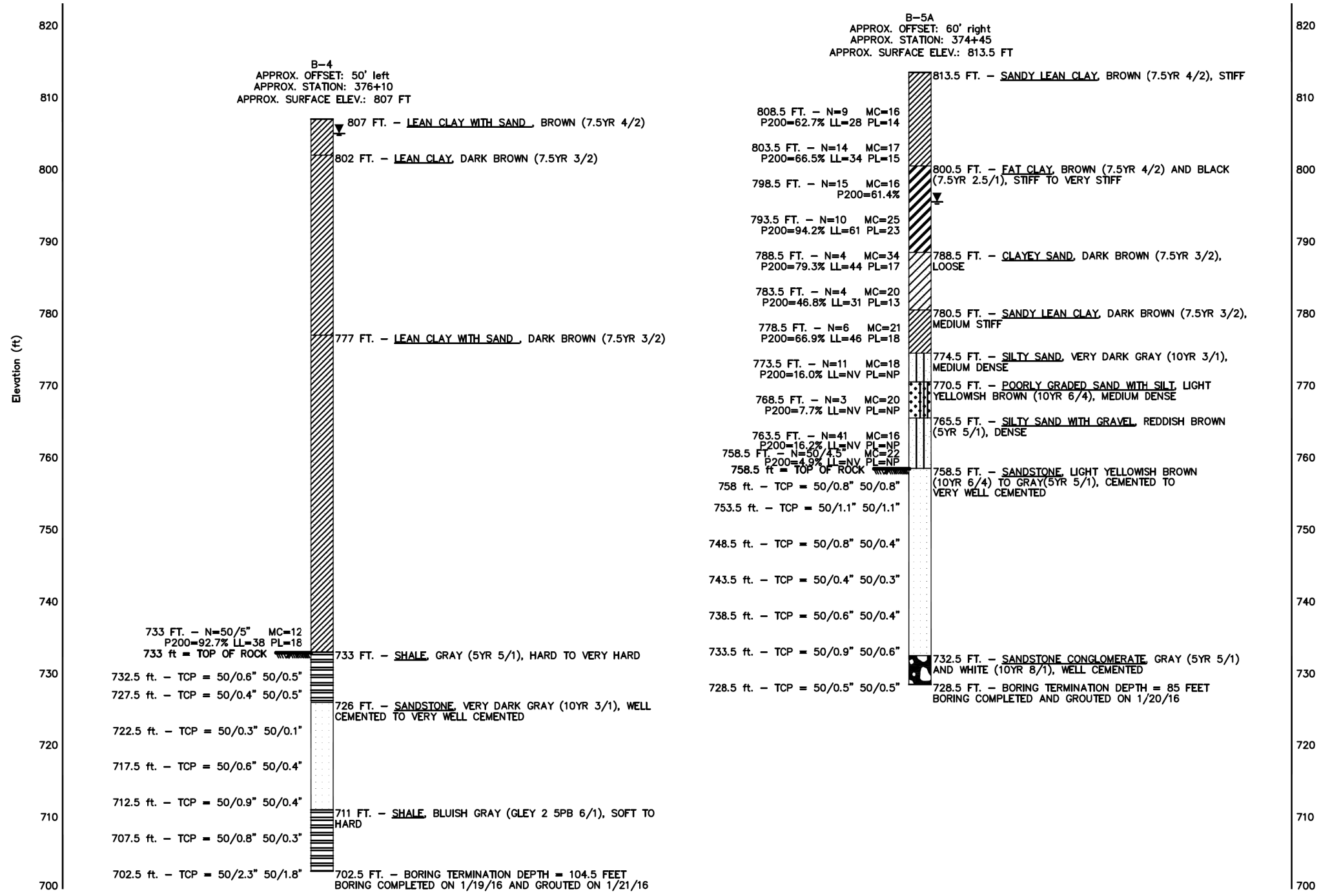
SUBSURFACE PROFILE
SHEET 2 OF 3

STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION

JOB PIECE NO. 21006(07) SHEET NO. B033

Design	JTU	4/19
Detail	JTU	4/19
Check	RAG	4/19
Squad:		
Engr.:		

REVISIONS		
REV. NO.	DESCRIPTION	DATE



LEGEND

SPT DENOTES STANDARD PENETRATION TEST, ASTM D1586
 N DENOTES NUMBER OF BLOW COUNTS PER 12 INCHES
 REC DENOTES RECOVERY IN ROCK CORING
 ROD DENOTES ROCK QUALITY DESIGNATION
 MC DENOTES MOISTURE CONTENT TESTS
 P200 DENOTES PERCENT PASSING NO 200 SIEVE
 LL DENOTES LIQUID LIMIT TESTS (NV=NO VALUE)
 PL DENOTES PLASTIC LIMIT TESTS (NP=NO PLASTICITY)
 DENOTES WATER ELEVATION DURING DRILLING
 DENOTES WATER ELEVATION IMMEDIATELY AFTER DRILLING
 DENOTES WATER ELEVATION HOURS AFTER DRILLING
 DENOTES TOP OF ROCK

■ NOTE: WATER ELEVATIONS SHOWN WERE OBTAINED AT THE TIME BORINGS WERE DRILLED AND MAY FLUCTUATE THROUGHOUT THE YEAR.
 ■■ NOTE: TOP OF ROCK LINE SHOWN FOR ESTIMATING PURPOSE ONLY
 ■■■ NOTE: ROCK CLASSIFICATION IS BASED ON DRILLING CHARACTERISTICS AND VISUAL OBSERVATION. PTEROGRAPHIC ANALYSIS OF THIN SECTIONS OF THE ROCK CORE SAMPLES MAY REVEAL OTHER TYPES.

GEOLOGIC REPORT

ALL GEOTECHNICAL INFORMATION CONTAINED ON THIS SHEET IS COVERED BY THE ENGINEERING SEAL AFFIXED TO AN ORIGINAL GEOTECHNICAL ENGINEERING REPORT THAT HAS BEEN STAMPED AND SEALED BY A PROFESSIONAL ENGINEER LICENSED IN OKLAHOMA. TO OBTAIN A COPY OF THE COMPLETE REPORT, CONTACT THE ODOT OFFICE ENGINEER AT (405) 521-2625. THE CONTRACTOR SHOULD BE FULLY AWARE OF THE SITE CONDITIONS PRIOR TO BEGINNING WORK. ANY ADDITIONAL GEOTECHNICAL INFORMATION WHICH MAY BE DESIRED IS THE RESPONSIBILITY OF THE CONTRACTOR.

US 270 OVER WEWOKA CREEK SEMINOLE COUNTY

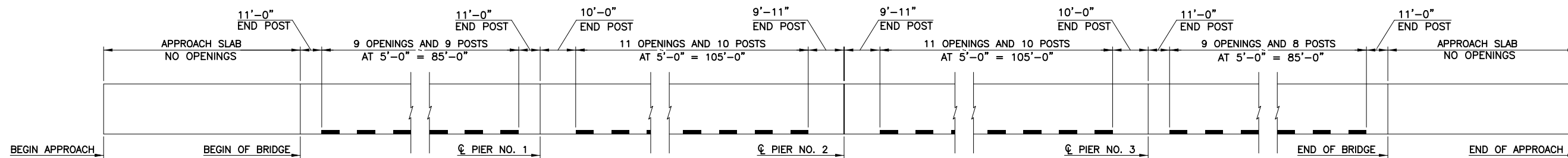
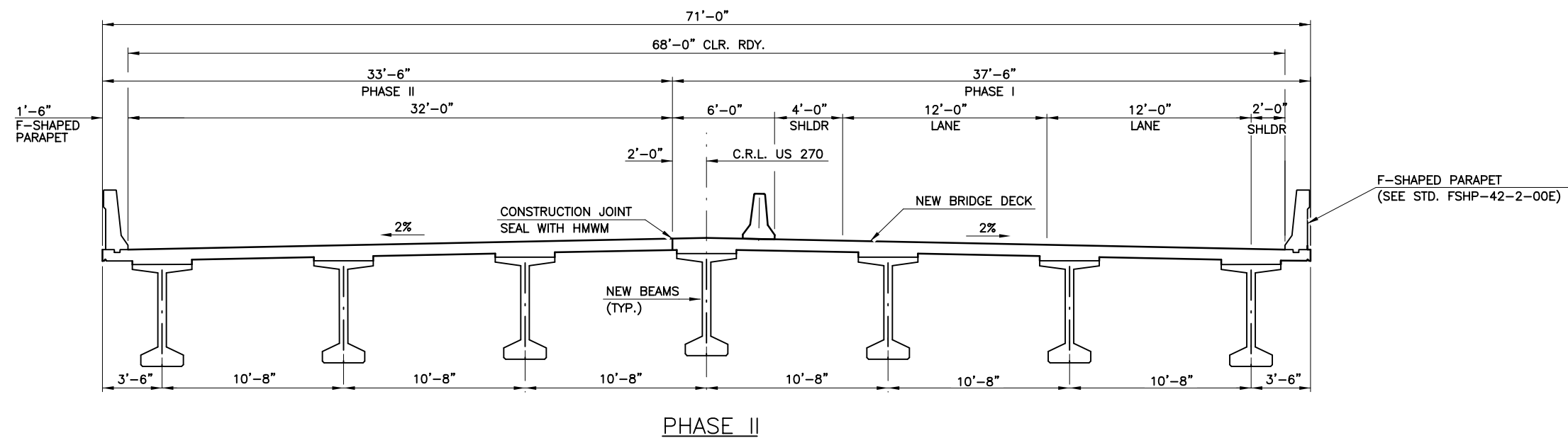
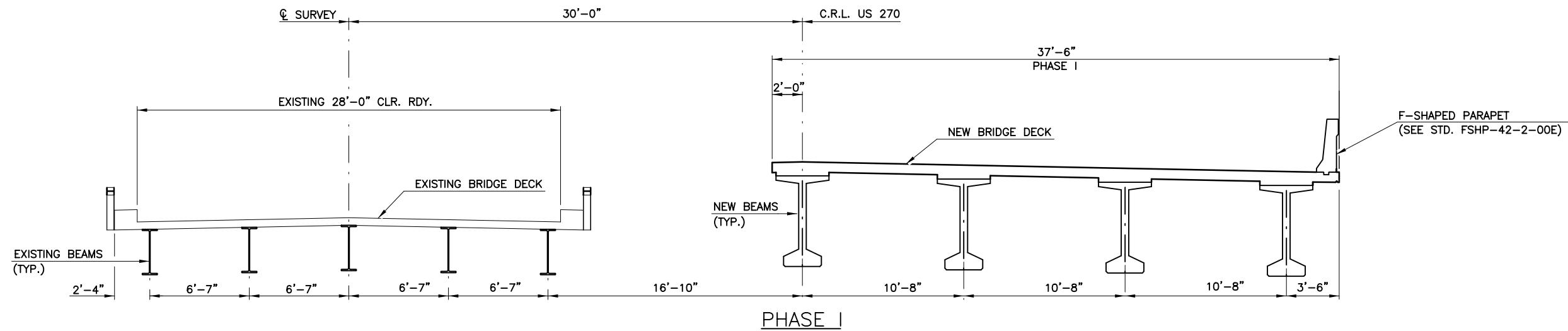
SUBSURFACE PROFILE
SHEET 3 OF 3

STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION

JOB PIECE NO. 21006(07) SHEET NO. B034

Design	JTU	4/19
Detail	JTU	4/19
Check	RAG	4/19
Squad:		
Engr.:		

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B035	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



TRAFFIC RAIL LAYOUT

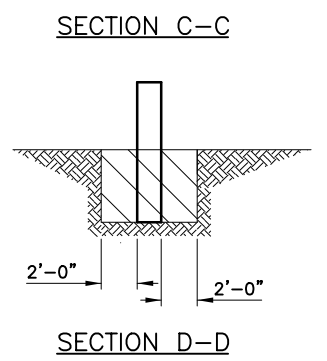
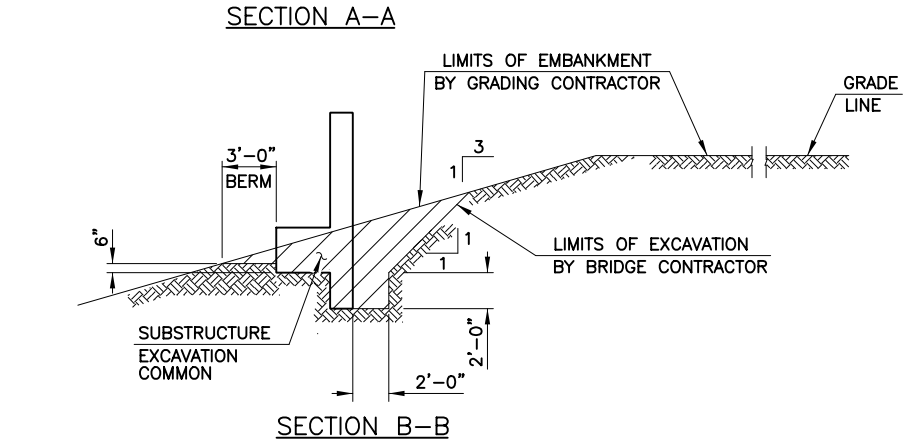
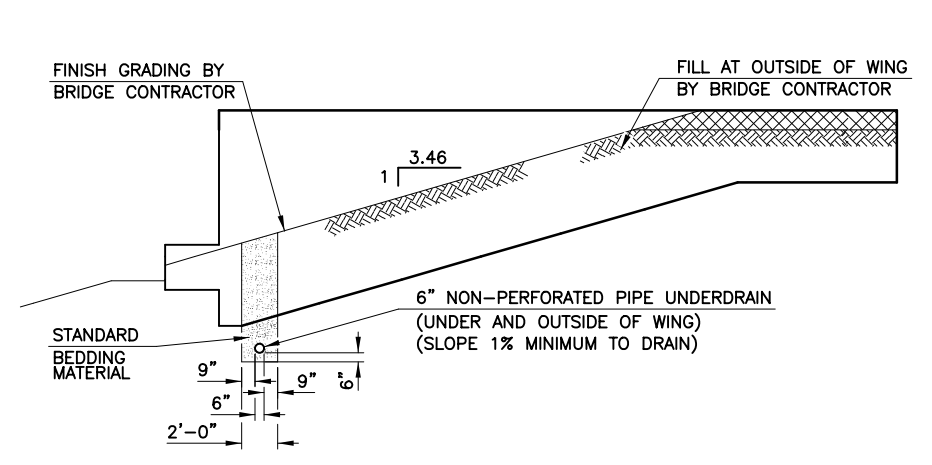
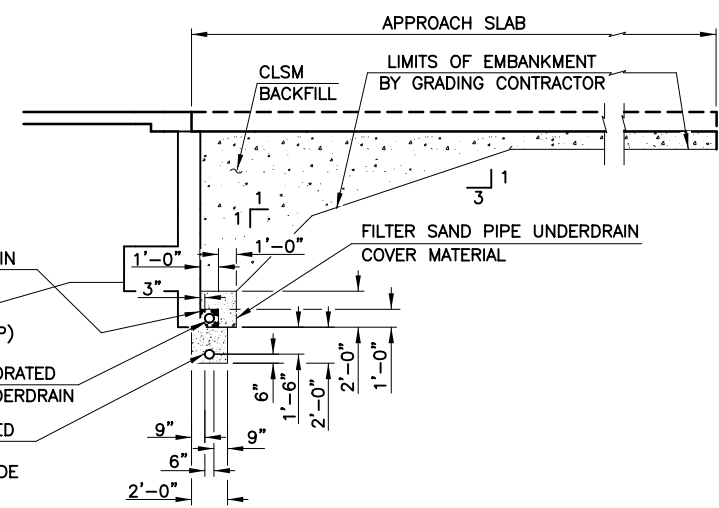
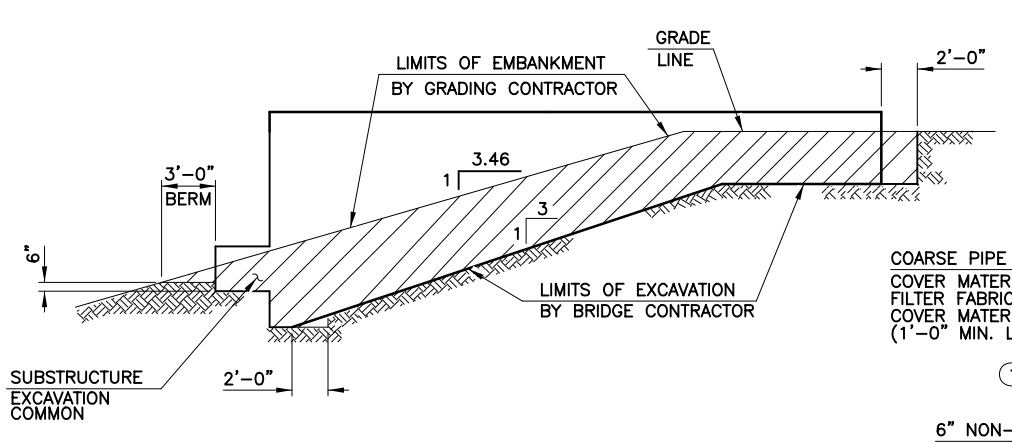
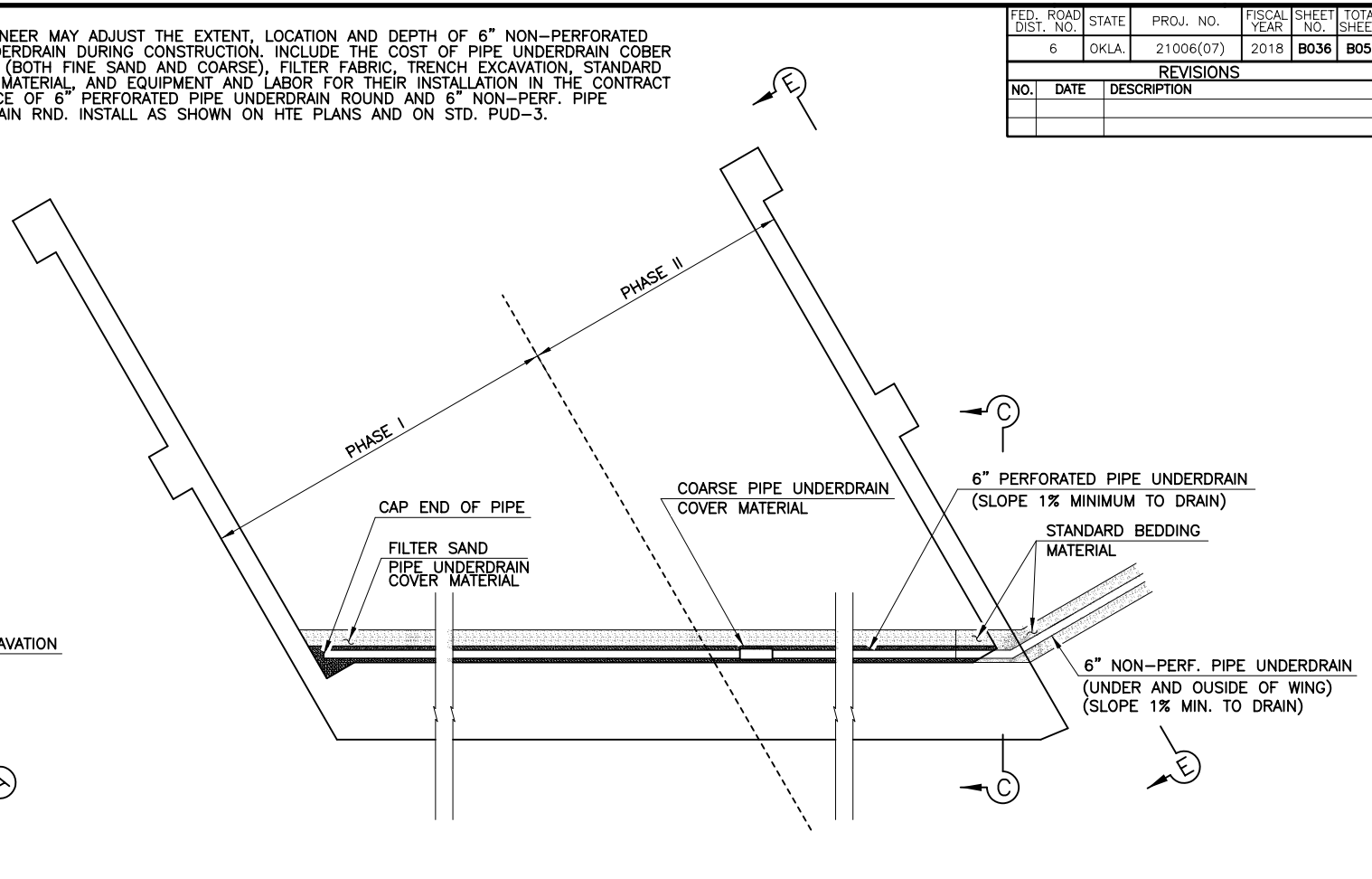
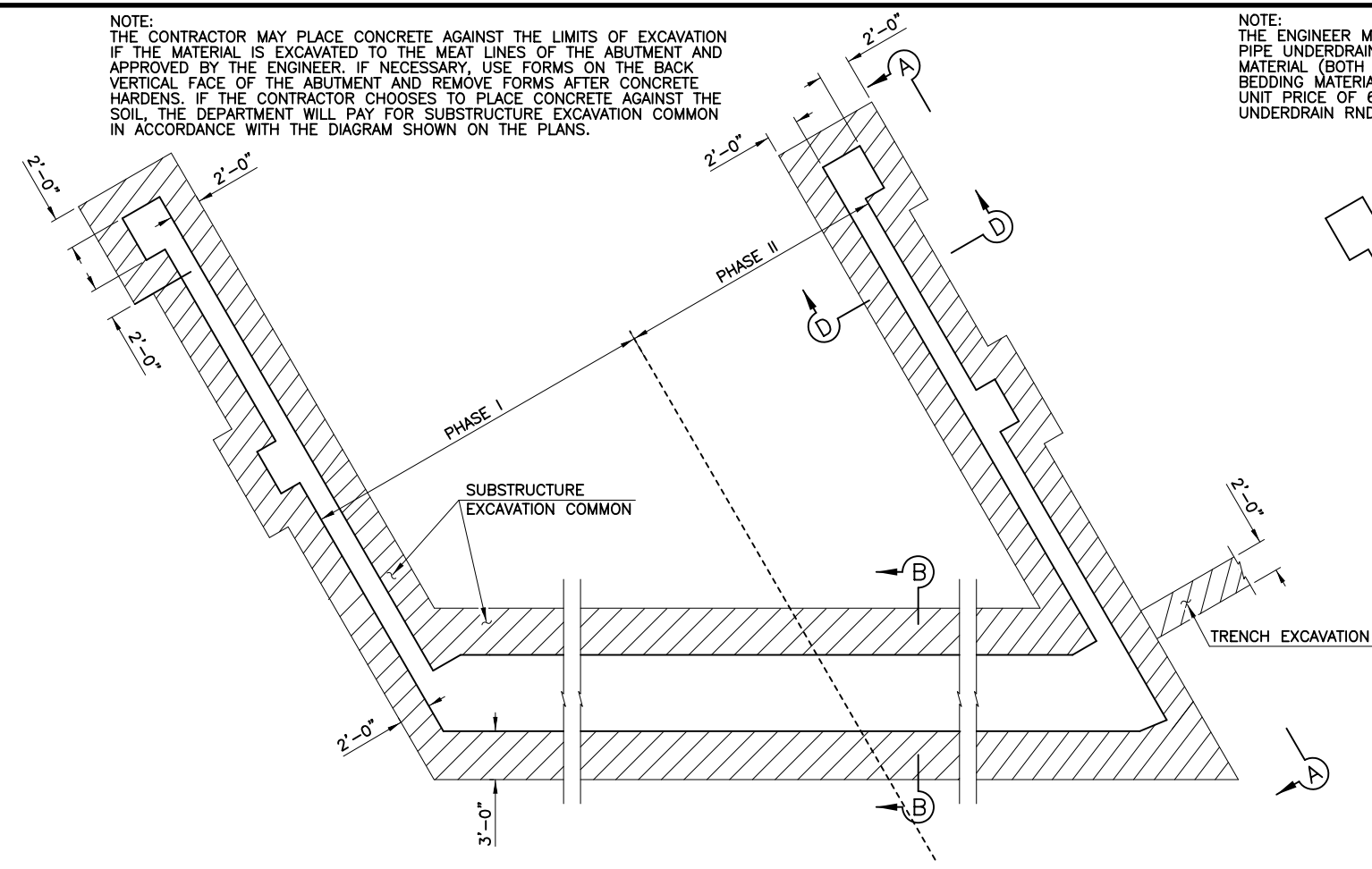
2:43:11 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRTD02.dgn

DESIGN: DKC	06/18	US 270 BRIDGE "B" SEMINOLE COUNTY BRIDGE CONSTRUCTION SEQUENCE STATE JOB PIECE NO: 21006(07)
DRAWN: LRJ	06/18	
CHECKED: DMN	06/18	
APPRVD: DMN	06/18	
		SHEET 1 OF 1 SHEET NO. B035

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B036	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

NOTE:
THE CONTRACTOR MAY PLACE CONCRETE AGAINST THE LIMITS OF EXCAVATION IF THE MATERIAL IS EXCAVATED TO THE MEAT LINES OF THE ABUTMENT AND APPROVED BY THE ENGINEER. IF NECESSARY, USE FORMS ON THE BACK VERTICAL FACE OF THE ABUTMENT AND REMOVE FORMS AFTER CONCRETE HARDENS. IF THE CONTRACTOR CHOOSES TO PLACE CONCRETE AGAINST THE SOIL, THE DEPARTMENT WILL PAY FOR SUBSTRUCTURE EXCAVATION COMMON IN ACCORDANCE WITH THE DIAGRAM SHOWN ON THE PLANS.

NOTE:
THE ENGINEER MAY ADJUST THE EXTENT, LOCATION AND DEPTH OF 6" NON-PERFORATED PIPE UNDERDRAIN DURING CONSTRUCTION. INCLUDE THE COST OF PIPE UNDERDRAIN COVER MATERIAL (BOTH FINE SAND AND COARSE), FILTER FABRIC, TRENCH EXCAVATION, STANDARD BEDDING MATERIAL, AND EQUIPMENT AND LABOR FOR THEIR INSTALLATION IN THE CONTRACT UNIT PRICE OF 6" PERFORATED PIPE UNDERDRAIN ROUND AND 6" NON-PERF. PIPE UNDERDRAIN RND. INSTALL AS SHOWN ON HTE PLANS AND ON STD. PUD-3.



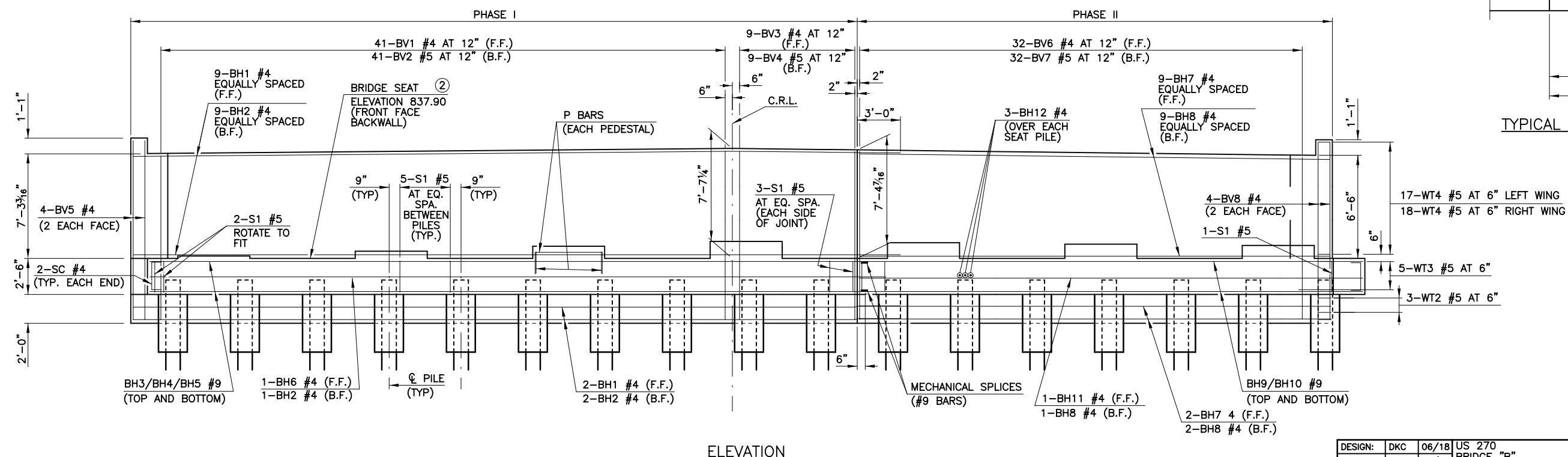
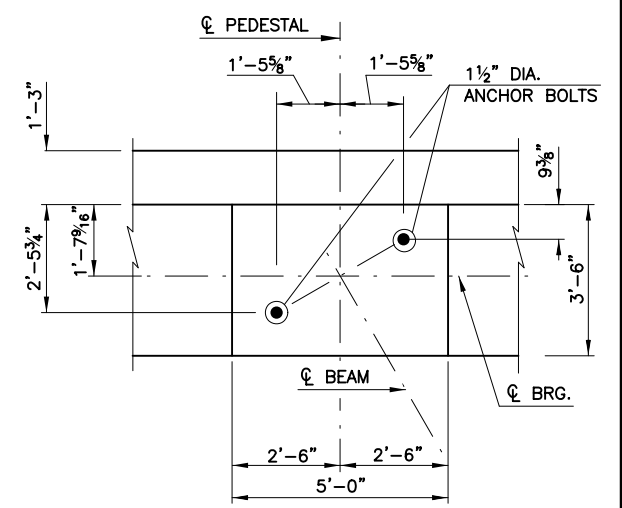
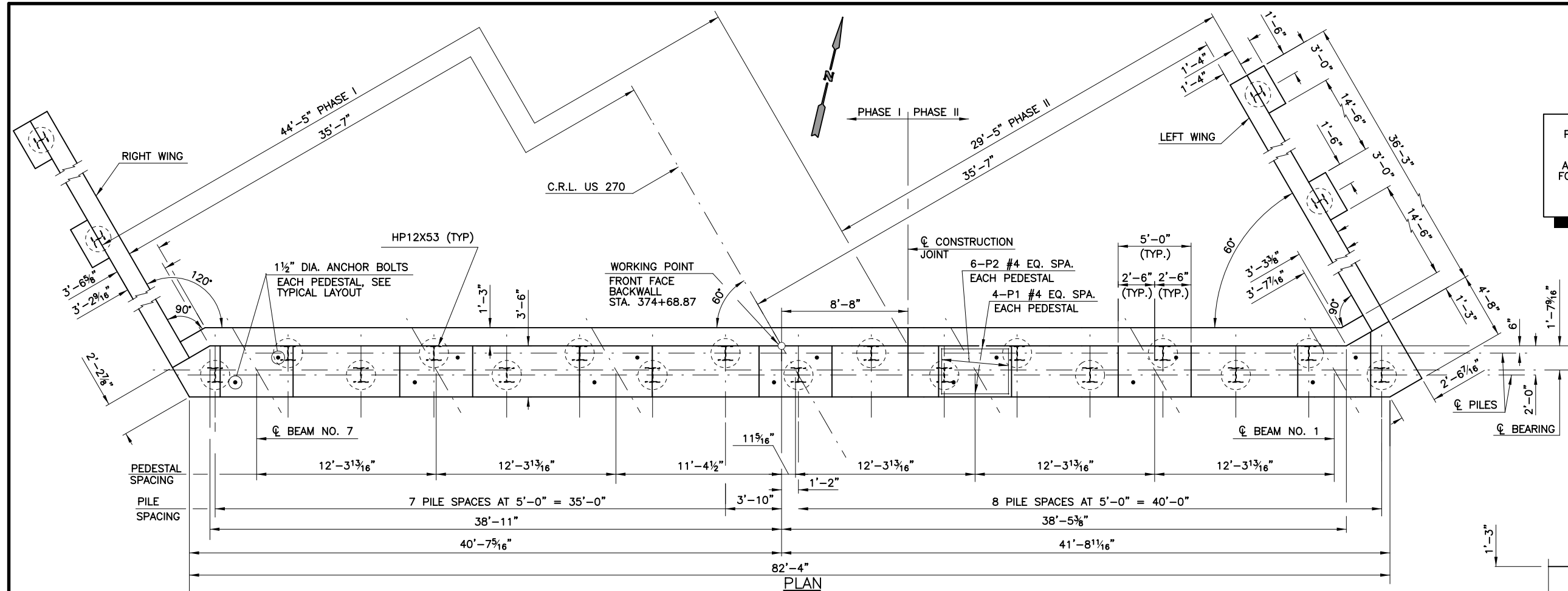
- 1 SET BOTTOM OF PIPE 3" ABOVE THE BOTTOM OF THE ABUTMENT AT THE LOW END.
- 2 GRADE LINE ASSUMED TO BE LOCATED 12" BELOW BOTTOM OF APPROACH SLAB FOR COMPUTING CLSM BACKFILL QUANTITY SHOWN ON PLANS. THE DEPARTMENT WILL PAY FOR CLSM BACKFILL IN ACCORDANCE WITH THE PLAN QUANTITY AND NO ADJUSTMENT WILL BE MADE FOR ACTUAL LOCATION OF GRADE LINE.

DESIGN:	DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN:	LRJ	06/18	BRIDGE "B"	
CHECKED:	DMN	06/18		
APPRVD:	DMN	06/18		
			SUBSTRUCTURE EXCAVATION	
			STATE JOB PIECE NO: 21006(07)	
			SHEET 1 OF 1	
			SHEET NO. B036	

2:43:17 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 cpydf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRFD03.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B037	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

PLACE ALL WT WING REINFORCING TIED TO ABUTMENT SEAT AND BACKWALL REINFORCING BEFORE PLACING ABUTMENT SEAT AND BACKWALL CONCRETE. FOR ADDITIONAL DETAILS AND BAR LIST, SEE ABUTMENT NO. 1 DETAILS (SHEET 2 OF 2).



② PEDESTAL HEIGHTS

BEAM NO.	TOP OF PED. ELEV.	HEIGHT
1	838.07	2"
2	838.41	6 1/8"
3	838.75	10 3/8"
4	839.08	1'-2 3/16"
5	838.99	1'-1 1/16"
6	838.90	11 13/16"
7	838.80	10 13/16"

- ① WT BARS ARE TYPICAL FOR BOTH WINGS
- ② BRIDGE SEAT ELEVATION AND PEDESTAL HEIGHTS TAKEN AT FRONT FACE OF BACKWALL

DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "B"	
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		

ABUTMENT NO. 1 DETAILS

SHEET 1 OF 2
SHEET NO. B037

STATE JOB PIECE NO: 21006(07)

6/5/2018 2:43:22 PM c:\p\w\ANSIB.tbl c:\p\w\ANSIB.plt
 Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.30.7 Bridge\21006BRBAD01.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B038	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

ABUTMENT NO. 1 BAR LIST-PHASE I

EPOXY COATED REINFORCING BARS

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
BH1	#4	11	BNT.	53'-9"	
BH2	#4	12	BNT.	54'-3"	
BH3	#9	2	STR.	47'-5"	
BH4	#9	2	STR.	50'-4"	
BH5	#9	6	STR.	49'-10" AVG	49'-8" TO 51'-0"
BH6	#4	1	STR.	52'-2"	
BH12	#4	30	BNT.	5'-1"	
BV1	#4	41	STR.	11'-6" AVG	11'-4" TO 11'-8"
BV2	#5	41	STR.	11'-6" AVG	11'-4" TO 11'-8"
BV3	#4	9	STR.	11'-6 1/2" AVG	11'-5" TO 11'-8"
BV4	#5	9	STR.	11'-6 1/2" AVG	11'-5" TO 11'-8"
BV5	#4	4	STR.	12'-5"	
P1	#4	16	BNT.	8'-8"	
P2	#4	24	BNT.	7'-2"	
S1	#5	50	BNT.	13'-11"	
SC	#4	2	BNT.	3'-5"	
WT1	#5	1	BNT.	10'-2"	
WT2	#5	3	BNT.	9'-0" AVG	6'-0" TO 12'-0"
WT3	#5	5	BNT.	15'-0"	
WT4	#5	18	BNT.	10'-6"	

WINGWALL-EPOXY COATED REINFORCING BARS

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
WH1	#5	2	BNT.	36'-5"	
WH2	#5	32	STR.	17'-0" AVG	5'-9" TO 28'-3"
WH3	#5	14	STR.	34'-8"	
WP1	#4	6	BNT.	8'-8"	
WP2	#4	4	STR.	1'-7"	
WP3	#4	4	STR.	2'-0 1/2" AVG	1'-7" TO 2'-6"
WV1	#4	14	STR.	3'-4"	
WV2	#4	58	STR.	7'-11" AVG	3'-6" TO 12'-4"

ABUTMENT NO. 1 BAR LIST-PHASE II

EPOXY COATED REINFORCING BARS

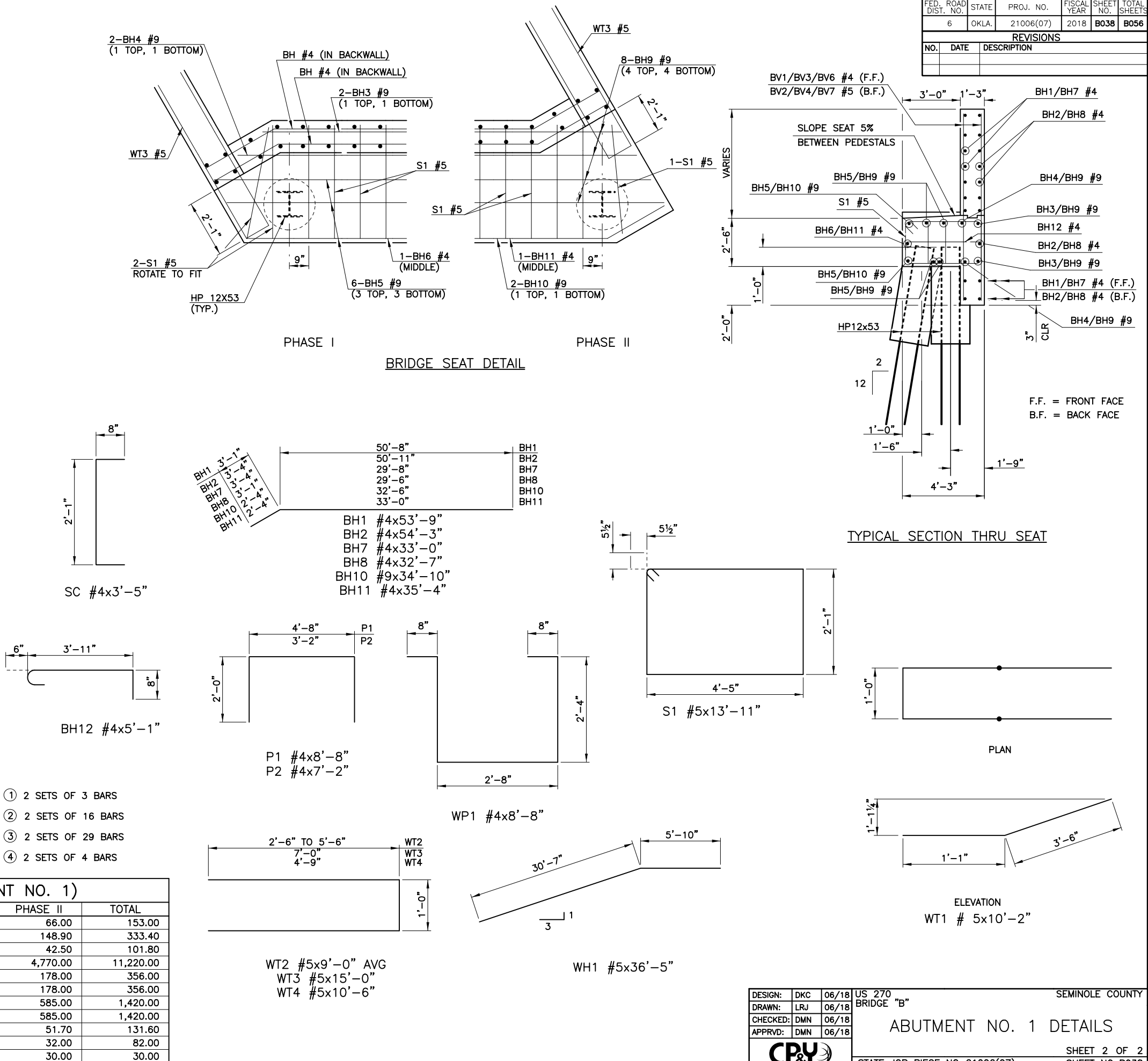
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
BH7	#4	11	BNT.	33'-0"	
BH8	#4	12	BNT.	32'-7"	
BH9	#9	8	STR.	33'-6" AVG	32'-7" TO 34'-5"
BH10	#9	2	BNT.	34'-10"	
BH11	#4	1	BNT.	35'-4"	
BH12	#4	21	BNT.	5'-1"	
BV6	#4	32	STR.	11'-0" AVG	10'-7" TO 11'-5"
BV7	#5	32	STR.	11'-0" AVG	10'-7" TO 11'-5"
BV8	#4	4	STR.	11'-8"	
P1	#4	12	BNT.	8'-8"	
P2	#4	18	BNT.	7'-2"	
S1	#5	34	BNT.	13'-11"	
SC	#4	2	BNT.	3'-5"	
WT1	#5	1	BNT.	10'-2"	
WT2	#5	3	BNT.	9'-0" AVG	6'-0" TO 12'-0"
WT3	#5	5	BNT.	15'-0"	
WT4	#5	17	BNT.	10'-6"	

WINGWALL-EPOXY COATED REINFORCING BARS

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
WH1	#5	2	BNT.	36'-5"	
WH2	#5	32	STR.	17'-0" AVG	5'-9" TO 28'-3"
WH3	#5	12	STR.	34'-8"	
WP1	#4	6	BNT.	8'-8"	
WP2	#4	4	STR.	1'-7"	
WP3	#4	4	STR.	2'-0 1/2" AVG	1'-7" TO 2'-6"
WV3	#4	14	STR.	2'-7"	
WV4	#4	58	STR.	7'-2 1/2" AVG	2'-10" TO 11'-7"

SUMMARY OF QUANTITIES (ABUTMENT NO. 1)

ITEM	UNIT	PHASE I	PHASE II	TOTAL
SUBSTRUCTURE EXCAVATION COMMON	CY	87.00	66.00	153.00
CSLM BACKFILL	CY	184.50	148.90	333.40
CLASS A CONCRETE	CY	59.30	42.50	101.80
EPOXY COATED REINFORCING STEEL	LB	6,450.00	4,770.00	11,220.00
PILES, FURNISHED (HP10x42)	LF	178.00	178.00	356.00
PILES, DRIVEN (HP10x42)	LF	178.00	178.00	356.00
PILES, FURNISHED (HP12x53)	LF	835.00	585.00	1,420.00
PILES, DRIVEN (HP12x53)	LF	835.00	585.00	1,420.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	79.90	51.70	131.60
6" PERFORATED PIPE UNDERDRAIN ROUND	LF	50.00	32.00	82.00
6" NON-PERF. PIPE UNDERDRAIN ROUND	LF		30.00	30.00



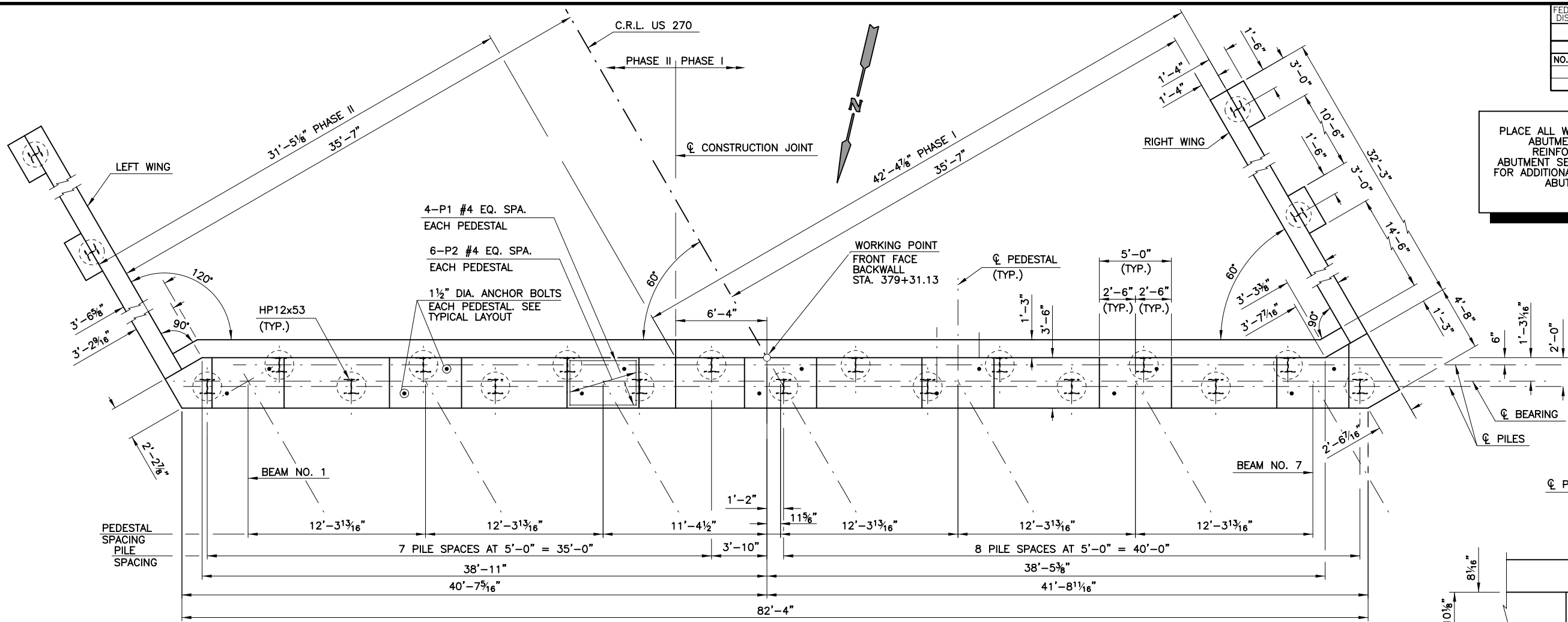
2:43:27 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRBADD04.dgn
 bmcanelly
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT113446.00\8.00

DESIGN: DKC	06/18	US 270 BRIDGE "B" ABUTMENT NO. 1 DETAILS STATE JOB PIECE NO: 21006(07)	SEMINOLE COUNTY SHEET 2 OF 2 SHEET NO. B038
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		

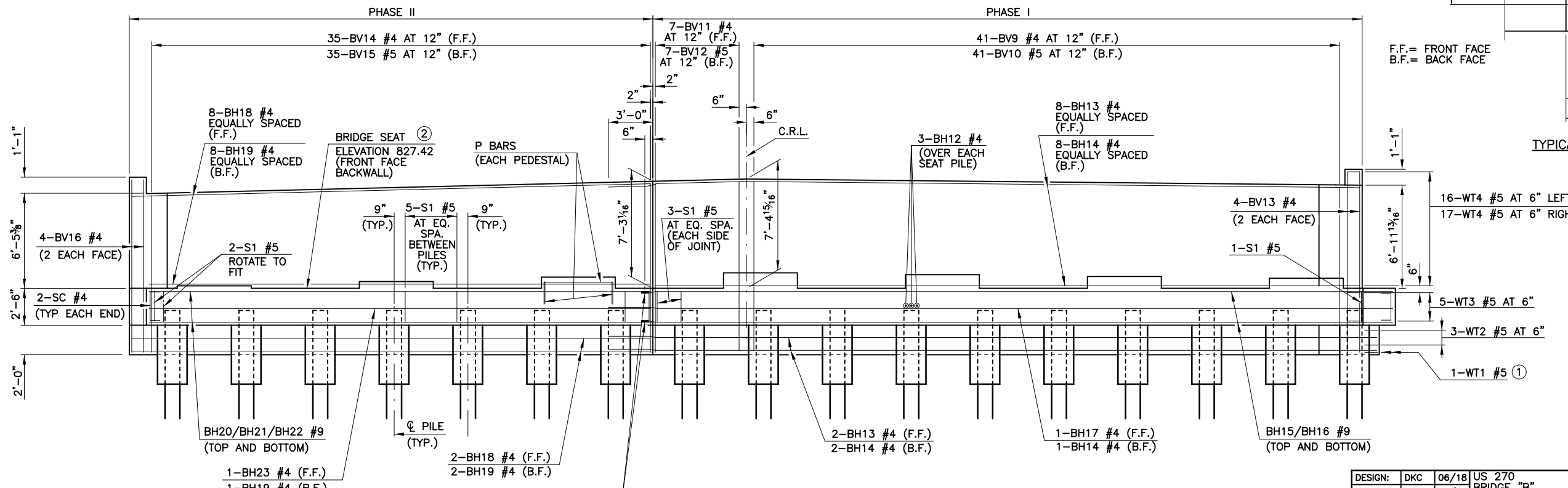
CP&Y

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B039	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

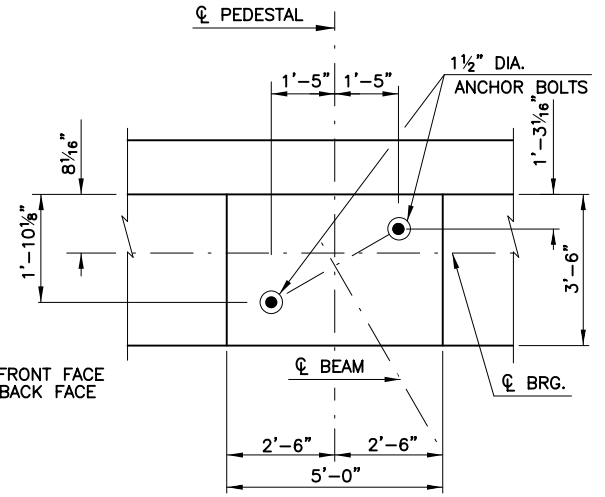
PLACE ALL WT WING REINFORCING TIED TO ABUTMENT SEAT AND BACKWALL REINFORCING BEFORE PLACING ABUTMENT SEAT AND BACKWALL CONCRETE. FOR ADDITIONAL DETAILS AND BAR LIST, SEE ABUTMENT NO. 2 DETAILS (SHEET 2 OF 2).



PLAN



ELEVATION



TYPICAL ANCHOR BOLT LAYOUT

BEAM NO.	TOP OF PED. ELEV.	HEIGHT
1	827.59	2"
2	827.88	5 1/2"
3	828.18	9 1/8"
4	828.48	1'-0 5/8"
5	828.35	11 1/8"
6	828.23	9 1/8"
7	828.11	8 3/8"

- ① WT BARS ARE TYPICAL FOR BOTH WINGS
- ② BRIDGE SEAT ELEVATION AND PEDESTAL HEIGHTS TAKEN AT FRONT FACE OF BACKWALL

DESIGN:	DKC	06/18
DRAWN:	LRJ	06/18
CHECKED:	DMN	06/18
APPRVD:	DMN	06/18

US 270 BRIDGE "B"
 SEMINOLE COUNTY
ABUTMENT NO. 2 DETAILS
 STATE JOB PIECE NO: 21006(07)
 SHEET 1 OF 2
 SHEET NO. B039

2:43:32 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRB\AD02.dgn

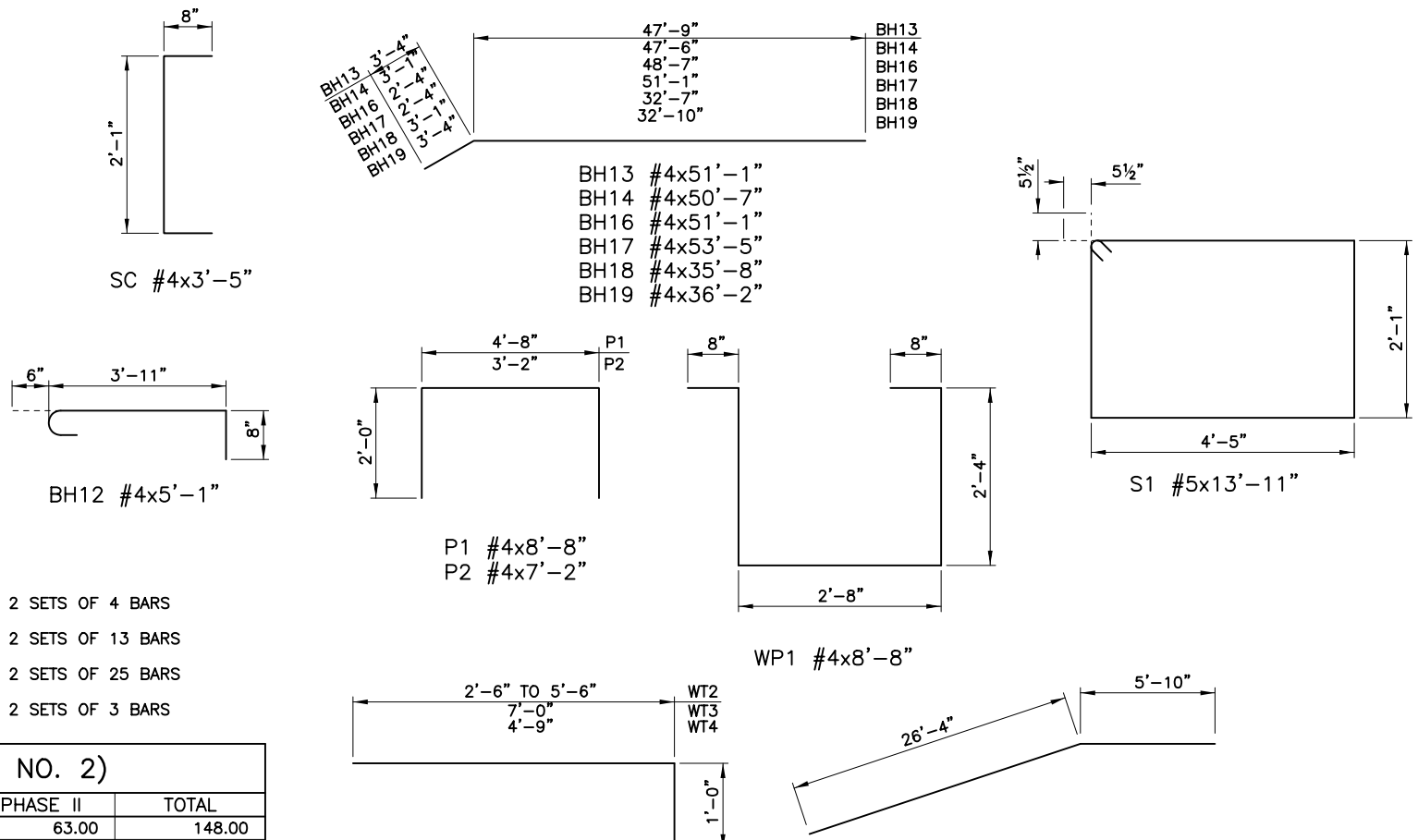
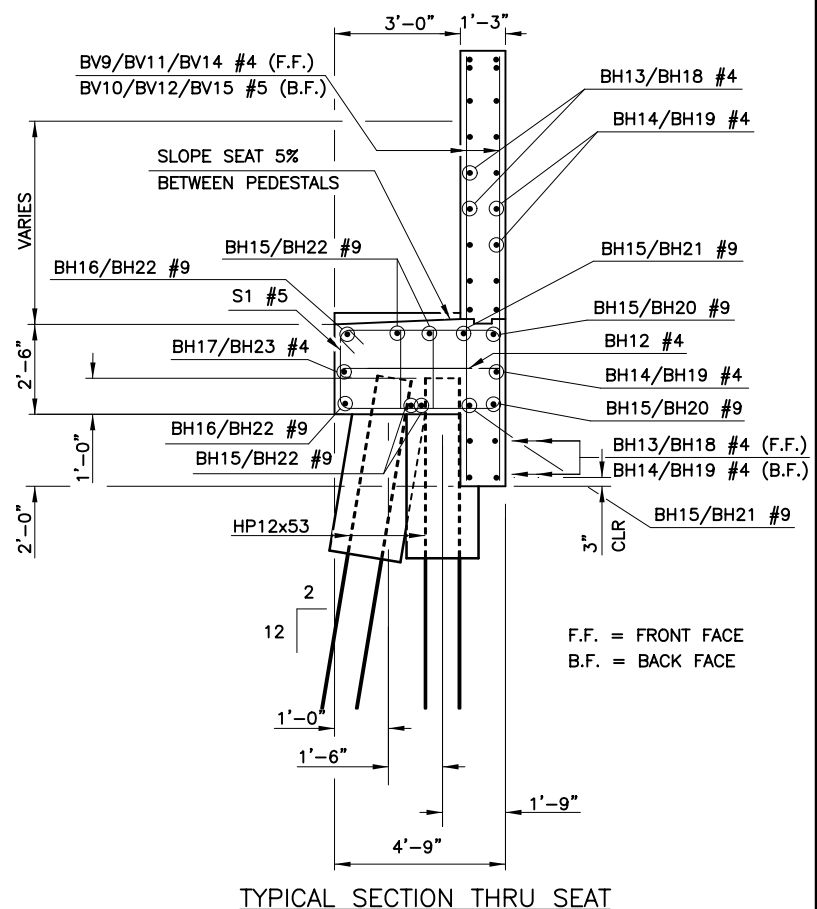
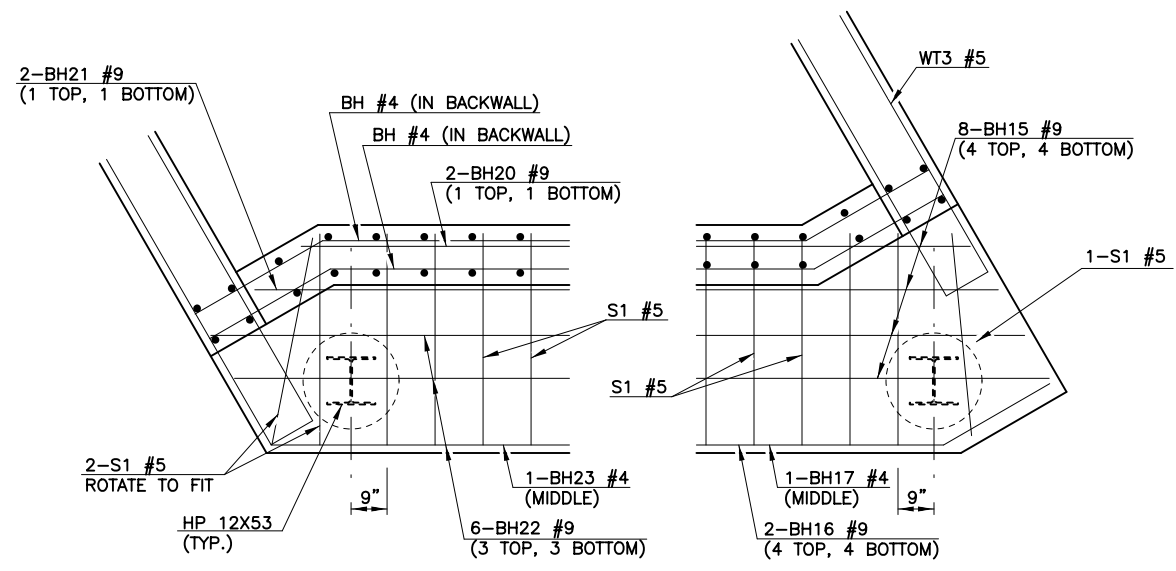
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B040	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

ABUTMENT NO. 2 BAR LIST-PHASE I					
EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
BH12	#4	30	BNT.	5'-1"	
BH13	#4	10	BNT.	51'-1"	
BH14	#4	11	BNT.	50'-7"	
BH15	#9	8	STR.	49'-7" AVG	48'-8" TO 50'-6"
BH16	#9	2	STR.	51'-1"	
BH17	#4	1	STR.	53'-5"	
BV9	#4	41	STR.	11'-3" AVG	11'-1" TO 11'-5"
BV10	#5	41	STR.	11'-3" AVG	11'-1" TO 11'-5"
BV11	#4	7	STR.	11'-4½" AVG	11'-4" TO 11'-5"
BV12	#5	7	STR.	11'-4½" AVG	11'-4" TO 11'-5"
BV13	#4	4	STR.	12'-1"	
P1	#4	16	BNT.	8'-8"	
P2	#4	24	BNT.	7'-2"	
S1	#5	49	BNT.	13'-11"	
SC	#4	2	BNT.	3'-5"	
WT1	#5	1	BNT.	10'-2"	
WT2	#5	3	BNT.	9'-0" AVG	6'-0" TO 12'-0"
WT3	#5	5	BNT.	15'-0"	
WT4	#5	16	BNT.	10'-6"	

ABUTMENT NO. 2 BAR LIST-PHASE II					
EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
WH4	#5	2	BNT.	32'-2"	
WH5	#5	26	STR.	15'-9" AVG	7'-9" TO 23'-9"
WH6	#5	18	STR.	30'-8"	
WP1	#4	6	BNT.	8'-8"	
WP2	#4	4	STR.	1'-7"	
WP3	#4	4	STR.	2'-0½" AVG	1'-7" TO 2'-6"
WV5	#4	14	STR.	3'-4"	
WV6	#4	50	STR.	7'-10½" AVG	3'-8" TO 12'-1"

WINGWALL-EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
WH4	#5	2	BNT.	32'-2"	
WH5	#5	26	STR.	15'-9" AVG	7'-9" TO 23'-9"
WH6	#5	18	STR.	30'-8"	
WP1	#4	6	BNT.	8'-8"	
WP2	#4	4	STR.	1'-7"	
WP3	#4	4	STR.	2'-0½" AVG	1'-7" TO 2'-6"
WV7	#4	14	STR.	2'-11"	
WV8	#4	50	STR.	7'-4½"	3'-3" TO 11'-6"

SUMMARY OF QUANTITIES (ABUTMENT NO. 2)				
ITEM	UNIT	PHASE I	PHASE II	TOTAL
SUBSTRUCTURE EXCAVATION COMMON	CY	85.00	63.00	148.00
CSLM BACKFILL	CY	199.30	122.60	321.90
CLASS A CONCRETE	CY	56.60	42.10	98.70
EPOXY COATED REINFORCING STEEL	LB	6,130.00	4,740.00	10,870.00
PILES, FURNISHED (HP10x42)	LF	189.00	189.00	378.00
PILES, DRIVEN (HP10x42)	LF	189.00	189.00	378.00
PILES, FURNISHED (HP12x53)	LF	900.00	631.00	1,531.00
PILES, DRIVEN (HP12x53)	LF	900.00	631.00	1,531.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	76.70	52.10	128.80
6" PERFORATED PIPE UNDERDRAIN ROUND	LF	47.00	35.00	82.00
6" NON-PERF. PIPE UNDERDRAIN ROUND	LF		30.00	30.00



- ① 2 SETS OF 4 BARS
- ② 2 SETS OF 13 BARS
- ③ 2 SETS OF 25 BARS
- ④ 2 SETS OF 3 BARS

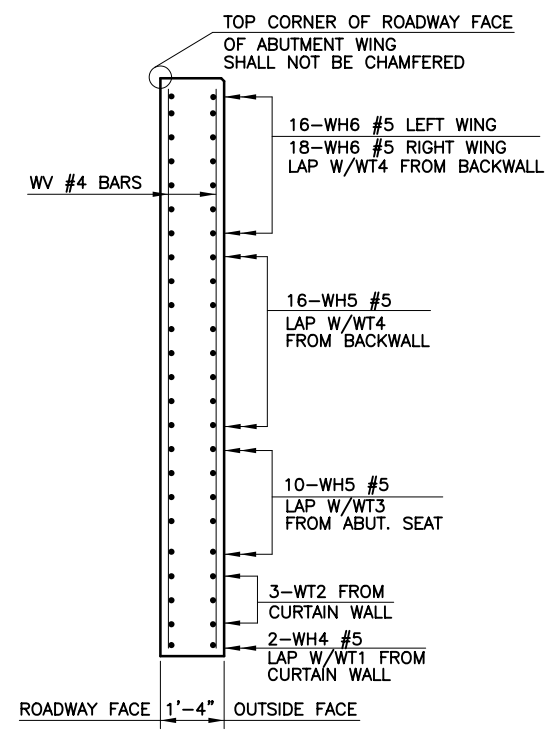
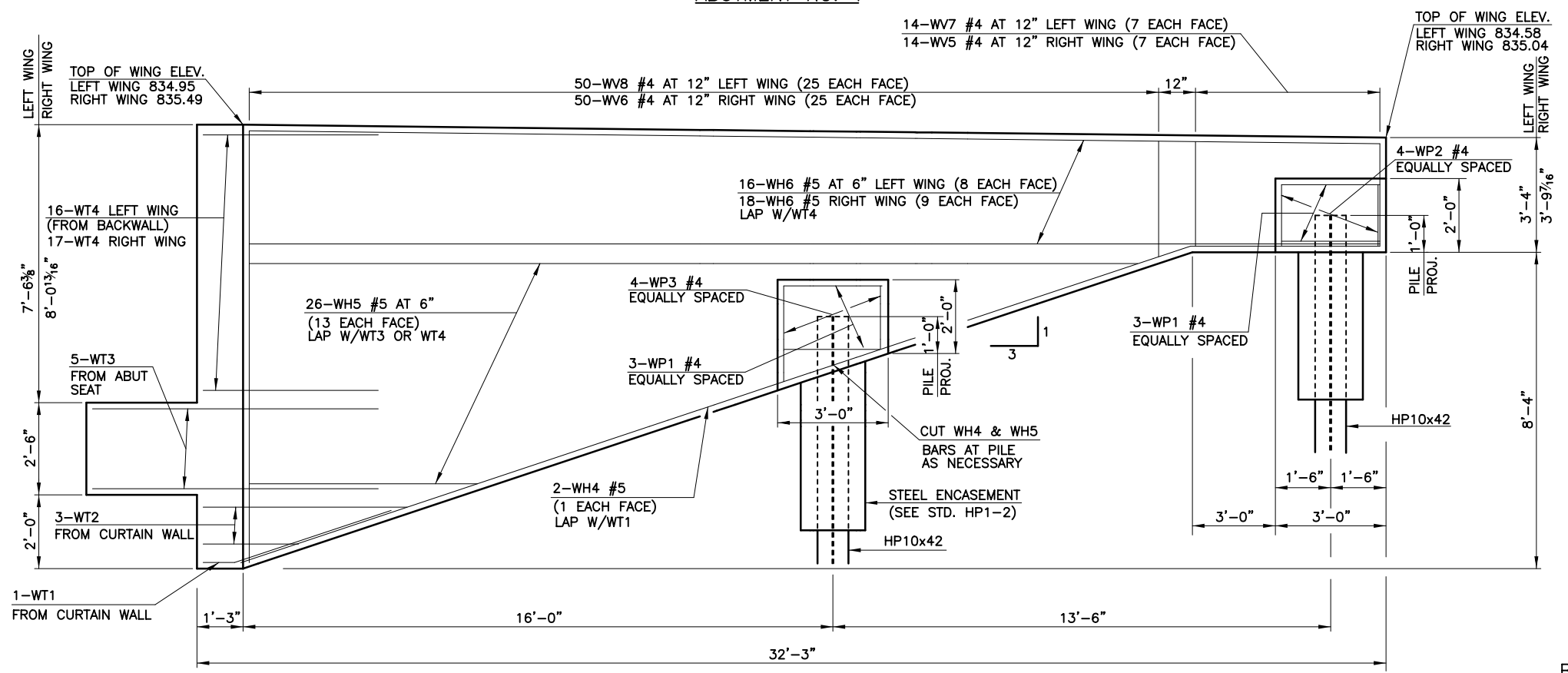
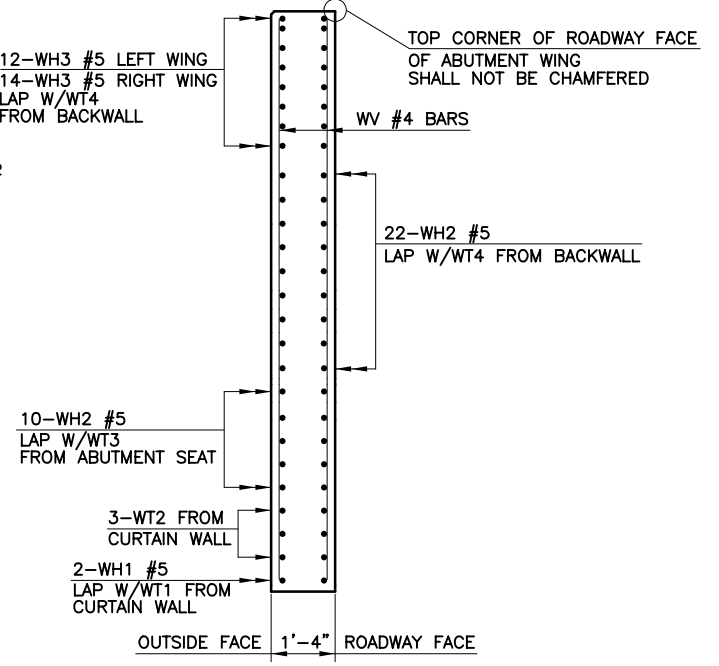
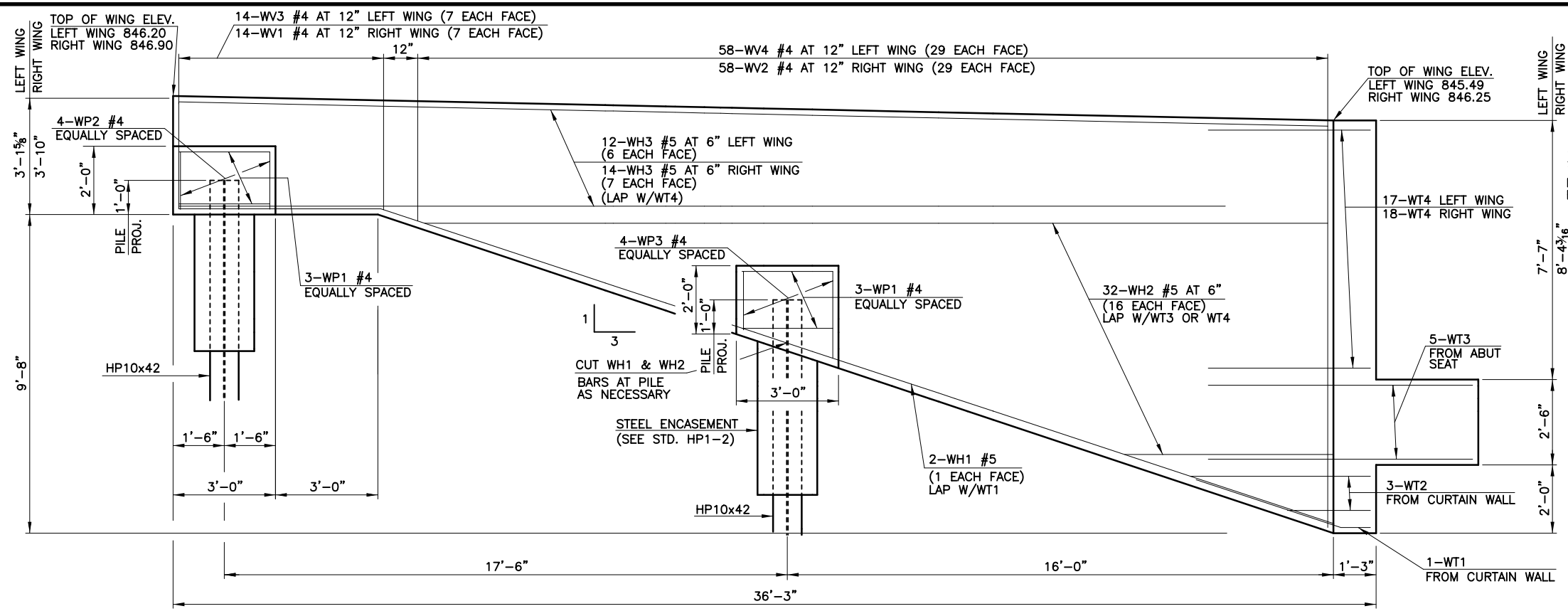
DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "B"	
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		

ABUTMENT NO. 2 DETAILS

SHEET 2 OF 2
STATE JOB PIECE NO: 21006(07)
SHEET NO. B040

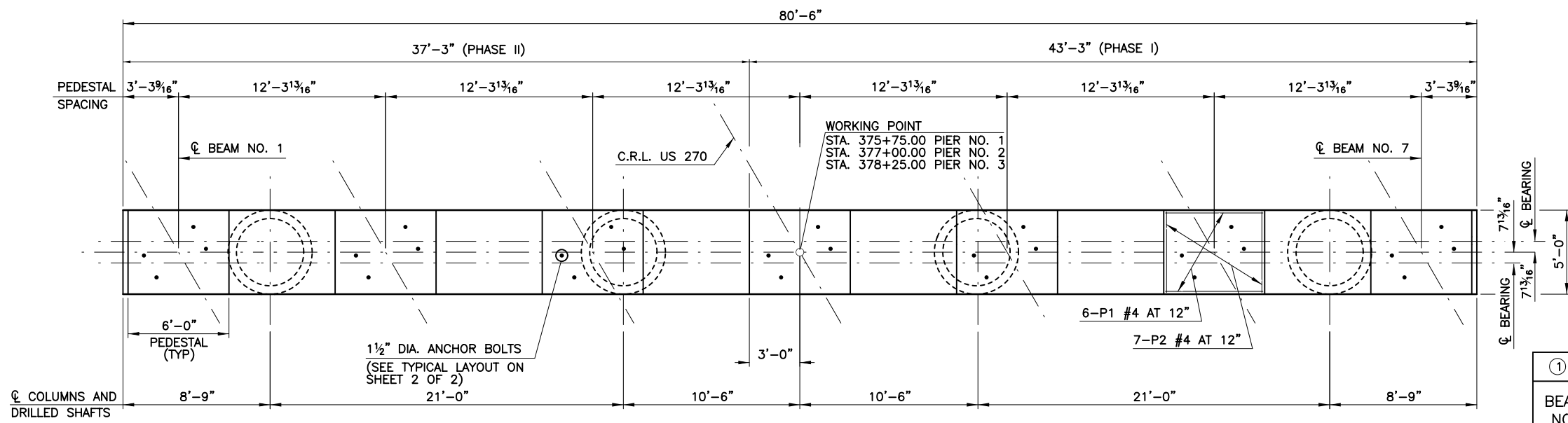
2:43:37 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT1.3446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRBADD05.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B041	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

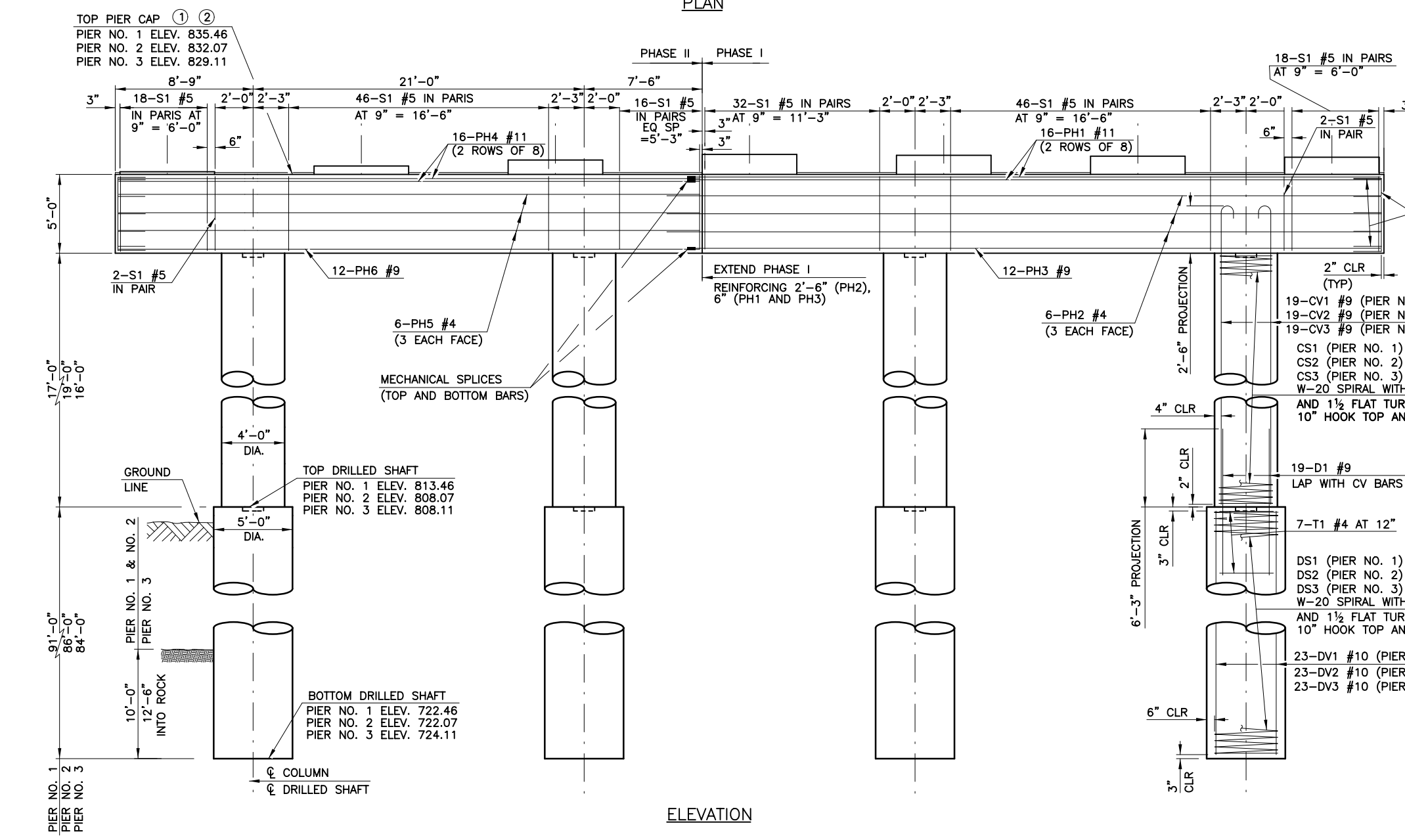


DESIGN: DKC 06/18	US 270 BRIDGE "B"	SEMINOLE COUNTY
DRAWN: LRJ 06/18		
CHECKED: DMN 06/18		
APPRVD: DMN 06/18		
CP&Y		SHEET 1 OF 1
STATE JOB PIECE NO: 21006(07)		SHEET NO. B041

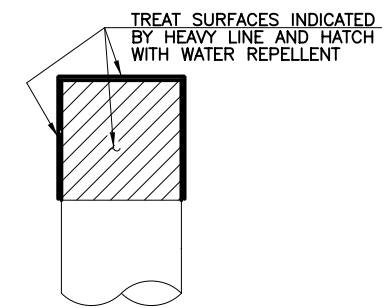
2:43:42 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRBADO3.dgn



PLAN



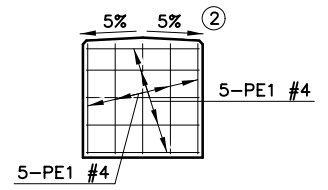
ELEVATION



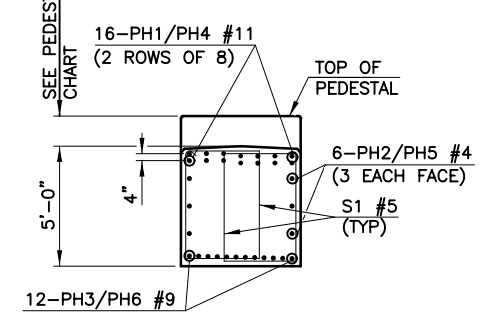
WATER REPELLENT TREATMENT DETAIL

PEDESTAL HEIGHTS						
BEAM NO.	PIER NO. 1		PIER NO. 2		PIER NO. 3	
	TOP OF PED. ELEV.	HEIGHT	TOP OF PED. ELEV.	HEIGHT	TOP OF PED. ELEV.	HEIGHT
1	835.63	2"	832.24	2"	829.27	2"
2	836.00	6 7/16"	832.62	6 9/16"	829.61	6"
3	836.37	10 15/16"	833.00	11 1/8"	829.94	10 1/2"
4	836.74	1'-3 3/16"	833.38	1'-3 3/4"	830.28	1'-2 1/8"
5	836.68	1'-2 5/8"	833.34	1'-3 3/16"	830.20	1'-1 1/8"
6	836.62	1'-1 7/8"	833.29	1'-2 5/8"	830.12	1'-0 1/8"
7	836.56	1'-1 1/8"	833.25	1'-2 1/8"	830.04	11 1/8"

- ① PIER CAP ELEVATION, PEDESTAL ELEVATIONS, AND PEDESTAL HEIGHTS ARE ESTABLISHED AT CENTER OF PIER CAP.
- ② SLOPE PIER CAP 5% EACH DIRECTION FROM CENTER BETWEEN PEDESTALS.



END VIEW



TYPICAL SECTION THROUGH CAP

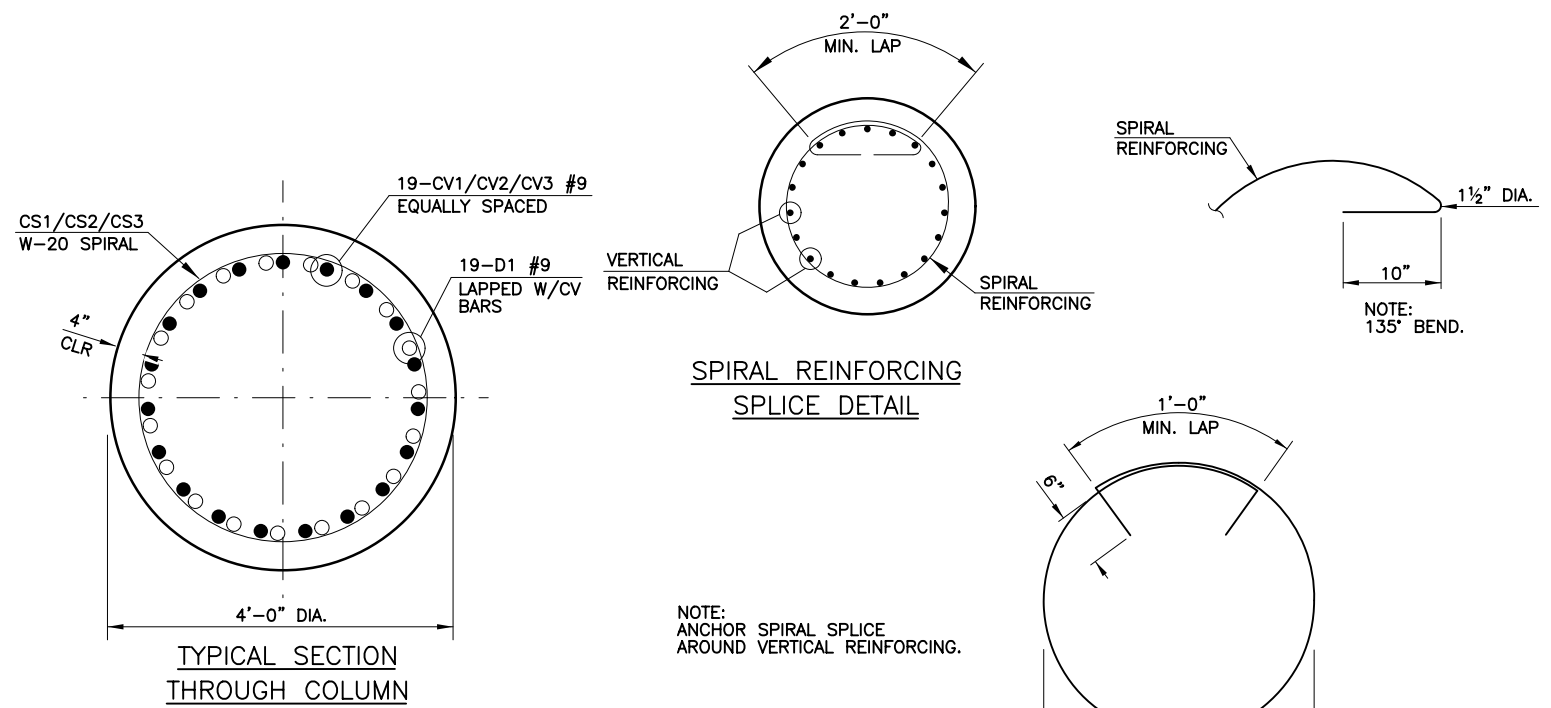
DESIGN: DKC 06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ 06/18	BRIDGE "B"	
CHECKED: DMN 06/18		
APPRVD: DMN 06/18		

PIER DETAILS

SHEET 1 OF 2
SHEET NO. B042

STATE JOB PIECE NO: 21006(07)

2:43:47 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpydf_ANSIB.plt
 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRPD01.dgn



PIER CAP AND COLUMN BAR LIST

MARK	SIZE	FORM	LENGTH	QUANTITY							
				PIER NO. 1		PIER NO. 2		PIER NO. 3			
				PHASE I	PHASE II	PHASE I	PHASE II	PHASE I	PHASE II		
CV1	#9	BNT.	20'-8"	38	38						
CV2	#9	BNT.	22'-8"			38	38				
CV3	#9	BNT.	19'-8"							38	38
P1	#4	BNT.	9'-8"	24	18	24	18	24	18		
P2	#4	BNT.	8'-8"	28	21	28	21	28	21		
PE1	#4	BNT.	6'-8"	10	10	10	10	10	10		
PH1	#11	BNT.	45'-4"	16		16		16			
PH2	#4	STR.	45'-7"	6		6		6			
PH3	#9	STR.	43'-7"	12		12		12			
PH4	#11	BNT.	37'-4"		16		16			16	
PH5	#4	STR.	37'-1"		6		6			6	
PH6	#9	STR.	36'-7"		12		12			12	
S1	#5	BNT.	16'-7"	98	82	98	82	98	82	98	82

PLAIN REINFORCING

MARK	SIZE	FORM	LENGTH	QUANTITY							
				PIER NO. 1		PIER NO. 2		PIER NO. 3			
				PHASE I	PHASE II	PHASE I	PHASE II	PHASE I	PHASE II		
CS1	W20	BNT.	384'-4"	2	2						
CS2	W20	BNT.	426'-8"			2	2				
CS3	W20	BNT.	363'-2"					2	2		

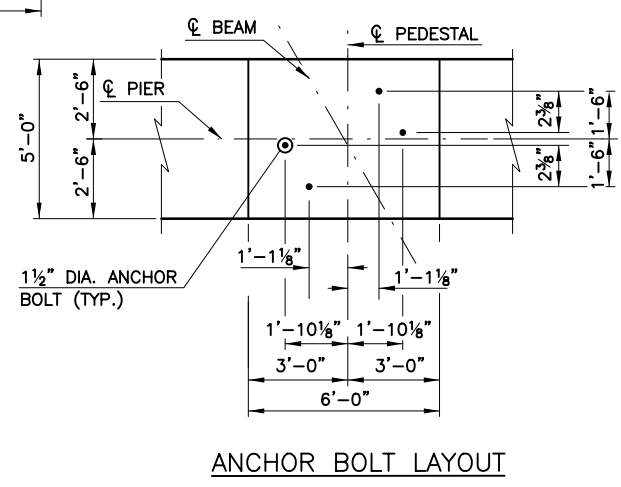
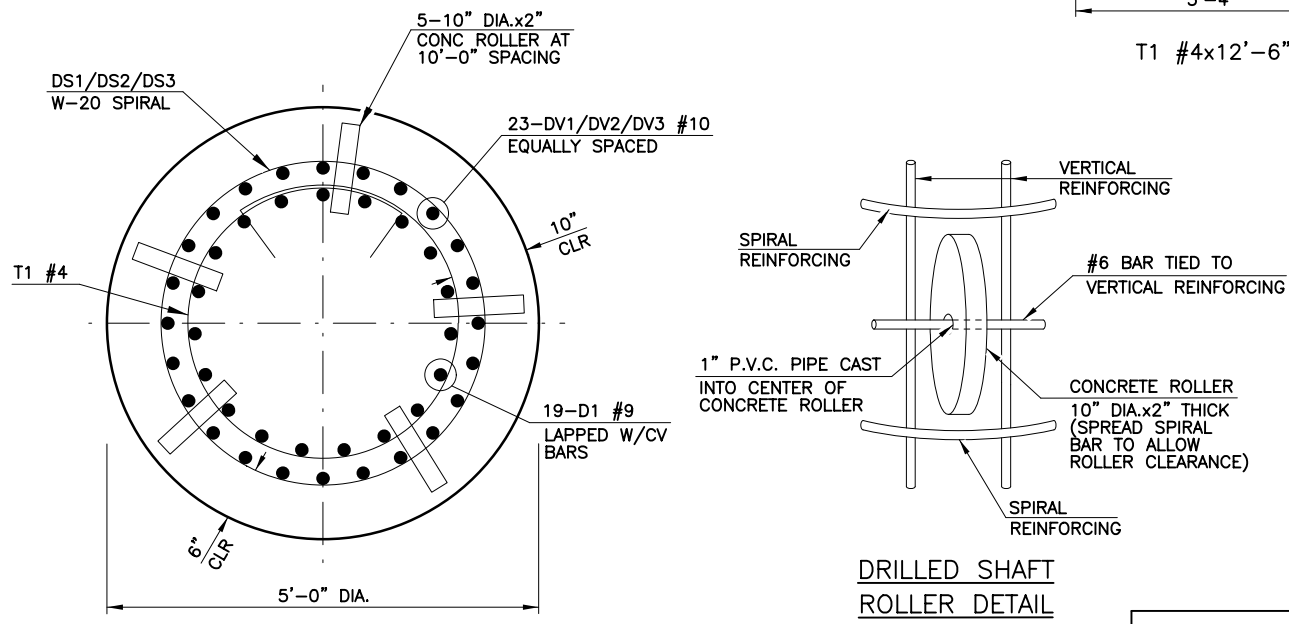
DRILLED SHAFT BAR LIST

MARK	SIZE	FORM	LENGTH	QUANTITY							
				PIER NO. 1		PIER NO. 2		PIER NO. 3			
				PHASE I	PHASE II	PHASE I	PHASE II	PHASE I	PHASE II		
D1	#9	STR.	12'-6"	38	38	38	38	38	38		

PLAIN REINFORCING

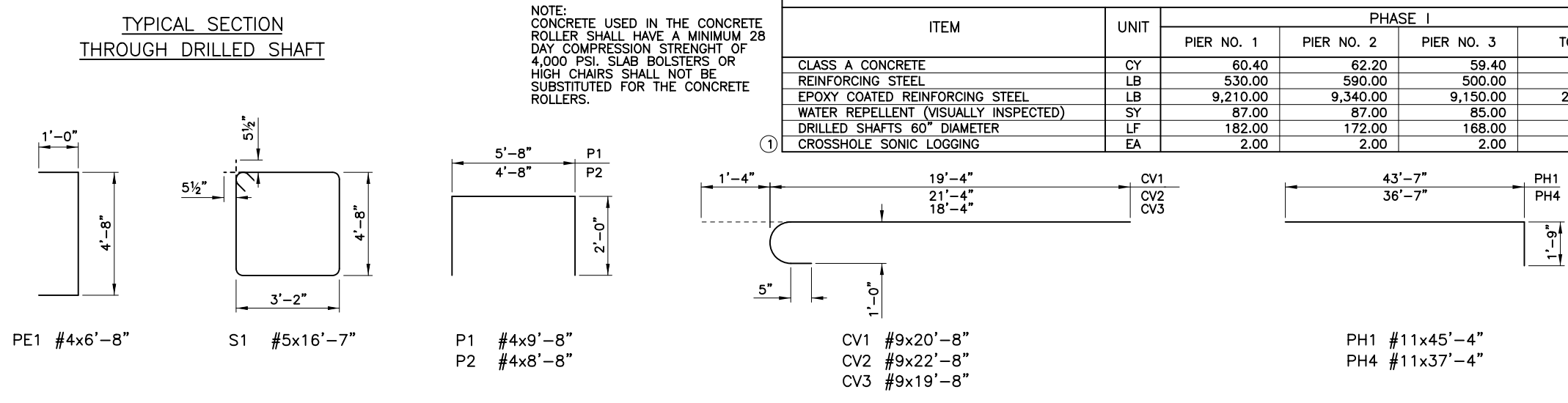
MARK	SIZE	FORM	LENGTH	QUANTITY							
				PIER NO. 1		PIER NO. 2		PIER NO. 3			
				PHASE I	PHASE II	PHASE I	PHASE II	PHASE I	PHASE II		
DS1	W20	BNT.	2,331'-7"	2	2						
DS2	W20	BNT.	2,205'-0"			2	2				
DS3	W20	BNT.	2,154'-4"					2	2		
DV1	#10	STR.	96'-6"	46	46						
DV2	#10	STR.	91'-6"			46	46				
DV3	#10	STR.	89'-6"					46	46		
T1	#4	BNT.	12'-6"	14	14	14	14	14	14	14	14

① INCLUDES ONE 6'-0" LAP



SUMMARY OF PIER QUANTITIES

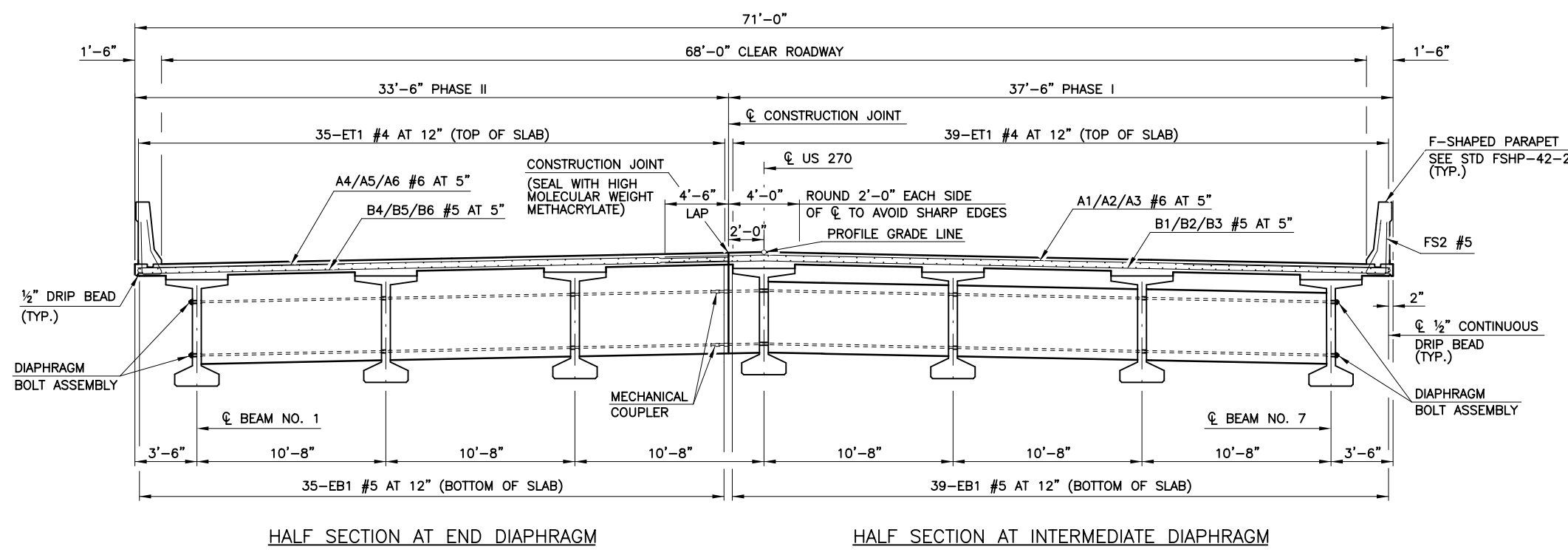
ITEM	UNIT	PHASE I				PHASE II			
		PIER NO. 1	PIER NO. 2	PIER NO. 3	TOTAL	PIER NO. 1	PIER NO. 2	PIER NO. 3	TOTAL
		CLASS A CONCRETE	CY	60.40	62.20	59.40	182.00	51.80	53.70
REINFORCING STEEL	LB	530.00	590.00	500.00	1,620.00	530.00	590.00	500.00	1,620.00
EPOXY COATED REINFORCING STEEL	LB	9,210.00	9,340.00	9,150.00	27,700.00	7,860.00	7,980.00	7,790.00	23,630.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	87.00	87.00	85.00	259.00	68.00	69.00	69.00	206.00
DRILLED SHAFTS 60" DIAMETER	LF	182.00	172.00	168.00	522.00	182.00	172.00	168.00	522.00
CROSSHOLE SONIC LOGGING	EA	2.00	2.00	2.00	6.00	2.00	2.00	2.00	6.00



① CSL TUBES SHALL EXTEND TO 3" CLEARANCE FROM BOTTOM OF DRILLED SHAFT.

2:43:52 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446\00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRPD03.dgn
 bmcanelly

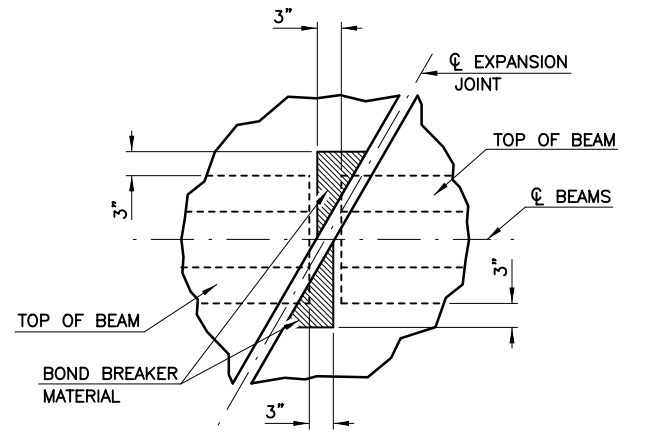
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B044	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



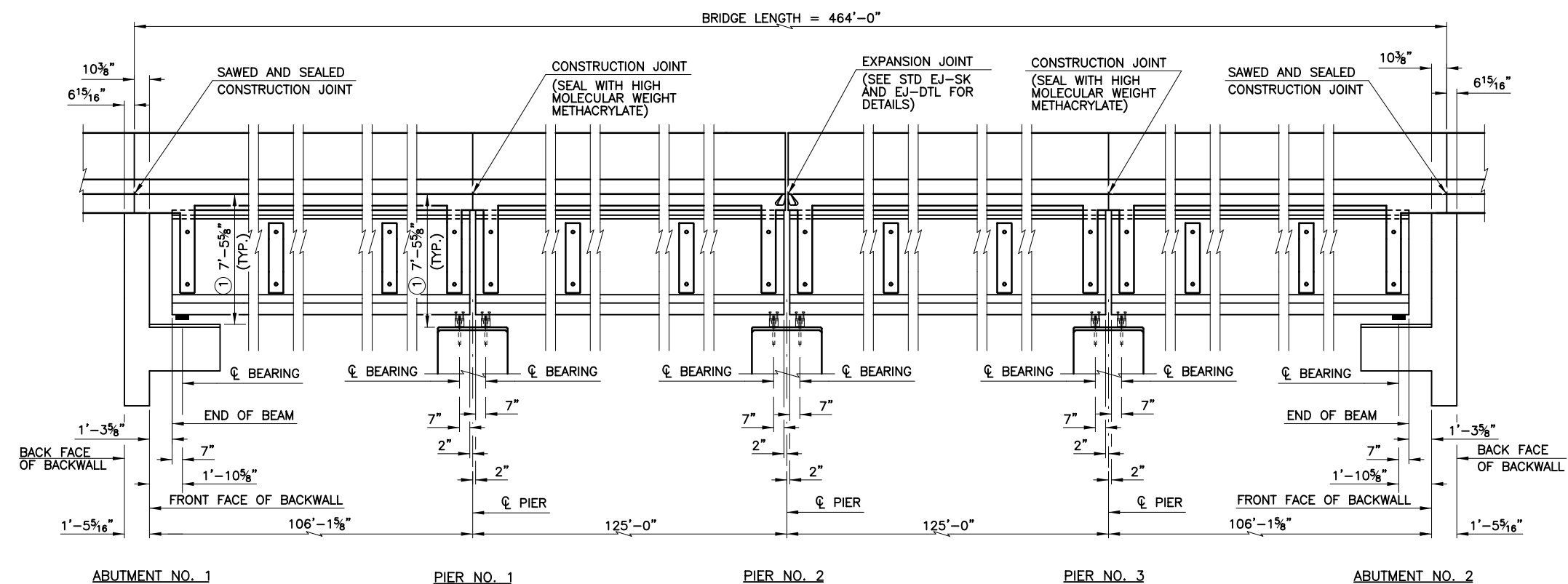
HALF SECTION AT END DIAPHRAGM

HALF SECTION AT INTERMEDIATE DIAPHRAGM

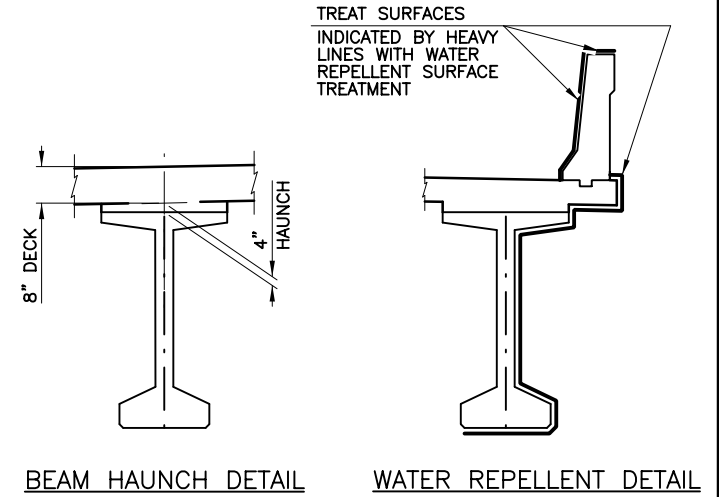
TYPICAL SECTION



DETAIL OF BOND BREAK AT BEAM CORNER



LONGITUDINAL SECTION



BEAM HAUNCH DETAIL

WATER REPELLENT DETAIL

NOTE:
 PLAN QUANTITIES FOR CLASS AA CONCRETE INCLUDE BEAM HAUNCHES. THE HAUNCH HEIGHT SHOWN IS THE THEORETICAL HAUNCH HEIGHT AT THE CENTERLINE BEARING ONLY, MEASURED FROM THE BOTTOM OF THE DECK SLAB TO THE TOP OF THE BEAM, AND VARIES ACROSS THE SPAN. DETERMINE THE ACTUAL HAUNCH HEIGHT (ACCOUNTING FOR BEAM CAMBER, DEAD LOAD DEFLECTION AND ROADWAY GRADE) AFTER ERECTION OF THE BEAMS AND SUBMIT TO THE ENGINEER FOR APPROVAL. THE ENGINEER WILL NOT MEASURE DIFFERENCE BETWEEN THE THEORETICAL AND THE ACTUAL HAUNCH HEIGHTS FOR PAYMENT.

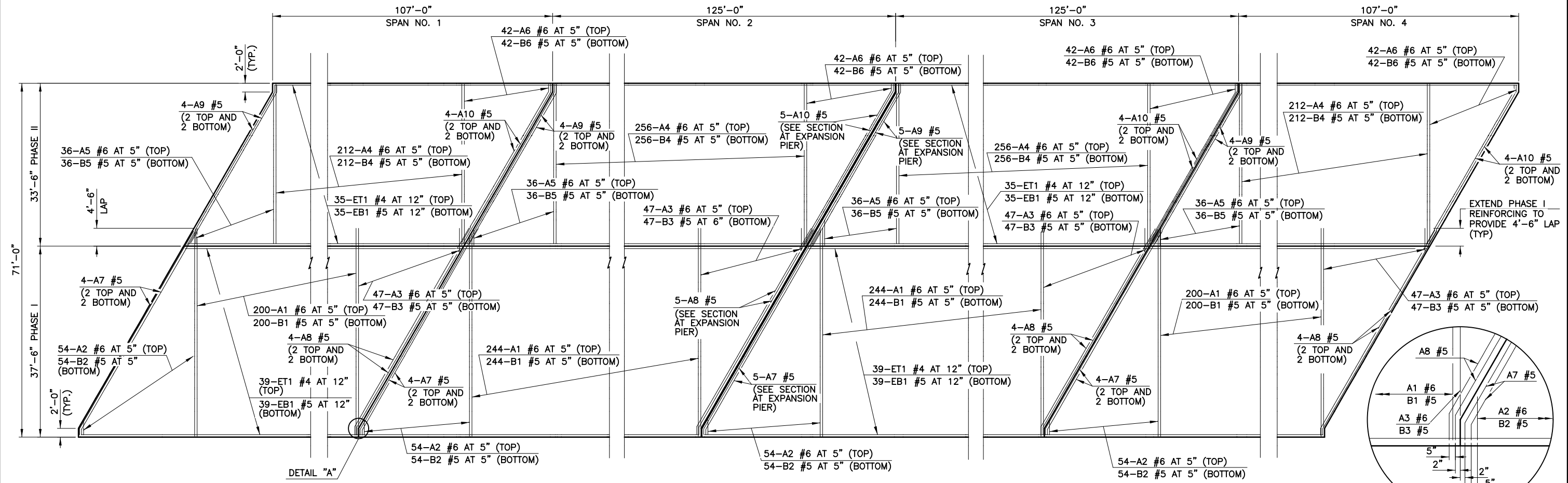
① DIMENSION IS FROM TOP OF DECK SLAB TO BOTTOM OF BEARING ASSEMBLY AT CL BEARING

DO NOT PLACE THE CONCRETE FOR THE DECK SLAB OR APPLY OTHER MASSIVE LOADS TO THE BEAMS OR DIAPHRAGMS UNTIL THE CONCRETE IN THE DIAPHRAGMS HAS BEEN IN PLACE A MINIMUM OF 10 DAYS OR AT THE DISCRETION OF THE ENGINEER. THE ENGINEER MAY APPROVE SHORTENED TIME IF THE BEAM AND DIAPHRAGM CONCRETE HAS ATTAINED 80% OF THE SPECIFIED COMPRESSIVE STRENGTH.

DESIGN:	DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN:	LRJ	06/18	BRIDGE "B"	
CHECKED:	DMN	06/18		
APPRVD:	DMN	06/18		
CP&Y			SUPERSTRUCTURE DETAILS	
			STATE JOB PIECE NO: 21006(07)	SHEET 1 OF 5
				SHEET NO. B044

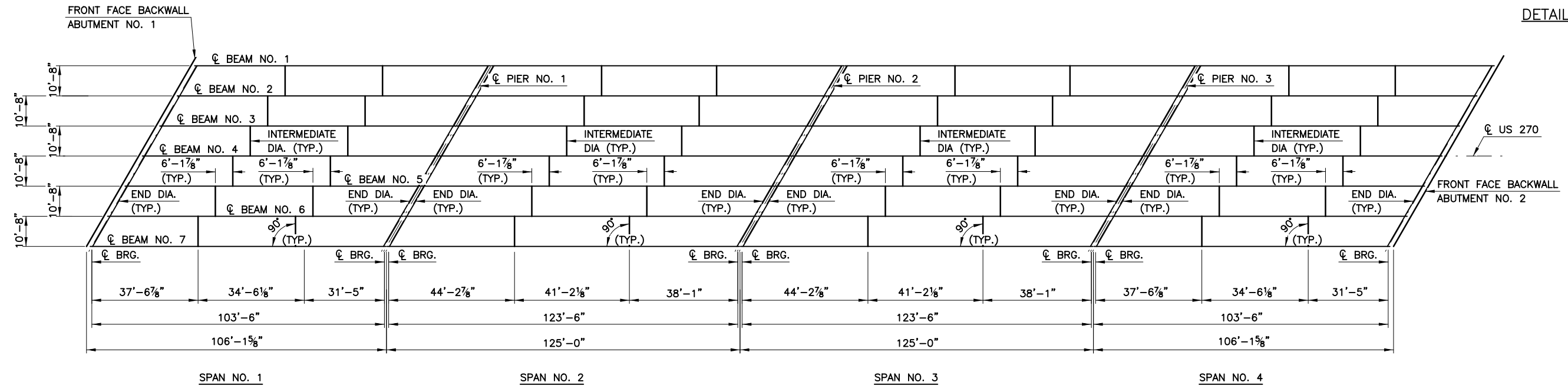
2:43:57 PM
 6/5/2018
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 Plans and Drawings/8.00
 Bridge/21006BRB01.dgn
 bmcanelly
 p:\Active Projects\ODOT13446.00\8.00

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B045	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



SLAB REINFORCING LAYOUT

DETAIL "A"

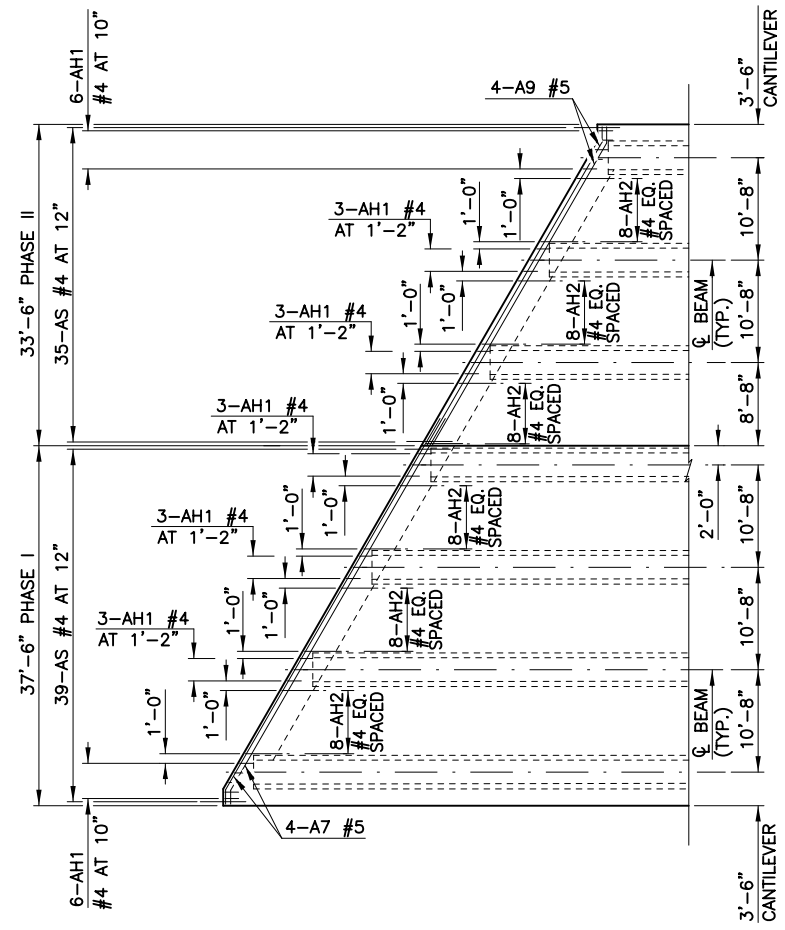


FRAMING PLAN

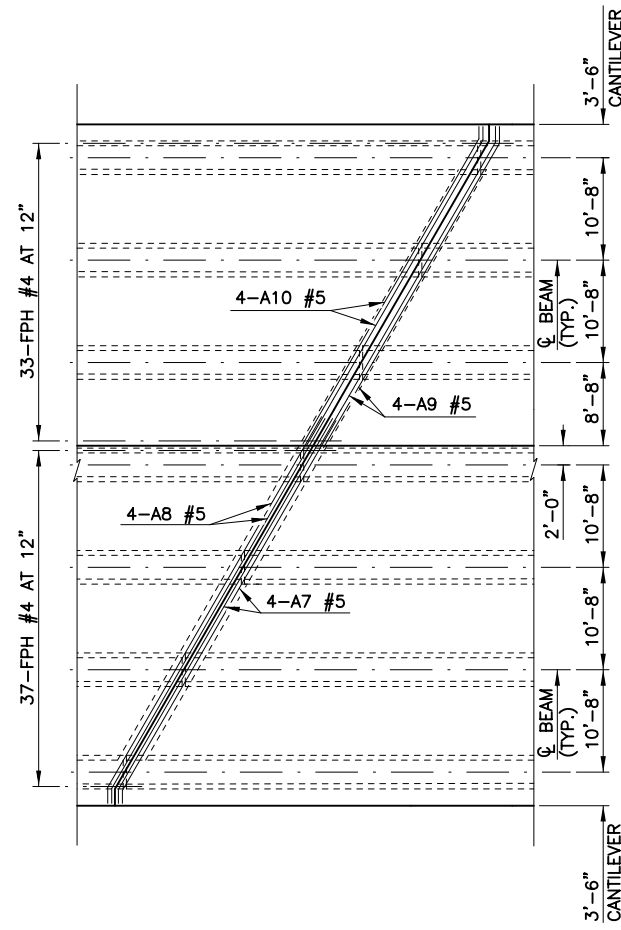
DESIGN: DKC	06/18	US 270 BRIDGE "B"	SEMINOLE COUNTY
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
CP&Y		STATE JOB PIECE NO: 21006(07)	
		SUPERSTRUCTURE DETAILS	
		SHEET 2 OF 5 SHEET NO. B045	

2:44:02 PM
 cpy:pdf_ANSIB.plt
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRB501.dgn

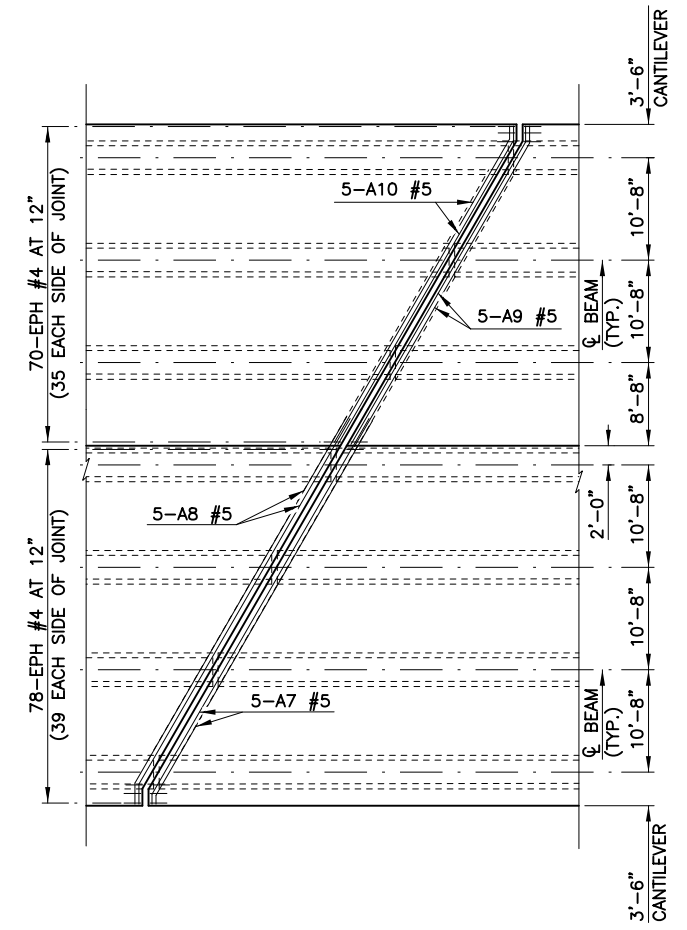
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B046	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



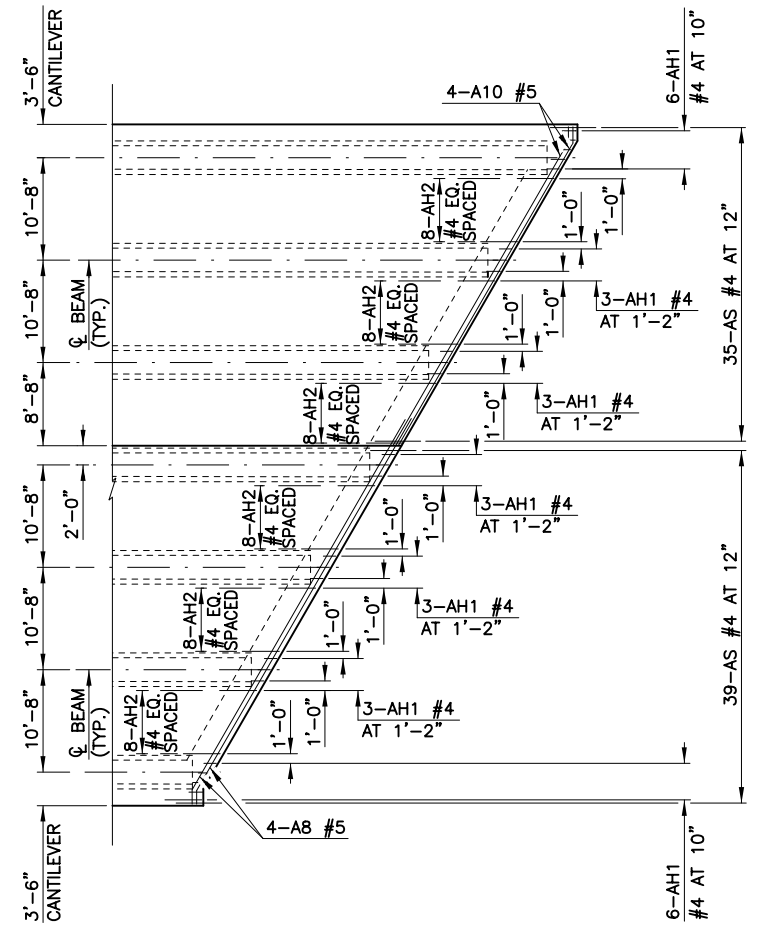
ABUTMENT NO. 1



PIER NO. 1 AND 3

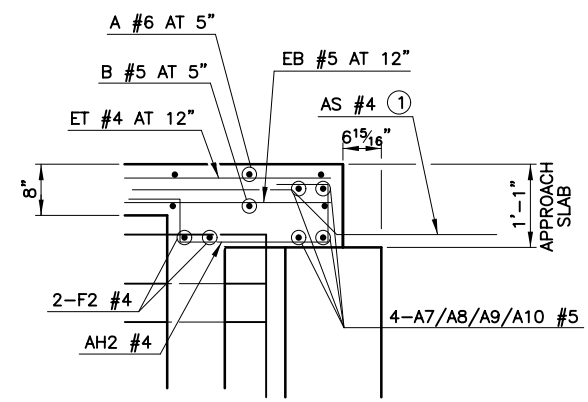


PIER NO. 2

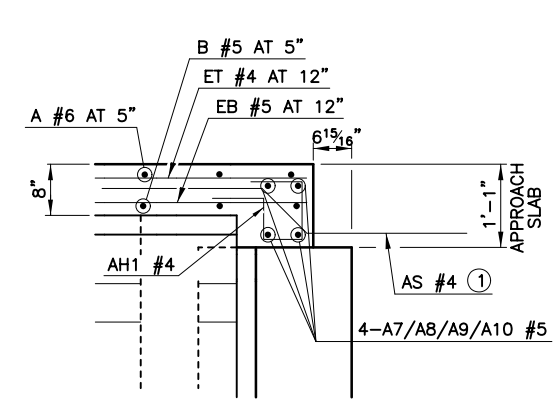


ABUTMENT NO. 2

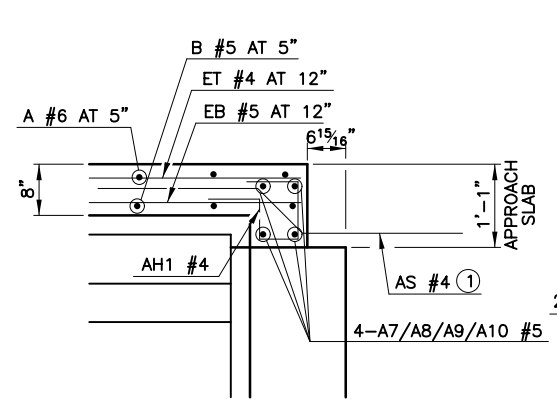
DIAPHRAGM REINFORCING PLAN



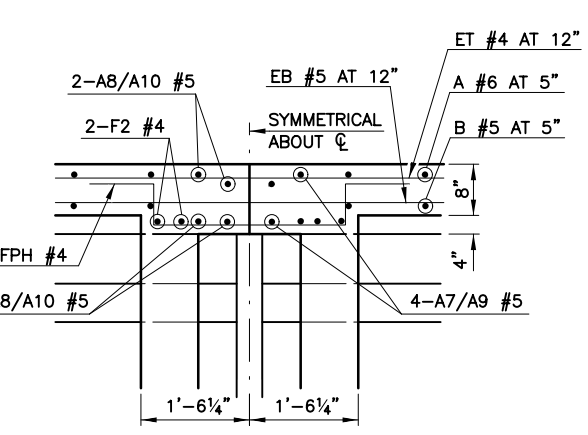
SECTION AT ABUTMENT BETWEEN BEAMS



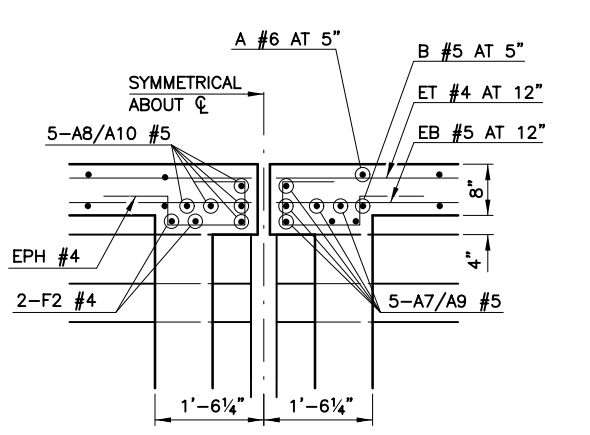
SECTION AT ABUTMENT THROUGH BEAMS



SECTION AT ABUTMENT THROUGH CANTILEVER



SECTION AT FIXED PIER



SECTION AT EXPANSION PIER

① TIE TO TOP REINFORCING OF DECK SLAB AND BOTTOM REINFORCING OF THE APPROACH SLAB. (PLACE BOTTOM LEG OF AS BAR THRU JOINT)

2:44:08 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 cpybdf_ANSIB.plt
 Plans and Drawings/8.00
 Bridge/21006BRB502.dgn
 Active Projects/ODOT13446.00/8.00

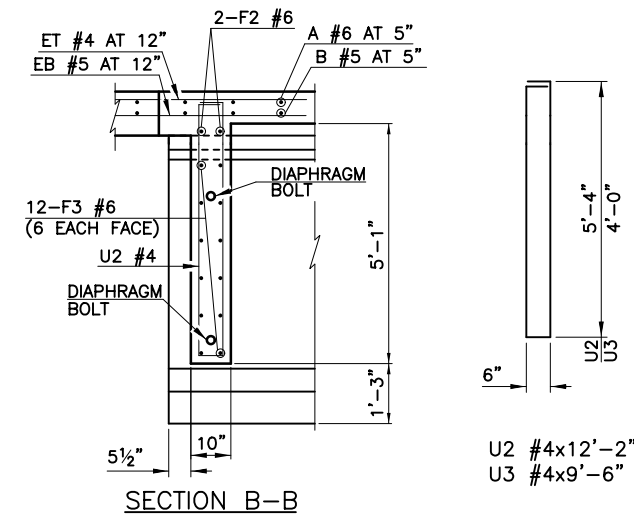
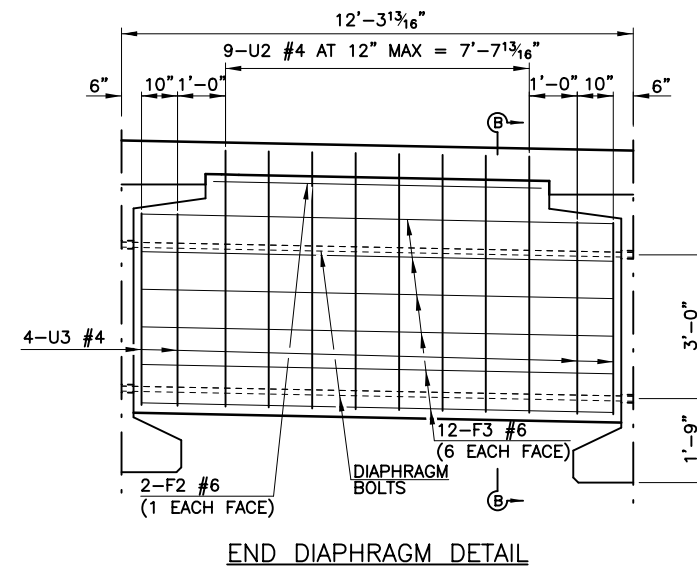
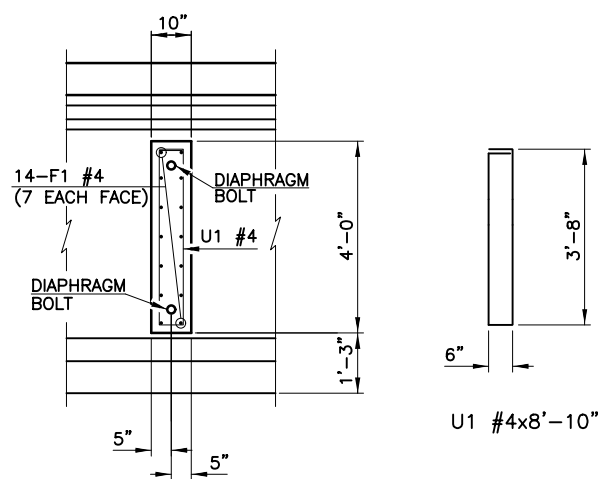
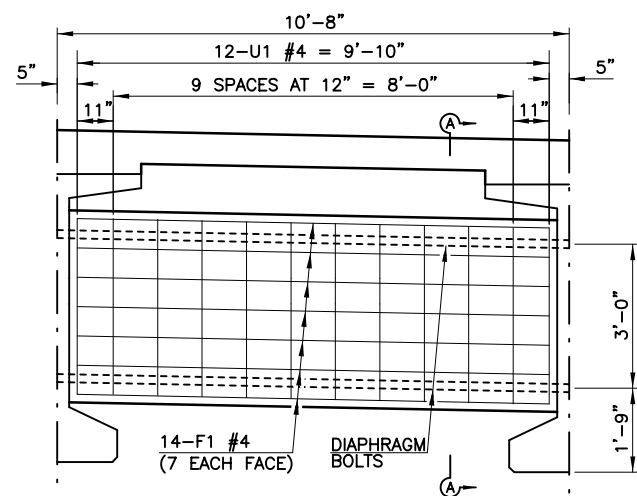
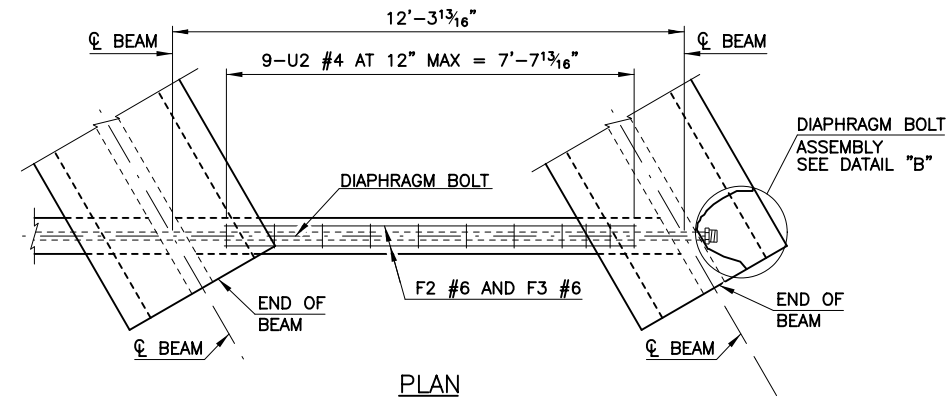
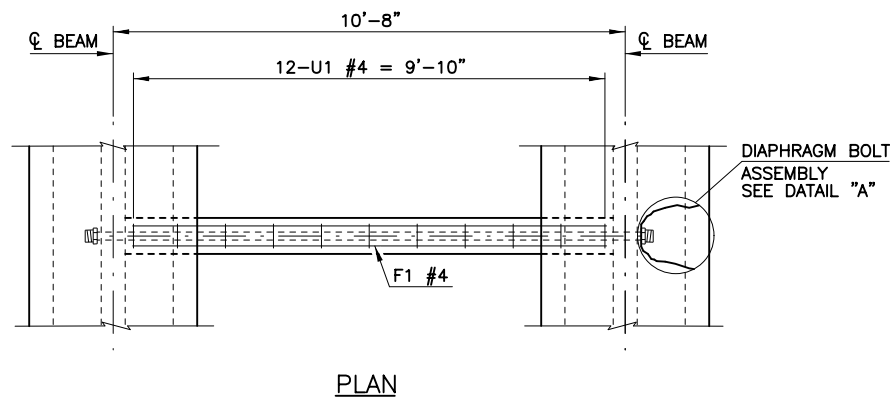
DESIGN: DKC 06/18	US 270 BRIDGE "B"	SEMINOLE COUNTY
DRAWN: LRJ 06/18		
CHECKED: DMN 06/18		
APPRVD: DMN 06/18		

SUPERSTRUCTURE DETAILS

SHEET 3 OF 5
SHEET NO. B046

STATE JOB PIECE NO: 21006(07)

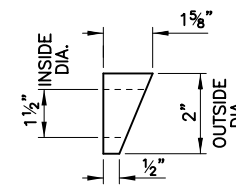
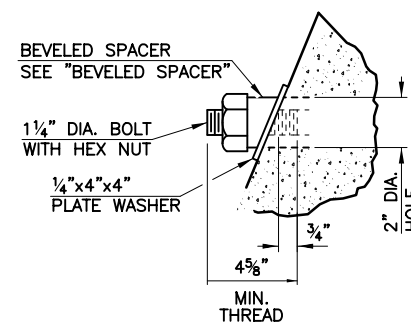
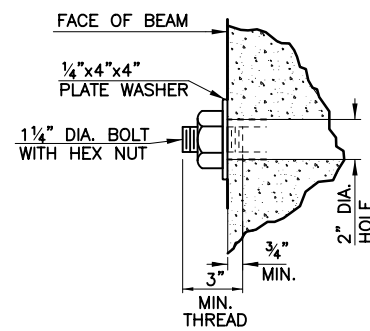
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B047	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



DIAPHRAGM BOLT NOTES

PROVIDE STRUCTURAL STEEL FOR DIAPHRAGM BOLTS AND PLATE WASHERS IN ACCORDANCE WITH AASHTO M270 (ASTM A709), GRADE 50W (WEATHERING STEEL, CHARPY V-NOTCH TESTING NOT REQUIRED). THE CONTRACTOR MAY SUBSTITUTE A #10 REINFORCING BAR IN ACCORDANCE WITH AASHTO M31, GRADE 60, AND THREADED AT THE ENDS AS SHOWN FOR THE DIAPHRAGM BOLT AT NO ADDITIONAL COST TO THE DEPARTMENT. PROVIDE HEX NUTS IN ACCORDANCE WITH AASHTO M291 (ASTM A563).

PAINT EXPOSED DIAPHRAGM BOLT, PLATE WASHER, AND HEX NUT WITH TWO (2) COATS OF ZINC-RICH PAINT (6 MIL MINIMUM THICKNESS) AFTER ASSEMBLY. INCLUDE ALL COST OF DIAPHRAGM BOLT, PLATE WASHER, AND HEX NUT IN THE CONTRACT UNIT PRICE FOR STRUCTURAL STEEL.

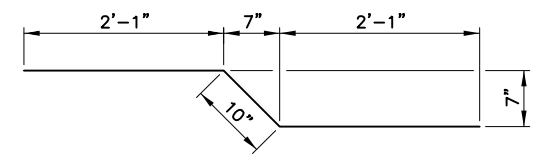


2:44:13 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRB503.dgn

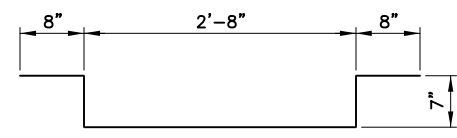
DESIGN: DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN: LRJ	06/18	BRIDGE "B"	
CHECKED: DMN	06/18	SUPERSTRUCTURE DETAILS	
APPRVD: DMN	06/18		
CP&Y		STATE JOB PIECE NO: 21006(07)	SHEET 4 OF 5 SHEET NO. B047

SUPERSTRUCTURE BAR LIST - PHASE 1					
EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	VARIANCE
A1	#6	888	BNT.	42'-6"	
A2	#6	216	BNT.	22'-11½" AVG	3'-10" TO 42'-1"
A3	#6	188	STR.	22'-9" AVG	6'-2" TO 39'-4"
A7	#5	8	BNT.	47'-3"	
A8	#5	8	BNT.	47'-3"	
AH1	#4	30	BNT.	3'-1"	
AH2	#4	48	BNT.	5'-8"	
AS	#4	78	BNT.	5'-0"	
B1	#5	888	STR.	41'-10"	
B2	#5	216	STR.	22'-3½" AVG	3'-2" TO 41'-5"
B3	#5	188	STR.	22'-9" AVG	6'-2" TO 39'-4"
EB1	#5	78	STR.	243'-9"	
ET1	#4	78	STR.	239'-9"	
EPH	#4	78	BNT.	3'-4"	
F1	#4	168	STR.	9'-10"	
F2	#4	48	STR.	7'-10"	
F3	#4	288	STR.	11'-4"	
FPH	#4	37	BNT.	5'-2"	
FS2	#5	308	BNT.	7'-4"	
L	#4	160	BNT.	1'-3"	
U1	#4	144	BNT.	8'-10"	
U2	#4	216	BNT.	12'-2"	
U3	#4	96	BNT.	9'-6"	

- ① 4 SETS OF 54 BARS
- ② 4 SETS OF 47 BARS
- ③ INCLUDES FOUR 3'-0" LAPS
- ④ INCLUDES FOUR 2'-0" LAPS



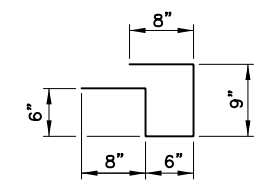
AS #4x5'-0"



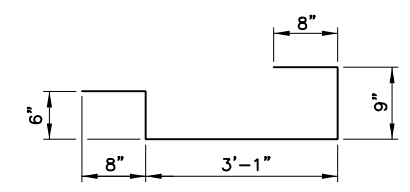
FPH #4x5'-2"

SUPERSTRUCTURE BAR LIST - PHASE 2					
EPOXY COATED REINFORCING BARS					
MARK	SIZE	NO.	FORM	LENGTH	VARIANCE
A4	#6	936	BNT.	34'-0"	
A5	#6	144	STR.	18'-2½" AVG	5'-7" TO 30'-10"
A6	#6	168	BNT.	18'-8" AVG	3'-10" TO 33'-6"
A9	#5	8	BNT.	38'-1"	
A10	#5	8	BNT.	38'-2"	
AH1	#4	24	BNT.	3'-1"	
AH2	#4	48	BNT.	5'-8"	
AS	#4	70	BNT.	5'-0"	
B1	#5	936	STR.	33'-4"	
B2	#5	144	STR.	18'-2½" AVG	5'-7" TO 30'-10"
B3	#5	168	STR.	18'-0" AVG	3'-2" TO 32'-10"
EB1	#5	70	STR.	243'-9"	
ET1	#4	70	STR.	239'-9"	
EPH	#4	70	BNT.	3'-4"	
F1	#4	168	STR.	9'-10"	
F2	#4	48	STR.	7'-10"	
F3	#4	288	STR.	11'-4"	
FPH	#4	33	BNT.	5'-2"	
FS2	#5	308	BNT.	7'-4"	
L	#4	160	BNT.	1'-3"	
U1	#4	144	BNT.	8'-10"	
U2	#4	216	BNT.	12'-2"	
U3	#4	96	BNT.	9'-6"	

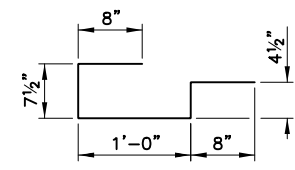
- ⑤ 4 SETS OF 36 BARS
- ⑥ 4 SETS OF 42 BARS



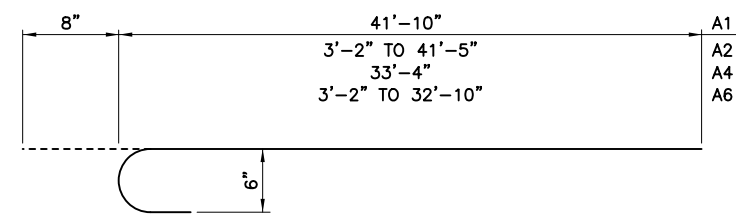
AH1 #4x3'-1"



AH2 #4x5'-8"



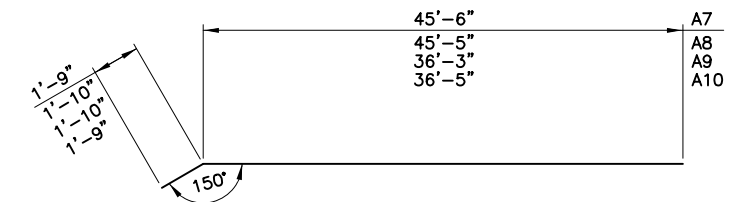
EPH #4x3'-4"



- A1 #6x42'-6"
- A2 #6x22'-11½" AVG.
- A4 #6x34'-0"
- A6 #6x18'-8" AVG.

BAR BENDS

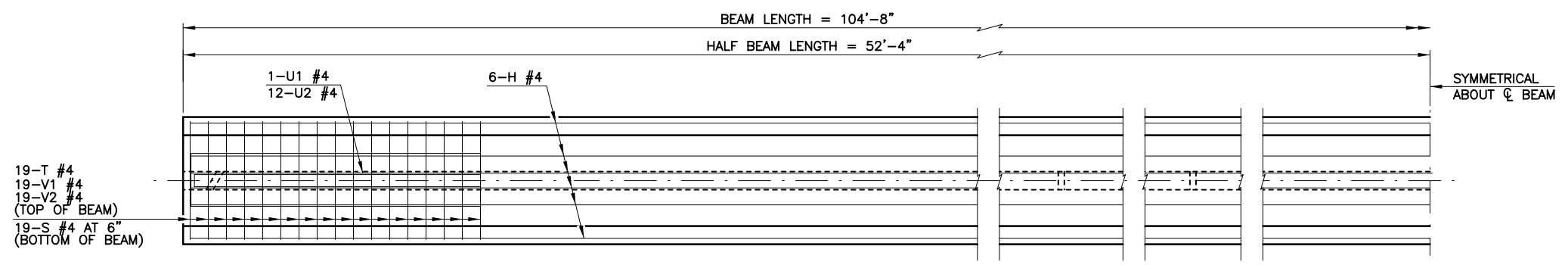
SUMMARY OF SUPERSTRUCTURE QUANTITIES				
ITEM	UNIT	PHASE I	PHASE II	TOTAL
PRESTRESSED CONCRETE BEAMS (TYPE J BT)	LF	1,834.67	1,376.00	3,210.67
SAW-CUT GROOVING	SY	1,856.00	1,650.00	3,506.00
SEALED EXPANSION JOINT	LF	42.00	37.40	79.40
42" F-SHAPED PARAPET	LF	463.90	463.90	927.80
STRUCTURAL STEEL	LB	2,640	2,400	5,040
STAINLESS STEEL FIXED BEARING ASSEMBLY	EA	24.00	18.00	42.00
STAINLESS STEEL EXPANSION BEARING ASSEMBLY	EA	8.00	6.00	14.00
CLASS AA CONCRETE	CY	543.80	482.40	1,026.20
EPOXY COATED REINFORCING STEEL	LB	161,950.00	134,500.00	296,450.00
WATER REPELLENT (VISUALLY INSPECTED)	SY	899.00	899.00	1,798.00
SEALER CRACK PREPARATION	LF	83.00	1,001.80	1,084.80
SEALER RESIN	GAL	1.00	11.20	12.20



- A7 #5x47'-3"
- A8 #5x47'-3"
- A9 #5x38'-1"
- A10 #5x38'-2"

2:44:16 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRB505.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B049	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



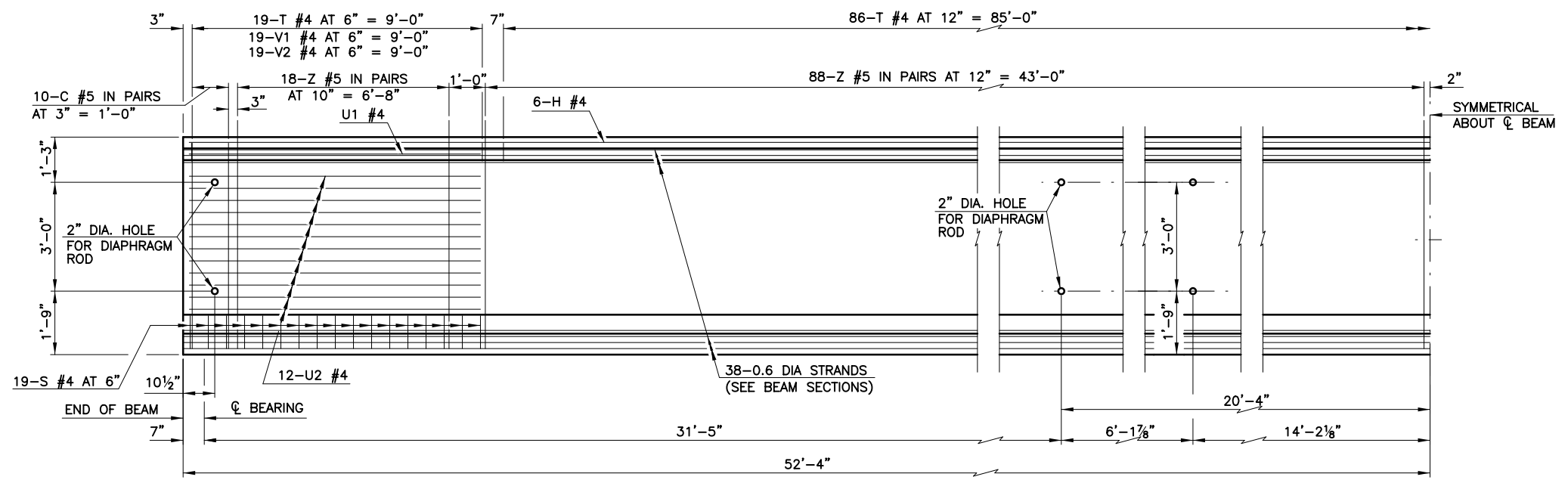
PLAN

PRESTRESSED CONCRETE BEAM NOTES

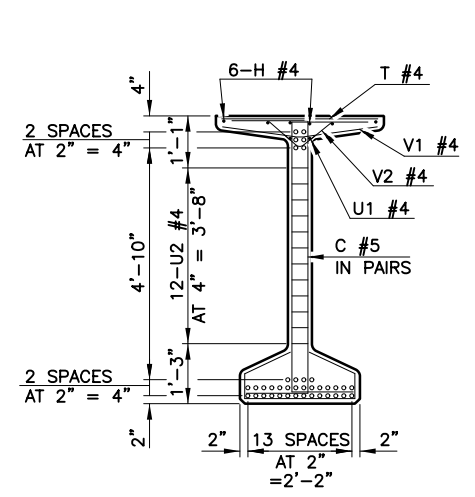
COMPRESSIVE STRENGTH
 PROVIDE CONCRETE WITH A COMPRESSIVE STRENGTH OF 6,000 P.S.I. AT TRANSFER OF PRESTRESS AND 8,000 P.S.I. AT 28 DAYS

STRAND TYPE
 PROVIDE LOW-RELAXATION STRANDS HAVING A NOIMINAL DIAMETER OF 0.6" WITH ULTIMATE TENSILE STRENGTH OF 270 K.S.I.

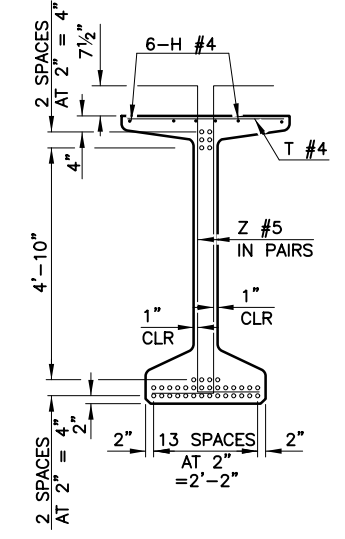
LRFR OPERATING RATING FACTOR = 2.41
 THE OPERATION RATING SHOWN IS BASED ON A NOIMINAL STRENGTH USING ONLY STRANDS THAT ARE BONDED FOR THE FULL LENGTH OF THE BEAM. ALL PARTIALLY BONDED STRANDS ARE NEGLECTED IN STRENGTH COMPUTATIONS.



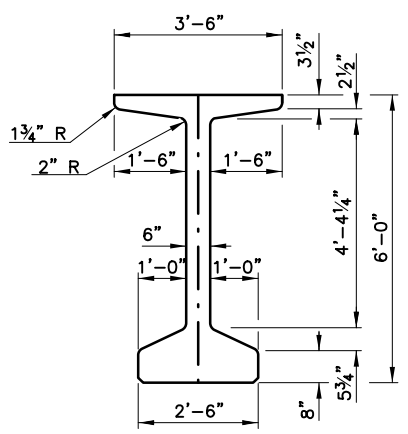
ELEVATION



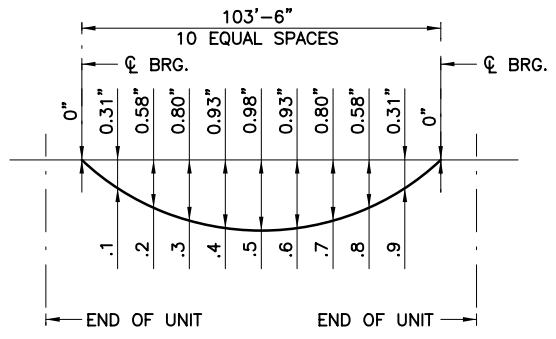
END SECTIONS



C SECTIONS



END VIEW



DEAD LOAD DEFLECTION DIAGRAM

NOTE:
 THE DEAD LOAD DEFLECTIONS SHOWN ABOVE AT THE TENTH POINTS ARE THE INITIAL DEFLECTIONS DUE TO DECK SLAB + HAUNCH + S.I.P. STEEL DECK FORM ALLOWANCE + CONCRETE TRAFFIC RAIL. IT DOES NOT INCLUDE THE BEAM WEIGHT OR FUTURE WEARING SURFACE.

BEAM SECTIONS
 (38-0.6" DIA. STRANDS)

DESIGN:	DKC	06/18	US 270 BRIDGE "B"	SEMINOLE COUNTY
DRAWN:	LRJ	06/18		
CHECKED:	DMN	06/18		
APPRVD:	DMN	06/18		

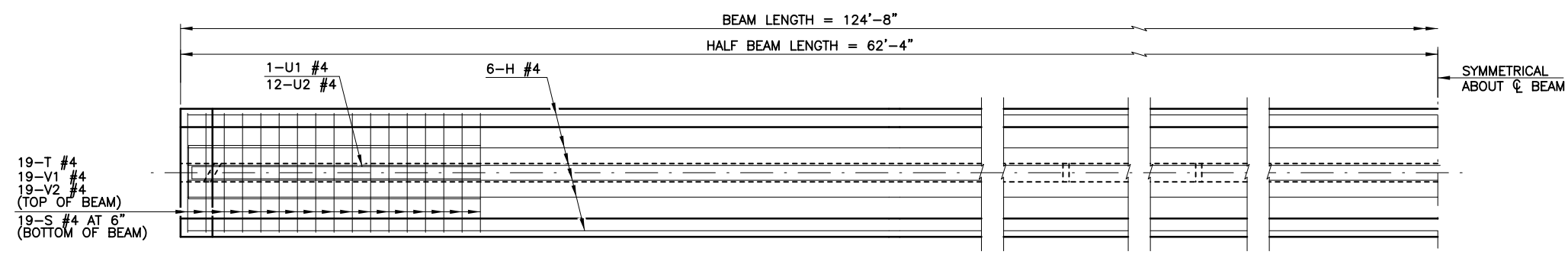
P.C. BEAM DETAILS
(105' SPAN)

STATE JOB PIECE NO: 21006(07)

SHEET 1 OF 3
SHEET NO. B049

2:44:22 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRBDD01.dgn
 bmcanelly

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B050	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



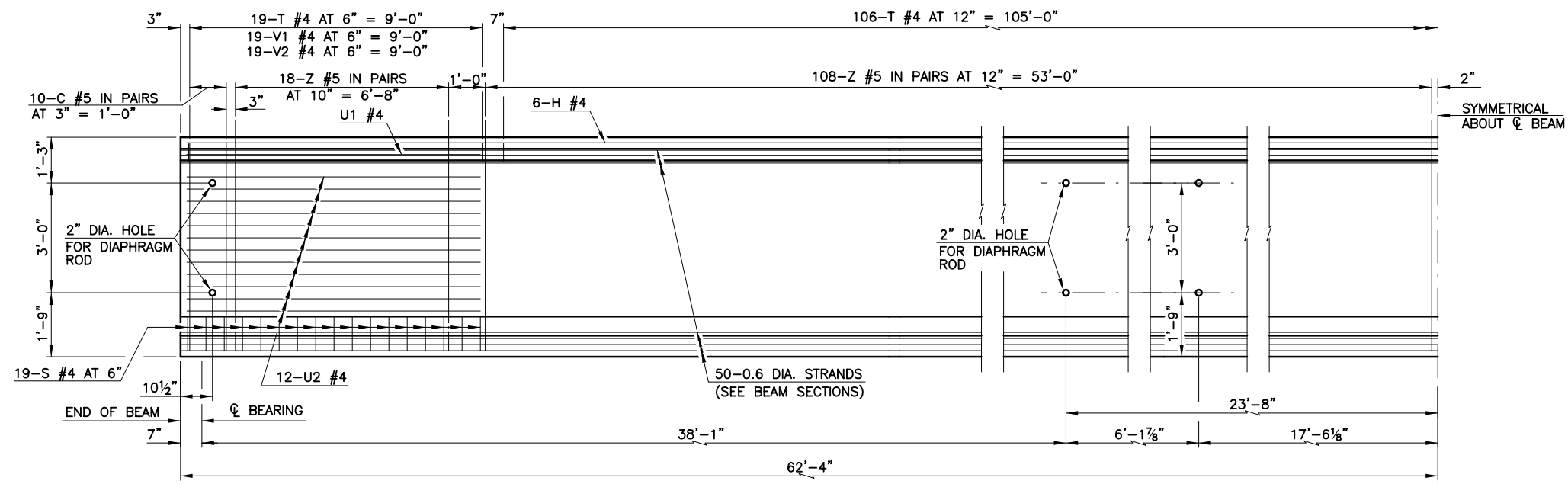
PLAN

PRESTRESSED CONCRETE BEAM NOTES

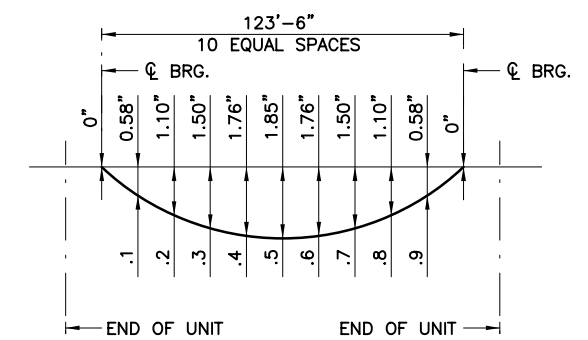
COMPRESSIVE STRENGTH
 PROVIDE CONCRETE WITH A COMPRESSIVE STRENGTH OF 7,000 P.S.I. AT TRANSFER OF PRESTRESS AND 10,000 P.S.I. AT 28 DAYS

STRAND TYPE
 PROVIDE LOW-RELAXATION STRANDS HAVING A NOIMINAL DIAMETER OF 0.6" WITH ULTIMATE TENSILE STRENGTH OF 270 K.S.I.

LRFR OPERATING RATING FACTOR = 2.46
 THE OPERATION RATING SHOWN IS BASED ON A NOIMINAL STRENGTH USING ONLY STRANDS THAT ARE BONDED FOR THE FULL LENGTH OF THE BEAM. ALL PARTIALLY BONDED STRANDS ARE NEGLECTED IN STRENGTH COMPUTATIONS.

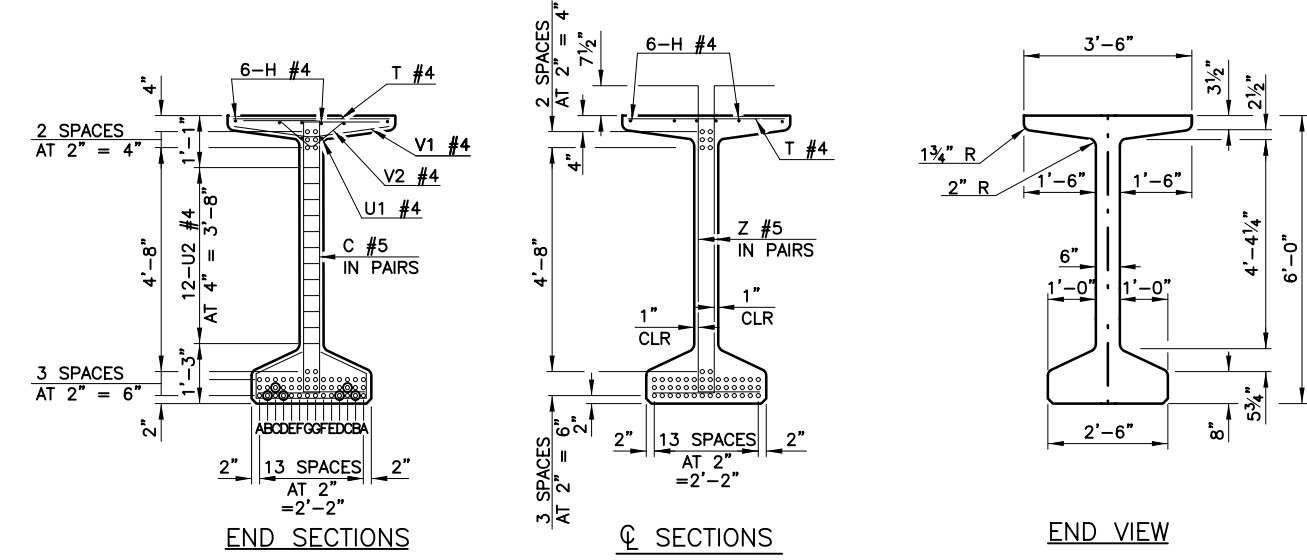


ELEVATION



DEAD LOAD DEFLECTION DIAGRAM

NOTE:
 THE DEAD LOAD DEFLECTIONS SHOWN ABOVE AT THE TENTH POINTS ARE THE INITIAL DEFLECTIONS DUE TO DECK SLAB + HAUNCH + S.I.P. STEEL DECK FORM ALLOWANCE + CONCRETE TRAFFIC RAIL. IT DOES NOT INCLUDE THE BEAM WEIGHT OR FUTURE WEARING SURFACE.



BEAM SECTIONS (50-0.6" DIA. STRANDS)

DEBOND SCHEDULE	
DEBOND PAIR	DEBOND LENGTH FROM END OF BEAM
B1	18'-0"
D1	12'-0"
C2	6'-0"

DESIGN: DKC 06/18	US 270 BRIDGE "B"	SEMINOLE COUNTY
DRAWN: LRJ 06/18		
CHECKED: DMN 06/18		
APPRVD: DMN 06/18		

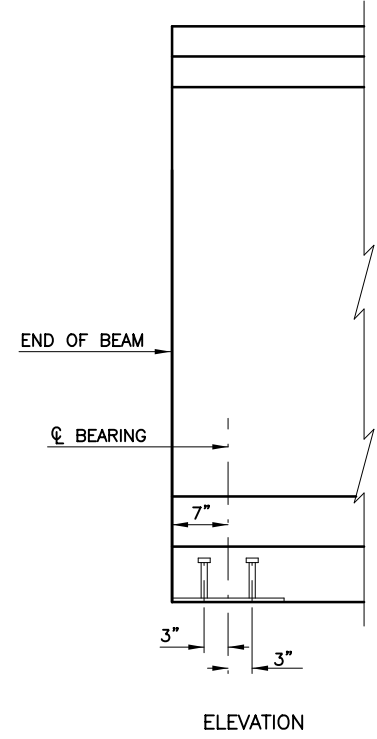
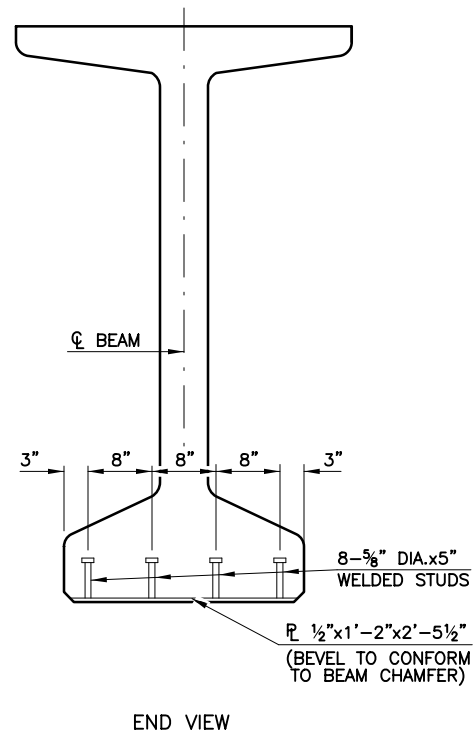
P.C. BEAM DETAILS
(125' SPAN)

SHEET 2 OF 3
SHEET NO. B050

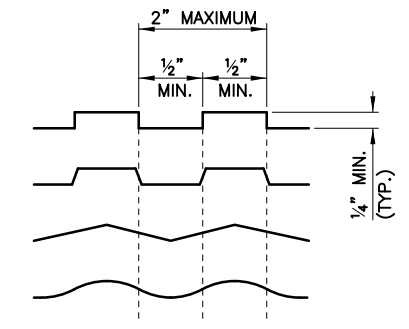
STATE JOB PIECE NO: 21006(07)

2:44:27 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT113446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRBDD02.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B051	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

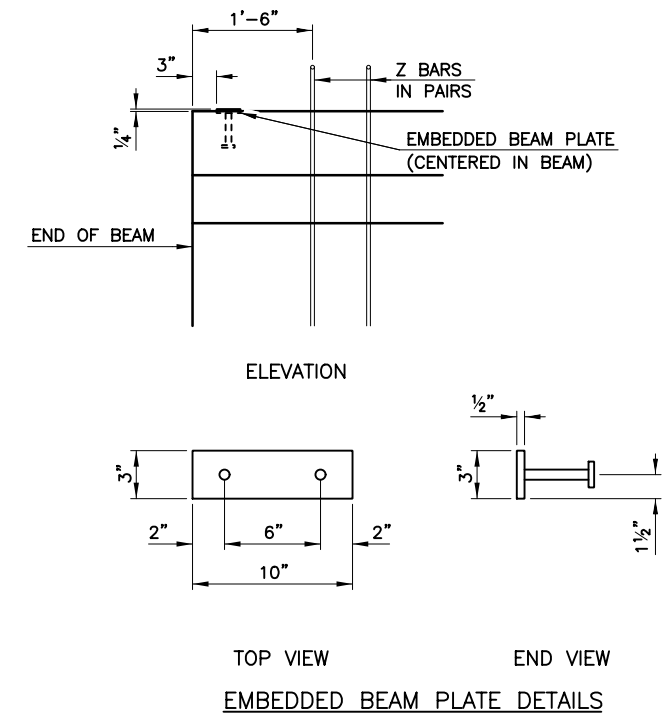
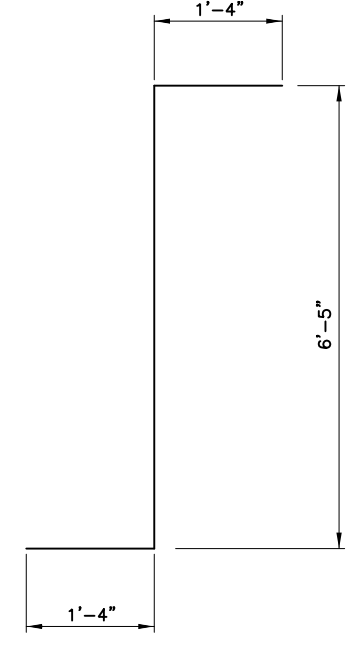
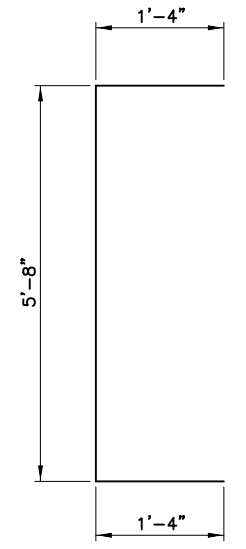
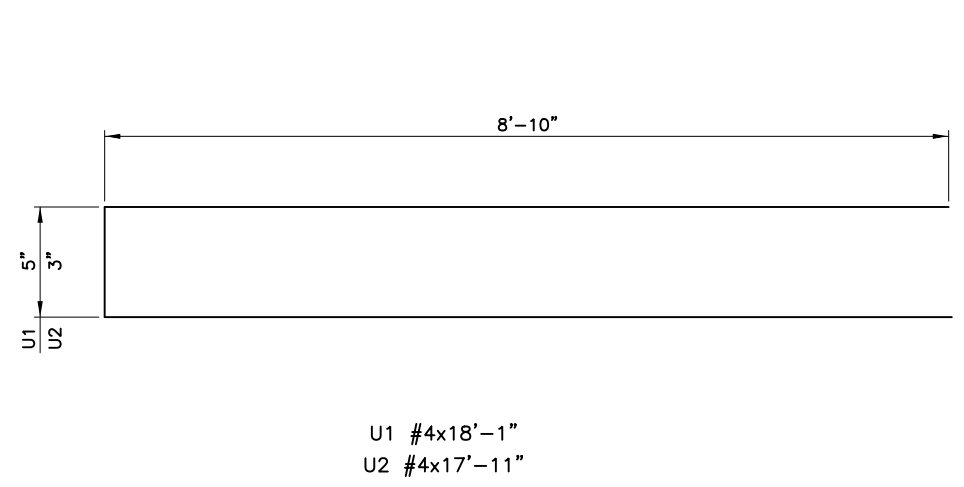


EMBEDDED SOLE PLATE DETAILS

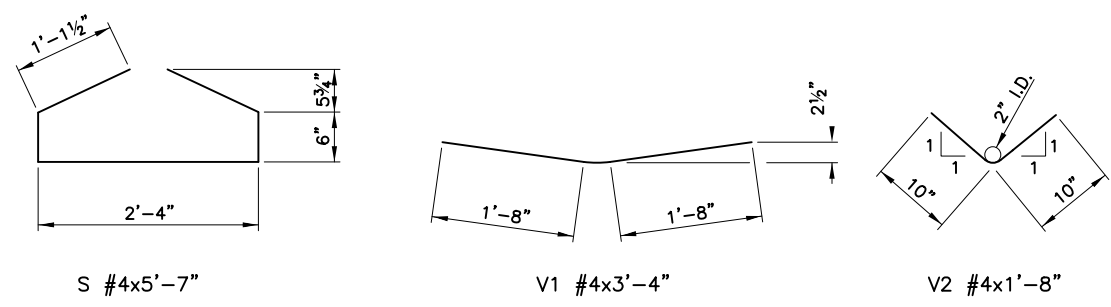


INTENTIONALLY ROUGHENED SURFACE DETAILS

INTENTIONALLY ROUGHEN THE ENTIRE TOP SURFACE OF P.C. BEAM TO A MINIMUM HEIGHT OF 1/4" OVER A MAXIMUM PITCH OF 2" MEASURED LONGITUDINALLY ALONG THE LENGTH OF THE BEAM. PROVIDE A CREST AND TROUGH ASSOCIATED WITH THE HEIGHT OF NOT LESS THAN 1/2". PRODUCE THE ROUGHENED SURFACE BY USING A SPECIAL TROWEL TO FORM ONE OF THE SURFACES SHOWN IN THE DETAILS, BY CLEANING THE CONCRETE SURFACE WITH A STIFF WIRE BRUSH (OR BLASTING) TO EXPOSE THE AGGREGATE TO A HEIGHT OF 1/4", OR BY USING ANOTHER APPROVED METHOD. SUBMIT THE METHOD TO BE USED FOR APPROVAL BY THE ENGINEER. REPAIR ANY DAMAGE TO REINFORCEMENT'S EPOXY COATING BEFORE PLACEMENT OF DECK CONCRETE.



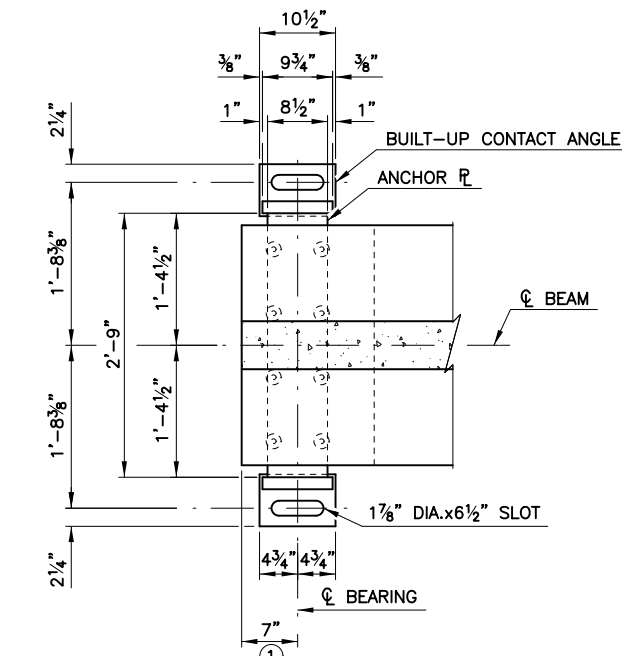
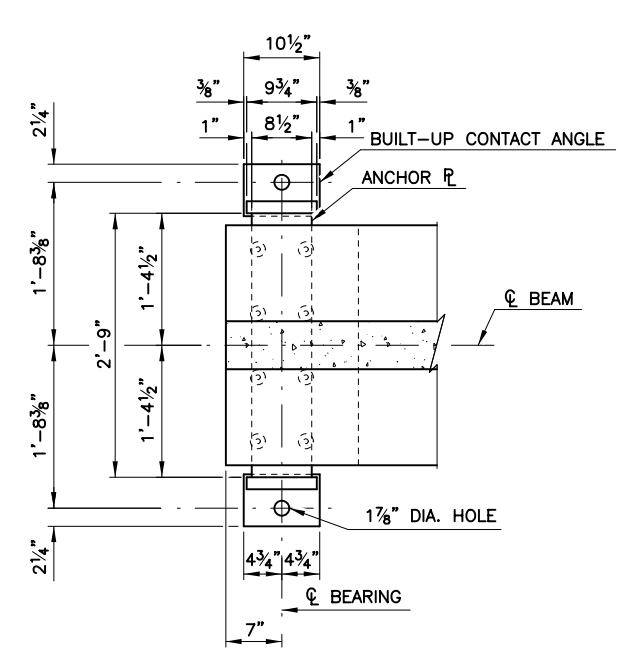
NOTE: PROVIDE AN EMBEDDED BEAM PLATE AT EXPANSION END ONLY.



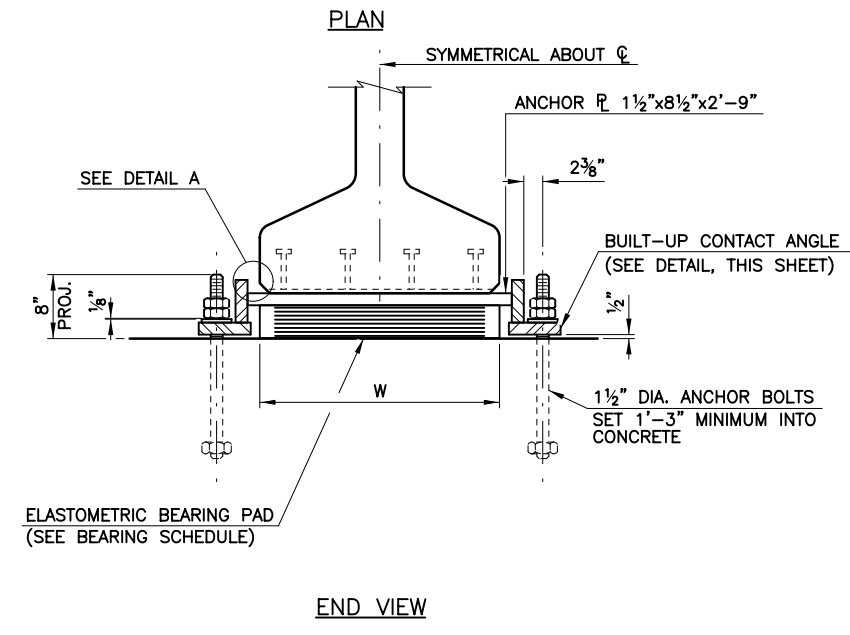
DESIGN: DKC	06/18	US 270 BRIDGE "B"	SEMINOLE COUNTY
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
CP&Y		P.C. BEAM DETAILS (105' AND 125' SPANS)	
		STATE JOB PIECE NO: 21006(07)	
		SHEET 3 OF 3 SHEET NO. B051	

2:44:32 PM
 6/5/2018
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Cut Sheets/8.30 Bridge/8.3.07 Bridge/21006BRBBD03.dgn

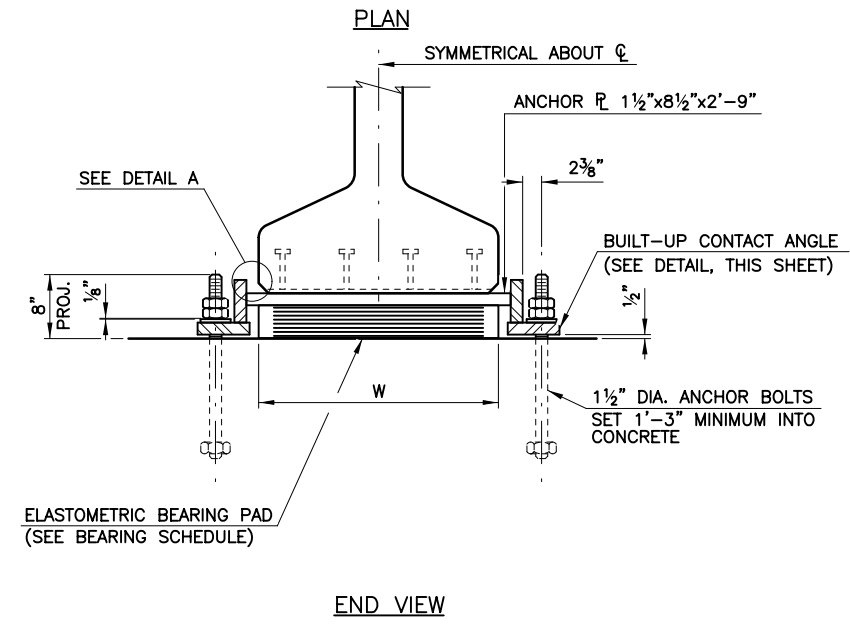
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B052	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



BEARING SCHEDULE				
SPAN	60 DUROMETER ELASTOMER BEARING PAD			
	SIZE (T x L x W)	COVER LAYER	INNER LAYER	LAMINATE PLATE
105'	4 1/8" x 7" x 2'-6"	2 - 1/4"	7 - 3/8"	8 - 1/8"
125'	4 1/8" x 8" x 2'-6"	2 - 1/4"	7 - 3/8"	8 - 1/8"



FIXED BEARING DETAILS

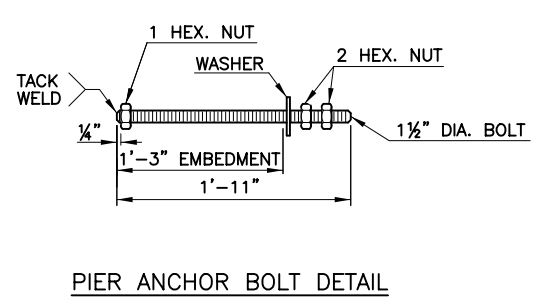


EXPANSION BEARING DETAILS

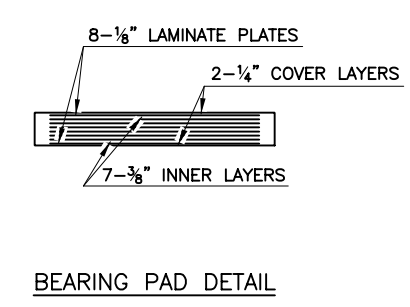
EXPANSION BEARING ASSEMBLY NOTES:
 PROVIDE STRUCTURAL STEEL FOR ANCHOR PLATES AND BUILT-UP CONTACT ANGLES IN ACCORDANCE WITH ASTM A240 (AUSTENITIC STAINLESS STEEL, TYPE 316, CHARPY V-NOTCH TESTING NOT REQUIRED). FOR ANCHOR BOLTS, PROVIDE CONTINUOUSLY THREADED BARS IN ACCORDANCE WITH ASTM A320, CLASS 2, GRADE B8M (AUSTENITIC STAINLESS STEEL, TYPE 316, CHARPY V-NOTCH TESTING NOT REQUIRED). USE AUSTENITIC STAINLESS STEEL NUTS AND WASHERS CONFORMING TO ASTM A194, GRADE 8M AND ASTM A320, RESPECTIVELY. PERFORM ALL WELDING CONSISTENT WITH PROCEDURES FOR STAINLESS STEEL.

FIXED BEARING ASSEMBLY NOTES:
 PROVIDE STRUCTURAL STEEL FOR ANCHOR PLATES IN ACCORDANCE WITH AASHTO M270 (ASTM A709), GRADE 50W (WEATHERING STEEL, CHARPY V-NOTCH TESTING NOT REQUIRED). FOR ANCHOR BARS PROVIDE A REINFORCING BAR IN ACCORDANCE WITH AASHTO M31, GRADE 60.

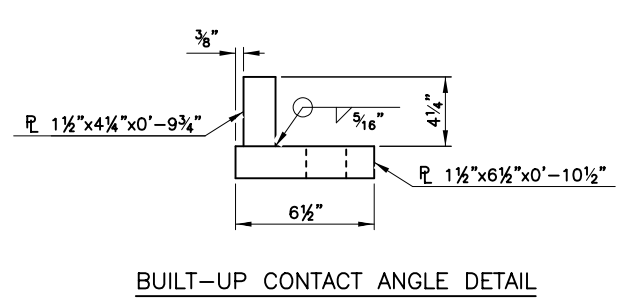
① CENTER ANCHOR BOLTS IN SLOTS DURING SETTING OF BEAMS. DIMENSION MAY VARY DEPENDING ON TEMPERATURE AT THE TIME OF BEAM SETTING.



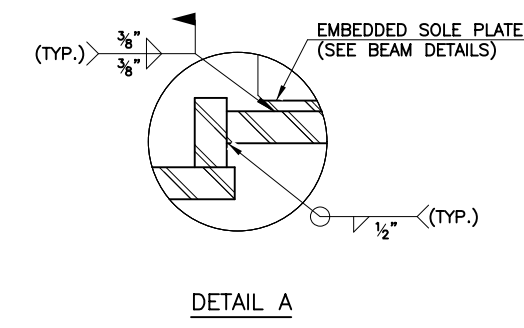
PIER ANCHOR BOLT DETAIL



BEARING PAD DETAIL



BUILT-UP CONTACT ANGLE DETAIL

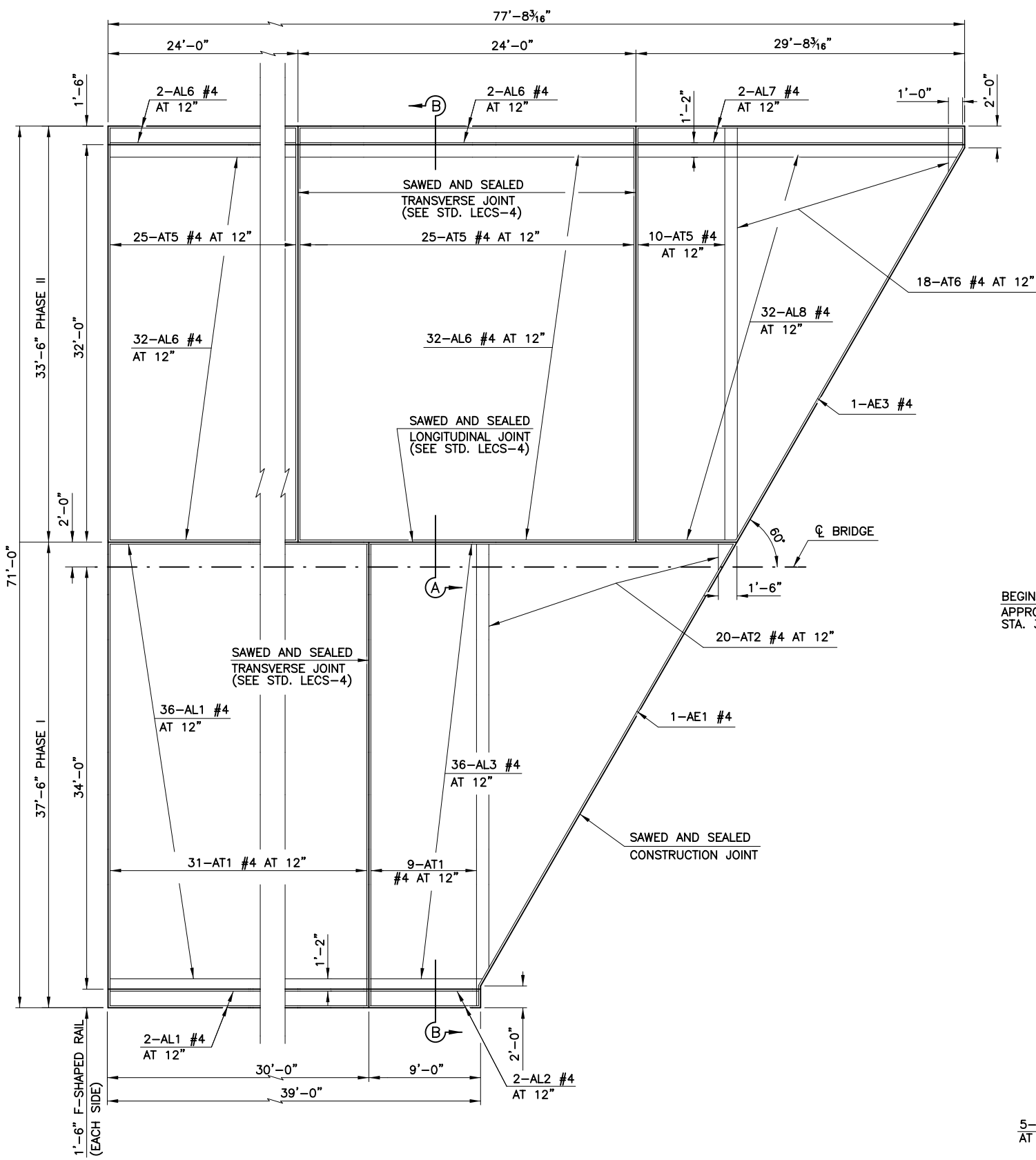


DETAIL A

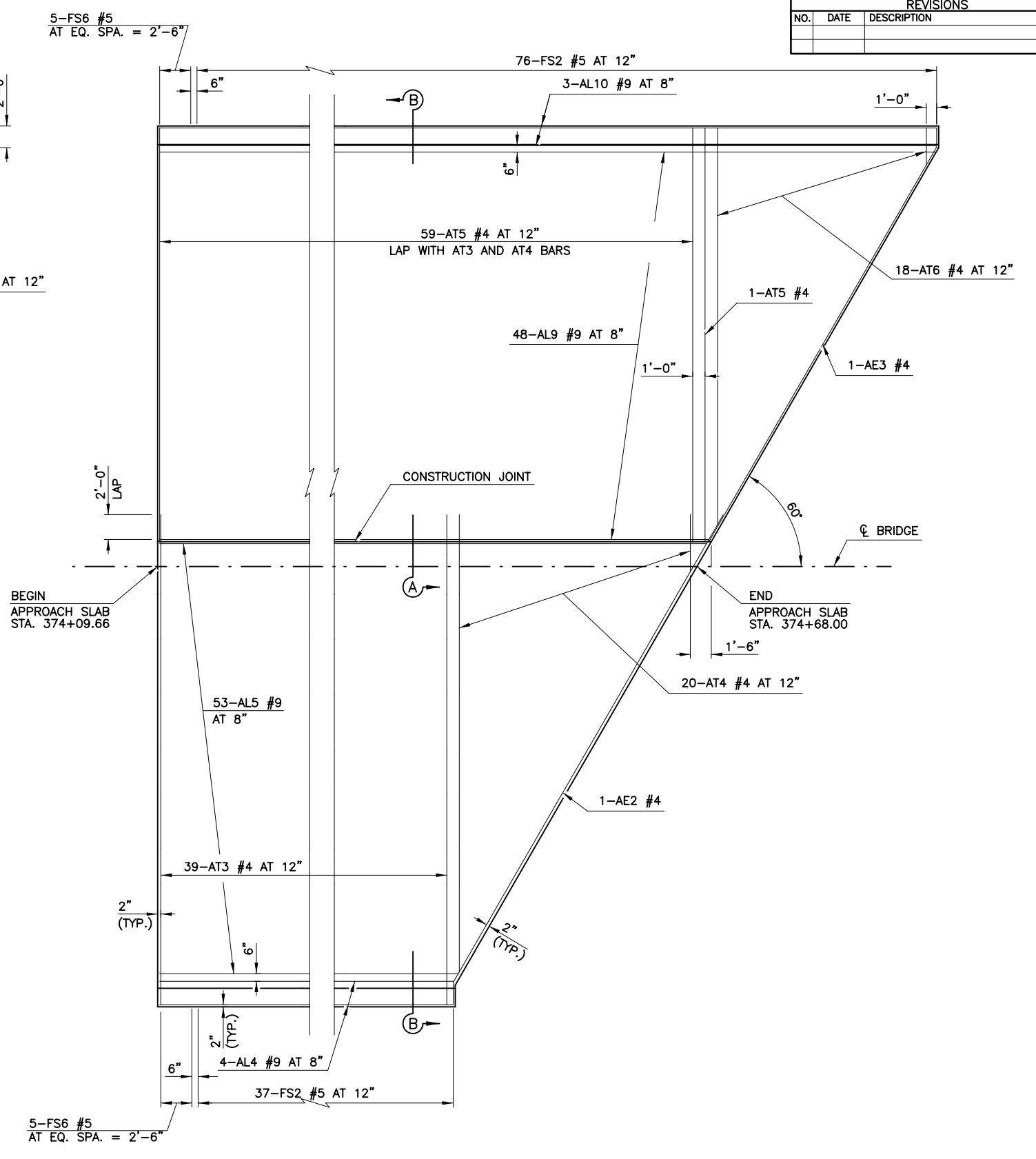
DESIGN: DKC	06/18	US 270 BRIDGE "B"	SEMINOLE COUNTY
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
CP&Y		BEARING DETAILS	
		STATE JOB PIECE NO: 21006(07)	SHEET 1 OF 1 SHEET NO. B052

2:44:38 PM 6/5/2018 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRBS04.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B053	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



TOP REINFORCING MAT DETAIL

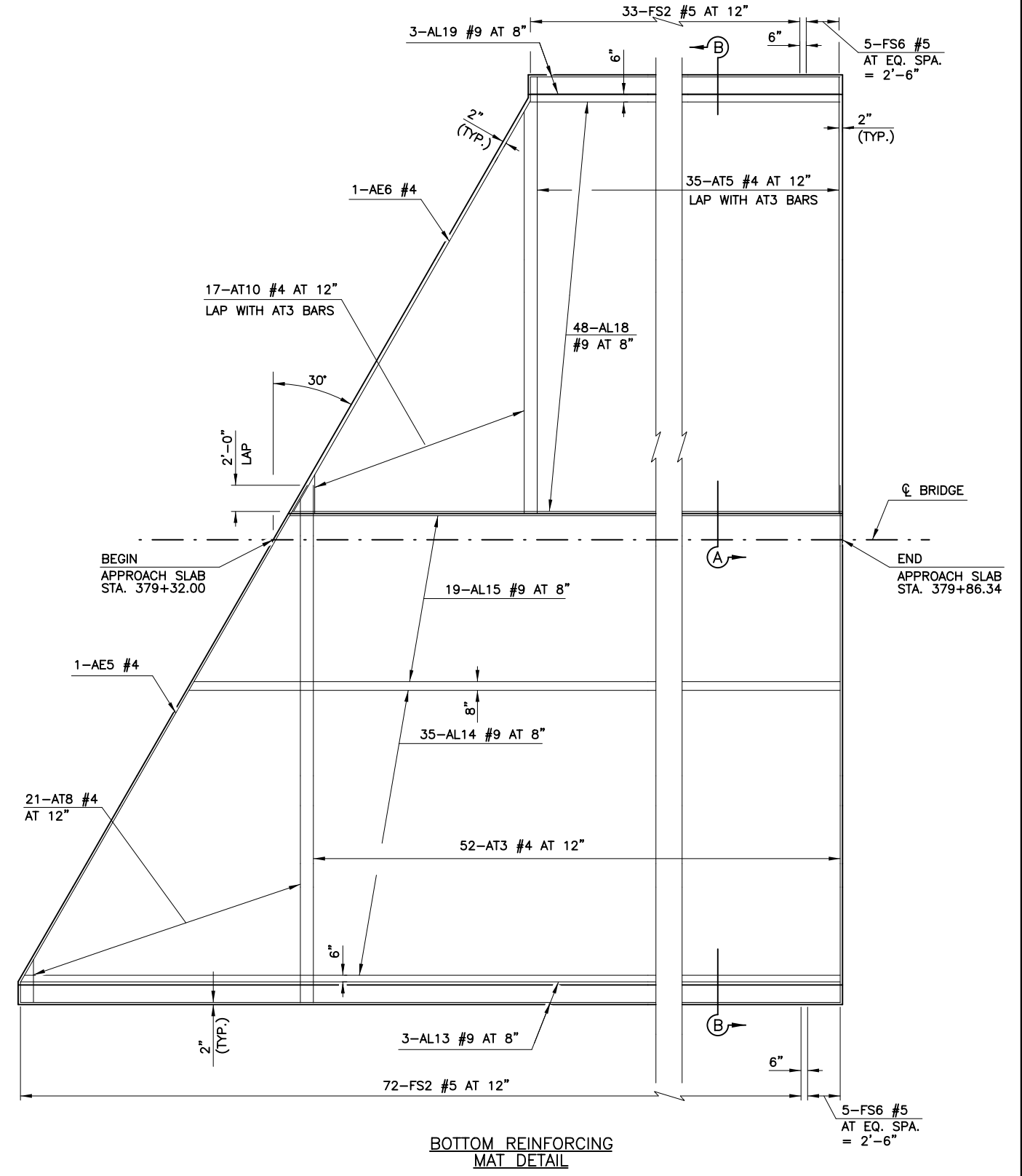
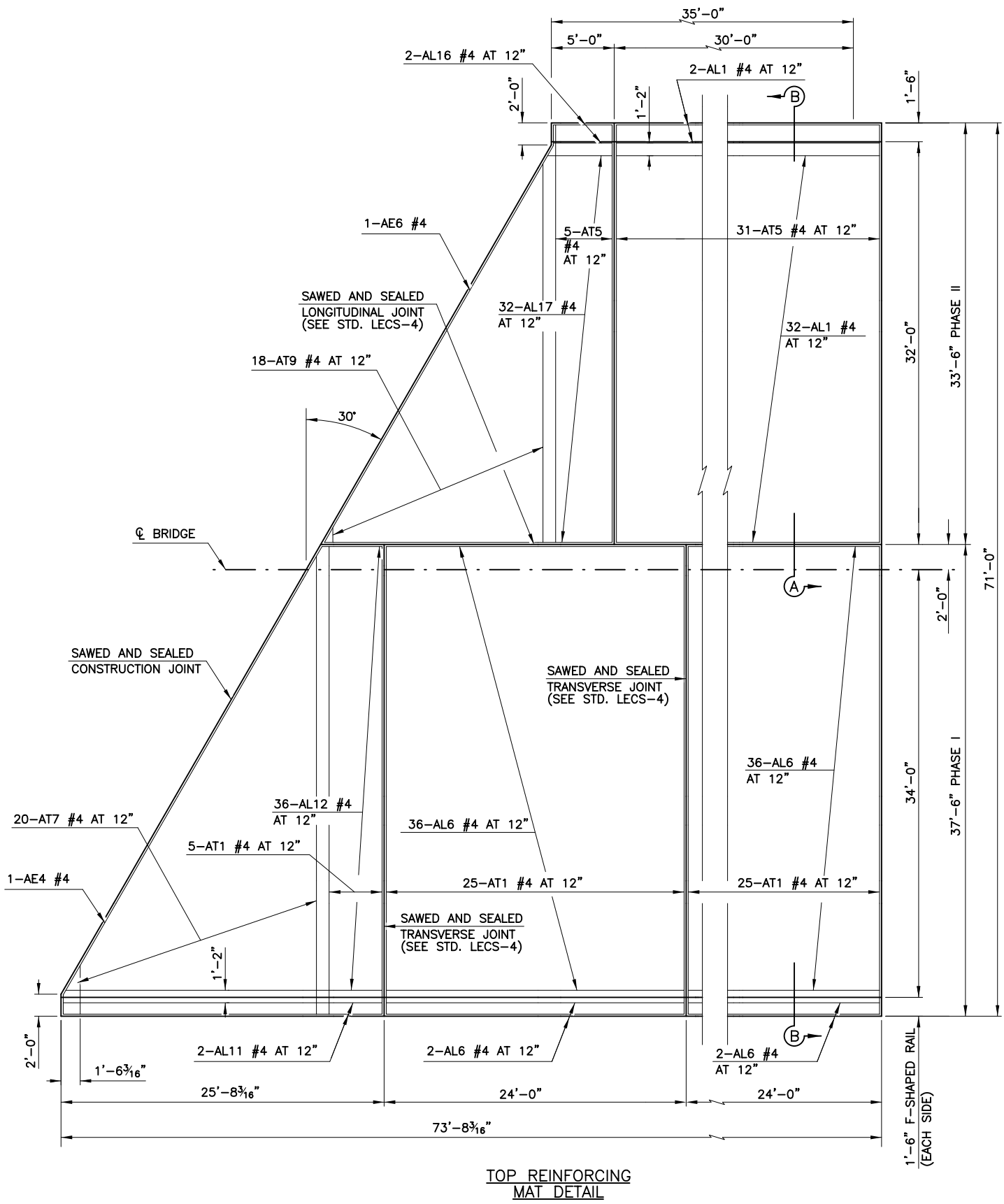


BOTTOM REINFORCING MAT DETAIL

2:44:43 PM 6/5/2018
 cpy:pdf_ANSIB.plt
 Plans and Drawings/8.30 Cut Sheets/8.3.07 Bridge/21006BRBTD01.dgn
 bmcanelly
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00

DESIGN: DKC	06/18	US 270 BRIDGE "B" APPROACH SLAB NO. 1 DETAILS	SEMINOLE COUNTY SHEET 1 OF 1 SHEET NO. B053
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
		STATE JOB PIECE NO: 21006(07)	

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B054	B056
REVISIONS					
NO.	DATE	DESCRIPTION			



6/5/2018 2:44:49 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRBTD02.dgn

DESIGN: DKC	06/18	US 270 BRIDGE "B"	SEMINOLE COUNTY
DRAWN: LRJ	06/18		
CHECKED: DMN	06/18		
APPRVD: DMN	06/18		
CP&Y		APPROACH SLAB NO. 2 DETAILS	
		STATE JOB PIECE NO: 21006(07)	SHEET 1 OF 1 SHEET NO. B054

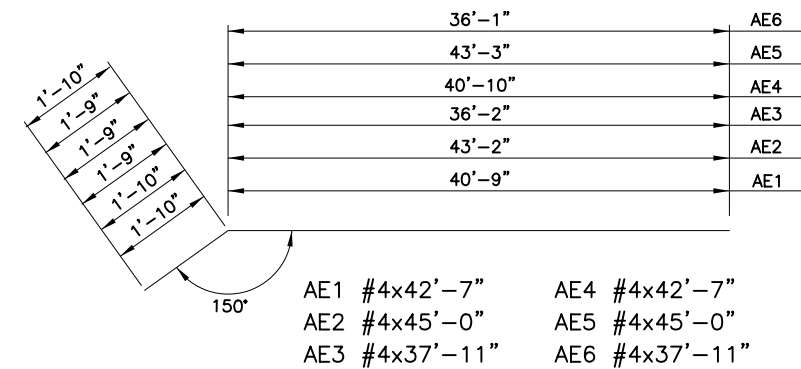
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B055	B056
REVISIONS					
NO.	DATE	DESCRIPTION			

APPROACH SLAB NO. 1 BAR LIST (PHASE I)

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING					
AE1	#4	1	BNT.	42'-7"	
AE2	#4	1	BNT.	45'-0"	
AL1	#4	38	STR.	29'-10"	
AL2	#4	2	STR.	8'-10"	
AL3	#4	36	STR.	19'-1" AVG	9'-0" TO 29'-2"
AL4	#9	4	STR.	38'-10"	
AL5	#9	53	STR.	49'-2" AVG	39'-2" TO 59'-2"
AT1	#4	40	STR.	37'-2"	
AT2	#4	20	STR.	18'-6½" AVG	2'-1" TO 35'-0"
AT3	#4	39	STR.	39'-6"	
AT4	#4	20	STR.	20'-10½" AVG	4'-5" TO 37'-4"
FS2	#5	37	BNT.	7'-4"	
FS6	#5	5	BNT.	7'-6½"	

APPROACH SLAB NO. 2 BAR LIST (PHASE I)

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING					
AE4	#4	1	BNT.	42'-7"	
AE5	#4	1	BNT.	45'-0"	
AL6	#4	76	STR.	23'-10"	
AL11	#4	2	STR.	25'-6"	
AL12	#4	36	STR.	15'-2" AVG	5'-1" TO 25'-3"
AL13	#9	3	STR.	79'-0"	
AL14	#9	35	STR.	72'-5" AVG	65'-11" TO 78'-11"
AL15	#9	19	STR.	56'-6½" AVG	53'-1" TO 60'-0"
AT1	#4	55	STR.	37'-2"	
AT3	#4	52	STR.	39'-6"	
AT7	#4	20	STR.	20'-6½" AVG	4'-1" TO 37'-0"
AT8	#4	21	STR.	21'-5" AVG	4'-1" TO 38'-9"
FS2	#5	72	BNT.	7'-4"	
FS6	#5	5	BNT.	7'-6½"	



APPROACH SLAB NO. 1 BAR LIST (PHASE II)

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING					
AE3	#4	2	BNT.	37'-11"	
AL6	#4	68	STR.	23'-10"	
AL7	#4	2	STR.	29'-6"	
AL8	#4	32	STR.	18'-4" AVG	9'-5" TO 27'-3"
AL9	#9	48	STR.	73'-0" AVG	64'-11" TO 82'-11"
AL10	#9	3	STR.	83'-0"	
AT5	#4	120	STR.	33'-2"	
AT6	#4	36	STR.	18'-2½" AVG	3'-6" TO 32'-11"
FS2	#5	76	BNT.	7'-4"	
FS6	#5	5	BNT.	7'-6½"	

APPROACH SLAB NO. 2 BAR LIST (PHASE II)

MARK	SIZE	NO.	FORM	LENGTH	LENGTH VARIATION
EPOXY COATED REINFORCING					
AE6	#4	2	BNT.	37'-11"	
AL1	#4	34	STR.	29'-10"	
AL16	#4	2	STR.	4'-10"	
AL17	#4	32	STR.	14'-0" AVG	5'-0" TO 23'-0"
AL18	#9	48	STR.	43'-10½" AVG	34'-10" TO 52'-11"
AL19	#9	3	STR.	34'-10"	
AT5	#4	71	STR.	33'-2"	
AT9	#4	18	STR.	15'-11½" AVG	1'-3" TO 30'-8"
AT10	#4	17	STR.	16'-10" AVG	3'-0" TO 30'-8"
FS2	#5	33	BNT.	7'-4"	
FS6	#5	5	BNT.	7'-6½"	

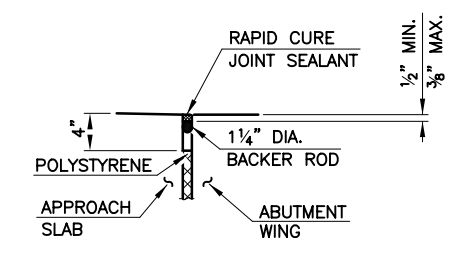
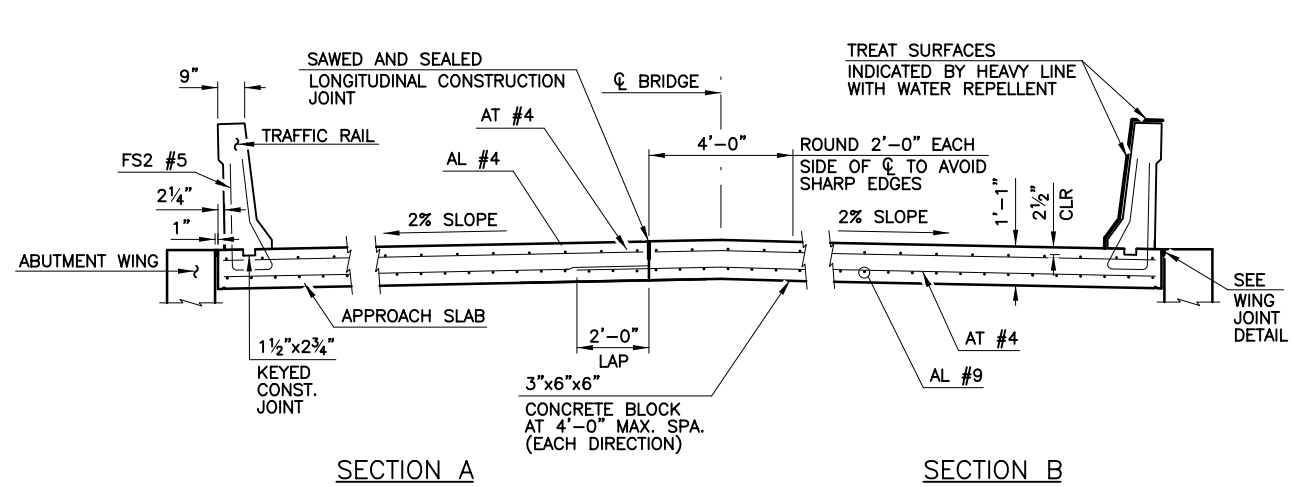
APPROACH SLAB NO. 1 QUANTITIES

ITEM	UNIT	PHASE I	PHASE II	TOTAL
APPROACH SLAB	SY	203.00	257.40	460.40
SAW-CUT GROOVING	SY	196.50	244.40	440.90
42" F-SHAPED PARAPET	LF	39.00	77.70	116.70
WATER REPELLENT (VISUALLY INSPECTED)	SY	20.00	38.00	58.00

APPROACH SLAB NO. 2 QUANTITIES

ITEM	UNIT	PHASE I	PHASE II	TOTAL
APPROACH SLAB	SY	266.60	162.20	428.80
SAW-CUT GROOVING	SY	254.30	156.30	410.60
42" F-SHAPED PARAPET	LF	73.70	35.00	108.70
WATER REPELLENT (VISUALLY INSPECTED)	SY	36.00	18.00	54.00

- ① INCLUDES ONE 5'-6" LAP
- ② TWO SETS OF 16 BARS



DESIGN:	DKC	06/18	US 270	SEMINOLE COUNTY
DRAWN:	LRJ	06/18	BRIDGE "B"	
CHECKED:	DMN	06/18		
APPRVD:	DMN	06/18		

CP&Y

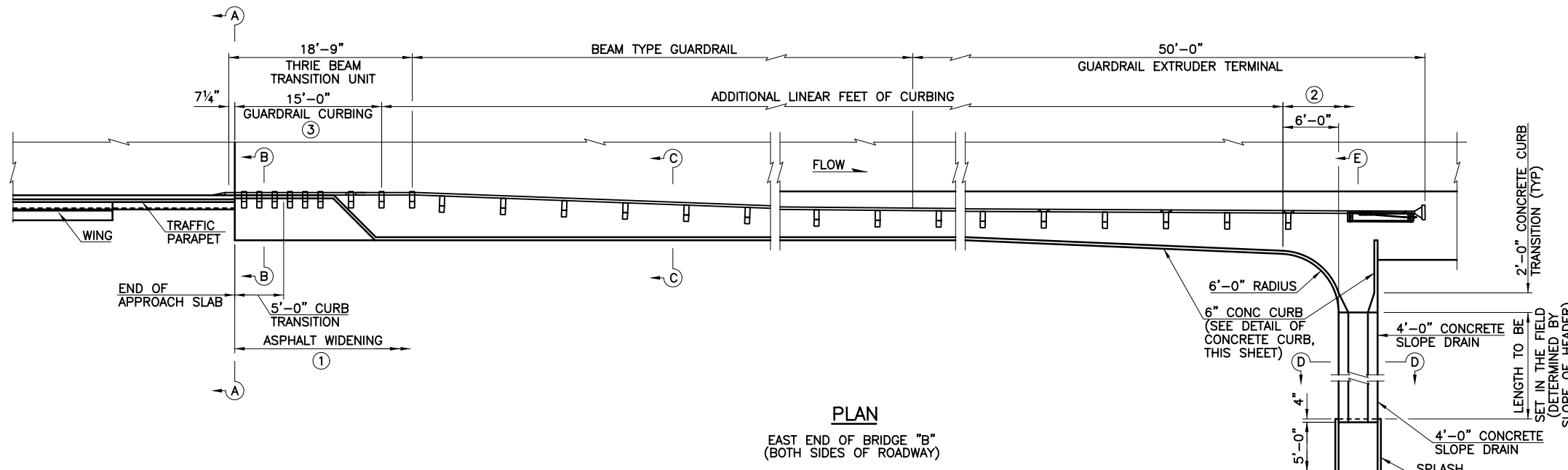
STATE JOB PIECE NO: 21006(07)

SHEET 1 OF 1
SHEET NO. B055

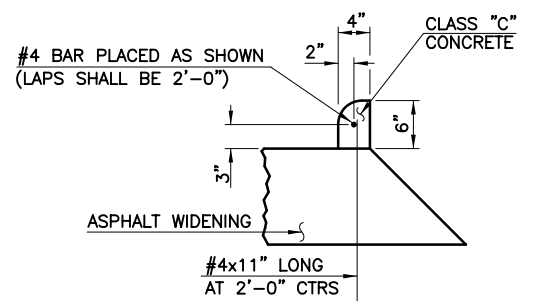
2:44:54 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 cpybdf_ANSIB.plt
 Plans and Drawings/8.00
 Bridge/8.3.07 Cut Sheets/8.30
 Bridge/21006BRBTD03.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	B056	B056

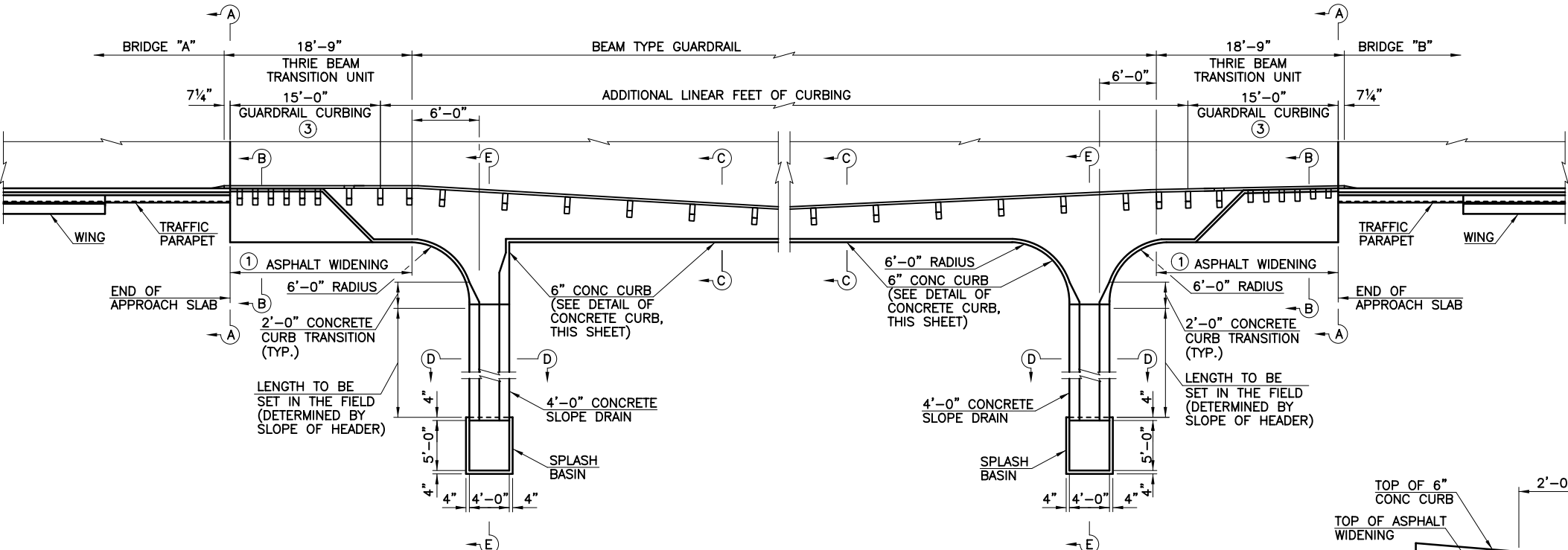
REVISIONS		
NO.	DATE	DESCRIPTION



PLAN
EAST END OF BRIDGE "B"
(BOTH SIDES OF ROADWAY)



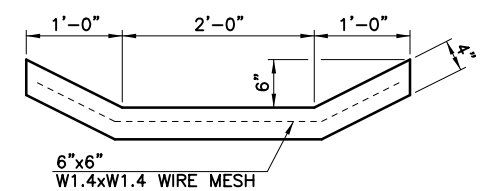
DETAIL OF CONCRETE CURB



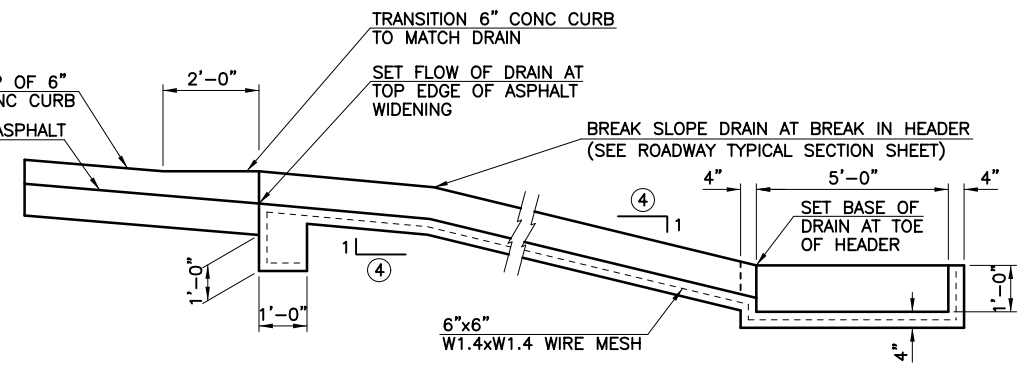
PLAN
EAST END OF BRIDGE "A"
(BOTH SIDES OF ROADWAY)

PLAN
WEST END OF BRIDGE "B"
(BOTH SIDES OF ROADWAY)

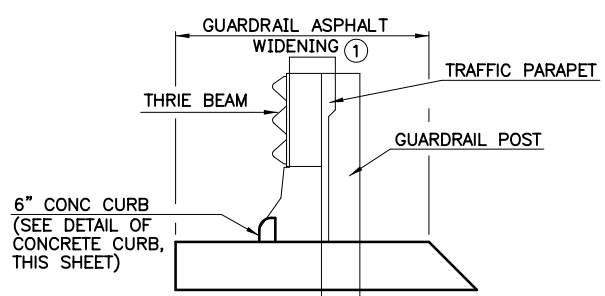
- GENERAL NOTES**
- ASPHALT WIDENING SHALL BE IN ACCORDANCE WITH ODOT STANDARDS GHW1-1-00, GHW2-1-00 AND THRI-1-00 EXCEPT AS SHOWN ON THIS SHEET. ALL COSTS OF ASPHALT WIDENING SHALL BE INCLUDED IN ROADWAY PAY ITEMS.
 - SLOPE DRAINS, SPLASH BASINS, AND CONCRETE CURBS SHALL BE CONSTRUCTED USING CLASS "C" CONCRETE AS SHOWN ON THIS SHEET. ALL COSTS OF THE SLOPE DRAINS, SPLASH BASINS, AND CONCRETE CURBS SHALL BE INCLUDED IN THE BRIDGE PAY ITEM FOR "CLASS "C" CONCRETE".
 - ADDITIONAL CURBING SHALL BE CONSTRUCTED AS SHOWN IN "DETAIL OF CONCRETE CURB", THIS SHEET. ALL COSTS OF MATERIALS AND INSTALLATION OF CLASS C CONCRETE CURB AND #4 BARS AS SHOWN SHALL BE PAID IN THE BRIDGE PAY ITEM FOR "CLASS C CONCRETE".
 - SLOPE TO MATCH SLOPE OF HEADER.



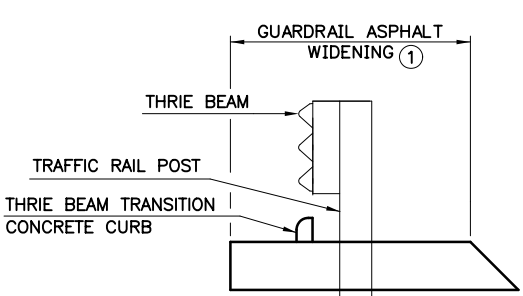
SECTION D-D



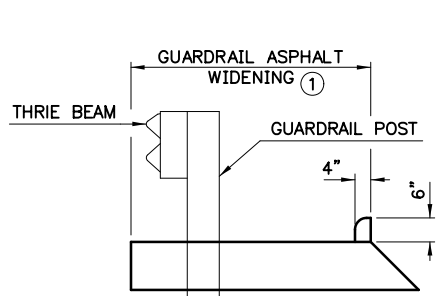
SECTION E-E



SECTION A-A



SECTION B-B



SECTION C-C

DESIGN: DKC	US 270	SEMINOLE COUNTY
DRAWN: LRJ	BRIDGE "B"	
CHECKED: DMN	DRAINS AT END OF BRIDGE	
APPRVD: DMN	STATE JOB PIECE NO: 21006(07)	SHEET 1 OF 1
CP&Y		SHEET NO. B056

2:44:57 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.07 Bridge\21006BRB01.dgn

REVISIONS		
REV. NO.	DESCRIPTION	DATE

U.S. ARMY CORPS OF ENGINEERS SECTION 404 PERMIT CONDITIONS

404 PERMIT INFORMATION

NATIONWIDE PERMIT NO. _____

TO BE PROVIDED AT A LATER DATE

SECTION 404 OF THE CLEAN WATER ACT REQUIRES PRIOR AUTHORIZATION FROM SECRETARY OF THE ARMY (CORPS) FOR THE DISCHARGE OF DREDGED OR FILL MATERIAL INTO WATERS OF THE UNITED STATES.

NO PRE-CONSTRUCTION NOTIFICATION REQUIRED: PROJECT DOES NOT REQUIRE NOTIFICATION TO THE US ARMY CORPS OF ENGINEERS (USACE) IN ORDER TO COMMENCE.

PRE-CONSTRUCTION NOTIFICATION REQUIRED: RESIDENT ENGINEER MUST NOTIFY THE USACE WITHIN 30 DAYS OF THE START OF CONSTRUCTION AND 30 DAYS PRIOR TO COMPLETION OF CONSTRUCTION, FORMS LOCATED IN THE CONTRACT.

INDIVIDUAL PERMIT: WILL BE MONITORED CLOSELY BY THE USACE.

GENERAL PERMIT: PROJECT WITHIN A DESIGNATED CRITICAL RESOURCE WATER AND WILL REQUIRE PRE-CONSTRUCTION NOTIFICATION SEE ABOVE FOR EXPLANATION OF PRE-CONSTRUCTION NOTIFICATION.

NO PERMIT REQUIRED

SWT TRACKING NO. _____

SPECIAL CONDITIONS

NAVIGABLE WATER OF THE U.S.

ON-SITE MITIGATION

ENDANGERED SPECIES PRESENT

HISTORIC PROPERTIES PRESENT

DESIGNATED CRITICAL RESOURCE WATERS

PERMIT GENERAL CONDITIONS

THE CONTRACTOR SHALL BE RESPONSIBLE BUT NOT LIMITED TO THE FOLLOWING HIGHLIGHTS OF THE 404 PERMIT (SEE CONTRACT FOR COMPLETE LIST):

TEMPORARY FILLS:

APPROPRIATE MEASURES MUST BE TAKEN TO MAINTAIN NORMAL DOWNSTREAM FLOWS AND MINIMIZE FLOODING TO THE MAXIMUM EXTENT PRACTICABLE. WHEN TEMPORARY STRUCTURES (WORK ROADS, WORK PADS, ETC.) WORK, AND DISCHARGES, INCLUDING COFFERDAMS, ARE NECESSARY FOR CONSTRUCTION ACTIVITIES, ACCESS FILLS, OR DE WATERING OF CONSTRUCTION SITES. TEMPORARY FILLS MUST CONSIST OF MATERIALS, AND BE PLACED IN A MANNER, THAT WILL NOT BE ERODED BY EXPECTED HIGH FLOWS. TEMPORARY FILLS MUST BE REMOVED IN THEIR ENTIRETY AND THE AFFECTED AREAS RETURNED TO PRE-CONSTRUCTION ELEVATIONS. THE AREAS AFFECTED BY TEMPORARY FILLS MUST BE RE VEGETATED, AS APPROPRIATE.

NAVIGATION:

NO ACTIVITY MAY CAUSE MORE THAN A MINIMAL ADVERSE EFFECT ON NAVIGATION WITHIN A NAVIGABLE WATER OF THE U.S. IF THIS PROJECT IS LOCATED WITHIN A NAVIGABLE WATER OF THE U.S., IT WILL BE IDENTIFIED IN THE SPECIAL CONDITIONS.

AQUATIC LIFE MOVEMENTS & ADVERSE EFFECTS FROM IMPOUNDMENTS:

NO ACTIVITY MAY LARGELY DISRUPT THE NECESSARY LIFE CYCLE MOVEMENTS OF THOSE SPECIES INDIGENOUS TO THE BODY OF WATER, INCLUDING THOSE SPECIES THAT NORMALLY MIGRATE THROUGH THE AREA. CULVERTS WILL BE DESIGNED TO PROVIDE SUFFICIENT PASSAGE FOR AQUATIC LIFE AND INSTALLED TO MAINTAIN LOW FLOW. RATE OF FLOW CANNOT BE MADE HIGHER THAN WHAT WAS PRIOR TO THE START OF CONSTRUCTION. EROSION CONTROL MEASURES SHOULD BE UTILIZED AROUND THE PERIMETER OF NEW STRUCTURES TO AVOID SILT BUILD UP. CAUTION SHOULD BE TAKEN TO MINIMIZE HARM IF CONSTRUCTION ACTIVITIES TAKE PLACE WITHIN A STREAM OR RIVER CHANNEL AND CREATE A CONFINED BODY OF WATER, CAUSE ADVERSE EFFECTS TO THE AQUATIC SYSTEM IN ANY WAY, AND/OR RESTRICTING ITS FLOW.

MANAGEMENT OF WATER FLOWS:

CONSTRUCTION ACTIVITIES MAY NOT IMPEDE THE PASSAGE OF NORMAL OR HIGH FLOWS. TO THE GREATEST EXTENT POSSIBLE, THE PRE- CONSTRUCTION COURSE, CONDITIONS, CAPACITY AND LOCATION OF OPEN WATERS MUST BE MAINTAINED. THIS INCLUDES STREAM CANALIZATION AND STORM WATER MANAGEMENT.

SUITABLE MATERIAL:

NO ACTIVITY MAY USE UNSUITABLE MATERIAL (E.G., TRASH, DEBRIS, CAR BODIES, ASPHALT, ETC.). MATERIALS USED FOR CONSTRUCTION OR DISCHARGED MUST BE FREE FROM TOXIC POLLUTANTS IN TOXIC AMOUNTS (SEE SECTION 307 OF CLEAN WATER ACT).

PROPER MAINTENANCE:

ANY AUTHORIZED STRUCTURE OR FILL SHALL BE PROPERLY MAINTAINED, INCLUDING MAINTENANCE TO ENSURE PUBLIC SAFETY AND COMPLIANCE WITH APPLICABLE NATION WIDE PERMIT GENERAL CONDITIONS, AS WELL AS ANY ACTIVITY- SPECIFIC CONDITIONS ADDED BY THE DISTRICT ENGINEER TO AN NATIONWIDE PERMIT AUTHORIZATION

HAZARDOUS MATERIALS:

HAZARDOUS MATERIALS, CHEMICALS, FUELS, LUBRICATING OILS AND OTHER SUCH SUBSTANCES SHOULD BE STORED AWAY FROM ANY STREAM OR RIVER CHANNEL (SEE SECTION 307 OF CLEAN WATER ACT)

EQUIPMENT:

HEAVY EQUIPMENT WORKING IN WETLANDS OR MUDFLATS MUST BE PLACED ON MATS, OR OTHER MEASURES MUST BE TAKEN TO MINIMIZE SOIL DISTURBANCE; FOR EXAMPLE IF WETLANDS ARE PRESENT WITHIN THE CONSTRUCTION, THE FOOTPRINT WILL BE SHOWN ON THE PLANS. MEASURES SHOULD BE TAKEN TO PREVENT DISCHARGE INTO ANY WATERS OF THE STATE (e.g. CONCRETE WASHOUT).

SOIL EROSION AND SEDIMENT CONTROLS:

APPROPRIATE SOIL EROSION AND SEDIMENT CONTROLS MUST BE USED AND MAINTAINED IN EFFECTIVE OPERATING CONDITION DURING CONSTRUCTION, AND ALL EXPOSED SOILS AND OTHER FILLS, AS WELL AS ANY WORK WITHIN STREAM OR RIVER CHANNELS OR BANKS, MUST BE PERMANENTLY STABILIZED AS SOON AS POSSIBLE.

404 COMPLIANCE:

IN ORDER TO REMAIN COMPLIANT WITH THE 404 PERMIT, THE PROJECT MUST COMPLY WITH ALL FEDERAL ENVIRONMENTAL PROTECTION LAWS ASSOCIATED AND, THE ENVIRONMENTAL COMMITMENTS AS SHOWN ON THE PLANS. THIS INCLUDES BUT IS NOT LIMITED TO COMPLIANCE WITH ALL ENVIRONMENTAL NOTES IN THE PLANS, INCLUDING CULTURAL RESOURCES, HAZARDOUS WASTE, BIOLOGICAL FOR PROTECTED SPECIES, AND DEQ STORM WATER REGULATIONS AS THEY PERTAIN TO THE SWMP SHEET WITHIN THE PLANS. ALL OF THE 404 PERMIT GENERAL AND SPECIFIC CONDITIONS MUST BE ADHERED TO. A COPY OF THESE CONDITIONS CAN BE FOUND IN THE CONTRACT WITH THE 404 PERMIT.

PERMIT GENERAL CONDITIONS

FUELING:

ALL FUELING AND SERVICING OF VEHICLES AND EQUIPMENT SHALL BE DONE ABOVE THE ORDINARY HIGH WATER MARK (OHWM).

MATERIAL STORAGE:

STORE MATERIAL AND FUEL OUTSIDE OF THE ORDINARY HIGH WATER MARK OR ANY AREA LIKELY TO FLOOD.

DEBRIS STORAGE:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR REMOVING ANY MATERIALS, DEBRIS, OR REFUSE WHICH HAS FALLEN INTO ANY STREAM OR RIVER CHANNELS RESULTING FROM THE EXECUTION OF THE PROJECT AS SOON AS POSSIBLE

SEE NATIONWIDE PERMIT 14 IN THE CONTRACT

401 CERTIFICATION CONDITIONS

THE CONTRACTOR SHALL BE RESPONSIBLE BUT NOT LIMITED TO THE FOLLOWING HIGHLIGHTS OF THE 401 CERTIFICATION (SEE CONTRACT FOR COMPLETE LIST):

ALL SPILLS OF FUEL OR POLLUTANTS IN EXCESS OF FIVE GALLONS SHALL BE REPORTED TO ODEQ WITHIN 24 HRS AND REPORTED TO POLLUTION PREVENTION HOTLINE (1-800-522-0206)

ALL FUELING AND SERVICING OF VEHICLES AND EQUIPMENT SHALL BE DONE OUTSIDE THE ORDINARY HIGH WATER MARK

THE PERMITTEE SHALL PROVIDE ACCESS TO THE PROPERTY TO ODEQ FOR INSPECTIONS.

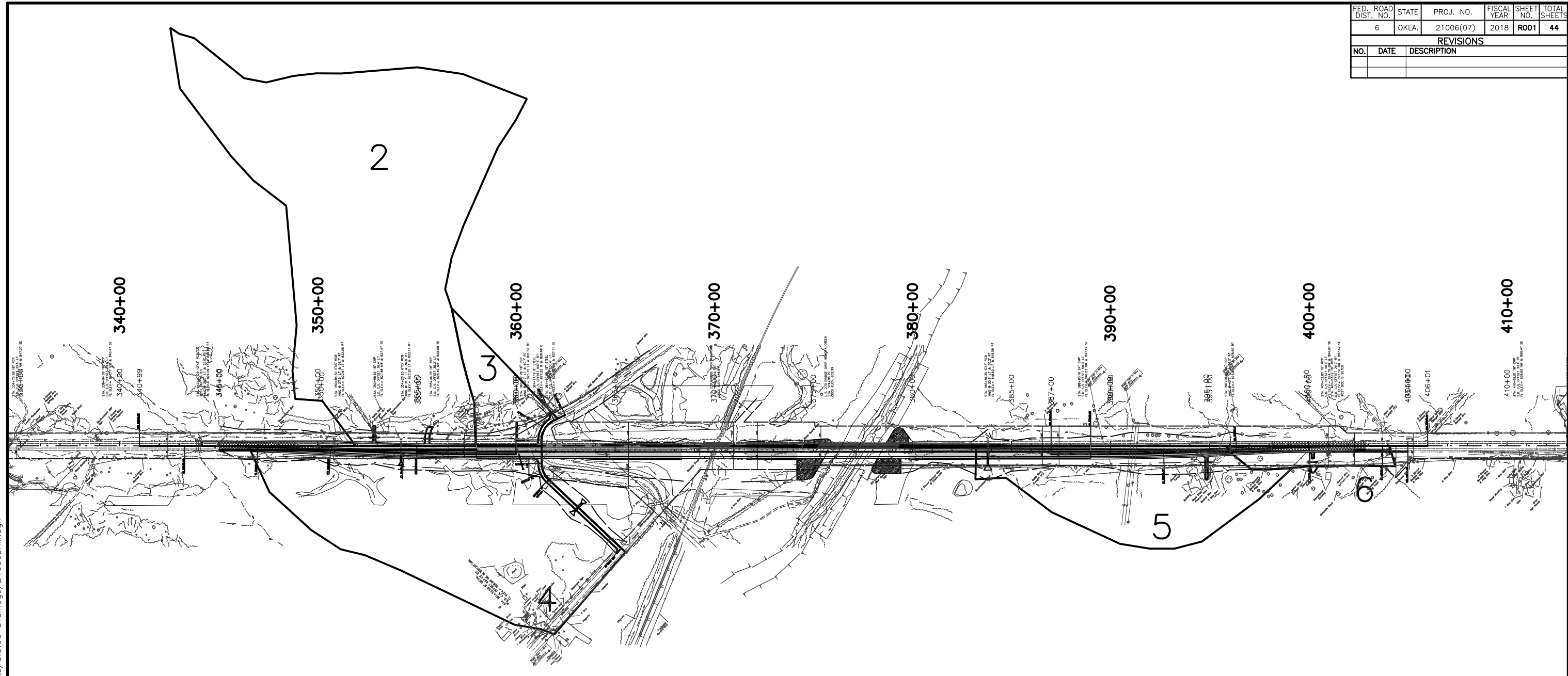
ANY STOCKPILE SHALL BE ABOVE ORDINARY HIGH WATER MARK AND REMOVED FROM LIKELY FLOOD ZONE

BEST MANAGEMENT PRACTICES SHOULD BE USED TO CONTROL SOIL EROSION AND MAINTAIN COMPLIANCE WITH WATER QUALITY STANDARDS.

FOR ANY PROJECT THAT INVOLVES BANK STABILIZATION, THE PERMITTEE SHALL CONSIDER INSTALLING BIOENGINEERING PRACTICES IN PLACE OF STRUCTURAL PRACTICES (RIPRAP) TO MINIMIZE IMPACTS TO AQUATIC RESOURCES

STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	DETAIL		
		REVIEW		
		APPROVED		
		ENVIRONMENTAL DIVISION		
		JOB/PIECE NO.	SHEET NO.	

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R001	44
REVISIONS					
NO.	DATE	DESCRIPTION			



DRAINAGE DESIGN RECORD

STRUCTURE NO.	DRAINAGE AREA NUMBER	DESIGN YEAR	CRL STATION	STRUCTURE SIZE AND TYPE	DRAINAGE AREA		ANTICIPATED LAND USE	k FACTOR	k' FACTOR	C RUNOFF COEFFICIENT	LENGTH OF OVERLAND FLOW		LENGTH OF CHANNEL FLOW		MANNING'S 'n' FOR CHANNEL	HYDRAULIC RADIUS OF CHANNEL	VELOCITY FOR CHANNEL FLOW		T ₁₀ TIME OF CONC. OVERLAND	T ₁₀ TIME OF CONC. CHANNEL	T ₁₀ TIME OF CONC. TOTAL	INTENSITY OF DESIGN YEAR RAINFALL	WRIGHT-MCLAUGHLIN FACTOR	DISCHARGE OF DESIGN YEAR RAINFALL		STRUCTURE CAPACITY	VELOCITY @ 50 YR	OUTLET STRUCTURE	OUTLET SLOPE
					ACRE						LF	FT/FT	LF	FT/FT			FT/SEC	MIN.						CFS	CFS				
2	2	50	354+20.23	EXTEND 6'x3' RCB	44.7	WOODLAND	0.942	0.01020	0.70	882	0.036	1686	0.0415	0.050	0.500	3.83	22.49	10.60	33.09	4.87	1.20	183.31	252.82	15.73	6'x3 RCB	1.51%			
4	3	50	360+02.78	EXTEND 18" RCP	3.8	WOODLAND	0.942	0.01020	0.71	730	0.047	N/A	N/A	0.050	0.500	0.00	19.89	0.00	19.89	6.30	1.20	20.21	25.12	15.81	18" RCP	5.72%			
5	5,6	50	383+81.53	EXTEND 5'x7' RCB	12.2	WOODLAND	0.942	0.01020	0.74	N/A	N/A	2075	0.0397	0.050	0.500	3.74	0.00	12.65	12.65	7.59	1.20	82.05	660.85	12.17	7'x5' RCB	1.65%			
8	2,3,4	50	SL 13+00.00	CONST. 8'x4' RCB	73.2	WOODLAND	0.942	0.01020	0.72	882	0.036	2910	0.0248	0.050	0.500	2.95	22.49	19.69	42.18	4.24	1.20	268.28	386.28	13.32	8'x4' RCB	0.76%			

2:45:05 PM
 cpy:pdf_ANSIB.tbl
 Plans and Drawings/8.30 Cut Sheets/8.3.06 Drainage/21006DAM.dgn
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		

DRAINAGE AREA MAP

SHEET 1 OF 1
SHEET NO. R001

STATE JOB PIECE NO: 21006(07)

STORM WATER MANAGEMENT PLAN

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	RO02	44
REVISIONS					
NO.	DATE	DESCRIPTION			

SITE DESCRIPTION

PROJECT LIMITS: PROJECT BEGINS IN SECTION 8 T-8-N, R-7-E AND EXTENDS SOUTHEAST 0.586 MILES IN SECTION 16 T-8-N, R-7-E.

PROJECT DESCRIPTION: GRADING, DRAINING, BRIDGE AND SURFACING OF 0.586 MILES OF UNDIVIDED HIGHWAY.

SUGGESTED SEQUENCE OF EROSION CONTROL ACTIVITIES:

1. VEGETATIVE STRIPPING
2. UNDERCUT & STOCKPILE EXISTING TOPSOIL
3. INSTALL PERIMETER EROSION CONTROL MEASURES
4. ROADWAY EXCAVATION AND EMBANKMENT
5. BRIDGE CONSTRUCTION
6. CULVERT TRENCHING AND CONSTRUCTION
7. INSTALL TEMP. SEDIMENT FILTERS, SOD DITCHES, & VEGETATIVE MULCH
8. CONST. FINISHED ROADWAY PAVING
9. SPREAD TOPSOIL
10. INSTALL SOLID SLAB SOD

SOIL TYPE: SILTY/SAND

TOTAL AREA OF THE CONSTRUCTION SITE: 39.70 AC

ESTIMATED AREA TO BE DISTURBED: 19.77 AC

OFFSITE AREA TO BE DISTURBED: (FOR CONTRACTOR USE)

TOTAL IMPERVIOUS AREA PRE-CONSTRUCTION: 3.67 AC

TOTAL IMPERVIOUS AREA POST-CONSTRUCTION: 6.49 AC

POST-CONSTRUCTION RUNOFF COEFFICIENT OF THE SITE: 0.49

LATITUDE & LONGITUDE OF CENTER OF PROJECT: 35° 10' 33.00" N AND 96° 34' 57.60" W

PROJECT WILL DISCHARGE TO:

NAME OF RECEIVING WATERS: WEWOKA CREEK

SENSITIVE WATERS OR WATERSHEDS: YES NO

303(d) IMPAIRED WATERS: YES NO

IF YES, LIST IMPAIRMENT:

LOCATED IN A TMDL: YES NO

LAKE THUNDERBIRD TMDL: YES NO

MS4 ENTITY: YES NO

IF YES, LOCATION:

NOTE: THIS SHEET SHOULD BE USED IN CONJUNCTION WITH A DRAINAGE MAP THAT ILLUSTRATES THE DRAINAGE PATTERNS/PATHWAYS AND RECEIVING WATERS FOR THIS PROJECT. THIS SHEET SHOULD ALSO BE USED WITH THE EROSION CONTROL SUMMARIES, PAY ITEMS, & NOTES.

EROSION AND SEDIMENT CONTROLS

SOIL STABILIZATION PRACTICES:

- TEMPORARY SEEDING
- PERMANENT SODDING, SPRIGGING OR SEEDING
- VEGETATIVE MULCHING
- SOIL RETENTION BLANKET
- PRESERVATION OF EXISTING VEGETATION

NOTE: TEMPORARY EROSION CONTROL METHODS MUST BE USED ON ALL DISTURBED AREAS WHERE CONSTRUCTION ACTIVITIES HAVE CEASED FOR OVER 14 DAYS. METHODS USED WILL BE AS SHOWN ON PLANS, OR AS DIRECTED BY THE ENGINEER.

STRUCTURAL PRACTICES:

- STABILIZED CONSTRUCTION EXIT
- TEMPORARY SILT FENCE
- TEMPORARY SILT DIKES
- TEMPORARY FIBER LOG
- DIVERSION, INTERCEPTOR OR PERIMETER DIKES
- DIVERSION, INTERCEPTOR OR PERIMETER SWALES
- ROCK FILTER DAMS
- TEMPORARY SLOPE DRAIN
- PAVED DITCH W/ DITCH LINER PROTECTION
- TEMPORARY DIVERSION CHANNELS
- TEMPORARY SEDIMENT BASINS
- TEMPORARY SEDIMENT TRAPS
- TEMPORARY SEDIMENT FILTERS
- TEMPORARY SEDIMENT REMOVAL
- RIP RAP
- INLET SEDIMENT FILTER
- TEMPORARY BRUSH SEDIMENT BARRIERS
- SANDBAG BERMS
- TEMPORARY STREAM CROSSINGS

OFFSITE VEHICLE TRACKING:

- HAUL ROADS DAMPENED FOR DUST CONTROL
- LOADED HAUL TRUCKS TO BE COVERED WITH TARPAULIN
- EXCESS DIRT ON ROAD REMOVED DAILY

NOTES:

THE CONTRACTOR SHALL ALSO BE RESPONSIBLE FOR THE FOLLOWING:

MAINTENANCE AND INSPECTION:

ALL EROSION AND SEDIMENT CONTROLS WILL BE MAINTAINED IN GOOD WORKING ORDER FROM THE BEGINNING OF CONSTRUCTION UNTIL AN ACCEPTABLE VEGETATIVE COVER IS ESTABLISHED. INSPECTION BY THE CONTRACTOR AND ANY NECESSARY REPAIRS SHALL BE PERFORMED ONCE EVERY 7 CALENDAR DAYS AND WITHIN 24 HOURS AFTER ANY STORM EVENT GREATER THAN 0.5 INCH AS RECORDED BY A NON-FREEZING RAIN GAUGE TO BE LOCATED ON SITE. POTENTIALLY ERODIBLE AREAS, DRAINAGEWAYS, MATERIAL STORAGE, STRUCTURAL DEVICES, CONSTRUCTION ENTRANCES AND EXITS ALONG WITH EROSION AND SEDIMENT CONTROL LOCATIONS ARE EXAMPLES OF SITES THAT NEED TO BE INSPECTED.

WASTE MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF CONSTRUCTION WASTE MATERIAL IS REQUIRED BY THE CONTRACTOR. MATERIALS INCLUDE STOCKPILES, SURPLUS, DEBRIS AND ALL OTHER BY-PRODUCTS FROM THE CONSTRUCTION PROCESS. PRACTICES INCLUDE DISPOSAL, PROPER MATERIALS HANDLING, SPILL PREVENTION AND CLEANUP MEASURES. CONTROLS AND PRACTICES SHALL MEET THE REQUIREMENTS OF ALL FEDERAL, STATE AND LOCAL AGENCIES.

HAZARDOUS MATERIALS:

PROPER MANAGEMENT AND DISPOSAL OF HAZARDOUS WASTE MATERIALS IS REQUIRED. THE CONTRACTOR IS RESPONSIBLE FOR FOLLOWING MANUFACTURER'S RECOMMENDATIONS, STATE AND FEDERAL REGULATIONS TO ENSURE CORRECT HANDLING, DISPOSAL, SPILL PREVENTION AND CLEANUP MEASURES. EXAMPLES INCLUDE BUT ARE NOT LIMITED TO: PAINTS, ACIDS, CLEANING SOLVENTS, CHEMICAL ADDITIVES, CONCRETE CURING COMPOUNDS AND CONTAMINATED SOILS.

GENERAL NOTES:

A STORM WATER POLLUTION PREVENTION PLAN (SWPPP) IS REQUIRED TO COMPLY WITH THE OKLAHOMA POLLUTION DISCHARGE ELIMINATION SYSTEM (OPDES) REGULATIONS. THIS PLAN IS INITIATED DURING THE DESIGN PHASE, CONFIRMED IN THE PRE-WORK MEETINGS AND AVAILABLE ON THE JOB SITE ALONG WITH COPIES OF THE NOTICE OF INTENT (NOI) FORM AND PERMIT CERTIFICATE THAT HAVE BEEN FILED WITH THE OKLAHOMA DEPARTMENT OF ENVIRONMENTAL QUALITY (ODEQ). THE PLAN MUST BE KEPT CURRENT WITH UP-TO-DATE AMENDMENTS DURING THE PROGRESSION OF THE PROJECT. ALL CONTRACTOR OFF-SITE OPERATIONS ASSOCIATED WITH THE PROJECT MUST BE DOCUMENTED IN THE SWPPP, I.E., BORROW PITS, WORK ROADS, DISPOSAL SITES, ASPHALT/CONCRETE PLANTS, ETC. THE BASIC GOAL OF STORM WATER MANAGEMENT IS TO IMPROVE WATER QUALITY BY REDUCING POLLUTANTS IN STORM WATER DISCHARGES. RUNOFF FROM CONSTRUCTION SITES HAS A POTENTIAL FOR POLLUTION DUE TO EXPOSED SOILS AND THE PRESENCE OF HAZARDOUS MATERIALS USED IN THE CONSTRUCTION PROCESS. THE PREVENTION OF SOIL EROSION, CONTAINMENT OF HAZARDOUS MATERIALS AND/OR THE INTERCEPTION OF THESE POLLUTANTS BEFORE LEAVING THE CONSTRUCTION SITE ARE THE BEST PRACTICES FOR CONTROLLING STORM WATER POLLUTION.

THE FOLLOWING SECTIONS OF THE 2009 ODOT STANDARD SPECIFICATIONS SHOULD BE NOTED:

- 103.05 BONDING REQUIREMENTS
- 104.10 FINAL CLEANING UP
- 104.12 CONTRACTOR'S RESPONSIBILITY FOR WORK
- 104.13 ENVIRONMENTAL PROTECTION
- 106.08 STORAGE AND HANDLING OF MATERIAL
- 107.01 LAWS, RULES AND REGULATIONS TO BE OBSERVED
- 107.20 STORM WATER MANAGEMENT
- 220 MANAGEMENT OF EROSION, SEDIMENTATION AND STORM WATER POLLUTION PREVENTION AND CONTROL
- 221 TEMPORARY SEDIMENT CONTROL

IN ADDITION:

"ODEQ GENERAL PERMIT (OKR10) FOR STORM WATER DISCHARGES FROM CONSTRUCTION ACTIVITIES WITHIN THE STATE OF OKLAHOMA." ODEQ, WATER QUALITY DIVISION, SEPTEMBER 13, 2017.

9:54:42 AM cpyb&w_ANSIB.tbl Plans and Drawings/8.30 Cut Sheets/8.3.11 SW3P/21006SW3P.dgn
 6/7/2018
 bmcanelly cpyb&w_ANSIB.tbl
 p.w./Active Projects/ODOT13446.00/8.00

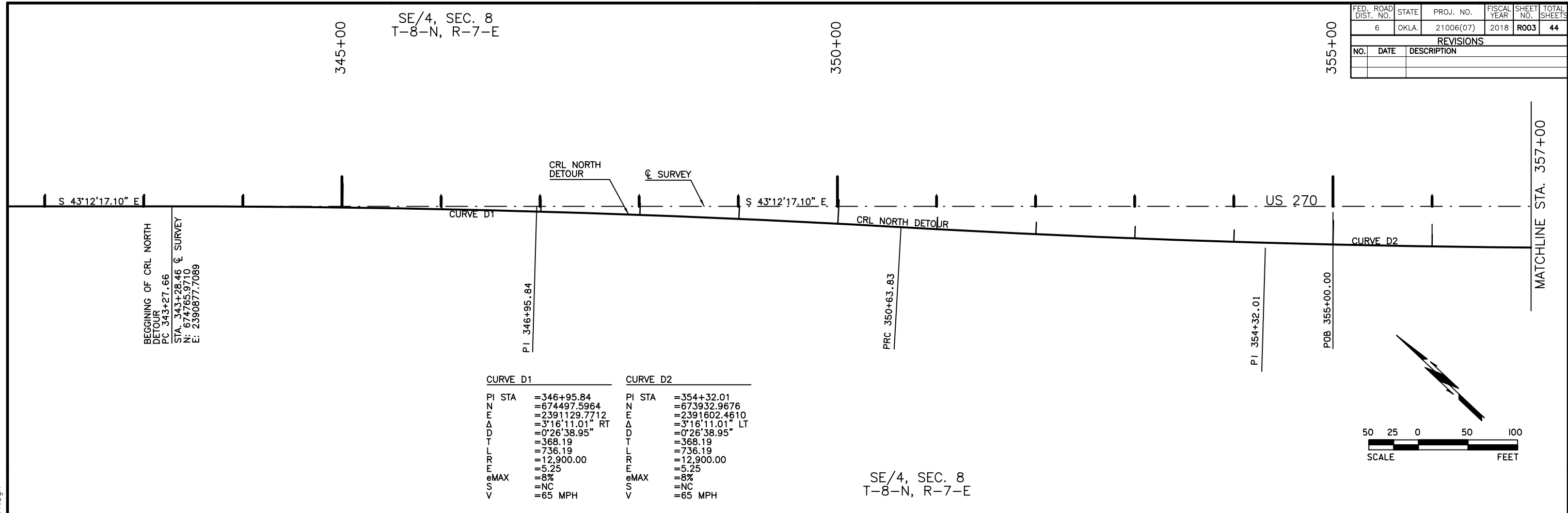
REVISED 08 / 18 / 2017

DESIGN: CPY	06/18	US 270 STORMWATER MANAGEMENT PLAN STATE JOB PIECE NO: 21006(07) SHEET NO. R002	SEMINOLE COUNTY
DRAWN: CPY	06/18		
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		

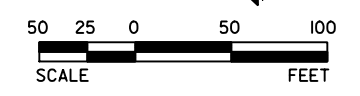
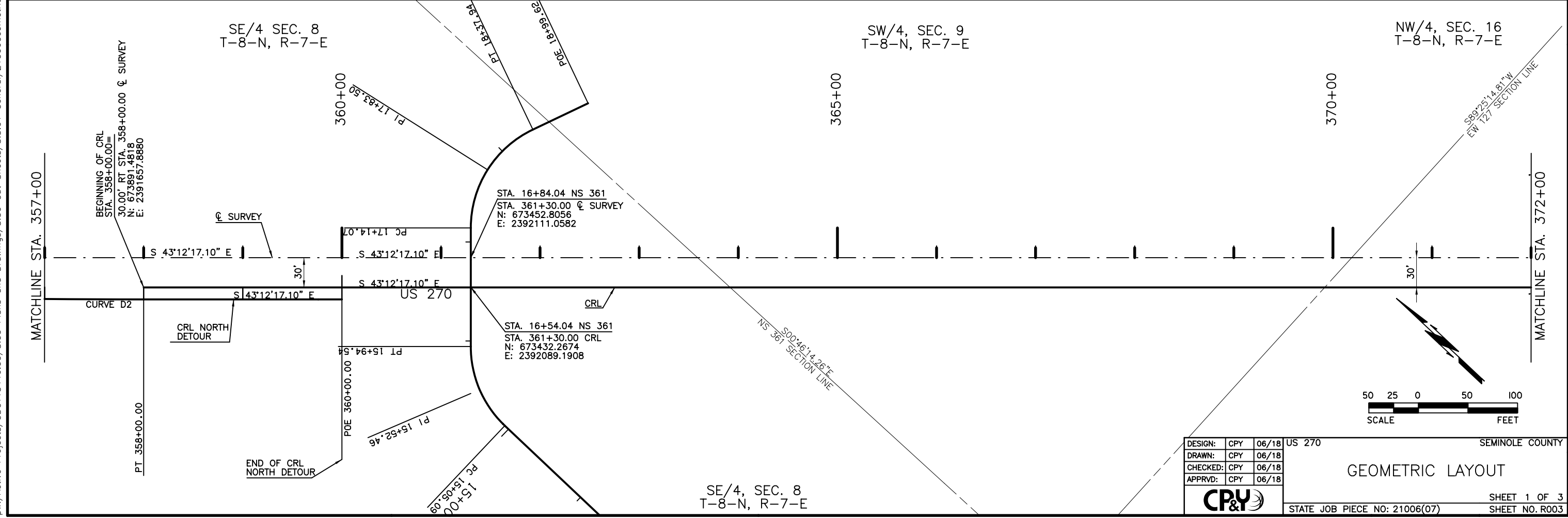
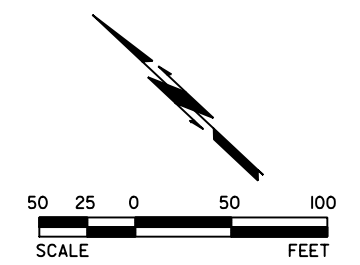


bmcanelly 6/5/2018 2:45:19 PM
 cpyb&w_ANSIB.tbl cpypdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.01 General\21006Geometric01.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	003	44
REVISIONS					
NO.	DATE	DESCRIPTION			



CURVE D1		CURVE D2	
PI STA	= 346+95.84	PI STA	= 354+32.01
N	= 674497.5964	N	= 673932.9676
E	= 2391129.7712	E	= 2391602.4610
Δ	= 3°16'11.01" RT	Δ	= 3°16'11.01" LT
D	= 0°26'38.95"	D	= 0°26'38.95"
T	= 368.19	T	= 368.19
L	= 736.19	L	= 736.19
R	= 12,900.00	R	= 12,900.00
E	= 5.25	E	= 5.25
eMAX	= 8%	eMAX	= 8%
S	= NC	S	= NC
V	= 65 MPH	V	= 65 MPH

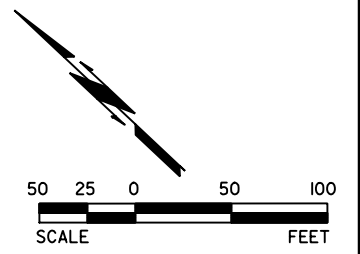
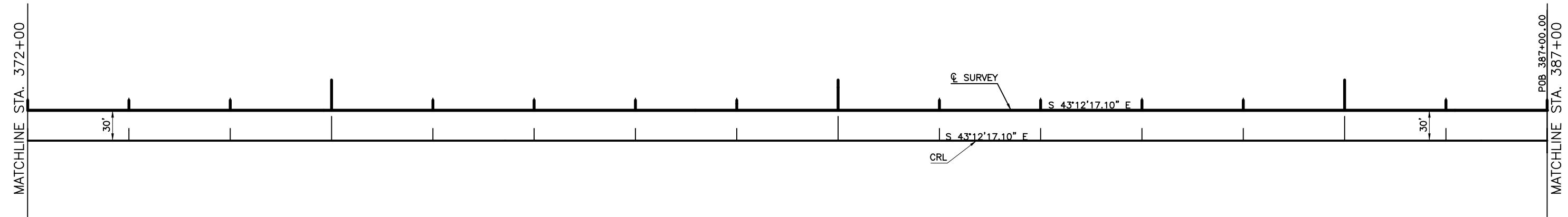


DESIGN: CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY	06/18		
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		
CP&Y		STATE JOB PIECE NO: 21006(07)	
		SHEET 1 OF 3	
		SHEET NO. R003	

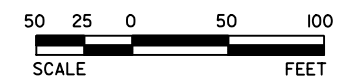
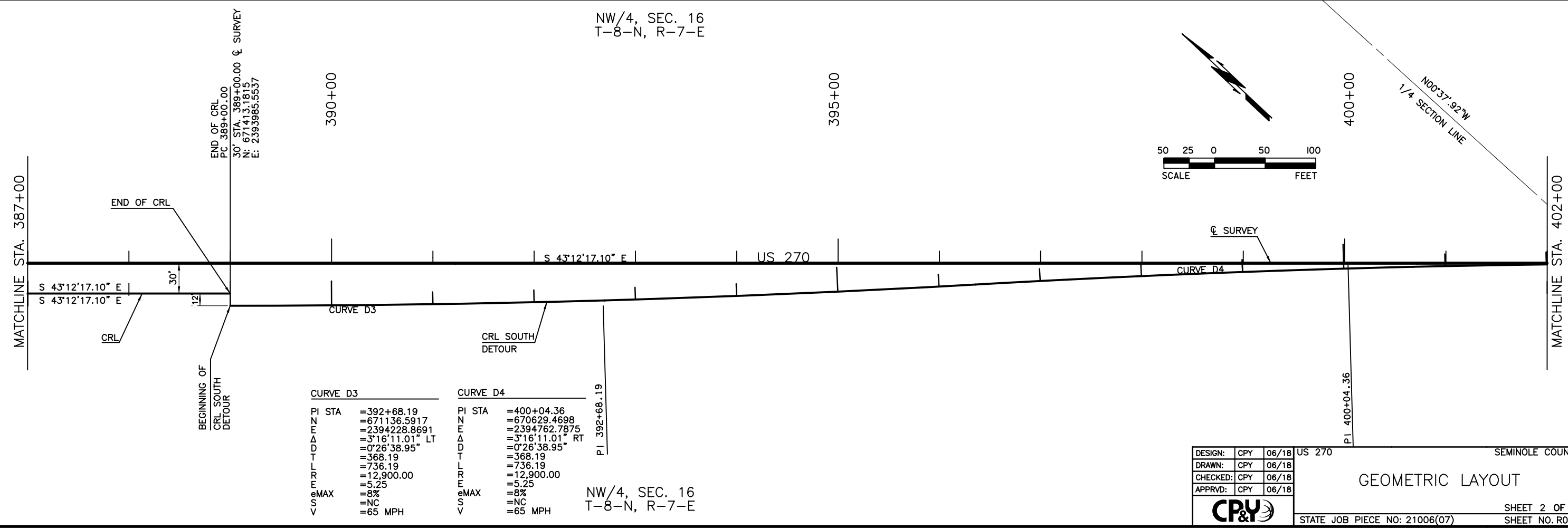
GEOMETRIC LAYOUT

6/5/2018 2:45:24 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.01 General\21006Geometric02.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R004	44
REVISIONS					
NO.	DATE	DESCRIPTION			



NW/4, SEC. 16 T-8-N, R-7-E



CURVE D3		CURVE D4	
PI STA	=392+68.19	PI STA	=400+04.36
N	=671136.5917	N	=670629.4698
E	=2394228.8691	E	=2394762.7875
Δ	=3°16'11.01" LT	Δ	=3°16'11.01" RT
D	=0°26'38.95"	D	=0°26'38.95"
T	=368.19	T	=368.19
L	=736.19	L	=736.19
R	=12,900.00	R	=12,900.00
E	=5.25	E	=5.25
eMAX	=8%	eMAX	=8%
S	=NC	S	=NC
V	=65 MPH	V	=65 MPH

DESIGN:	CPY	06/18
DRAWN:	CPY	06/18
CHECKED:	CPY	06/18
APPRVD:	CPY	06/18

US 270 SEMINOLE COUNTY
 GEOMETRIC LAYOUT
 STATE JOB PIECE NO: 21006(07)
 SHEET 2 OF 3
 SHEET NO. R004

NW/4, SEC. 16
 T-8-N, R-7-E

NE/4, SEC. 16
T-8-N, R-7-E

SE/4, SEC. 16
T-8-N, R-7-E

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	0005	44

REVISIONS		
NO.	DATE	DESCRIPTION

MATCHLINE STA. 402+00

CRL SOUTH DETOUR

CURVE D-4 S 43°12'17.10" E US 270

Q SURVEY

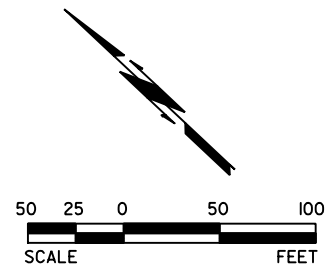
END OF CRL SOUTH DETOUR
PT. 403+72.34
STA. 403+71.5387 Q SURVEY
N: 670361.0953
E: 2395014.8498

N00°55'37.92"W
1/4 SECTION LINE

NW/4, SEC. 16
T-8-N, R-7-E

CURVE D-4

PI STA	= 400+04.36
N	= 670629.4698
E	= 2394762.7875
Δ	= 3°16'11.01" RT
D	= 0'26'38.95"
T	= 368.19
L	= 736.19
R	= 12,900.00
E	= 5.25
eMAX	= 8%
S	= NC
V	= 65 MPH



SE/4, SEC. 16
T-8-N, R-7-E

2:45:30 PM
 6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.01 General/21006Geometric03.dgn

SE/4, SEC. 8
T-8-N, R-7-E

BEGIN CRL NS 3610
POB 10+00.00
N: 672806.6518
E: 2392009.3614

N 0°12'01.44" E
NS 3610 RD.

15+00
PC 15+05.09

PI 15+62.46

CRL DETOUR

PT 15+94.54

CRL SECTION
LINE RD

STA. 16+54.0446 NS 3610
STA. 361+30.00 CRL
N: 673432.2674
E: 2392089.1908

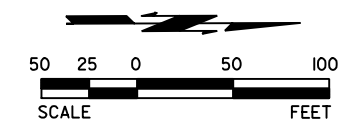
STA. 16+84.0446 NS 3610
STA. 361+30.00 Q SURVEY
N: 673452.8056
E: 2392111.0582

CURVE S-1

PI STA	= 15+52.46
N	= 673359.10
E	= 2392011.29
Δ	= 46°35'41.46" RT
D	= 52°05'13.46"
T	= 47.37
L	= 89.46
R	= 110.00
E	= 9.77
eMAX	= n/a
S	= NC
V	= n/a

CURVE S2

PI STA	= 17+83.50
N	= 673520.89
E	= 2392183.55
Δ	= 64°31'06.4" RT
D	= 52°05'13.5"
T	= 69.43
L	= 123.87
R	= 110.00
E	= 20.08
eMAX	= n/a
S	= NC
V	= n/a



NS 3610 RD.

S89°25'14.81"W
E 1270 SECTION LINE ROAD

CRL

US 270

S 43°12'17.10" E

PC 17+14.07

Q SURVEY

S 68°41'11" E

PI 17+83.50

NS 3610 RD.

PT 18+37.94

N00°46'32.04"W
NS 361 SECTION LINE ROAD

END OF CRL NS 3610
POE 18+99.62
N: 673473.2354
E: 2392305.7017

SW/4, SEC. 9
T-8-N, R-7-E

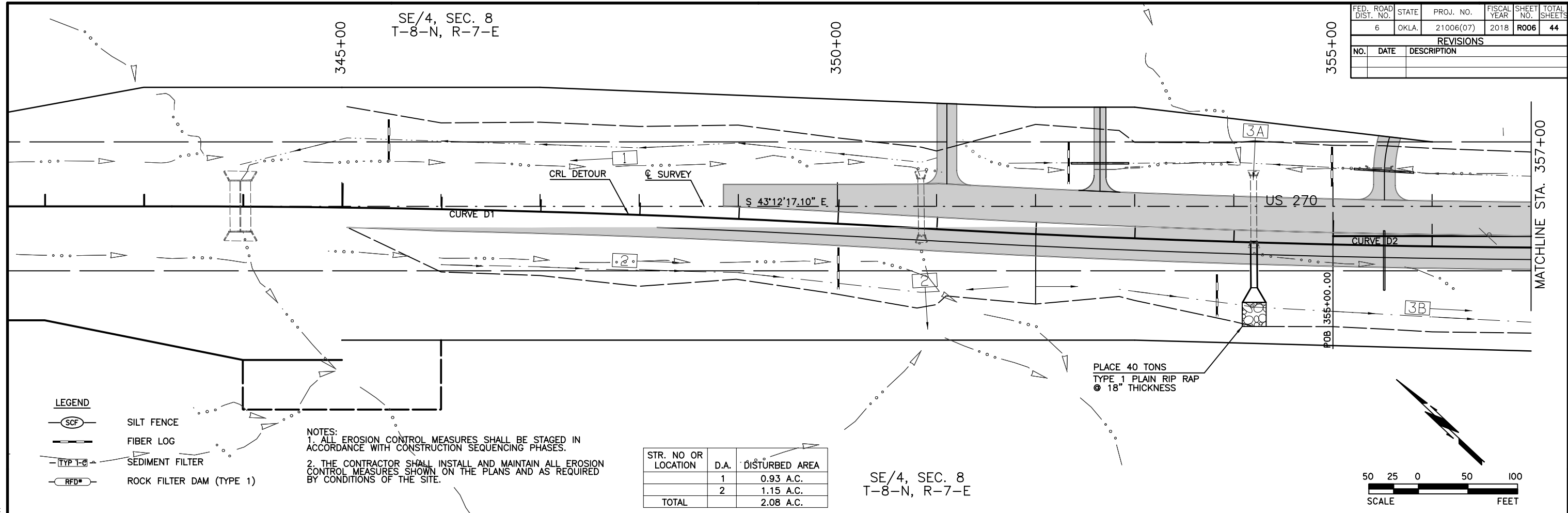
DESIGN: CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY	06/18		
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		

GEOMETRIC LAYOUT

STATE JOB PIECE NO: 21006(07)

SHEET 3 OF 3
SHEET NO. R005

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R006	44
REVISIONS					
NO.	DATE	DESCRIPTION			

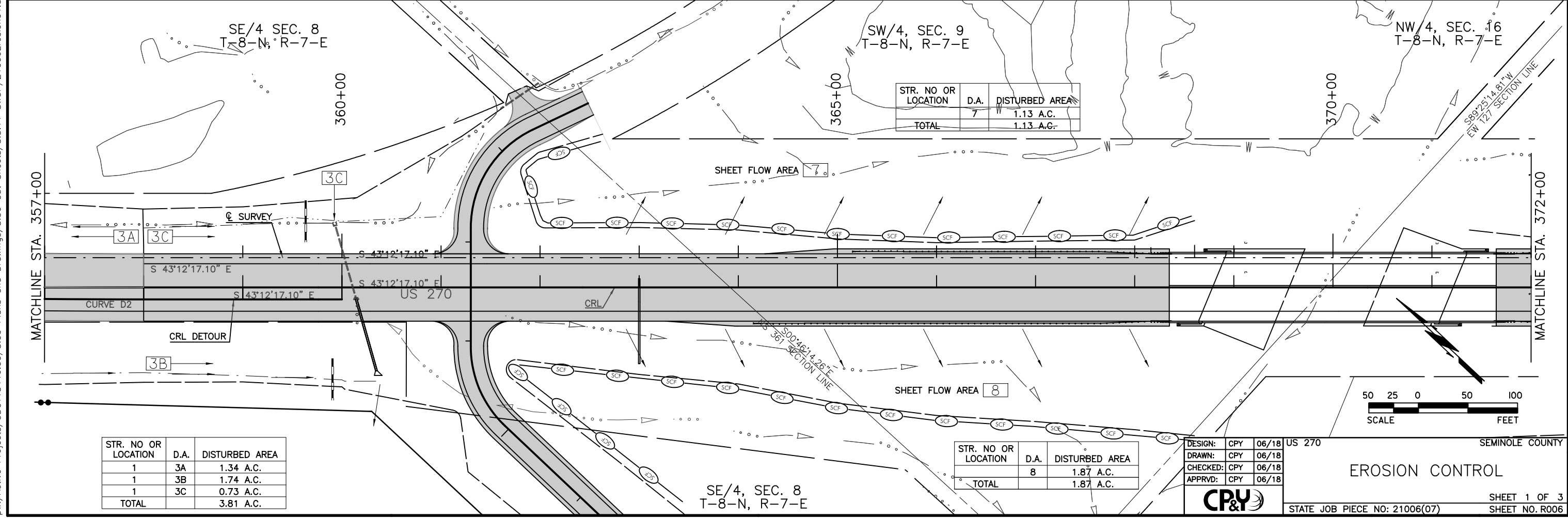
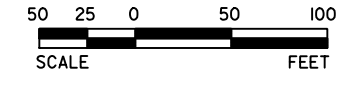


LEGEND

- SILT FENCE
- FIBER LOG
- SEDIMENT FILTER
- ROCK FILTER DAM (TYPE 1)

NOTES:
 1. ALL EROSION CONTROL MEASURES SHALL BE STAGED IN ACCORDANCE WITH CONSTRUCTION SEQUENCING PHASES.
 2. THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES SHOWN ON THE PLANS AND AS REQUIRED BY CONDITIONS OF THE SITE.

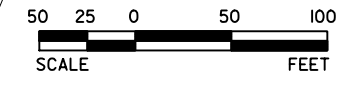
STR. NO OR LOCATION	D.A.	DISTURBED AREA
1		0.93 A.C.
2		1.15 A.C.
TOTAL		2.08 A.C.



STR. NO OR LOCATION	D.A.	DISTURBED AREA
1	3A	1.34 A.C.
1	3B	1.74 A.C.
1	3C	0.73 A.C.
TOTAL		3.81 A.C.

STR. NO OR LOCATION	D.A.	DISTURBED AREA
7		1.13 A.C.
TOTAL		1.13 A.C.

STR. NO OR LOCATION	D.A.	DISTURBED AREA
8		1.87 A.C.
TOTAL		1.87 A.C.



DESIGN:	CPY	06/18
DRAWN:	CPY	06/18
CHECKED:	CPY	06/18
APPRVD:	CPY	06/18

SEMINOLE COUNTY
EROSION CONTROL
 STATE JOB PIECE NO: 21006(07)
 SHEET 1 OF 3
 SHEET NO. R006

6/5/2018 2:45:35 PM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.11 SW3P\21006Erosion01.dgn

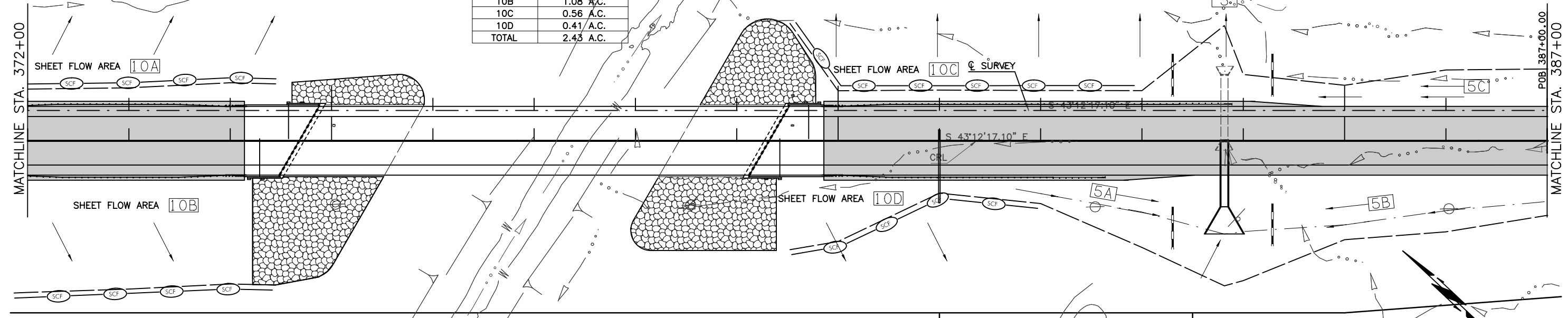
NW/4, SEC. 16
T-8-N, R-7-E

NW/4, SEC. 16
T-8-N, R-7-E

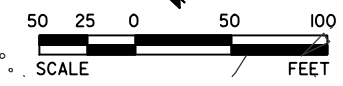
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	007	44
REVISIONS					
NO.	DATE	DESCRIPTION			

D.A.	DISTURBED AREA
10A	0.38 A.C.
10B	1.08 A.C.
10C	0.56 A.C.
10D	0.41 A.C.
TOTAL	2.43 A.C.

STR. NO OR LOCATION	D.A.	DISTURBED AREA
1	5A	0.43 A.C.
1	5B	4.24 A.C.
1	5C	1.98 A.C.
TOTAL		6.65 A.C.



- NOTES:
- ALL EROSION CONTROL MEASURES SHALL BE STAGED IN ACCORDANCE WITH CONSTRUCTION SEQUENCING PHASES.
 - THE CONTRACTOR SHALL INSTALL AND MAINTAIN ALL EROSION CONTROL MEASURES SHOWN ON THE PLANS AND AS REQUIRED BY CONDITIONS OF THE SITE.

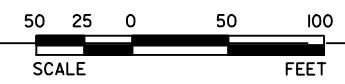
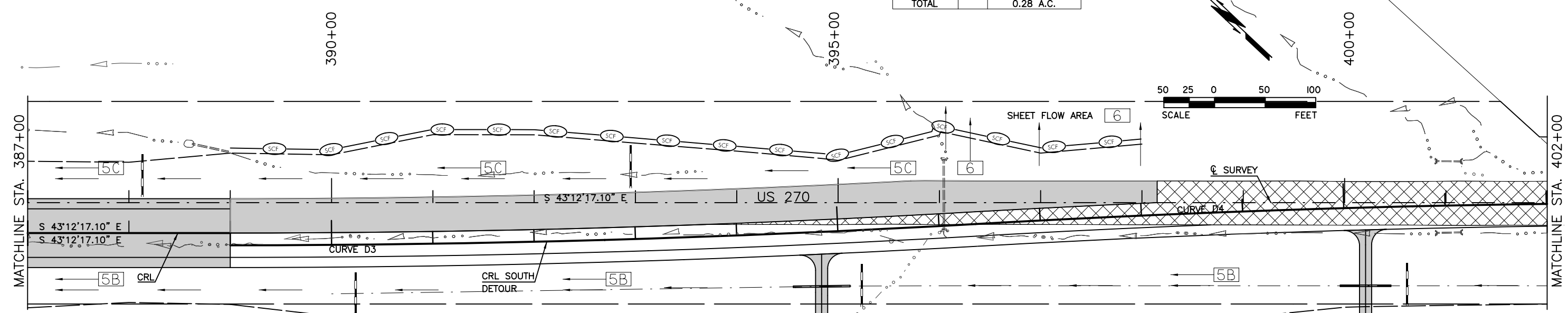


NW/4, SEC. 16
T-8-N, R-7-E

NW/4, SEC. 16
T-8-N, R-7-E

NW/4, SEC. 16
T-8-N, R-7-E

STR. NO OR LOCATION	D.A.	DISTURBED AREA
1	6	0.28 A.C.
TOTAL		0.28 A.C.



CURVE D3		CURVE D4	
PI STA	=392+68.19	PI STA	=400+04.36
N	=671136.5917	N	=670629.4698
E	=2394228.8691	E	=2394762.7875
Δ	=3°16'11.01" LI	Δ	=3°16'11.01" RI
D	=0°26'38.95"	D	=0°26'38.95"
T	=368.19	T	=368.19
L	=736.19	L	=736.19
R	=12,900.00	R	=12,900.00
E	=5.25	E	=5.25
eMAX	=8%	eMAX	=8%
S	=NC	S	=NC
V	=65 MPH	V	=65 MPH

NW/4, SEC. 16
T-8-N, R-7-E

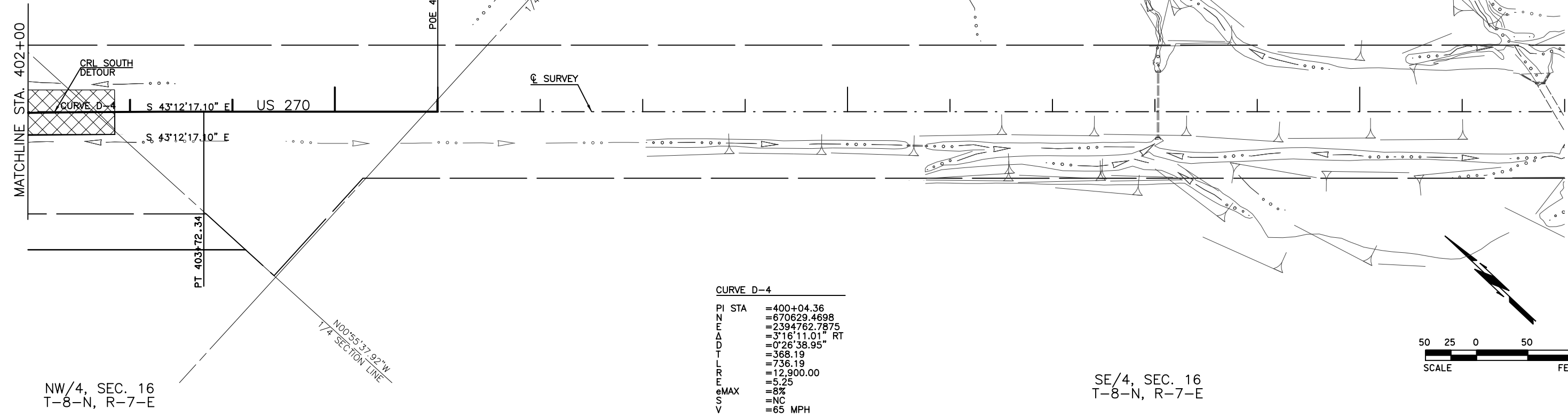
DESIGN: CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY	06/18	EROSION CONTROL	
CHECKED: CPY	06/18	STATE JOB PIECE NO: 21006(07)	
APPRVD: CPY	06/18	SHEET 2 OF 3	
CP&Y		SHEET NO. R007	

6/5/2018 2:45:40 PM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.11 SW3P\21006Erosion02.dgn

NE/4, SEC. 16
T-8-N, R-7-E

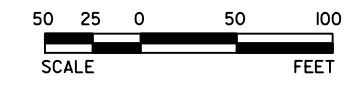
SE/4, SEC. 16
T-8-N, R-7-E

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	RO08	44
REVISIONS					
NO.	DATE	DESCRIPTION			



CURVE D-4

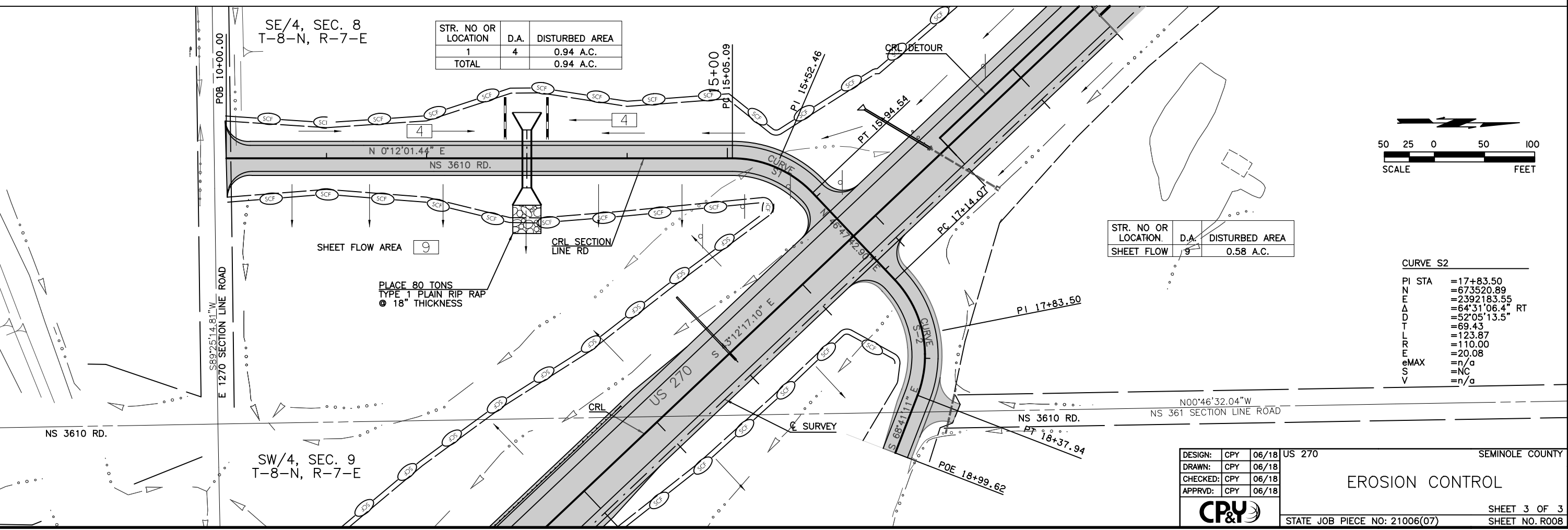
PI STA	= 400+04.36
N	= 670629.4698
E	= 2394762.7875
Δ	= 3°16'11.01" RT
D	= 0'26'38.95"
L	= 368.19
R	= 736.19
E	= 12,900.00
S	= 5.25
eMAX	= 8%
S	= NC
V	= 65 MPH



NW/4, SEC. 16
T-8-N, R-7-E

SE/4, SEC. 16
T-8-N, R-7-E

STR. NO OR LOCATION	D.A.	DISTURBED AREA
1	4	0.94 A.C.
TOTAL		0.94 A.C.



STR. NO OR LOCATION	D.A.	DISTURBED AREA
SHEET FLOW	9	0.58 A.C.



CURVE S2

PI STA	= 17+83.50
N	= 673520.89
E	= 2392183.55
Δ	= 64°31'06.4" RT
D	= 52°05'13.5"
L	= 69.43
R	= 123.87
E	= 110.00
S	= 20.08
eMAX	= n/a
S	= NC
V	= n/a

PLACE 80 TONS
TYPE 1 PLAIN RIP RAP
@ 18" THICKNESS

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		

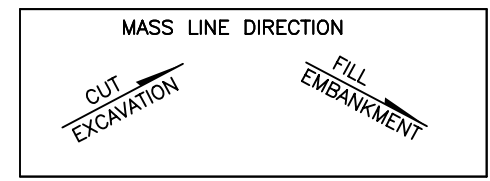
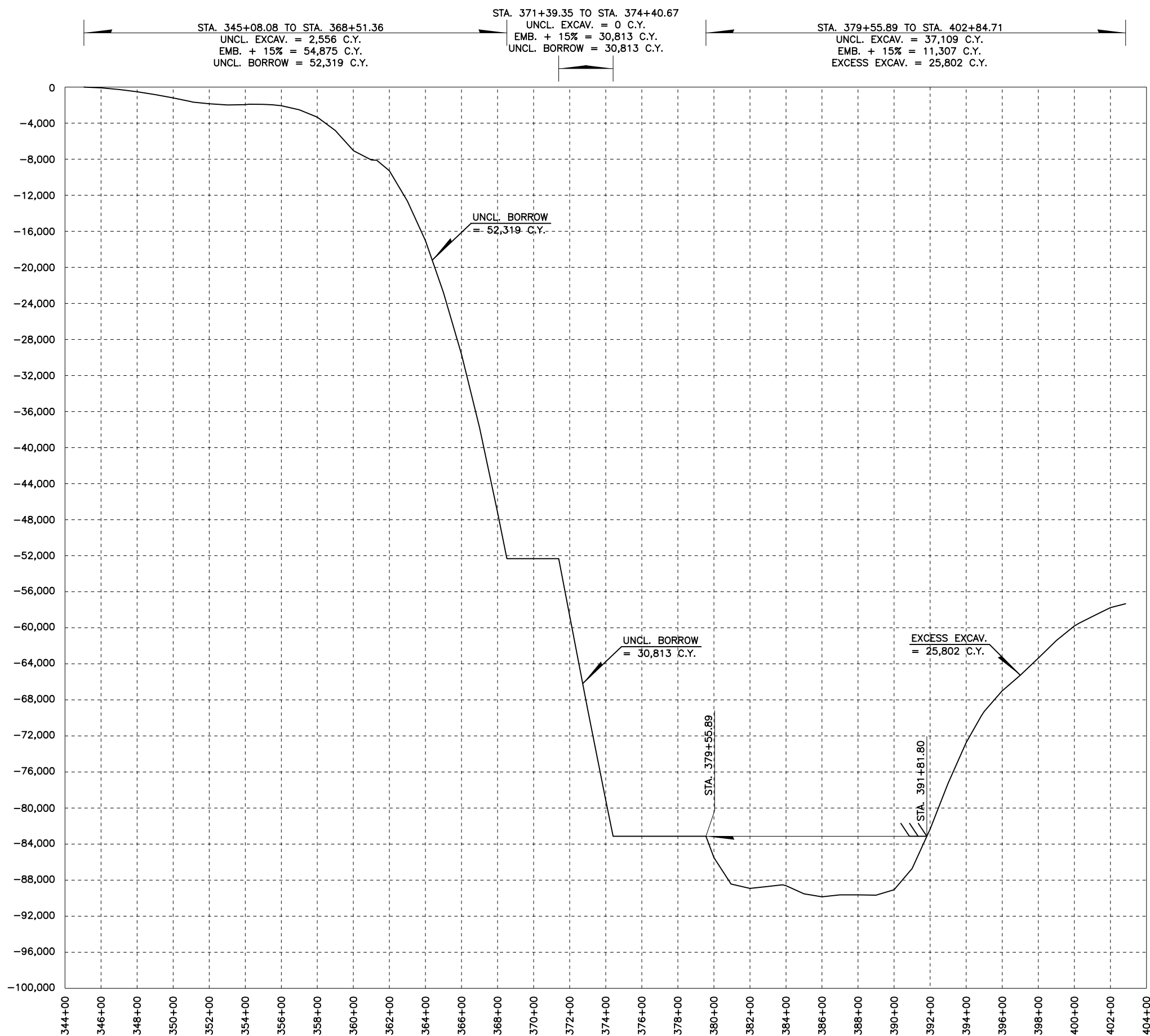
EROSION CONTROL

SHEET 3 OF 3
SHEET NO. R008

STATE JOB PIECE NO: 21006(07)

6/5/2018 2:45:46 PM
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.11 SW3P\21006Erosion03.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R009	44
REVISIONS					
NO.	DATE	DESCRIPTION			



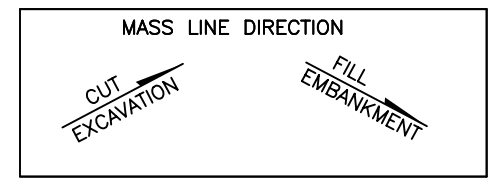
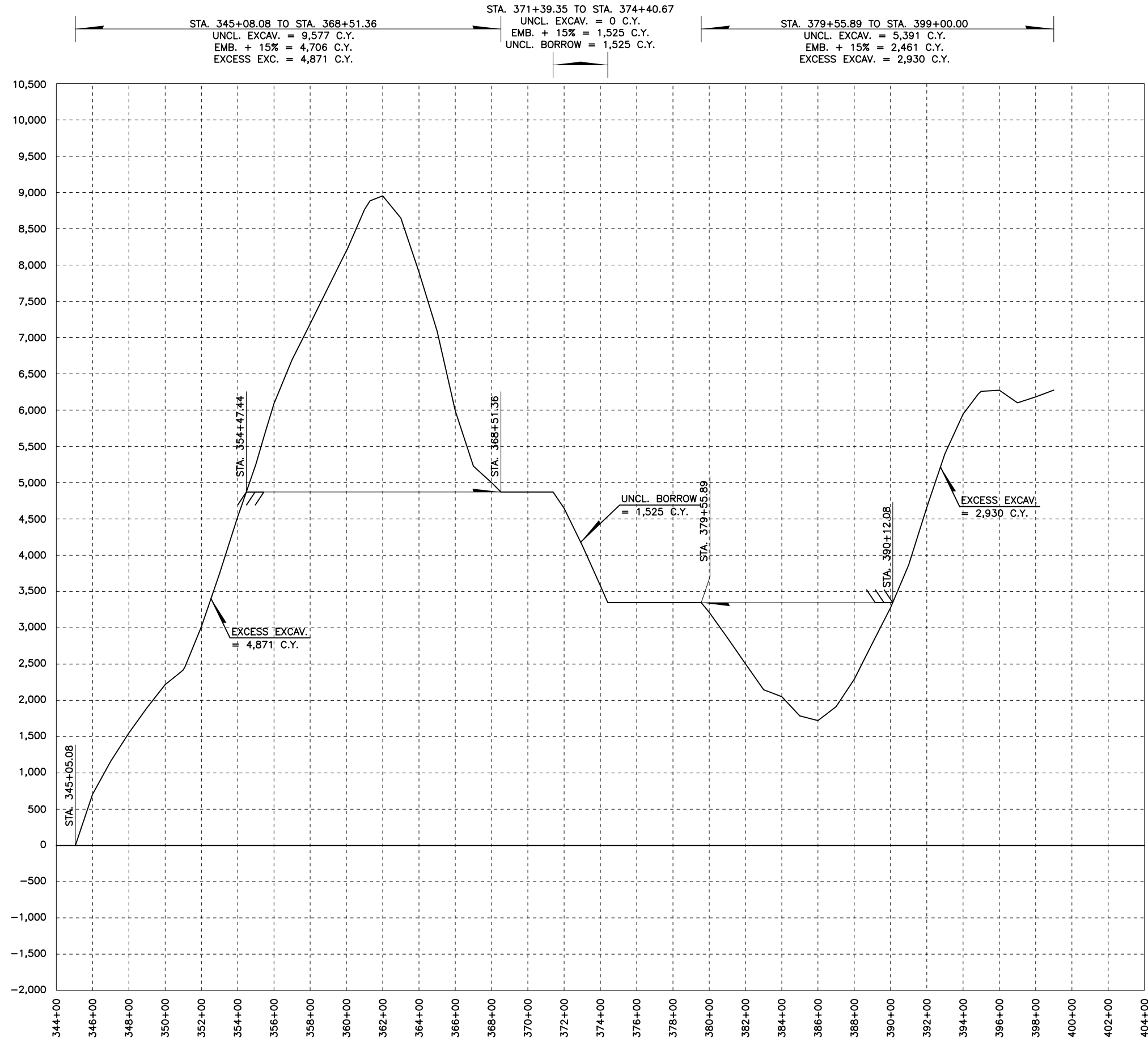
MASS DIAGRAM (PHASE 1)

MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTION FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.

DESIGN:	CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN:	CPY	06/18		
CHECKED:	CPY	06/18		
APPRVD:	CPY	06/18		
CP&Y			STATE JOB PIECE NO: 21006(07)	SHEET NO. R009

bmcanelly 6/5/2018 2:45:50 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.02 Summaries/MassDiagram1.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R010	44
REVISIONS					
NO.	DATE	DESCRIPTION			



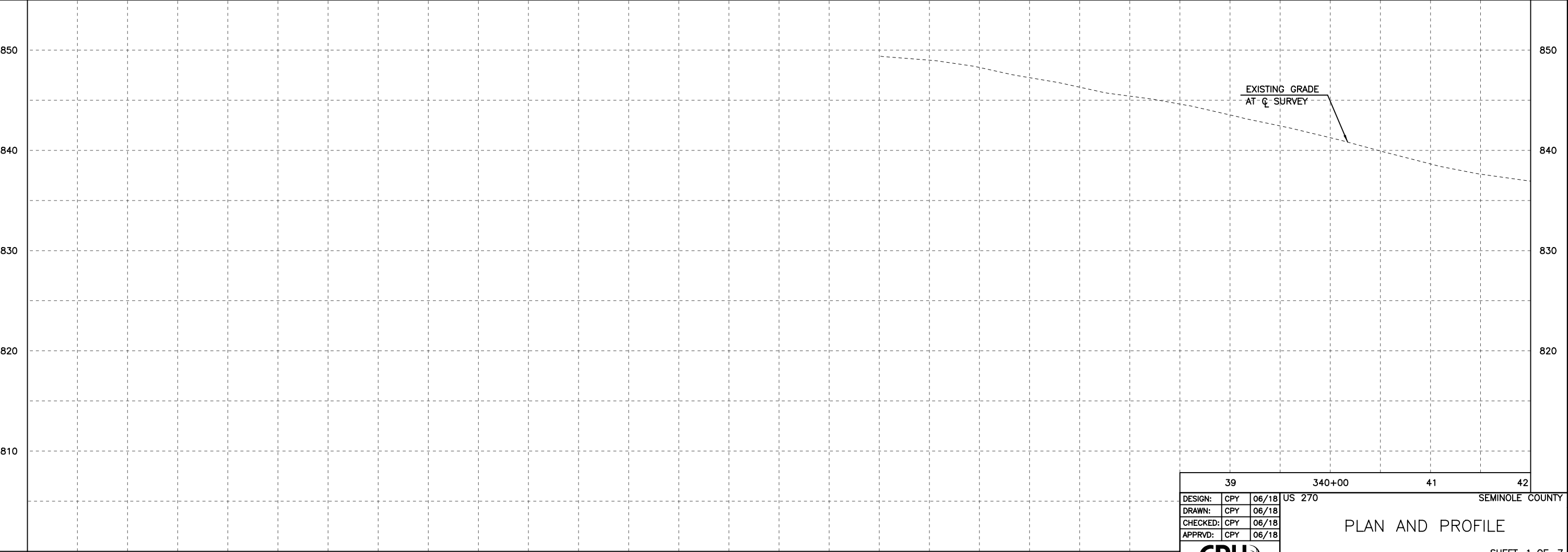
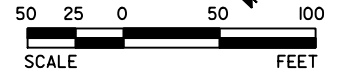
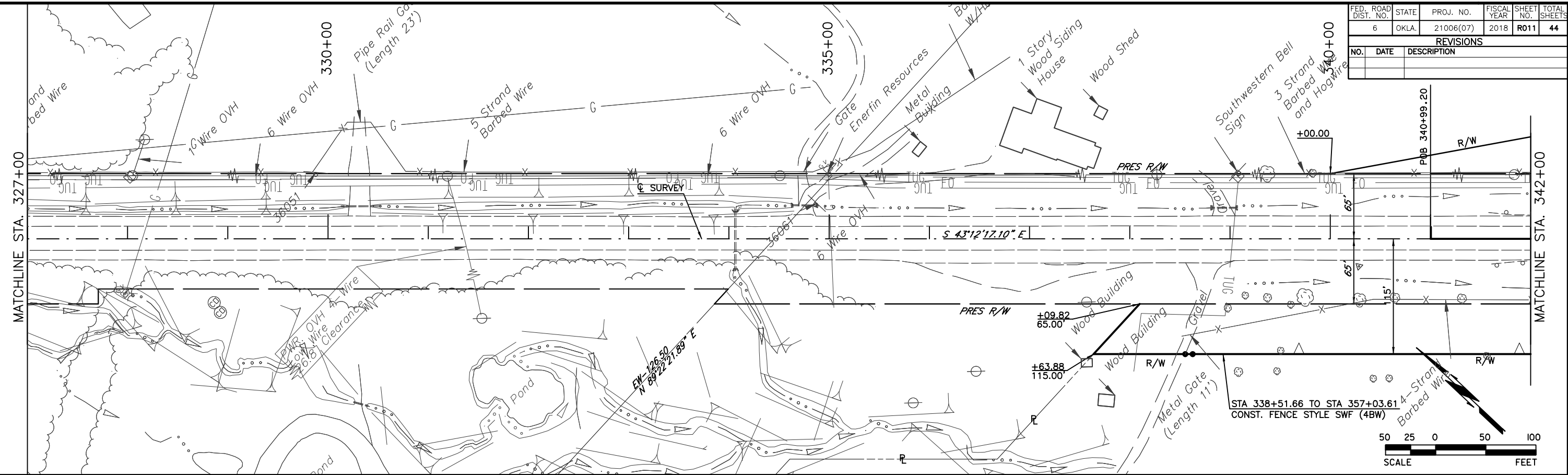
MASS DIAGRAM (PHASE 2)

MASS DIAGRAM PROVIDED FOR BIDDING PURPOSES ONLY. ACTUAL BALANCE POINTS TO BE DETERMINED BY CONTRACTOR AND VOLUME OF MATERIAL ENCOUNTERED DURING GRADING OPERATIONS. WHENEVER POSSIBLE, THE CONTRACTOR SHALL SEQUENCE EARTHWORK OPERATIONS IN ORDER TO OBTAIN THE MATERIAL FROM THE CUT SECTION FOR USE AS FILL RATHER THAN OBTAINING UNCLASSIFIED BORROW. MATERIAL DEPICTED AS WASTE SHALL ONLY BE CONSIDERED WASTE ONCE ALL EARTHWORK OPERATIONS HAVE BEEN COMPLETED. THIS MATERIAL SHALL BE USED TO REDUCE THE NEED FOR UNCLASSIFIED BORROW AT ANY LOCATION AND TIME THROUGH THE DURATION OF THE PROJECT.

DESIGN:	CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN:	CPY	06/18		
CHECKED:	CPY	06/18		
APPRVD:	CPY	06/18		
CP&Y			STATE JOB PIECE NO: 21006(07)	SHEET NO. R010

6/5/2018 2:45:53 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.02 Summaries/MassDiagram2.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R011	44
REVISIONS					
NO.	DATE	DESCRIPTION			

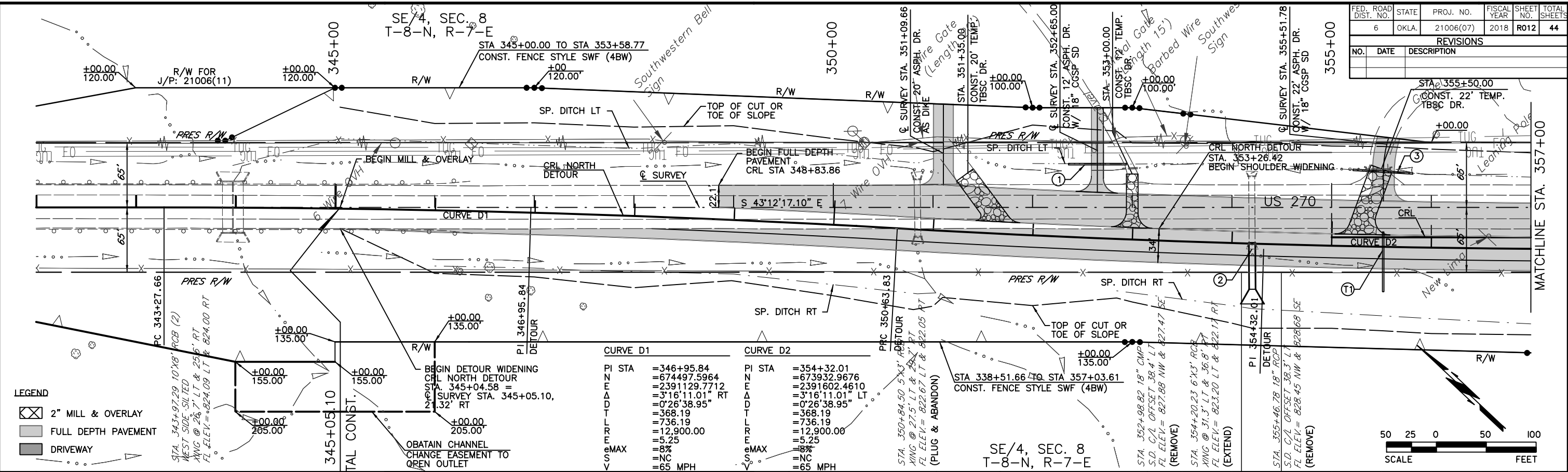


DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
PLAN AND PROFILE		
STATE JOB PIECE NO: 21006(07)		SHEET 1 OF 7 SHEET NO. R011

6/5/2018 2:46:00 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.04 Roadway\21006CORPP01A.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R012	44

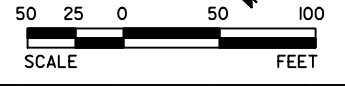
REVISIONS		
NO.	DATE	DESCRIPTION



LEGEND

- 2" MILL & OVERLAY
- FULL DEPTH PAVEMENT
- DRIVEWAY

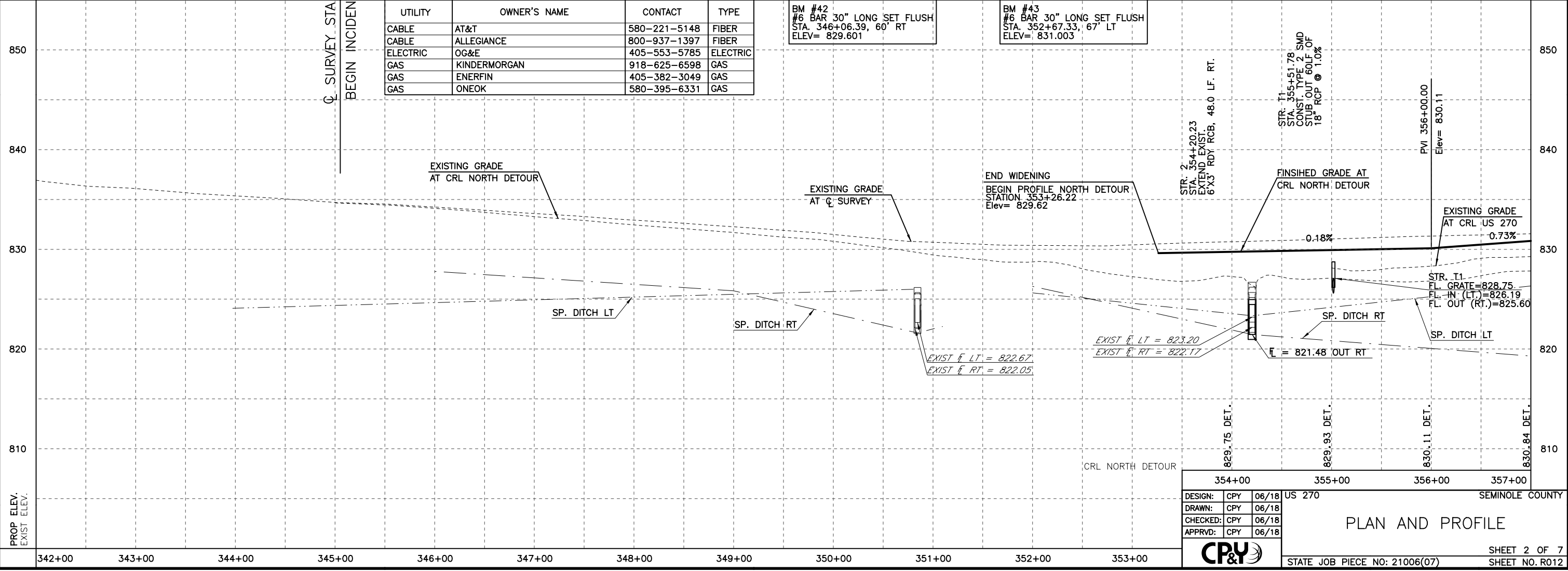
CURVE D1		CURVE D2	
PI STA	=346+95.84	PI STA	=354+32.01
N	=674497.5964	N	=673932.9676
E	=2391129.7712	E	=2391602.4610
Δ	=3°16'11.01" RT	Δ	=3°16'11.01" LT
D	=0°26'38.95"	D	=0°26'38.95"
T	=368.19	T	=368.19
L	=736.19	L	=736.19
R	=12,900.00	R	=12,900.00
F	=5.25	F	=5.25
eMAX	=8%	eMAX	=8%
S	=NC	S	=NC
V	=65 MPH	V	=65 MPH



UTILITY	OWNER'S NAME	CONTACT	TYPE
CABLE	AT&T	580-221-5148	FIBER
CABLE	ALLEGIANCE	800-937-1397	FIBER
ELECTRIC	OG&E	405-553-5785	ELECTRIC
GAS	KINDERMORGAN	918-625-6598	GAS
GAS	ENERFIN	405-382-3049	GAS
GAS	ONEOK	580-395-6331	GAS

BM #42
#6 BAR 30" LONG SET FLUSH
STA. 346+06.39, 60' RT
ELEV= 829.601

BM #43
#6 BAR 30" LONG SET FLUSH
STA. 352+67.33, 67' LT
ELEV= 831.003



DESIGN: CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY	06/18		
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		

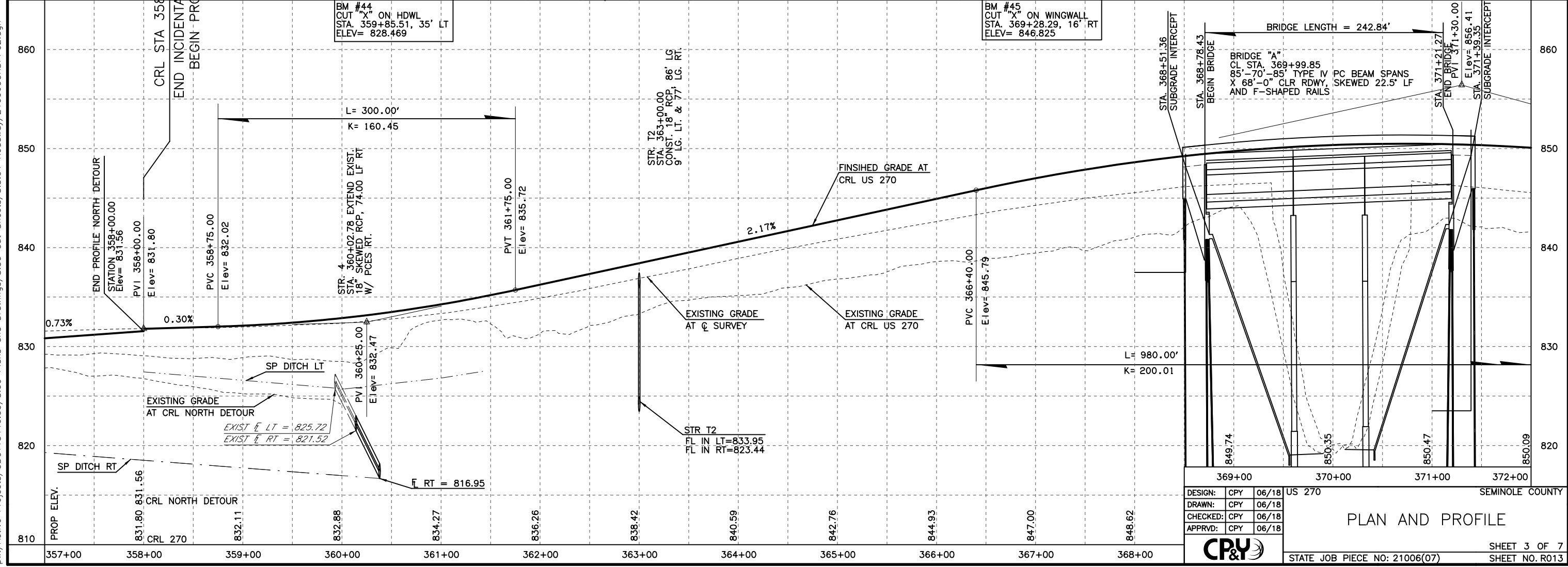
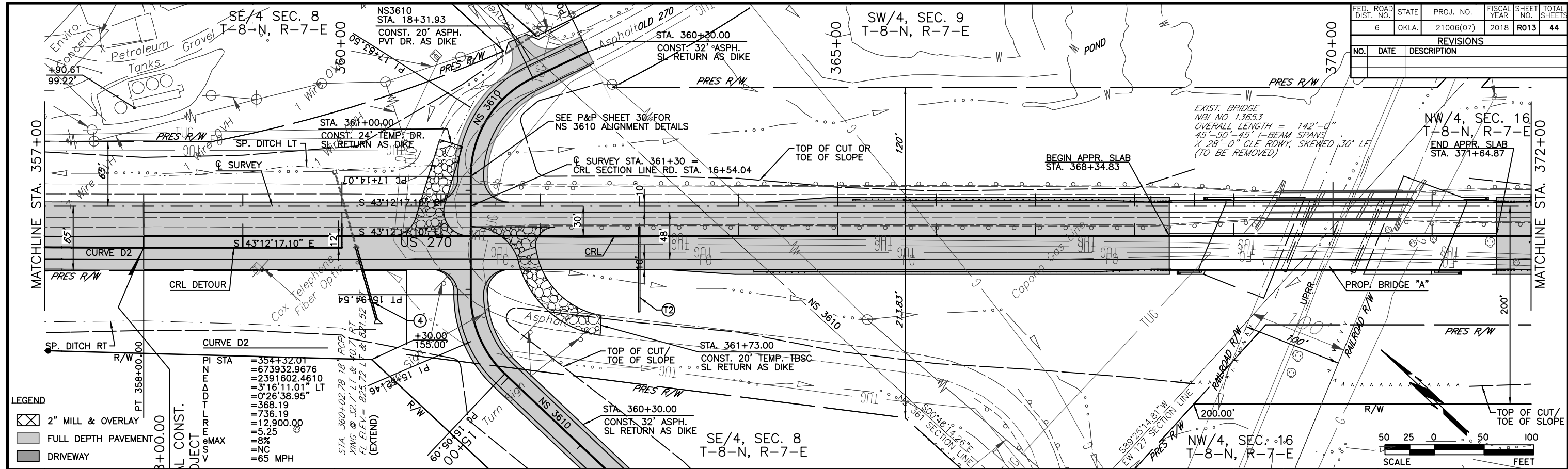
CP&Y

STATE JOB PIECE NO: 21006(07)

SHEET 2 OF 7
SHEET NO. R012

6/5/2018 2:46:05 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT\13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.04 Roadway\21006CORRPP01.dgn

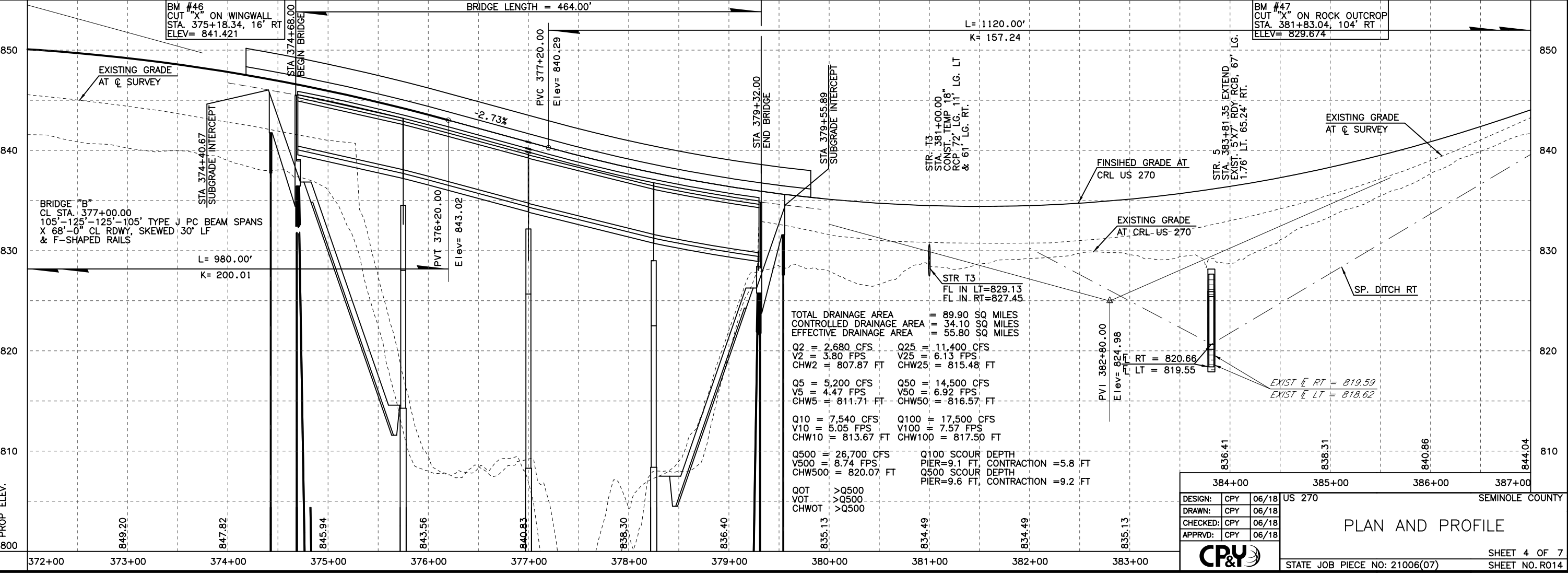
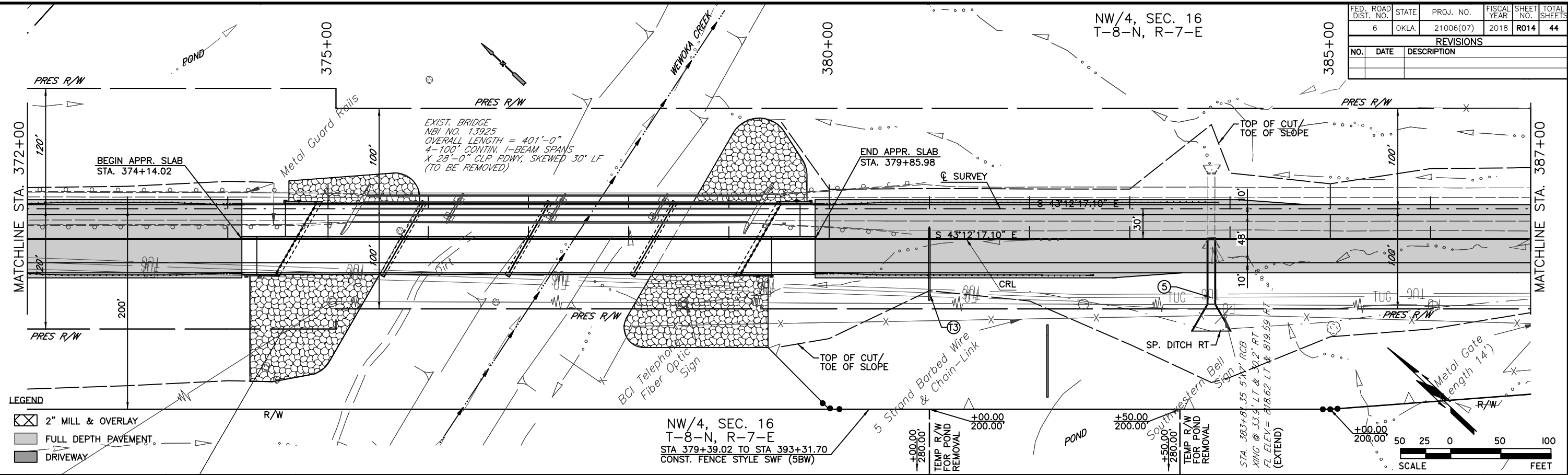
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R013	44
REVISIONS					
NO.	DATE	DESCRIPTION			



6/5/2018 2:46:11 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.04 Roadway\21006CORPP02.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R014	44
REVISIONS					
NO.	DATE	DESCRIPTION			

NW/4, SEC. 16
T-8-N, R-7-E



DESIGN: CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY	06/18		
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		
CP&Y		SHEET 4 OF 7	
STATE JOB PIECE NO: 21006(07)		SHEET NO. R014	

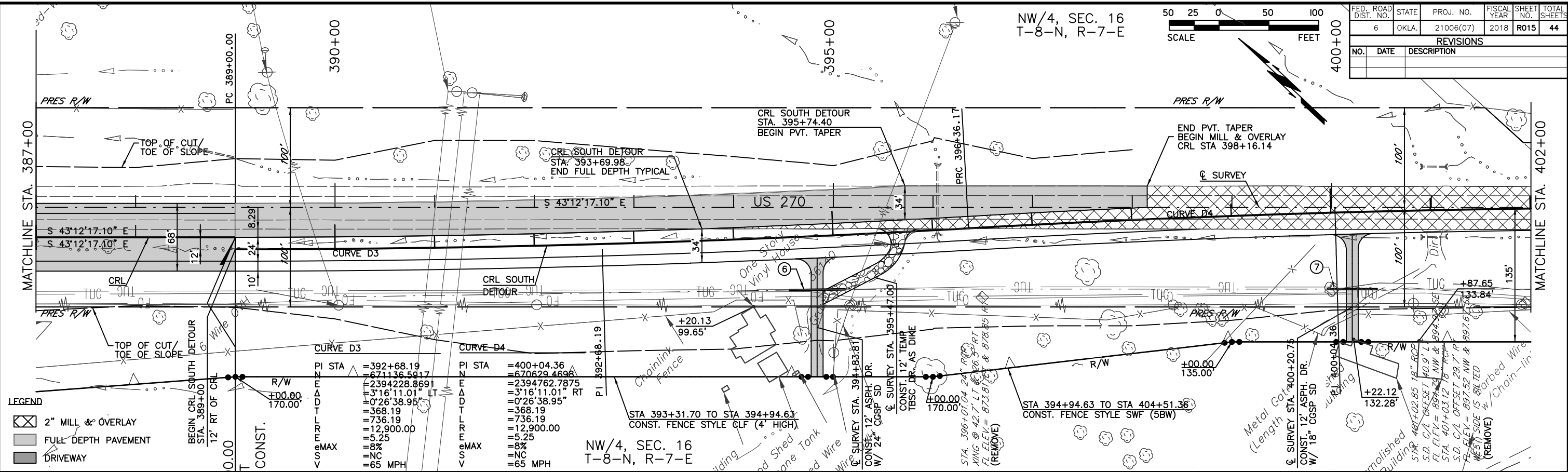
bmcanelly 6/5/2018 2:46:16 PM
 cpyb&w_ANSIB.tbl cpypdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.04 Roadway\21006CORPP03.dgn

NW/4, SEC. 16
T-8-N, R-7-E



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R015	44

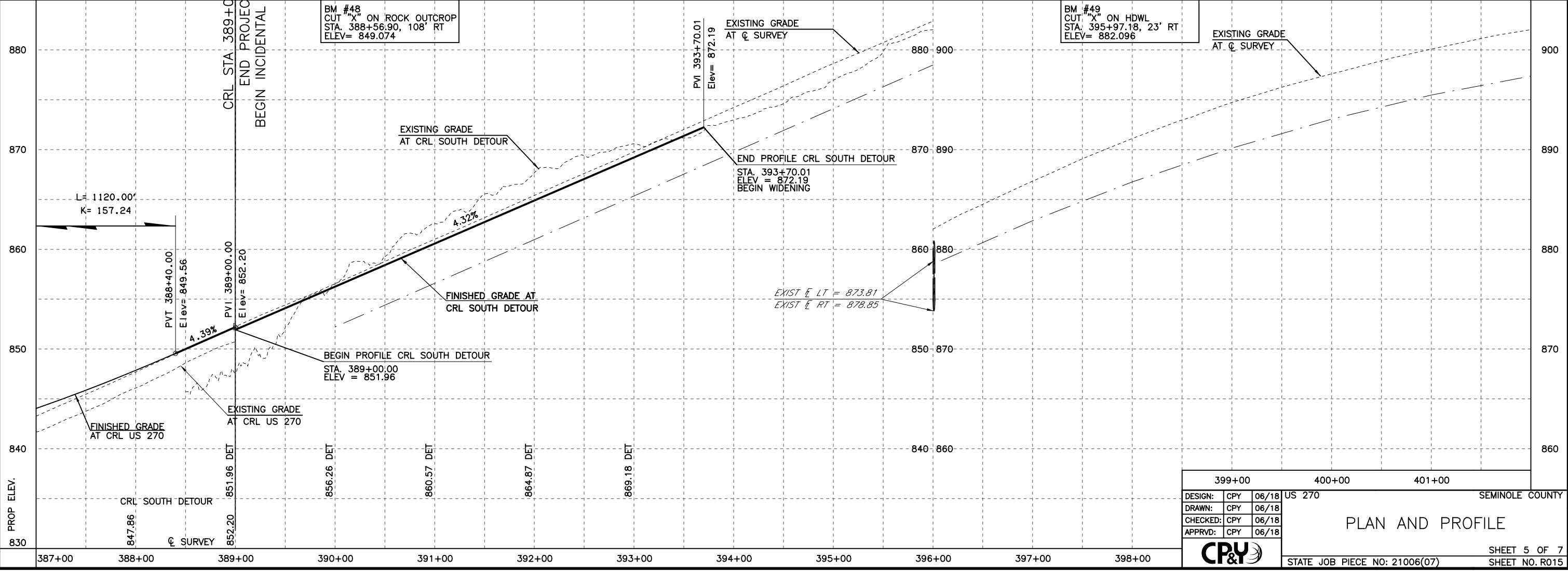
REVISIONS		
NO.	DATE	DESCRIPTION



LEGEND

- 2" MILL & OVERLAY
- FULL DEPTH PAVEMENT
- DRIVEWAY

CURVE D3		CURVE D4	
PI STA	=392+68.19	PI STA	=400+04.36
PC	=2394228.8691	PC	=2394762.7875
PT	=316111.01" LT	PT	=316111.01" RT
EA	=368.19	EA	=368.19
EB	=736.19	EB	=736.19
EC	=12,900.00	EC	=12,900.00
EMAX	=5.25	EMAX	=5.25
SMAX	=8%	SMAX	=8%
SV	=NC	SV	=NC
VS	=65 MPH	VS	=65 MPH



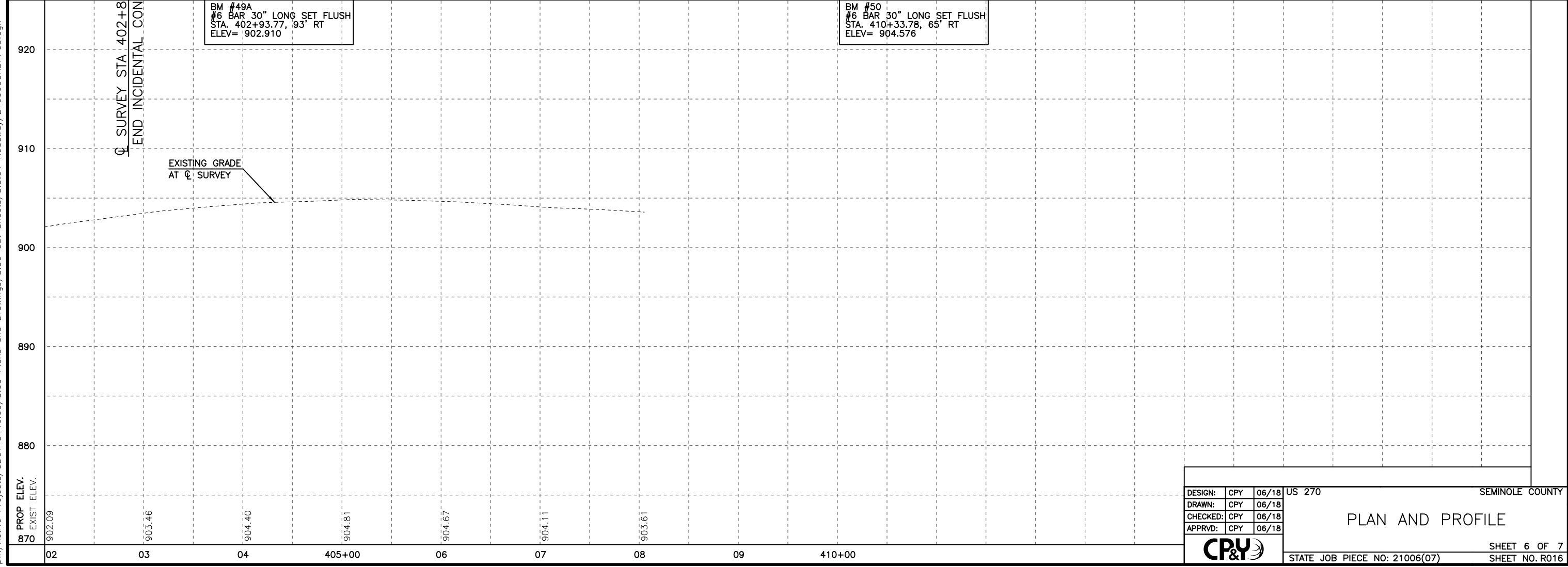
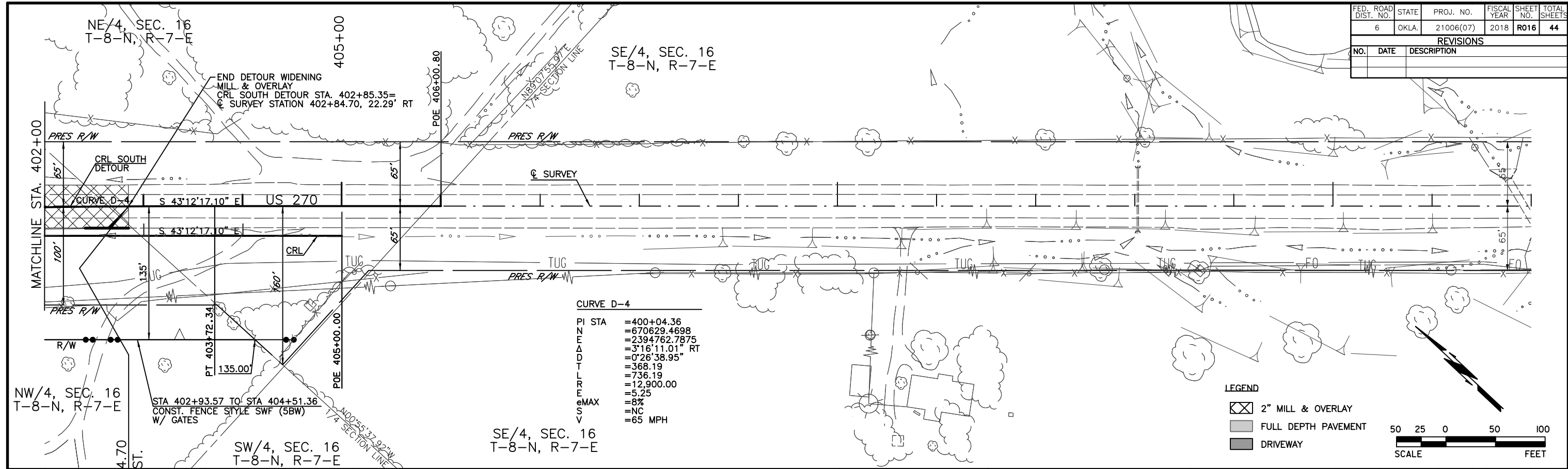
DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		

PLAN AND PROFILE

SHEET 5 OF 7
STATE JOB PIECE NO: 21006(07)
SHEET NO. R015

bmcanelly 6/5/2018 2:46:20 PM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.04 Roadway\21006CORPP04.dgn

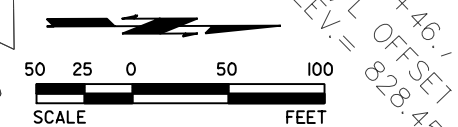
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R016	44
REVISIONS					
NO.	DATE	DESCRIPTION			



bmcanelly 6/5/2018 2:46:26 PM
c:\p\w\ANSIB.tbl c:\p\w\ANSIB.plt
p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.04 Roadway\21006CORPP05.dgn

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
STATE JOB PIECE NO: 21006(07)		SHEET 6 OF 7 SHEET NO. R016

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R017	44
REVISIONS					
NO.	DATE	DESCRIPTION			



LEGEND

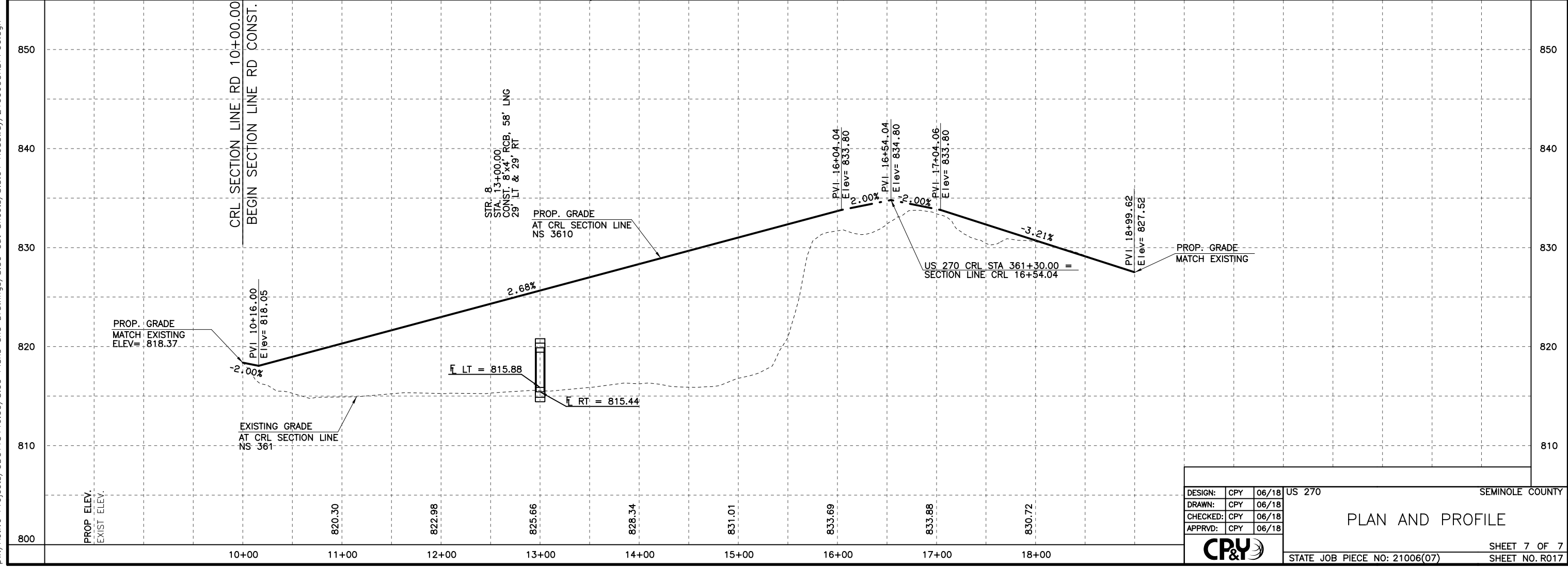
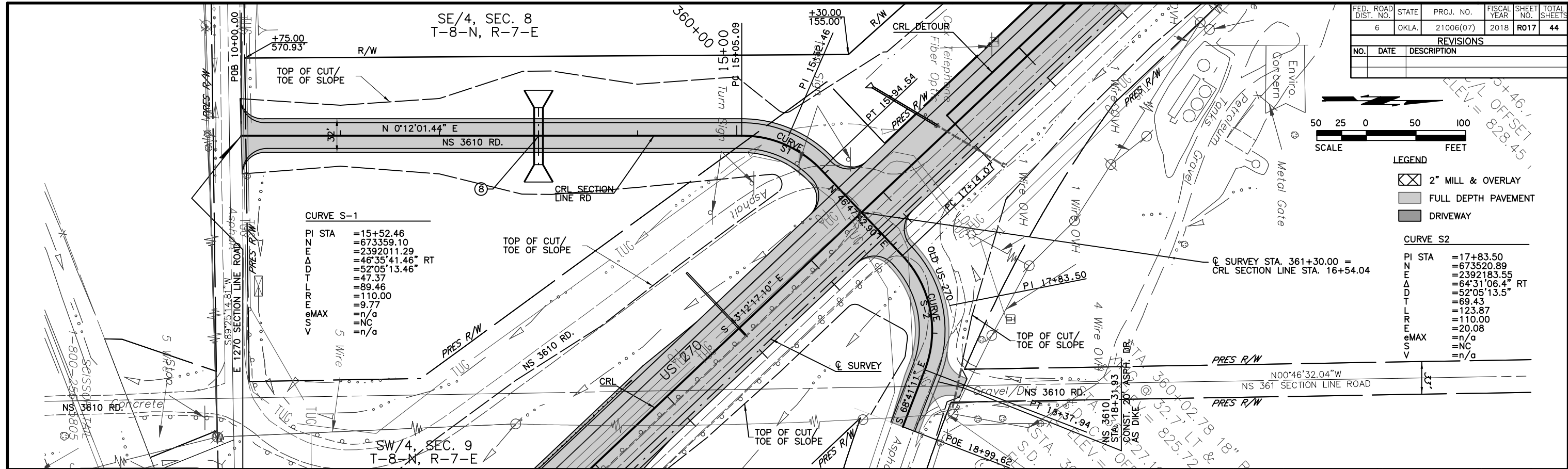
- 2" MILL & OVERLAY
- FULL DEPTH PAVEMENT
- DRIVEWAY

CURVE S2

PI STA	=17+83.50
N	=673520.89
E	=2392183.55
Δ	=64°31'06.4" RT
D	=52°05'13.5"
T	=69.43
R	=123.87
F	=110.00
L	=20.08
R	=n/a
E	=n/a
S	=n/a
V	=n/a

CURVE S-1

PI STA	=15+52.46
N	=673359.10
E	=2392011.29
Δ	=46°35'41.46" RT
D	=52°05'13.46"
T	=47.37
R	=89.46
F	=110.00
L	=9.77
R	=n/a
E	=n/a
S	=n/a
V	=n/a



DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		

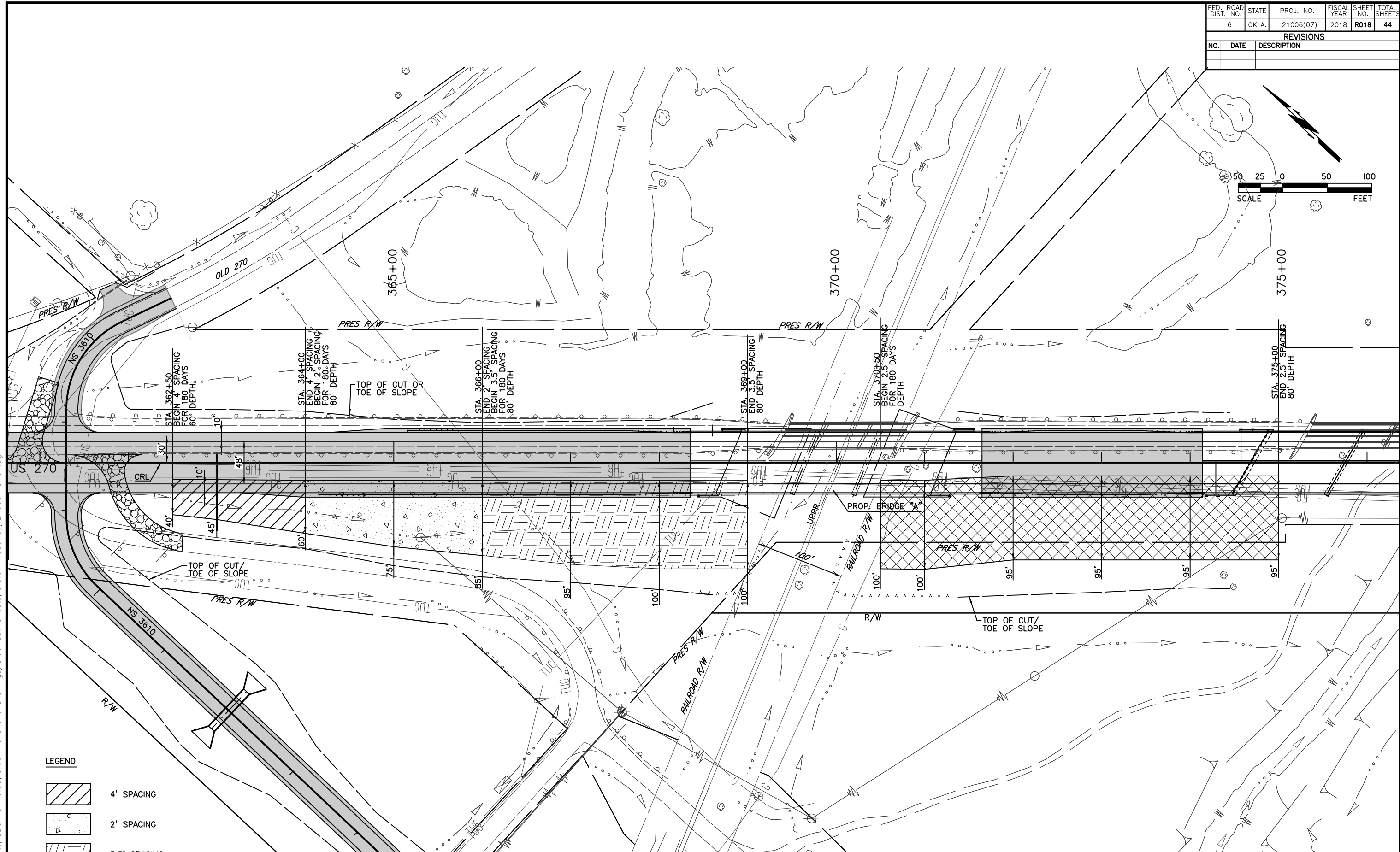
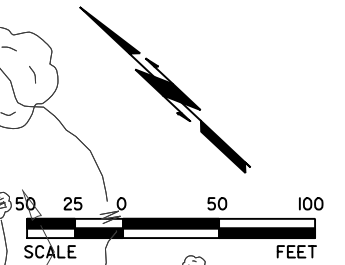
PLAN AND PROFILE

STATE JOB PIECE NO: 21006(07)

SHEET 7 OF 7
SHEET NO. R017

6/5/2018 2:46:32 PM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446\00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.04 Roadway\21006CORPP06.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R018	44
REVISIONS					
NO.	DATE	DESCRIPTION			



9:54:48 AM
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 6/7/2018
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.04 Roadway/21006WICK01.dgn

LEGEND

	4' SPACING
	2' SPACING
	3.5' SPACING
	2.5' SPACING

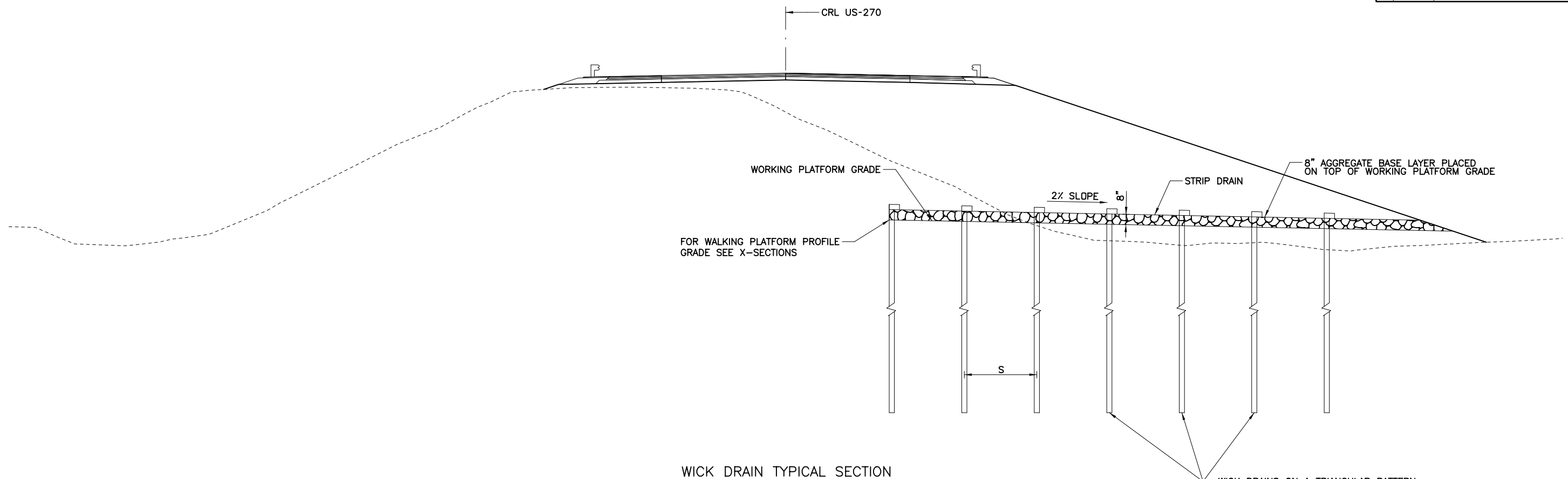
DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		

WICK DRAIN PLAN

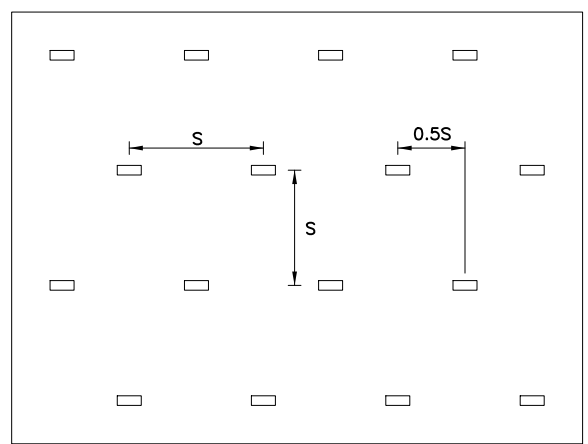
SHEET 1 OF 1
SHEET NO. R018

STATE JOB PIECE NO: 21006(07)

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	R019	44
REVISIONS					
NO.	DATE	DESCRIPTION			



WICK DRAIN TYPICAL SECTION



PLAN VIEW WICK DRAIN LAYOUT

S VARIES FROM 2' TO 4'
FOR SPACING DISTANCE SEE WICK DRAIN PLAN SHEET

bmcanelly 6/5/2018 2:46:40 PM
 cpyb&w_ANSIB.tbl cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.01 General/21006WickDrainDetail.dgn

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
CP&Y		
STATE JOB PIECE NO: 21006(07)		SHEET 1 OF 1 SHEET NO. R019

WICK DRAIN DETAIL

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5001	5047

DESCRIPTION	REVISIONS	DATE

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION

SWO 4879(1) J/P 21006(11) ; CO. Seminole

HORIZONTAL CONTROL:
 Oklahoma Coordinate System of 1927 Zone.
 Oklahoma Coordinate System of 1983(2011) South Zone.
 Oklahoma Dept. of Transportation Plane Coordinate System of 1927 Zone.
 Oklahoma Dept. of Transportation Plane Coordinate System of 1983 Zone.
 Arbitrary Coordinate System

HORIZONTAL PLANE DATUM DEFINITION:
 The horizontal control for this survey is the NGS Oklahoma State Plane Coordinate System, NAD83(2011), Lambert Projection (South Zone). The combined scale factor for conversion to geodetic distances is 1.00000786.

- Primary Control** adjusted to NGS (2nd) Order
 Stations OKAR, OKMU, OKPR, K 149 and SEMINOLE
 A) Closure before adjustment X ; Y ; Angles ; Accuracy 1:20,000
 Trav. Length ; No. Angles ; Order before adjustment
 B) ; Is ; Order before adjustment
 C) Method of Distance Measurement:
 Electronic GPS Triangulation Chained
 D) Instrument used for angles Trimble R6
- Secondary Control** adjusted to Primary Control () Order
 Stations S-67-978, S-67-979, S-67-980, S-67-981, S-67-982 & S-67-983
 A) Closure before adjustment X ; Y ; Angles ; Tied to
 B) ; Is ; Order ;
 C) Method of Distance Measurement:
 Electronic GPS Triangulation Chained
 D) Instrument used for angles Trimble R6, Trimble S6

VERTICAL CONTROL IS (3rd) order. Level Line taken from NGS G-149 through one BM from FAP No. F-222(15) plane, NGS U-149 and project BMs, to NGS T-149 ; NGVD 29 datum ; NGVD 83 datum

ACCURACY DEFINITION:
 (1) HORIZONTAL: (2nd Order = Class I = 1 : 20,000'
 (3rd Order = Class II = 1 : 10,000'
 (3rd Order = Class III = 1 : 5,000'
 (1st Order = 0.017 Ft. x sqrt. of MI.)
 (2nd Order = 0.035 Ft. x sqrt. of MI.)
 (3rd Order = 0.050 Ft. x sqrt. of MI.)
 (2) VERTICAL: () NGVD 29 datum ; (X) NAVD 83 datum

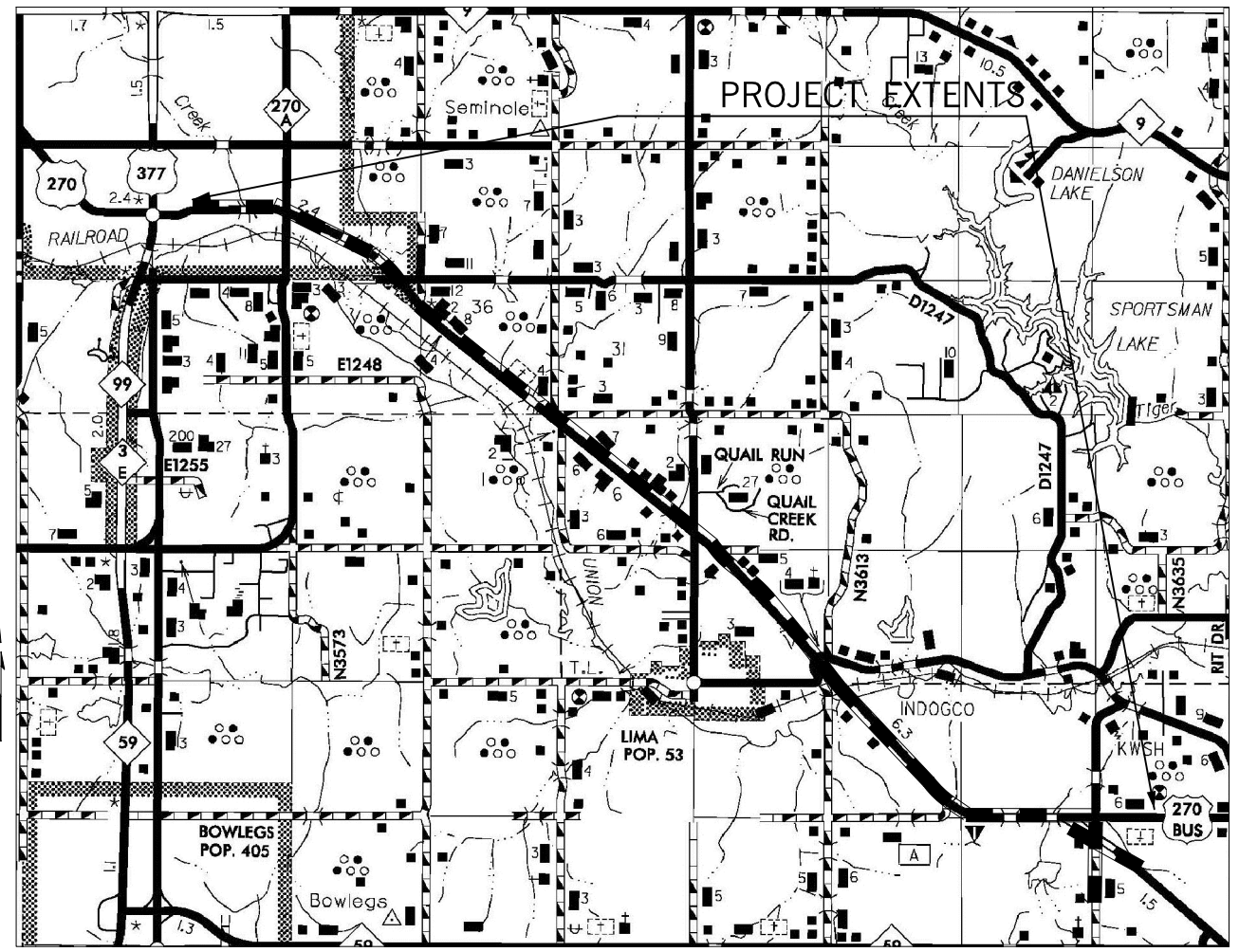
Distribution:
 Copy w/survey reports
 Copy in each Alignment
 and level book



(FORM SD #20)
Rev. 11/03

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

SURVEY OF
U.S. 270
SWO 4879(1)
J/P NO. 21006(07)
SEMINOLE
U.S. 270, FROM 0.5 MILES EAST OF S.H. 99,
EAST TO U.S. 270-B
R-6-E R-7-E



PROJECT LENGTH 44702.943 Ft. 8.47 MI.
 BEGINNING STATION : 52+97.057
 ENDING STATION : 500+00.000

INDEX OF SHEETS

1	TITLE SHEET
2-5	SURVEY INFORMATION (Notes, Letters)
6-7	ALIGNMENT DATA
8-11	CGO POINTS
12-13	CHECK LEVELS
14	HORIZONTAL CONTROL DIAGRAM & STATIC DATA
15-47	SURVEY DATA SHEETS

SURVEY BEGAN: 2/27/2014
 SURVEY COMPLETED: 3/31/2015

PERSONNEL:
 SHAWN SMITH, PLS PROF. LAND SURVEYOR
 CLARK FISHER, PLS PROF. LAND SURVEYOR
 JASON HARVEY, PLS PROF. LAND SURVEYOR
 JEREMY BONE SURVEY TECH
 SHAUN KOONCE, CST I SURVEY TECH
 ANDREW MAYHUE TITLE RESEARCHER
 JACOB ANDREWS, CST I SURVEY TECH
 PIERCE TRANUM SURVEY TECH
 BOB BLEDSOE, LSIT SURVEY TECH
 LUKE BRUNGARD SURVEY TECH
 RANDALL TOLLISON SURVEY TECH
 BRANDON SECHRIST SURVEY TECH

EQUIPMENT:
 TRIMBLE R6 BASE & ROVER GPS
 TRIMBLE S6 ROBOTIC TOTAL STATION
 SOKKIA SDL-30 DIGITAL LEVEL

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION
 SWO 4879(1) Job/Piece 21006(11) Engr. Contract No. 1468.A

LAND SURVEYOR'S CERTIFICATION

I hereby certify that all land and property sub-division distances, angles, corners, and monumentation made or used in conjunction with this survey and depicted or remarked herein or hereon were recovered, established or re-established in substantial conformity with:

- Applicable instructions contained in the U.S. Government Bureau of Land Management publication "Manual of Survey Instruction".
- Its supplement, "Restoration of Lost or Obliterated Corners and Sub-division of Sections".
- "Oklahoma Minimum Standards for the Practice of Land Surveying" as adopted by the State Board of Licensure for Professional Engineers and Land Surveyors; and
- Sound land surveying practices; including a thorough search, study, analysis and consideration of all existing records and field evidence.

I further certify that all survey monuments depicted exist and that all land survey work was done by me or under my direct supervision.

Dated this 31st day of March, 2015.



Land Surveyor Shawn Smith
 Signature
 Shawn Smith
 Printed Name

Oklahoma Licensed Land Surveyor No. 1663
 Certificate of Authorization No. 6975

Utilities

Utility	Phone Number	Utility	Phone Number
Telephone Lines:		Gas Lines:	
AT&T	800-246-8464	OneOK	800-666-3041
Southwestern Bell	800-522-6543	Sunoco	800-753-5531
Cox	405-600-7676	Blue Knight Energy Partners	855-999-2537
BCI Allegiance	800-937-1397	Atlantic Richfield	405-382-3049
Midcontinent	800-722-2606	Enerfin Resources	405-382-3049
Semcrude Telephone	918-524-8100	Centerpoint Energy	888-876-5786
Electric Lines:		Scissor Tail	800-256-3805
Canadian Valley Elec. Co-op	405-382-3680	COPANO	580-332-3791
Sanitary Sewer		Water Lines:	
City of Seminole	405-382-3434	Bowlegs Lima Water Dist., Inc.	405-398-4469
		City of Seminole	405-382-3434

THIS SURVEY MEETS THE OKLAHOMA MINIMUM STANDARDS FOR THE PRACTICE OF LAND SURVEYING AS ADOPTED BY THE OKLAHOMA STATE BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS AND LAND SURVEYORS, JULY 25, 2013.

SPECIFICATIONS FOR SURVEYS FOR PRIMARY AND SECONDARY HIGHWAYS DATED MAY 1, 1999 GOVERN.
 SDS / OF 47

Electronic File Transfer Disclaimer:
 These Files, Drawings and/or Notes are provided for information only. The Oklahoma Department of Transportation (ODOT) and the Owner cannot be held responsible for the content or accuracy of these Files, Drawings and/or Notes due to conversions, software translations, or any other manipulation of said Files, Drawings and/or Notes. ODOT expressly disclaims any responsibility arising from any use of these Files, Drawings and/or Notes. To the full extent permitted by applicable law, the recipient of these Files, Drawings and/or Notes hereby agrees to defend, indemnify, and hold harmless ODOT and the Owner from and against any and all claims, suits, actions, damages, loss, liability or costs of every nature or description (including reasonable attorney's fees) arising from, or in any way attributable to or connected with any of these Files, Drawings and/or Notes.



PLS	SS		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION SURVEY DATA SHEET
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH, JA, LB, RT, BS, PT, BB		

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5001

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5002	5047

DESCRIPTION	REVISIONS	DATE

Oklahoma Department of Transportation
Survey Division (405) 521-2621

March 31, 2015

To: Mr. W. (Leroy) Tackett, PLS, Chief of Surveys

From: Shawn Smith, Professional Land Surveyor

Subject: SWO 4879(1), JP 21006(11), U.S. 270. From 0.5 Mile East of S.H. 99, East to U.S. 270-B, Seminole County

- I. GENERAL:**
- Survey Began: February 27, 2014
Survey Completed: March 31, 2015
- The measurement unit for this project was the U.S. Survey Foot.
- II. SURVEY ASSIGNMENT:**
- This survey was assigned to Lenke Land Surveying, Inc. (LLS) under Engineering Contract Number 1468-A.
- III. PURPOSE OF SURVEY:**
- The purpose of this survey was to furnish sufficient data to develop plans to add capacity to present U.S. 270 southeast of Seminole.
- IV. SURVEY LIMITS:**
- U.S. 270: Survey began at P.C. Sta. 52+97.057, as established under SWO 1739(1) survey & shown on FAP No. F-222(7) plans, and extended southeasterly to P.O.T. Sta. 500+00.000, as established under SWO 2219(1) survey & shown on FAP No. 222(15) plans.
- S.H. 270-A: Survey began at P.O.T. Sta. 26+87.240 (C.R.L.) and extended north to Sta. 35+00.000 as shown on FAP No. DBS-67B(239) plans.
- U.S. 270-B: Survey began at Sta. 482+00.000 (U.S. 270 Main Line Survey) and extended east to P.O.T. Sta. 500+00.000 as established under SWO 1739(1) survey and shown on FAP No. F-222(9) plans.
- NS-363 Section Line: Survey began at P.O.T. Sta. 17+00.000 and extended north to P.O.T. Sta. 31+00.000 as established under SWO 2219(1) survey and shown on FAP No. F-222(15) plans.
- V. ALIGNMENT:**
- U.S. 270: The centerline referenced under SWO 1739(1) survey & SWO 2219(1) survey and shown on FAP No. F-222(7) plans, FAP No. F-222(8) plans, FAP No. F-222(9) plans and FAP No. F-222(15) plans was re-established using monuments and references found from said previous ODOT surveys and verified with existing bridges, paving and right-of-way occupation. This alignment was approved by Mr. Larry Williams and Mr. Leroy Tackett in a meeting at the ODOT main building on July 17, 2014.
- S.H. 270-A: The centerline of survey, construction reference line (C.R.L.), shown on FAP No. DBS-67B(239) plans was re-established using monuments found and tied to U.S. 270 centerline.
- U.S. 270-B: The centerline referenced under SWO 1739(1) and shown on FAP No. F-222(9) plans was re-established using monuments found and tied to U.S. 270 centerline.

NS-363 Section Line: The centerline referenced under SWO 2219(1) survey and shown on FAP No. F-222(15) plans was re-established using existing monuments which fit well with ties shown on said survey and plans.

- VI. STATIONING:**
- U.S. 270: As directed by the Special Provisions, the stationing was taken from SWO 1739(1) survey & SWO 2219(1) survey and FAP No. F-222(7) plans, FAP No. F-222(8) plans, FAP No. F-222(9) plans and FAP No. F-222(15) plans. The PI at station 78+53.28 was found and held. Stations were then calculated back and ahead from PI.
- S.H. 270-A: As directed by the Special Provisions, the stationing was taken from FAP No. DSB-67B(239) plans. A station of 26+40.000 was held at the 1/4 section corner as per said plans.
- U.S. 270-B: As directed by the Special Provisions, the stationing was taken from SWO 1739(1) survey and FAP No. F-222(9) plans. Stationing was a continuation of the stationing for U.S. 270 centerline.
- NS-363 Section Line: As directed by the Special Provisions, the stationing was taken from SWO 2219(1) survey and FAP No. F-222(15) plans. A station value of 31+00.000 was held where NS-363 intersected U.S. 270-B and stationing was calculated back south per survey and plans.
- VII. HORIZONTAL CONTROL:**
- Horizontal control for this survey was established by static GPS observations using multiple sessions. The primary control stations used in the static network were OKAR, OKMU, OKPR, K 149, and SEMINOLE. Coordinates shown on this survey are NGS Oklahoma State Plane Coordinate System NAD83(2011) Lambert Projection South Zone. The distances and coordinates shown on this survey are in U.S. Survey Feet. All angles and bearings are shown in degrees, minutes, and seconds. Primary control for this survey was established following ODOT Survey Division Standards.
- Secondary control points were established by multiple observations using RTK and by Robotic Total Stations.
- VIII. VERTICAL CONTROL:**
- A Vertical control for this survey is NAVD88.
- B Benchmarks held for this survey are, NGS G-149, NGS U-149, NGS T-149 and BM X on headwall from FAP No. F-222(15) plans. Differential leveling techniques were used to establish elevations for the survey vertical control.
- C A benchmark list depicting newly established benchmarks as well as the results of the leveling has been placed on the Survey Data Sheets.
- IX. PHOTO CONTROLS:**
- Aerial control targets were established by static GPS observations utilizing the primary control points. Differential leveling runs were used to establish vertical control for the targets.
- X. TOPOGRAPHY AND DTM:**
- Topographic data was collected by aerial film photographs, which were scanned to digital aerial imagery (Provided by Aerial Mapping Technologies) and supplemented by conventional field methods.

DTM data was collected as follows:

U.S. 270: 200' right and left of centerline of survey from the beginning of survey to Sta. 61+00, 500' right and left of centerline of survey from Sta. 61+00 to Sta. 71+00, 200' right and left of centerline of survey from Sta. 71+00 to Sta. 82+00, 250' right and left of centerline of survey from Sta. 82+00 to Sta. 110+00, 500' right and left of centerline of survey from Sta. 110+00 to Sta. 133+00, 250' right and left of centerline of survey from Sta. 133+00 to Sta. 184+00, 500' right and left of centerline of survey from Sta. 184+00 to Sta. 194+00, 250' right and left of centerline of survey from Sta. 194+00 to Sta. 200+00, 500' right and left of centerline of survey from Sta. 200+00 to Sta. 210+00, 250' right and left of centerline of survey from Sta. 210+00 to Sta. 271+00, 500' right and left of centerline of survey from Sta. 271+00 to Sta. 281+00, 250' right and left of centerline of survey from Sta. 281+00 to Sta. 339+00, 500' right and left of centerline of survey from Sta. 339+00 to Sta. 349+00, 250' right and left of centerline of survey from Sta. 349+00 to Sta. 365+00, 500' right and left of centerline of survey from Sta. 365+00 to Sta. 382+00, 250' right and left of centerline of survey from Sta. 382+00 to Sta. 470+00, 500' right and left of centerline of survey from Sta. 470+00 to Sta. 484+00, and 250' right and left of centerline of survey from Sta. 484+00 to the end of survey.

S.H. 270-A: 150' right and left of centerline of survey from the beginning of survey to the end of survey.

U.S. 270-B: 150' right and left of centerline of survey from the beginning of survey to the end of survey.

NS-363 Section Line: 100' right and left of centerline of survey from the beginning of survey to the end of survey.

- XI. LAND TIES:**
- A Complete land tie information was obtained by conventional field methods as per the Survey Special Provisions within the following sections or partial sections:
- In T-9-N, R-6-E, I.M., Section 1.
 - In T-9-N, R-7-E, I.M., Sections 5, 6, 8, 9, 14, 15, 16, 17, 21, 22 and 23.
 - In T-9-N, R-6-E, I.M., Sections 26, 27, 35 and 36.
 - In T-9-N, R-7-E, I.M., Section 31
- The following is a detailed explanation of how each corner was re-established:
- NW Corner of Section 27, T-9-N, R-6-E, I.M., Found and accepted ODOT brass monument as shown on corner record filed by Jerry G. Anderson, PLS 1080.
- N/4 Corner of Section 27, T-9-N, R-6-E, I.M., Found and accepted PK nail as shown on corner record filed by Timothy G. Pollard, PLS 1474. This monument appears to be in the same location as shown on corner record filed by Jerry G. Anderson, PLS 1080.
- NE Corner of Section 27, T-9-N, R-6-E, I.M., Found and accepted cut 'X' in concrete as shown on corner record filed by Robby L. Johnson, PLS 1539 and by Bruce Ira Williams, PLS 1280. This monument appears to be in the same location as shown on corner record filed by Jerry G. Anderson, PLS 1080.
- W/4 Corner of Section 27, T-9-N, R-6-E, I.M., Found and accepted #5 rebar shown as found on corner record filed by Jerry G. Anderson, PLS 1080.

March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5002

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5003	5047

DESCRIPTION	REVISIONS	DATE

E/4 Corner of Section 27, T-9-N, R-6-E, I.M.,
Found and accepted PK nail. This monument appears to be in the same location as shown on corner record filed by Jerry G. Anderson, PLS 1080.

SW Corner of Section 27, T-9-N, R-6-E, I.M.,
Found and accepted railroad spike. This monument appears to be in the same location as shown on corner record filed by Jerry G. Anderson, PLS 1080.

S/4 Corner of Section 27, T-9-N, R-6-E, I.M.,
Found and accepted railroad spike. This monument appears to be in the same location as shown on corner record filed by Jerry G. Anderson, PLS 1080 and by Rodger Whited, PLS 1298.

SE Corner of Section 27, T-9-N, R-6-E, I.M.,
Found and accepted #5 rebar as shown on corner record filed by Jerry G. Anderson, PLS 1080.

N/4 Corner of Section 26, T-9-N, R-6-E, I.M.,
Found and accepted #5 rebar as shown on corner record filed by Jerry G. Anderson, PLS 1080 and by Bruce Ira Williams, PLS 1280. This monument appears to be in the same location as shown on corner record filed by James B. Marshall, PLS 113.

NE Corner of Section 26, T-9-N, R-6-E, I.M.,
Found and accepted #5 rebar as shown on corner record filed by James B. Marshall, PLS 113, Jerry G. Anderson, PLS 1080 and Bruce Ira Williams, PLS 1280.

E/4 Corner of Section 26, T-9-N, R-6-E, I.M.,
Found and accepted #5 rebar as shown on corner record filed by Jerry G. Anderson, PLS 1080. Monument shown on corner record filed by James B. Marshall, PLS 113 was not found.

S/4 Corner of Section 26, T-9-N, R-6-E, I.M.,
Found and accepted #5 rebar as shown on corner record filed by Jerry G. Anderson, PLS 1080.

SE Corner of Section 26, T-9-N, R-6-E, I.M.,
Found damaged nail at the location shown on corner record filed by Robby L. Johnson, PLS 1539. Replaced damaged nail with #4 rebar with cap stamped CA 6975. This monument matches the location shown on corner record filed by Jerry G. Anderson, PLS 1080 and by James B. Marshall, PLS 113.

W/4 Corner of Section 35, T-9-N, R-6-E, I.M.,
Found and accepted railroad spike as shown on corner record filed by Billy Jack Willingham, PLS 754.

E/4 Corner of Section 35, T-9-N, R-6-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by Rodger Whited, PLS 1298.

SW Corner of Section 35, T-9-N, R-6-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Billy Jack Willingham, PLS 754.

S/4 Corner of Section 35, T-9-N, R-6-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using proportionate measurement. There was no corner record on file.

SE Corner of Section 35, T-9-N, R-6-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using surrounding monuments and checks well with GLO distances. There was no corner record on file.

N/4 Corner of Section 36, T-9-N, R-6-E, I.M.,
Set mag nail with washer stamped CA 6975. This monument was re-established at the location shown on corner record filed by Robby L. Johnson, PLS 1539 using existing references and section data.

NE Corner of Section 36, T-9-N, R-6-E, I.M.,
Found and accepted 80d nail. This monument appears to be in the same location as shown on corner record filed by Rodger Whited, PLS 1298.

E/4 Corner of Section 36, T-9-N, R-6-E, I.M.,
Found and accepted #4 rebar. This monument fits well with surrounding occupation lines and GLO distances. There was no corner record on file.

S/4 Corner of Section 36, T-9-N, R-6-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using proportionate measurement. There was no corner record on file.

SE Corner of Section 36, T-9-N, R-6-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by James B. Marshall, PLS 113.

N/4 Corner of Section 31, T-9-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using proportional measurement. There was no corner record on file.

NE Corner of Section 31, T-9-N, R-7-E, I.M.,
Found and accepted PK nail as shown on corner record filed by Johnny Lee Pack, PLS 1252. This monument appears to be in the same location as shown on corner record filed by Bobby L. Goforth, PLS 340 and by James B. Marshall, PLS 113.

E/4 Corner of Section 31, T-9-N, R-7-E, I.M.,
Found and accepted PK nail as shown on corner record filed by Johnny Lee Pack, PLS 1252.

S/4 Corner of Section 31, T-9-N, R-7-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Billy Jack Willingham, PLS 754 and by Dan W. Rogers, PLS 1200.

SE Corner of Section 31, T-9-N, R-7-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by Rodger Whited, PLS 1298 and by Johnny Lee Pack, PLS 1252. Also found 1-1/2" pipe 4.7' south and 6.9' east of corner.

NW Corner of Section 1, T-8-N, R-6-E, I.M.,
Found BLM brass monument as closing corner as shown on the 2007 dependent resurvey. There was no corner record on file.

N/4 Corner of Section 1, T-8-N, R-6-E, I.M.,
Found BLM brass monument as shown on the 2007 dependent resurvey. This monument is 0.5' north of the calculated position for corner. There was no corner record on file.

NE Corner of Section 1, T-8-N, R-6-E, I.M.,
Found BLM brass monument as closing corner as shown on the 2007 dependent resurvey. This monument is 1.3' north of the calculated position for corner. There was no corner record on file.

W/4 Corner of Section 1, T-8-N, R-6-E, I.M.,
Found and accepted BLM brass monument as shown on the 2007 dependent resurvey. This monument was set at the location shown on corner record filed by Rodger Whited, PLS 1298.

E/4 Corner of Section 1, T-8-N, R-6-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established at the location shown on the 2007 dependent resurvey. There was no corner record on file.

SW Corner of Section 1, T-8-N, R-6-E, I.M.,
Found and accepted BLM brass monument as shown on the 2007 dependent resurvey. This monument was set at the location shown on corner record filed by Jerry G. Anderson, PLS 1080.

S/4 Corner of Section 1, T-8-N, R-6-E, I.M.,
Found and accepted BLM brass monument as shown on the 2007 dependent resurvey. Monuments shown on corner record filed by Jerry G. Anderson, PLS 1080 and by David F. Heavner, PLS 964 were not found.

SE Corner of Section 1, T-8-N, R-6-E, I.M.,
Found and accepted BLM brass monument as shown on the dependent resurvey. This monument was set at the location shown on corner record filed by David F. Heavner, PLS 964.

N/4 Corner of Section 6, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Dan W. Rogers, PLS 1200. Also found 1-1/2" pipe 2.1' south and 4.0' west of corner.

NE Corner of Section 6, T-8-N, R-7-E, I.M.,
Found and accepted #6 rebar. This monument matches the location shown on corner record filed by Johnny Lee Pack, PLS 1252.

E/4 Corner of Section 6, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by Frank Magner, PLS 1564. Also found mag nail 0.3' north and 8.7' east of corner.

S/4 Corner of Section 6, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This corner was re-established using proportionate measurement. There was no corner record on file.

SE Corner of Section 6, T-8-N, R-7-E, I.M.,
Found and accepted railroad spike as shown on corner record filed by Frank Magner, PLS 1564.

N/4 Corner of Section 5, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using proportionate measurement. There was no corner record on file.

NE Corner of Section 5, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Johnny Lee Pack, PLS 1252.

E/4 Corner of Section 5, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Johnny Lee Pack, PLS 1252.

S/4 Corner of Section 5, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar. This monument fits well with existing occupation evidence. There was no corner record on file.

PLS	SS	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	SK	SURVEY DATA SHEET
CHECKED	SS	
APPROVED	SS	
CREW	JH,JA,LB,RT BS,PT,BB	

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5003

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5004	5047

DESCRIPTION	REVISIONS	DATE

SE Corner of Section 5, T-8-N, R-7-E, I.M.,
Found and accepted 16p nail inside 2-1/2" iron pipe as shown on corner record filed by Johnny Lee Pack, PLS 1252.

W/4 Corner of Section 8, T-8-N, R-7-E, I.M.,
Set mag nail with washer stamped CA 6975. This monument was re-established using proportionate measurement. There was no corner record on file.

E/4 Corner of Section 8, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using section information provided by Marshall Surveying at the location shown on corner record filed by David F. Heavner, PLS 964.

SW Corner of Section 8, T-8-N, R-7-E, I.M.,
Found and accepted BLM brass monument as shown on the 2005 dependent resurvey. This monument was set at the point shown on corner record filed by Terry M. Marshall, PLS 1322.

S/4 Corner of Section 8, T-8-N, R-7-E, I.M.,
Set mag nail with washer stamped CA 6975. This monument was re-established using proportional measurement and fits existing occupation evidence. There was no corner record on file.

SE Corner of Section 8, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Rodger Whited, PLS 1298 and by Kelly K. Schmidt, PLS 1507.

N/4 Corner of Section 9, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Johnny Lee Pack, PLS 1252.

NE Corner of Section 9, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Johnny Lee Pack, PLS 1252.

E/4 Corner of Section 9, T-8-N, R-7-E, I.M.,
Found and accepted railroad spike as shown on corner record filed by David F. Heavner, PLS 964.

S/4 Corner of Section 9, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using proportional measurement. There was no corner record on file.

SE Corner of Section 9, T-8-N, R-7-E, I.M.,
Found and accepted bull prick as shown on corner record filed by Kelly K. Schmidt, PLS 1507.

W/4 Corner of Section 17, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using proportional measurement. There was no corner record on file.

E/4 Corner of Section 17, T-8-N, R-7-E, I.M.,
Found and accepted railroad spike as shown on corner record filed by Jesse L. Carroll, PLS 1071.

SW Corner of Section 17, T-8-N, R-7-E, I.M.,
Found and accepted BLM brass monument as shown on the 2005 dependent resurvey. Also found #3 rebar 17.1' south and 7.6' east of corner as shown on corner record filed by Johnny Lee Pack, PLS 1252.

S/4 Corner of Section 17, T-8-N, R-7-E, I.M.,
Found and accepted BLM brass monument as shown on the 2005 dependent resurvey. There was no corner record on file.

SE Corner of Section 17, T-8-N, R-7-E, I.M.,
Found and accepted BLM brass monument as shown on the 2005 dependent resurvey. This monument was set at the location shown on corner record filed by Johnny Lee Pack, PLS 1252 and by Rodger Whited, PLS 1298.

E/4 Corner of Section 16, T-8-N, R-7-E, I.M.,
Found and accepted original stone. There was no corner record on file.

S/4 Corner of Section 16, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was set using surrounding monuments and checks with GLO distances and existing occupation lines.

SE Corner of Section 16, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established from SWO 1736(1) survey. After discussion with Bearing Tree Land Surveying, it was determined that the distance to corner from highway PI was misread. Therefore the previous corner was pulled and new location accepted.

N/4 Corner of Section 15, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established online between section corners, using existing occupation evidence. There was no corner record on file.

NE Corner of Section 15, T-8-N, R-7-E, I.M.,
Found and accepted broke off mag nail for witness corner as shown on corner record filed by Kelly K. Schmidt, PLS 1507. Witness monuments set by Fred R. Smith, Jr., PLS 917 and by Virgil C. Vaughn, PLS 405 were not found.

E/4 Corner of Section 15, T-8-N, R-7-E, I.M.,
Found and accepted mag nail as shown on corner record filed by Virgil C. Vaughn, PLS 405, by Kelly K. Schmidt, PLS 1507 and by Jacob Royce Carroll, PLS 1522. This monument appears to be at the same location as shown on corner record filed by Fred R. Smith, Jr., PLS 917.

S/4 Corner of Section 15, T-8-N, R-7-E, I.M.,
Found and accepted mag nail. This monument appears to be in the same location as shown on corner record filed by Jacob Royce Carroll, PLS 1522.

SE Corner of Section 15, T-8-N, R-7-E, I.M.,
Found and accepted mag nail as shown on corner record filed by Jacob Royce Carroll, PLS 1522. This monument matches the location shown on corner record filed by Virgil C. Vaughn, PLS 405 and by Kelly K. Schmidt, PLS 1507.

N/4 Corner of Section 14, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar as shown on corner record filed by Fred R. Smith, Jr., PLS 917, by Kelly K. Schmidt, PLS 1507 and by Jacob Royce Carroll, PLS 1522.

NE Corner of Section 14, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by Timothy G. Pollard, PLS 1474.

E/4 Corner of Section 14, T-8-N, R-7-E, I.M.,
Found and accepted 2" pipe. This monument fits well with existing occupation evidence and GLO distances. There was no corner record on file.

S/4 Corner of Section 14, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by Jacob Royce Carroll, PLS 1522. Also found #4 rebar 0.4' south and 4.2' east of corner.

SE Corner of Section 14, T-8-N, R-7-E, I.M.,
Found hole in pavement at the location shown on corner record filed by Jacob Royce Carroll, PLS 1522. Set mag nail with washer stamped CA 6975 in hole. The monument shown on corner record filed by Fred R. Smith, Jr., PLS 917 was not found.

W/4 Corner of Section 21, T-8-N, R-7-E, I.M.,
Found and accepted BLM brass monument as shown on the 2005 dependent resurvey. There was no corner record on file.

E/4 Corner of Section 21, T-8-N, R-7-E, I.M.,
Set #4 rebar with cap stamped CA 6975. This monument was re-established using proportionate measurement. There was no corner record on file.

SW Corner of Section 21, T-8-N, R-7-E, I.M.,
Found and accepted BLM brass cap as shown on the 2005 dependent resurvey. This monument matches the location shown on corner record filed by Jacob Royce Carroll, PLS 1522 and by Johnny Lee Pack, PLS 1252.

S/4 Corner of Section 21, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by Johnny Lee Pack, PLS 1252 and by Jacob Royce Carroll, PLS 1522. Also found #4 rebar 7.7' south and 0.8' west of corner.

SE Corner of Section 21, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by Johnny Lee Pack, PLS 1252 and by Jacob Royce Carroll, PLS 1522. Also found #4 rebar 12.4' north and 7.7' east of corner.

E/4 Corner of Section 22, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar. This monument matches the location shown on corner record filed by Jacob Royce Carroll, PLS 1522.

S/4 Corner of Section 22, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by Jacob Royce Carroll, PLS 1522.

SE Corner of Section 22, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar as shown on corner record filed by David F. Heavner, PLS 964. The monument on corner record filed by Bobby L. Goforth, PLS 340 was not found.

E/4 Corner of Section 23, T-8-N, R-7-E, I.M.,
Found and accepted #3 rebar. This monument fits existing occupation evidence. There was no corner record on file.

S/4 Corner of Section 23, T-8-N, R-7-E, I.M.,
Found and accepted #4 rebar. This monument fits existing occupation evidence. There was no corner record on file.

SE Corner of Section 23, T-8-N, R-7-E, I.M.,
Found and accepted #5 rebar as shown on corner record filed by David F. Heavner, PLS 964. Also found #6 rebar 12.1' north and 8.2' west of corner.

B All property divisions, including existing right-of-way lines, adjacent to and/or crossing the Survey Centerline throughout the project limits were computed mathematically based upon the best available information.

PLS	SS	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	SK	SURVEY DATA SHEET
CHECKED	SS	
APPROVED	SS	
CREW	JH,JA,LB,RT BS,PT,BB	

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. S004

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5005	5047

DESCRIPTION	REVISIONS	DATE

XII. EXISTING RIGHT OF WAY:

The right of way along U.S. 270, S.H. 270-A, U.S. 270-B and NS-363 section line was established from the existing plans, right of way documents, and right of way occupation.

XIII. UTILITIES:

CALL OKIE was contacted on June 2, 3, 4 and 5, 2014 with ticket numbers 14060216533090, 14060216573120, 14060217213206, 14060217293216, 14060218013244, 14060219043291, 14060309330792, 14060316112801, 14060316532984, 14060412061621 and 14060505040016. A follow up request was made June 26, 2014 with ticket numbers 14062609090635 and 14062609120646. Utilities notified according to the CALL OKIE ticket are Enerfin Resources Seminole, Smoco PL/Midcont Fuel, City of Seminole, BCI Allegiance Engineering, USIC OG&E Shawnee, USIC AT&T, Centerpoint Energy OK, Suddenlink Seminole, Blueknight Energy Partners, Scissortail Copano Ada, ONEOK Gas Shawnee, Bowlegs-Lima Water District, ONEOK NGL Pipeline and Seminole County Conservation District. All utilities marked, along with any overhead lines, are depicted in the submitted digital file.

There are several pipeline blanket easements located throughout the project. There are also several easements that do not provide enough information to be accurately plotted.

XIV. ENVIRONMENTAL CONCERNS:

There are several areas throughout the project that are potential environmental concern areas. These areas are flagged on the submitted .dgn file.

XV. DRAINAGE:

Drainage areas for all drains crossing the Survey Centerline were taken from USGS quad maps that have been scanned into a Microstation Design File.

High water information was observed on three structures within the limits of the project. The high water data is reported within the SWO4879_1_V1_DRA drawing.

XVI. SURVEY DATA SHEETS:

Survey Data Sheets were submitted in the form of a Microstation Design File as per ODOT Survey Division Standards. These were incorporated into a set of design drawings and are in substantial conformity with the ODOT Survey Division Standards for Survey Data Sheets.

XVII. DATA SUBMITTED:

- A Reports
 - 1 Historical Letter & Written Report
 - 2 Form SD-1, Transmittal Letter
 - 3 Form SD-7, Public and Privately Owned Utilities List
 - 4 Form SD-11, Position and Description of Survey Monuments
 - 5 Form SD-10, Survey Control Data Statement
 - 6 Form SD-41, Surveyor's Certification
 - 7 Cogo Point List
 - 8 Alignment Report
 - 9 Benchmark & Check Levels List
 - 10 Oklahoma Certified Corner Record Forms
- B Computer Files
 - 1 Digital files submitted on compact disk

XVIII. PERSONNEL:

- Shawn Smith, PLS Professional Land Surveyor
- Clark Fisher, PLS Professional Land Surveyor
- Jason Harvey, PLS Professional Land Surveyor
- Jeremy Bone Survey Technician
- Shawn Kocmur, CST I Survey Technician
- Andrew Mayhew Title Researcher
- Jacob Andrews, CST I Survey Technician
- Pierce Trunum Survey Technician
- Seb Dietsche, LSIT Survey Technician
- Luke Brangard Survey Technician
- Randall Tollison Survey Technician
- Brandon Sechrist Survey Technician



 Shawn Smith, PLS

 Professional Land Surveyor

March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5005

Alignment Report

Project Name: SWO4879_1_V1
 Description: U.S. 270
 Horizontal Alignment Name: A001
 Description: Centerline of Survey U.S. 270
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Curve			
POB (301)	52+97.057	2367781.80740	691039.46903
PI (302)	55+63.467	2368048.20100	691036.52100
PT (305)	58+29.493	2368313.71876	691058.30541
Curve Length	532.436		
Radius	5729.578		
Delta	5°19'27.71"		
Tangent Direction:	S 89°21'57.48" E		
Tangent Length:	266.410		
Element: Linear			
PT (305)	58+29.493	2368313.71876	691058.30541
PC (307)	73+77.106	2369856.14899	691184.85415
Tangent Direction:	N 85°18'34.82" E		
Tangent Length:	1547.613		
Element: Curve			
PC (307)	73+77.106	2369856.14899	691184.85415
PI (308)	78+53.280	2370330.72800	691223.79100
PT (309)	83+10.425	2370768.03757	691035.37217
Curve Length	933.319		
Radius	1909.859		
Delta	27°59'58.43"		
Tangent Direction:	N 85°18'34.82" E		
Tangent Length:	476.174		
Element: Linear			
PT (309)	83+10.425	2370768.03757	691035.37217
PC (313)	107+88.529	2373043.84700	690054.78500
Tangent Direction:	S 66°41'26.75" E		
Tangent Length:	2478.104		
Element: Curve			
PC (313)	107+88.529	2373043.84700	690054.78500
PI (316)	122+21.966	2374360.32913	689487.60051
PT (319)	135+97.740	2375254.59491	688367.31920
Curve Length	2809.211		
Radius	5729.578		
Delta	28°05'31.59"		
Tangent Direction:	S 66°41'26.75" E		
Tangent Length:	1433.437		

Element: Linear			
PT (319)	135+97.740	2375254.59491	688367.31920
PC (322)	151+52.958	2376224.83544	687151.86140
Tangent Direction:	S 38°35'55.16" E		
Tangent Length:	1555.218		
Element: Curve			
PC (322)	151+52.958	2376224.83544	687151.86140
PI (323)	154+93.215	2376437.10890	686885.93823
PT (324)	158+30.300 Back	2376705.76653	686677.13633
	157+72.600 Forward		
Curve Length	677.342		
Radius	2864.789		
Delta	13°32'48.61"		
Tangent Direction:	S 38°35'55.16" E		
Tangent Length:	340.257		
Element: Linear			
PT (324)	157+72.600	2376705.76653	686677.13633
PC (331)	195+89.954	2379719.84200	684334.56890
Tangent Direction:	S 52°08'43.77" E		
Tangent Length:	3817.354		
Element: Curve			
PC (331)	195+89.954	2379719.84200	684334.56890
PI (332)	198+00.895	2379886.39395	684205.13871
PT (333)	200+11.646	2380042.97763	684063.79629
Curve Length	421.692		
Radius	5729.578		
Delta	4°13'00.90"		
Tangent Direction:	S 52°08'43.77" E		
Tangent Length:	210.941		
Element: Linear			
PT (333)	200+11.646	2380042.97763	684063.79629
PC (334)	204+92.022	2380399.56565	683741.91718
Tangent Direction:	S 47°55'42.87" E		
Tangent Length:	480.376		
Element: Curve			
PC (334)	204+92.022	2380399.56565	683741.91718
PI (335)	207+02.745	2380555.98766	683600.72068
PT (336)	209+13.279 Back	2380722.35894	683471.39593
	209+18.721 Forward		
Curve Length	421.257		
Radius	5729.578		
Delta	4°12'45.24"		
Tangent Direction:	S 47°55'42.87" E		
Tangent Length:	210.723		

Alignment Report

Project Name: SWO4879_1_V1
 Description: U.S. 270
 Horizontal Alignment Name: A001
 Description: Centerline of Survey U.S. 270
 Style: Centerline

	STATION	EASTING	NORTHING
Element: Curve			
POB (301)	52+97.057	2367781.80740	691039.46903
PI (302)	55+63.467	2368048.20100	691036.52100
PT (305)	58+29.493	2368313.71876	691058.30541
Curve Length	532.436		
Radius	5729.578		
Delta	5°19'27.71"		
Tangent Direction:	S 89°21'57.48" E		
Tangent Length:	266.410		
Element: Linear			
PT (305)	58+29.493	2368313.71876	691058.30541
PC (307)	73+77.106	2369856.14899	691184.85415
Tangent Direction:	N 85°18'34.82" E		
Tangent Length:	1547.613		
Element: Curve			
PC (307)	73+77.106	2369856.14899	691184.85415
PI (308)	78+53.280	2370330.72800	691223.79100
PT (309)	83+10.425	2370768.03757	691035.37217
Curve Length	933.319		
Radius	1909.859		
Delta	27°59'58.43"		
Tangent Direction:	N 85°18'34.82" E		
Tangent Length:	476.174		
Element: Linear			
PT (309)	83+10.425	2370768.03757	691035.37217
PC (313)	107+88.529	2373043.84700	690054.78500
Tangent Direction:	S 66°41'26.75" E		
Tangent Length:	2478.104		
Element: Curve			
PC (313)	107+88.529	2373043.84700	690054.78500
PI (316)	122+21.966	2374360.32913	689487.60051
PT (319)	135+97.740	2375254.59491	688367.31920
Curve Length	2809.211		
Radius	5729.578		
Delta	28°05'31.59"		
Tangent Direction:	S 66°41'26.75" E		
Tangent Length:	1433.437		

March 31st, 2015

PLS	SS		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,RR	SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5006	

SURVEY DATA SHEET

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5007	5047

DESCRIPTION	REVISIONS	DATE

Element: Curve			
PCC (407)	482+06.092	2402320.19002	667567.40779
PI (408)	487+32.970	2402812.02567	667378.46833
PT (413)	492+48.203 Back	2403204.45366	667026.89942
POT	492+47.888 Forward		
	12.504 Left	2403195.90019	667017.78373
Curve Length	1042.110		
Radius	2864.789		
Delta	20°50'31.95"		
Tangent Direction:	S 48°08'36.35" E		
Tangent Length:	526.878		
Element: Linear			
POT	492+47.888	2403195.90019	667017.78373
POE (414)	500+00.000	2403744.62430	666502.99484
Tangent Direction:	S 46°49'39.39" E		
Tangent Length:	752.400		
Project Name: SWO4879_1_V1 Description: U.S. 270 A Horizontal Alignment Name: A002 Description: Construction Reference Line U.S. 270 A Style: Centerline			
	STATION	EASTING	NORTHING
Element: Linear			
POB (390)	26+87.240	2370693.62973	691078.34042
PC (392)	28+02.400	2370708.52384	691192.53320
Tangent Direction:	N 07°25'52.00" E		
Tangent Length:	115.160		
Element: Curve			
PC (392)	28+02.400	2370708.52384	691192.53320
PI (393)	29+05.629	2370721.87491	691294.89556
PT (394)	30+08.502	2370720.41021	691398.11455
Curve Length	206.103		
Radius	1432.395		
Delta	8°14'38.75"		
Tangent Direction:	N 07°25'52.00" E		
Tangent Length:	103.229		
Element: Linear			
PT (394)	30+08.502	2370720.41021	691398.11455
POE (398)	35+00.000	2370713.43645	691889.56264
Tangent Direction:	N 00°48'46.75" W		
Tangent Length:	491.498		

Project Name: SWO4879_1_V1 Description: U.S. 270 B Horizontal Alignment Name: A003 Description: Centerline of Survey U.S. 270 B Style: Centerline			
	STATION	EASTING	NORTHING
Element: Linear			
POB (419)	482+00.000	2401797.57677	667674.73977
POE (385)	500+00.000	2403597.30116	667706.20766
Tangent Direction:	N 88°59'53.86" E		
Tangent Length:	1800.000		
Project Name: SWO4879_1_V1 Description: NS-363 Section Line Horizontal Alignment Name: A004 Description: Centerline of Survey NS-363 Section Line Style: Centerline			
	STATION	EASTING	NORTHING
Element: Linear			
POB (416)	17+00.000	2402902.20390	666293.84952
POE (383)	31+00.000	2402882.80400	667693.71510
Tangent Direction:	N 00°47'38.32" W		
Tangent Length:	1400.000		

March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION		SURVEY DIVISION	
PLS	SS		
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,BB	SWO 4879 (1)	PROJECT NO. 21006 (07) SHEET NO. 5007

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
300	691050.10900	2366820.34500	343	681641.76720	2383076.10200
301	691039.46900	2367781.80740	344	681027.97500	2383865.67900
302	691036.52100	2368048.20100	345	680742.82730	2384232.55670
303	691043.05750	2368057.63750	346	680618.34010	2384392.70470
304	691037.30150	2368057.71400	347	680414.29000	2384655.20000
305	691058.30540	2368313.71880	348	679800.59800	2385444.69600
306	691137.66570	2369280.99650	349	679186.88100	2386234.21800
307	691184.85420	2369856.14900	350	678658.82880	2386913.53990
308	691223.79100	2370330.72800	351	678573.12000	2387023.78200
309	691035.37220	2370768.03760	352	678014.19680	2387742.83400
310	691030.40380	2370779.56880	353	677909.44750	2387877.59000
311	690643.77300	2371676.81400	354	677772.05820	2388054.33600
312	690248.09600	2372595.25100	355	677608.88120	2388207.59480
313	690054.78500	2373043.84700	356	676900.56800	2388872.82600
314	689908.22270	2373357.63970	357	676171.64000	2389557.44700
315	689919.68690	2373357.48080	358	676137.98310	2389589.08970
316	689487.60050	2374360.32910	359	675442.69700	2390242.04300
317	688448.55260	2375188.52610	360	675409.73940	2390273.06960
318	688448.56820	2375189.73780	361	673984.86000	2391611.26600
319	688367.31920	2375254.59490	362	673285.90660	2392267.81280
320	687583.96800	2375879.88300	363	673256.00400	2392295.95200
321	687384.97250	2376038.75420	364	672804.84530	2392719.63450
322	687151.86140	2376224.83540	365	671798.17700	2393665.16200
323	686885.93820	2376437.10890	366	671069.28600	2394349.74600
324	686677.13630	2376705.76650	367	670439.56480	2394941.14980
325	686157.45540	2377374.42070	368	670340.32000	2395034.38900
326	686077.32750	2377477.50300	369	670202.53530	2395163.77230
327	685842.54260	2377779.60730	370	669611.42900	2395718.95200
328	685463.66870	2378267.07700	371	668882.57400	2396403.62800
329	685120.53620	2378708.58600	372	668526.75080	2396737.70020
330	684850.01470	2379056.61600	373	667601.46380	2397605.74690
331	684334.56890	2379719.84200	374	667886.89340	2397625.36890
332	684205.13870	2379886.39400	375	667623.65580	2398875.96160
333	684063.79630	2380042.97760	376	667624.76250	2398941.32340
334	683741.91720	2380399.56570	377	667640.60600	2399847.86500
335	683600.72070	2380555.98770	378	667647.68200	2400252.50300
336	683471.39590	2380722.35890	379	667658.17200	2400847.74100
337	683245.44110	2381013.04110	380	667670.69860	2401567.65350
338	682869.15030	2381497.07900	381	667675.63000	2401847.59800
339	682749.69290	2381650.80190	382	667686.72190	2402482.86510
340	682255.46380	2382286.61900	383	667693.71510	2402882.80400
341	681935.22140	2382698.58790	384	667705.21770	2403540.68270
342	681756.49250	2382928.51570	385	667706.20770	2403597.30120

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
386	691126.77040	2370549.08340	7603	691090.00600	2368197.02600
387	691089.11040	2370678.22400	7604	691101.81710	2368354.88280
388	691222.81260	2370693.03210	7605	691001.73040	2368358.21270
389	691029.32530	2370702.64110	7606	691106.73490	2368414.82280
390	691078.34040	2370693.62970	7607	691115.25900	2368518.71860
391	691135.84180	2370701.12960	7608	691120.27040	2368518.65200
392	691192.53320	2370708.52380	7609	691004.08430	2368558.19890
393	691294.89560	2370721.87490	7610	691132.55170	2368668.34210
394	691398.11450	2370720.41020	7611	691152.48470	2368666.70660
395	691232.67160	2370699.75560	7612	691009.10250	2368984.53790
396	691403.28770	2370697.33450	7613	691048.35110	2368987.30130
397	691889.23630	2370690.43880	7614	691120.07180	2369861.46410
398	691889.56260	2370713.43650	7615	691249.63650	2369850.83390
399	691518.35410	2370750.70730	7616	691250.33370	2370165.06710
400	691159.00800	2370764.83780	7617	691242.43850	2370666.61370
401	691064.54990	2370768.55220	7618	691980.76120	2370656.13670
402	691031.33030	2370845.65310	7619	691007.73990	2370662.87520
403	691031.34140	2370846.44620	7620	689259.40000	2370682.02580
404	691027.14460	2370855.36790	7621	689259.86970	2370762.02940
405	667662.45660	2401781.48870	7622	690975.28170	2370743.23560
406	667667.32790	2402060.08410	7623	691254.67160	2370799.45350
407	667567.40780	2402320.19000	7624	691188.70060	2370809.79810
408	667378.46830	2402812.02570	7625	691058.55410	2371004.09290
409	667310.29900	2402888.11750	7626	691033.97600	2371034.35710
410	667306.48230	2402888.17040	7627	690977.54340	2371103.84510
411	667272.34030	2402888.64360	7628	690971.17600	2371099.00190
412	667161.11200	2403043.12360	7629	690923.51840	2371159.09890
413	667026.89940	2403204.45370	7630	690933.07190	2371169.73940
414	666502.99480	2403744.62430	7631	689995.10740	2373018.16550
415	666373.59530	2402901.09880	7632	690114.49710	2373069.60560
416	666293.84950	2402902.20390	7633	689981.42830	2373356.62510
417	691062.30700	2370702.17310	7634	689834.79030	2373358.65740
418	691063.75940	2370702.15250	7635	689745.71860	2373359.89190
419	667674.73980	2401797.57680	7636	689747.90060	2373521.72250
7400	691201.70000	2370593.35200	7637	688563.25520	2375176.86690
7401	680974.76100	2383844.85000	7638	688463.78940	2375090.85680
7402	677189.06300	2388692.08700	7639	688441.61220	2373377.96620
7403	670355.48800	2394956.79100	7640	688408.61540	2373378.42210
7404	667108.96100	2402969.29800	7641	688425.74840	2374701.71890
7405	686371.84700	2377030.61100	7642	688431.14350	2375118.42390
7600	691078.19250	2368057.17080	7643	688468.62700	2375464.50000
7601	690998.19950	2368058.23350	7644	688475.66450	2376008.05330
7602	691079.84020	2368197.16110	7645	688442.66780	2376008.51710

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
7646	688435.97700	2375491.74550	7689	683266.58520	2381360.06050
7647	688334.17880	2375364.21890	7690	683795.01790	2381351.82280
7648	687643.02970	2375915.92950	7691	684570.72100	2381339.73040
7649	687724.38390	2376017.84510	7692	685891.35790	2381319.14310
7650	687300.07350	2376023.35540	7693	685891.82370	2381352.13990
7651	687138.89050	2376025.44850	7694	684571.18650	2381372.72720
7652	685818.61450	2376042.59420	7695	683267.04730	2381393.05730
7653	685819.04170	2376075.59140	7696	683286.37070	2382696.14800
7654	686027.70570	2376072.88160	7697	683296.15660	2383356.07550
7655	686918.37240	2376061.31510	7698	683305.93790	2384015.69170
7656	687046.31470	2376059.65360	7699	683275.38460	2384180.93290
7657	687469.87160	2376054.15310	7700	683256.57710	2382912.62550
7658	687685.68680	2376051.35040	7701	683248.62610	2382376.43680
7659	688443.12290	2376041.51400	7702	683243.43660	2382026.47520
7660	687192.41240	2376275.63520	7703	683238.01720	2382661.01540
7661	686240.64800	2377373.30190	7704	683237.52790	2381628.01900
7662	686023.06720	2377376.22800	7705	683234.69340	2381434.53990
7663	685840.22390	2377611.48570	7706	683233.40430	2381346.54930
7664	685843.97790	2377883.68260	7707	683230.65460	2381137.97510
7665	685294.58050	2378590.57190	7708	682845.80990	2381633.06290
7666	685290.63260	2378587.50360	7709	682819.89660	2381666.39930
7667	685261.79070	2378624.61340	7710	682679.48920	2381635.20450
7668	685265.73850	2378627.68170	7711	681932.66080	2381644.82080
7669	685203.75900	2378707.42850	7712	680611.25980	2381661.83560
7670	684986.09940	2378710.45600	7713	680611.50020	2381694.83520
7671	684385.90660	2379759.72870	7714	681932.90230	2381677.82050
7672	684192.21910	2379731.91220	7715	682653.57600	2381668.54090
7673	683895.14130	2380071.31520	7716	681934.98150	2382592.98480
7674	684112.04640	2380086.53130	7717	681935.46130	2382804.19100
7675	683790.16730	2380443.11930	7718	681839.50410	2382927.63630
7676	683522.71510	2380762.25060	7719	681673.48100	2382929.39500
7677	683260.36120	2380887.93510	7720	680826.48080	2384230.85150
7678	683256.49640	2380594.76450	7721	680659.17380	2384234.26190
7679	683254.92570	2380475.61700	7722	680618.07240	2384287.13720
7680	683231.97730	2378734.85480	7723	680618.60770	2384498.27210
7681	683202.75020	2379021.27470	7724	678755.78500	2386894.72130
7682	683220.04130	2380332.89810	7725	679003.88400	2386888.80030
7683	683222.21120	2380497.49520	7726	679228.38790	2386883.44240
7684	683223.76970	2380615.71760	7727	679437.03850	2386878.46280
7685	683224.04520	2380636.61100	7728	680624.57040	2386850.12160
7686	683227.90870	2380929.68400	7729	680624.80010	2386883.12550
7687					

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
7732	679322.51330	2386914.20540	7775	673622.00700	2392246.76070
7733	679323.09150	2386937.69830	7776	674051.10650	2392240.95150
7734	678711.02320	2386952.30570	7777	674216.09130	2392238.71790
7735	678477.91940	2386957.86890	7778	675431.07930	2392222.26930
7736	678019.96340	2386968.79830	7779	675431.50740	2392255.26660
7737	677986.96840	2386969.38850	7780	673522.00530	2392281.11750
7738	678588.01670	2386898.72520	7781	672986.82800	2392255.35970
7739	678148.83010	2386909.20670	7782	672813.77170	2392257.70260
7740	678019.29770	2386912.29810	7783	672806.72800	2391560.96980
7741	678011.65710	2385791.22020	7784	672804.61810	2391352.27050
7742	678007.04620	2385114.67670	7785	672800.58890	2390953.72480
7743	678005.91380	2384948.52840	7786	672787.23920	2389633.22970
7744	678001.41500	2384288.43780	7787	672754.24200	2389633.67350
7745	677155.34720	2386979.39460	7788	672767.59160	2390954.16030
7746	676688.69090	2386985.00950	7789	672780.66440	2392247.26170
7747	678485.94980	2387030.03030	7790	672685.79770	2392214.59140
7748	678028.96600	2387617.92230	7791	672668.04320	2392214.14890
7749	677996.54140	2387659.63530	7792	672560.00220	2392211.46020
7750	678031.85230	2387826.03270	7793	672485.61530	2392209.60620
7751	678034.48990	2388016.21070	7794	671586.67750	2392260.92060
7752	678055.95270	2389563.75680	7795	671486.98240	2392275.55130
7753	678022.95650	2389564.25850	7796	671478.20710	2392275.66850
7754	678004.68510	2388246.82540	7797	670159.14110	2392293.41100
7755	677999.42770	2387867.74580	7798	670159.55800	2392326.40830
7756	677960.76660	2387917.48170	7799	671478.63610	2392308.66570
7757	677858.12840	2387837.69830	7800	671488.16420	2392308.53750
7758	677653.38070	2388254.97410	7801	671488.61540	2392342.08310
7759	677564.38170	2388160.21560	7802	671588.89690	2392352.09530
7760	677558.68290	2388343.91600	7803	672611.78170	2392374.49290
7761	677575.79810	2388362.13880	7804	672620.77850	2392370.92080
7762	677174.89660	2388738.67300	7805	672713.72630	2392334.02050
7763	677157.78140	2388720.45020	7806	672781.48140	2392307.11900
7764	676234.27860	2389587.82110	7807	672785.90060	2392572.79870
7765	676041.68760	2389590.35820	7808	673450.20890	2392523.45670
7766	675856.92090	2389763.89440	7809	673296.05270	2393295.26500
7767	675867.19000	2389774.82810	7810	673298.32050	2393851.94530
7768	675408.99600	2390205.17240	7811	673308.11050	2393849.90680
7769	675408.77300	2390184.80320	7812	673525.15490	2394892.28690
7770	675410.70570	2390361.33660	7813	673198.07320	2393730.57440
7771	673688.04320	2391979.29260	7814	673217.60230	2393279.59580
7772	673455.70770	2392019.15840	7815	673371.75840	2392507.78750
7773	673397.51610	2391957.20090	7816	673430.60460	2392296.53870
7774	673535.71780	2392247.92880	7817	672823.78990	2392866.47030

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
7818	672857.64850	2394902.04120	7861	666543.59690	2402882.24120
7819	672824.65240	2394902.54930	7862	665053.19760	2402902.89580
7820	672791.29300	2392896.99200	7863	665053.63210	2402935.89290
7821	672530.60400	2393141.83600	7864	666112.76760	2402921.21500
7822	672516.91190	2393127.25770	7865	666373.74260	2402917.59830
7823	672366.29820	2392966.89710	7866	666544.05420	2402915.23800
7824	672379.99030	2392981.47530	7867	666544.65690	2402958.73390
7825	670588.19500	2394938.74440	7868	667110.52170	2402950.89190
7826	670536.17440	2394939.58630	7869	666703.74330	2403384.48540
7827	670290.93460	2394943.55530	7870	666662.24050	2403414.10630
7828	670199.22220	2394945.03950	7871	666380.85480	2403714.04320
7829	670201.20350	2395075.84910	7872	666383.63440	2404025.31790
7830	670203.86700	2395251.69550	7873	666836.14460	2403542.97800
7831	668482.25130	2396690.32090	7874	666849.60300	2403521.32450
7832	668571.25030	2396785.07940	7875	666857.125030	2402936.90670
7833	667967.59760	2397606.71730	7876	667629.59470	2402933.69740
7834	670238.88710	2397563.72310	7877	667758.41850	2402865.28550
7835	670239.47730	2397602.46310	7878	669006.55050	2402845.75870
7836	669580.14080	2397613.50760	7879	670319.67470	2402825.21090
7837	667951.39810	2397640.79070	7880	670320.10390	2402858.20830
7838	667838.11760	2397576.17900	7881	669006.97850	2402878.75610
7839	667617.57830	2397579.87320	7882	667758.99540	2402898.28450
7840	667578.04680	2394987.46100	7883	667770.21020	2403539.68790
7841	664965.13230	2397672.90000	7884	667781.71360	2404197.59290
7842	667585.63320	2397646.80610	7885	667641.21760	2403598.43750
7843	667594.95970	2398177.61090	7886	691028.18540	2370605.79460
7844	667697.06900	2398603.42990	7887	691402.81940	2370664.33780
7845	667712.02160	2398604.62170	8000	690993.68000	2367674.26010
7846	667523.67110	2398877.70980	8001	690853.69240	2367676.12000
7847	667524.80320	2398942.45720	8002	690857.81170	2368026.09570
7848	667703.64360	2398874.56300	8003	690997.79940	2368024.23590
7849	667547.73140	2400253.77500	8004	691071.65650	2367685.13240
7850	667582.72890	2400253.32960	8005	691596.61020	2367678.15790
7851	667727.71260	2400251.12900	8006	691598.25800	2367818.14840
7852	667750.70890	2401566.34060	8007	691073.30430	2367825.12290
7853	667609.46470	2401782.41520	8008	691073.53970	2367845.12150
7854	667592.46730	2401782.71240	8009	691598.49330	2367838.14700
7855	667759.81300	2402087.02690	8010	691600.14110	2367978.13750
7856	667744.81530	2402087.28910	8011	691075.18740	2367985.11190
7857	667558.97350	2402107.68690	8012	691203.18150	2368055.51020
7858	667557.32340	2402156.42370	8013	691204.82920	2368195.50050
7859	667239.82810	2402829.08840	8014	691183.83110	2368195.77940
7860	666542.99410	2402838.74540	8015	690858.21190	2368060.09340

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
8018	690861.74280	2368360.07260	8050	690960.05780	2370363.24950
8019	690389.93950	2368366.34090	8051	691092.64170	2370362.85430
8020	690454.00050	2368554.25920	8052	691253.15180	2370199.41460
8021	690457.81760	2368565.45650	8053	691462.58660	2370196.44270
8022	690864.02090	2368560.05980	8054	691523.18050	2370195.58280
8023	690864.68520	2368610.05530	8055	691925.07330	2369979.85910
8024	691004.67280	2368608.19540	8056	691924.57710	2369939.86220
8025	691185.69060	2368353.76850	8057	691971.15540	2369979.20520
8026	691182.39720	2368413.81760	8058	692401.00910	2369933.10150
8027	691183.62120	2368517.81030	8059	692410.83270	2370683.03720
8028	691423.60000	2368514.62200	8070	692459.12780	2370682.35190
8029	691441.01710	2368726.90870	8071	692449.90050	2370021.11370
8030	691481.01360	2368726.37730	8072	693332.83040	2370009.98680
8031	691497.94000	2368932.68410	8073	693328.66360	2369648.01080
8032	692113.81640	2368924.50170	8074	693661.67430	2369643.81420
8033	692109.11170	2368489.60710	8075	691981.22950	2370689.13340
8034	692326.09260	2368486.72430	8076	691464.07640	2370301.43210
8035	692321.26710	2368040.65540	8077	691436.67910	2370301.82090
8036	692308.93650	2366900.81710	8078	691261.75740	2370304.30310
8037	692847.21900	2366893.66560	8079	691440.26320	2370554.39550
8038	692857.57840	2367793.60600	8080	691401.26720	2370554.94880
8039	693637.43610	2367783.24490	8081	691401.93970	2370602.34410
8040	691174.65920	2368936.97920	8082	691403.28770	2370697.33450
8041	691006.04920	2368725.12730	8083	691324.94750	2370603.43660
8042	690514.47550	2368731.65830	8084	691324.66370	2370583.43860
8043	690552.13850	2368842.14020	8085	691260.31910	2370584.35170
8044	691007.34380	2368835.11970	8086	690992.06210	2370703.04940
8045	690686.00240	2369234.82020	8087	690691.36050	2370706.34380
8046	691211.48040	2369385.77170	8088	691899.50040	2370790.30320
8047	691510.47580	2369361.24070	8089	691810.50940	2370791.56600
8048	691223.74600	2369535.26940	8090	691660.52450	2370793.69430
8049	691423.07620	2369518.91540	8091	691100.00300	2370952.69190
8050	691236.01150	2369684.76710	8092	691068.01140	2370992.44780
8051	691435.34180	2369668.41300	8093	691061.93780	2371245.43810
8052	691535.00690	2369660.23600	8094	691356.57480	2371241.25720
8053	691537.45230	2369985.35960	8095	691385.24920	2371198.64090
8054	691520.20080	2369985.60440	8096	691666.14600	2371194.65490
8055	691256.11630	2369989.35180	8097	691816.13090	2371192.52660
8056	690824.99870	2370488.79750	8098	691905.12190	2371191.26380
8057	690793.33140	2370705.22660	8099	691036.11330	2371187.19720
8058					

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5010	5047

DESCRIPTION	REVISIONS	DATE

COORDINATE POINT LIST
SWO 4879(1) Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
8102	690887.34360	2371275.66130	8145	688741.05250	2375018.05970
8103	690887.92400	2371317.03720	8146	689116.00980	2375684.08430
8104	690869.41540	2371317.26610	8147	689006.02620	2375685.98360
8105	690819.59160	2371432.88790	8148	691098.74910	2375654.41000
8106	690889.53620	2371432.02280	8149	689010.39360	2376016.46780
8107	691039.52470	2371430.16770	8150	688442.24690	2374701.49820
8108	691040.78370	2371520.16440	8151	688447.46710	2375104.69250
8109	690890.79510	2371522.01400	8152	688452.75050	2375512.75830
8110	690621.90740	2371563.39770	8153	688286.08940	2374703.58730
8111	690714.85000	2371676.22020	8154	688098.30280	2374706.09950
8112	690892.92700	2371674.01060	8155	687970.32110	2374707.81170
8113	691042.91550	2371672.14950	8156	687122.52920	2374719.15360
8114	691044.66860	2371797.13720	8157	687130.04360	2375319.10650
8115	690894.68020	2371798.99830	8158	687499.05700	2375314.16980
8116	691072.82190	2372021.76440	8159	688106.27750	2374981.73950
8117	691091.32090	2373341.23850	8160	687815.95560	2375355.28580
8118	691047.82430	2372022.11500	8161	687694.13900	2375488.79710
8119	690563.22720	2372028.12810	8162	687517.96620	2375625.10860
8120	690383.53060	2371744.40240	8163	687189.24710	2375933.33280
8121	690149.58450	2371902.64980	8164	687135.83520	2375781.51370
8122	690228.54140	2371964.01600	8165	687137.84380	2375941.88640
8123	690174.96830	2372032.94580	8166	687138.55540	2375998.70060
8124	690265.22570	2372031.82580	8167	687139.09710	2376041.94730
8125	690420.91320	2372029.89400	8168	687279.05970	2376040.12960
8126	690383.27780	2372117.24360	8169	687490.88540	2376037.37880
8127	690156.59150	2372643.36980	8170	687685.44530	2376034.85210
8128	689814.94850	2372496.16530	8171	687059.27040	2376042.98390
8129	689471.86070	2372937.59930	8172	686931.32810	2376044.64540
8130	689630.95330	2372934.40680	8173	686027.79660	2376056.37910
8131	689491.51330	2373122.78250	8174	686024.08580	2376730.36890
8132	689951.67110	2373116.40490	8175	686295.57790	2376862.64160
8133	689009.06330	2373370.10160	8176	687688.37320	2376234.83070
8134	689137.09390	2373368.32720	8177	687578.38240	2376236.25910
8135	688434.57910	2374109.26190	8178	687579.84640	2376336.24840
8136	688436.65430	2374269.54790	8179	687359.86490	2376339.10520
8137	690149.54570	2372988.25990	8180	687366.32390	2376780.25790
8138	690488.30750	2372983.56490	8181	687697.29380	2376775.95980
8139	690493.43830	2373349.52890	8182	687705.75110	2377353.59900
8140	689751.31820	2373775.19500	8183	687065.27410	2376784.16740
8141	689763.54960	2374682.34960	8184	687041.95130	2376784.47030
8142	689102.89830	2374691.92390	8185	686810.27490	2376645.26510
8143	689108.16980	2375090.82910	8186	686755.43400	2376711.49720
8144	688966.09810	2375077.34700	8187	687037.09370	2376819.61620

COORDINATE POINT LIST
SWO 4879(1) Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
8188	686848.61540	2377056.70740	8231	684273.91520	2381096.85650
8189	686567.98210	2376952.13330	8232	683537.16070	2380743.74620
8190	687157.84840	2377360.96730	8233	683244.04900	2380908.92020
8191	686542.43590	2377369.24340	8234	683239.98990	2380605.31590
8192	685895.07850	2377377.94920	8235	683238.42660	2380486.76640
8193	685837.01390	2377378.73000	8236	683983.02010	2379427.47630
8194	685838.01570	2377451.36980	8237	683985.81030	2379636.16760
8195	686130.49630	2377515.03000	8238	683777.12050	2379639.07040
8196	686289.04600	2377616.05480	8239	683774.33030	2379430.37900
8197	686297.38530	2377716.15810	8240	683795.23850	2381368.32140
8198	686748.66100	2377931.44910	8241	683793.09950	2381208.33570
8199	686743.19430	2378086.35270	8242	683246.83320	2381117.16200
8200	686858.89540	2378249.48840	8243	683248.16570	2381216.83060
8201	687124.96080	2378227.16730	8244	683249.89720	2381346.33690
8202	687124.60820	2377902.16750	8245	683251.15750	2381434.32790
8203	687176.59970	2378679.98730	8246	683259.93520	2382026.26280
8204	685849.55960	2378288.40610	8247	683265.12470	2382376.22430
8205	685632.49790	2378155.78610	8248	683736.50190	2382690.24480
8206	685424.46760	2378423.45090	8249	683746.28790	2383350.17220
8207	685379.67400	2378041.10100	8250	683279.65800	2383356.29180
8208	685124.69870	2378044.64760	8251	683240.40540	2380636.40030
8209	684603.08890	2378715.78340	8252	683189.93670	2380637.05020
8210	684931.90830	2379057.20870	8253	683029.66040	2380731.68030
8211	685163.25280	2379237.01060	8254	683035.61510	2381177.06230
8212	685313.44490	2379291.98210	8255	682974.24330	2381256.01480
8213	685866.19050	2379494.29140	8256	682568.76520	2380950.53490
8214	685151.86780	2379567.40590	8257	681925.19320	2381040.73870
8215	685099.14120	2379706.56170	8258	681926.45290	2381142.64440
8216	684944.36190	2379969.54430	8259	683066.85750	2381348.69380
8217	684779.58620	2380195.77820	8260	682997.75550	2381437.59070
8218	684911.65280	2379560.73490	8261	682832.85330	2381649.73110
8219	684990.60990	2379622.10080	8262	682666.53260	2381651.87270
8220	684902.39630	2379940.09190	8263	682765.16620	2382382.66190
8221	684730.57190	2380161.17190	8264	682759.97670	2382032.70040
8222	684588.83560	2380061.97200	8265	682532.88820	2382035.62450
8223	684304.03280	2379862.64090	8266	682365.67830	2382038.91010
8224	684343.10620	2380346.80290	8267	681933.73610	2382044.81860
8225	684108.51690	2380606.68850	8268	681934.23360	2382263.81800
8226	683794.21760	2380438.63230	8269	682193.60940	2382260.27010
8227	684113.26190	2379670.54640	8270	682320.14510	2382309.31030
8228	683895.14130	2380071.31520	8271	682497.78820	2382447.39690
8229	684564.07400	2380850.27560	8272	682375.04450	2382605.30190
8230	684567.48240	2381101.25250	8273	682197.40140	2382467.21520

COORDINATE POINT LIST
SWO 4879(1) Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
8274	682105.34370	2382585.64400	8317	679318.18890	2385279.66180
8275	682282.98680	2382723.73060	8318	679319.59260	2385579.65850
8276	682221.61490	2382802.68310	8319	679307.59550	2385579.92390
8277	682043.97180	2382664.59650	8320	679306.04220	2385247.94750
8278	682126.85160	2382924.59250	8321	679169.00890	2385250.08720
8279	682052.84240	2383019.80250	8322	679031.02600	2385434.72460
8280	681960.78460	2383138.23130	8323	679011.92180	2385511.65810
8281	681935.74440	2382928.77920	8324	679257.03310	2385697.53580
8282	681933.00010	2382926.64600	8325	679189.10840	2385855.53380
8283	681875.19920	2382881.71590	8326	679311.64240	2385967.80880
8284	681783.14150	2383000.14460	8327	679301.42870	2384261.93830
8285	681789.19810	2381066.98720	8328	679304.51210	2384920.93110
8286	681806.34250	2381165.50660	8329	677989.41580	2384948.87900
8287	681778.01820	2382794.91200	8330	677990.46780	2385103.23620
8288	681650.50990	2382695.79650	8331	678463.71970	2385429.82110
8289	681528.02500	2382697.51330	8332	678635.15670	2385791.22020
8290	680614.07470	2382710.32390	8333	677995.15670	2385791.22020
8291	680607.79650	2381374.07800	8334	678973.24790	2386403.14020
8292	680606.57740	2381272.78730	8335	678587.49620	2386022.90660
8293	680595.62180	2380362.52610	8336	678140.56840	2386925.90860
8294	681511.33380	2382737.79190	8337	678574.94470	2386915.54190
8295	681626.14040	2382827.35600	8338	678920.01010	2386683.45210
8296	681704.98570	2382888.86550	8339	679003.35830	2386661.46300
8297	681549.42560	2382926.04660	8340	679003.92580	2386905.30400
8298	681555.32210	2382930.64660	8341	678742.71300	2386911.53800
8299	681628.27090	2382987.55610	8342	679228.42980	2386899.94610
8300	679954.97370	2382947.59890	8343	679227.90060	2386691.23670
8301	679959.68460	2384248.52030	8344	679436.55120	2386686.25720
8302	679961.49410	2384750.03850	8345	679437.08030	2386894.96650
8303	680349.51980	2384844.44340	8346	679548.48860	2386892.30770
8304	680507.42480	2384967.18700	8347	679792.43920	2386886.48560
8305	680384.68110	2385125.09200	8348	679922.47130	2387112.15320
8306	680226.77610	2385002.34840	8349	679845.39780	2387149.85350
8307	679962.86520	2385130.03600	8350	679875.56860	2387227.17570
8308	679781.13700	2385363.82220	8351	679874.17310	2387381.56940
8309	679691.11560	2385256.60220	8352	679897.41940	2387421.37920
8310	679526.25550	2385263.39680	8353	679791.57610	2387508.67770
8311	679505.87180	2384768.81660	8354	679816.84090	2387534.46340
8312	680621.27640	2385550.86390	8355	679807.19410	2387613.77890
8313	679786.0385				

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5011	5047

DESCRIPTION	REVISIONS	DATE

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
8360	679471.21810	2387957.22670	8403	675244.67010	2389810.87510
8361	679319.94670	2388109.02080	8404	675242.37110	2389600.88770
8362	679113.44420	2388247.25600	8405	674978.39400	2389604.86520
8363	679085.40340	2388343.24410	8406	674980.69300	2389814.35260
8364	679087.62440	2388451.79140	8407	674086.54890	2389616.11360
8365	679091.24190	2388628.59440	8408	674072.65400	2388296.38180
8366	679223.98100	2389102.21490	8409	675387.91700	2388279.80580
8367	679437.03890	2389483.87280	8410	674808.54340	2390926.89810
8368	679483.69870	2389539.14170	8411	674107.28450	2391585.53350
8369	679676.58520	2389767.61730	8412	674105.42510	2391408.93180
8370	679774.83950	2389849.56070	8413	674100.44940	2390936.35520
8371	680658.38360	2389831.48290	8414	672784.10730	2390953.94510
8372	678592.53440	2388058.82360	8415	672788.11950	2391352.49230
8373	678484.19980	2388019.70780	8416	672996.81060	2391349.68610
8374	678396.04040	2388008.81320	8417	672998.92050	2391558.38540
8375	678017.99300	2388016.54830	8418	672790.22940	2391561.19160
8376	678015.64000	2387846.88920	8419	672963.66660	2392272.17480
8377	678012.75370	2387638.77880	8420	673528.73520	2392264.52570
8378	678021.18380	2388246.61750	8421	673620.11130	2392082.39360
8379	677855.89090	2388248.70080	8422	673622.19730	2392263.26050
8380	677874.16340	2389566.21860	8423	674051.32500	2392257.45130
8381	677657.38880	2388251.20260	8424	674049.57670	2392125.46290
8382	677173.35910	2388527.47130	8425	674112.96020	2392124.60490
8383	677014.95950	2388409.29940	8426	674214.56160	2392123.22950
8384	677014.95950	2388259.29940	8427	674216.30990	2392255.21790
8385	676704.55040	2388263.21160	8428	673278.39080	2393728.84090
8386	676693.98480	2387411.66660	8429	673851.65710	2393721.09130
8387	676688.19470	2386945.01260	8430	673858.30480	2394121.03600
8388	677154.85090	2386939.39770	8431	673365.95090	2394127.69180
8389	677160.64100	2387406.05180	8432	671465.01710	2390971.86120
8390	677042.02310	2388829.17260	8433	671478.37400	2392292.16770
8391	677051.30250	2389577.05830	8434	671499.28010	2393607.91430
8392	676720.90620	2389581.41070	8435	671502.42280	2393805.70310
8393	676735.86410	2390901.15760	8436	671506.71840	2394076.05020
8394	678052.26760	2390883.57640	8437	671520.18620	2394923.66090
8395	676750.82210	2392220.90550	8438	670618.78710	2394635.63010
8396	675424.03430	2391578.77140	8439	670553.57210	2394280.09840
8397	675416.80860	2390918.77430	8440	670189.23300	2394285.55520
8398	675413.86070	2390649.50880	8441	670179.26510	2393627.47490
8399	675098.86990	2390654.21800	8442	668840.28150	2392327.61120
8400	675099.50770	2390475.27100	8443	668860.33350	2393646.99750
8401	675097.20870	2390265.28360	8444	667541.40190	2393666.52000
8402	675248.09350	2390123.56980	8445	669579.87870	2397597.00970

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
8446	669621.52750	2400218.61630	9005	690967.09140	2365415.32170
8447	667624.76250	2398941.32340	9006	690998.19950	2368058.23350
8448	664986.99440	2398971.24190	9007	691029.32530	2370702.64110
8449	668963.57900	2400229.91180	9008	691066.32330	2373341.58500
8450	668985.20030	2401546.08420	9009	691103.38130	2375984.81300
8451	670299.70200	2401524.51480	9010	688327.83930	2365453.87160
8452	667670.69860	2401567.65350	9011	688359.30890	2368093.29350
8453	669006.82160	2402862.25650	9012	688390.65950	2370731.55000
8454	669013.08580	2403520.66500	9013	688425.11380	2373378.19490
8455	669019.35010	2404179.07340	9014	688459.38000	2376024.80150
8456	667764.10880	2403190.73210	9015	688497.98570	2378661.60760
8457	667972.79320	2403187.46660	9016	688531.71820	2381294.47150
8458	667976.44200	2403396.14470	9017	688550.68230	2382627.10910
8459	667767.75750	2403399.41020	9018	688569.44020	2383945.24760
8460	667699.21810	2403540.77450	9019	688606.95580	2386581.52570
8461	667773.79470	2403744.69210	9020	687211.65180	2381315.05650
8462	667982.48130	2403741.56940	9021	687230.71170	2382644.41590
8463	667990.40020	2404194.47020	9022	685754.80420	2370769.99220
8464	668153.34890	2404192.03190	9023	685786.80900	2373414.52490
8465	668151.48330	2403983.33020	9024	685818.81420	2376059.09300
8466	668360.17000	2403980.20750	9025	685855.21360	2378698.36710
8467	668362.03560	2404188.90920	9026	685891.58540	2381335.64160
8468	666373.59530	2402901.09880	9027	685910.74100	2382661.73060
8469	666112.62030	2402904.71540	9028	685929.79200	2383980.57530
8470	666114.48660	2403113.70710	9029	685967.89390	2386618.26660
8471	666375.46150	2403110.09040	9030	684570.94510	2381356.22900
8472	666397.11940	2405535.41140	9031	684590.30650	2382679.04750
8473	667056.42320	2405525.04870	9032	683115.13700	2370808.37400
8474	667053.57720	2405195.91150	9033	683147.40140	2373450.87000
8475	667712.97550	2405185.70080	9034	683179.66580	2376093.36610
8476	693693.11430	2371985.01510	9035	683183.73900	2376398.02450
8477	677464.92380	2388253.62830	9036	683201.27420	2377709.57170
8478	676713.19180	2388959.66890	9037	683214.98520	2378735.09120
8479	673902.81430	2391599.22760	9038	683218.80940	2379021.11890
8480	673617.62800	2391867.07990	9039	683236.34520	2380332.71410
8481	667704.72110	2404198.74500	9040	683250.30470	2381376.81630
8482	691025.32820	2370363.05500	9041	683254.27120	2381644.30480
8483	684858.06430	2378712.23690	9042	683269.87200	2382696.36440
9000	693606.55680	2365380.50180	9043	683273.07640	2382912.45070
9001	688640.51900	2368023.12800	9044	683289.43940	2384015.91250
9002	693675.00540	2370665.09830	9045	683291.88150	2384180.59660
9003	693711.22320	2373304.93190	9046	683311.45000	2385500.22760
9004	693747.40400	2375944.75630	9047	683328.57410	2386655.00660

SWO 4879(1) COORDINATE POINT LIST Job Piece 21006(11)

PT NO.	EASTING	NORTHING	PT NO.	EASTING	NORTHING
9048	683330.66850	2386819.86520	9091	672744.04120	2386992.46880
9049	683347.51690	2388146.05120	9092	672770.74060	2389633.44700
9050	683364.36520	2389472.23710	9093	672797.43990	2392274.42520
9051	683381.21360	2390798.42310	9094	672841.15020	2394902.28230
9052	683398.06200	2392124.60900	9095	672884.86050	2397530.13930
9053	681867.89960	2376406.10030	9096	672915.09620	2400162.07270
9054	681884.14220	2377720.00410	9097	672945.41100	2402800.88600
9055	681900.38470	2379033.90790	9098	673003.28950	2405439.17550
9056	681916.62470	2380347.61370	9099	673014.30600	2408080.29000
9057	681932.86480	2381661.31960	9100	673105.85160	2387029.41490
9058	681935.73950	2382926.61690	9101	673132.57980	2389669.66250
9059	681938.65110	2384208.18100	9102	673159.30810	2392309.91020
9060	681941.64510	2385525.99820	9103	673199.22220	2394945.03950
9061	681944.63910	2386843.81550	9104	673239.22400	2397585.96500
9062	681959.38830	2388170.01040	9105	673279.47590	2400207.32070
9063	681974.13750	2389496.20530	9106	673319.92800	2402841.70900
9064	681988.82380	2390816.74730	9107	673348.03040	2405477.09470
9065	682003.51010	2392137.28930	9108	673376.16100	2408115.12940
9066	680548.10150	2376414.20030	9109	667467.66200	2387066.36100
9067	680579.78530	2379046.71790	9110	667494.27800	2389705.88000
9068	680611.45830	2381678.33440	9111	667521.25490	2392345.31210
9069	680614.65860	2382940.61100	9112	667561.54890	2394987.72800
9070	680617.94050	2384235.10240	9113	667601.84300	2397630.14380
9071	680624.61220	2386866.62530	9114	667647.68200	2400252.50300
9072	680654.82370	2389518.95090	9115	667693.71510	2402882.80400
9073	680684.78460	2392149.28020	9116	667715.72700	2405514.68600
9074	679289.40800	2381695.35750	9117	667737.95500	2408147.00400
9075	679295.28890	2382954.58690	9118	664886.57300	2392380.10900
9076	677906.88920	2376432.25450	9119	664925.71920	2395018.62480
9077	677936.92450	2379072.35400	9120	664964.85450	2397656.40190
9078	677967.35770	2381712.38060	9121	665009.13440	2400286.08180
9079	677975.91910	2382968.56280	9122	665053.47540	2402919.39350
9080	677984.91690	2384288.77410	9123	665078.51180	2405556.13670
9081	678002.91250	2386929.19380	9124	665103.46800	2408184.43800
9082	678039.45510	2389564.04120	9125	662246.11200	2392415.21800
9083	678065.08000	2392203.11160	9126	662281.70950	2395049.61750
9084	678105.40900	2394825.35600	9127	662327.86600	2397682.66000
9085	678146.57000	2397458.76700	9128	662371.50200	2400319.64900
9086	675373.4				

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5012	5047

DESCRIPTION	REVISIONS	DATE

CHECK LEVELS					BENCHMARK LIST		
SWO 4879(1)					NAVD 88 DATUM		
BM NO.	RUN 1	RUN 2	DIFF.	MEAN DIFF.	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
NGS G-149						870.540	870.540 PER NGS DATASHEET STANDARD BRASS CAP AT CHICAGO/ROCK ISLAND & PACIFIC RAILWAY STATION
TO	-4.941	-4.929	-0.012	-4.935			
BM1					865.603		CUT "X" ON CARWASH PAD - 44' RT. STA 58+20.86
TO	1.720	1.739	-0.019	1.730			
BM 2					867.331		CUT SQUARE SW COR. MAGNOLIA CREEK BRIDGE - 15' RT. STA 65+61.86
TO	-0.460	-0.472	0.012	-0.466			
BM 3					866.863		#6 BAR 30" LONG SET FLUSH - 64' LT. STA 72+36.69
TO	9.539	9.533	0.006	9.536			
BM 4					876.397		#6 BAR 30" LONG SET FLUSH - 117' LT. STA 79+12.03
TO	4.647	4.651	0.004	4.649			
7400					881.048		#4 BAR W/ ALUM CAP "LEMKE LAND SURVEYING" - 96' LT. STA 80+94.60
TO	12.721	12.720	0.001	-12.720			
BM 5					868.325		#6 BAR 30" LONG SET FLUSH - 78' RT. STA 85+85.46
TO	-7.393	-7.383	-0.010	-7.388			
BM 6					860.935		#6 BAR 30" LONG SET FLUSH - 66' RT. STA 93+22.86
TO	6.397	6.399	-0.002	6.398			
BM 7					867.331		#6 BAR 30" LONG SET FLUSH - 56' RT. STA 100+20.74
TO	1.492	1.490	0.002	1.491			

BM 8						868.820	#6 BAR 30" LONG SET FLUSH - 61' RT. STA 107+33.49
TO	-8.000	-7.994	-0.006	-7.997			
BM 9						860.821	CUT "X" SW WINGWALL OF CARTER CREEK BRIDGE - 17' RT. STA 114+53.79
TO	-8.123	-8.124	0.001	-8.123			
BM 10						852.696	#6 BAR 30" LONG SET FLUSH - 63' RT. STA 121+44.89
TO	3.822	3.816	0.006	3.819			
BM 11						856.513	CUT "X" NE WINGWALL OF BOX - 26' LT. STA 127+62.91
TO	6.163	6.170	-0.007	6.167			
BM 12						862.678	#6 BAR 30" LONG SET FLUSH - 60' LT. STA 134+35.61
TO	11.905	11.895	0.010	11.900			
BM 13						874.576	#6 BAR 30" LONG SET FLUSH - 26' LT. STA 141+32.95
TO	1.078	1.082	-0.004	1.080			
BM 14						875.654	#6 BAR 30" LONG SET FLUSH - 70' RT. STA 148+26.52
TO	-25.797	-25.804	0.007	-25.800			
BM 15						849.851	#6 BAR 30" LONG SET FLUSH - 66' LT. STA 155+33.64
TO	3.917	3.935	-0.018	3.926			
BM 16						853.775	#6 BAR 30" LONG SET FLUSH - 99' RT. STA 161+45.46
TO	0.728	0.729	0.001	0.728			
7405						853.047	#4 BAR W/ ALUM CAP "LEMKE LAND SURVEYING" - 42' RT. STA 162+16.34
TO	-4.055	-4.057	0.002	-4.056			

BM 17						848.991	CUT "X" CENTER OF HDWL - 31' LT. STA 167+95.81
TO	1.704	1.709	-0.005	1.706			
BM 18						850.696	#6 BAR 30" LONG SET FLUSH - 64' LT. STA 174+39.67
TO	-5.928	-5.933	0.005	-5.930			
BM 19						844.763	CUT "X" CENTER OF HDWL - 39' LT. STA 181+02.57
TO	-4.570	-4.568	-0.002	-4.569			
BM 20						840.192	#6 BAR 30" LONG SET FLUSH - 110' RT. STA 188+23.52
TO	3.110	3.108	0.002	3.109			
BM 21						843.303	#6 BAR 30" LONG SET FLUSH - 92' RT. STA 195+15.74
TO	-2.374	-2.378	0.004	-2.376			
BM 22						840.927	#6 BAR 30" LONG SET FLUSH - 65' LT. STA 201+18.26
TO	4.474	4.471	0.001	4.473			
BM 23						845.398	CUT "X" CENTER OF HDWL - 30' RT. STA 209+18.88
TO	5.847	5.838	0.009	5.842			
BM 24						851.238	CUT "X" ON CURB - 74' RT. STA 216+35.12
TO	15.717	15.733	-0.016	15.725			
BM 25						866.962	#6 BAR 30" LONG SET FLUSH - 64' RT. STA 223+69.51
TO	4.636	4.643	-0.007	4.639			
BM 26						871.599	#6 BAR 30" LONG SET FLUSH - 63' LT. STA 230+74.87
TO	4.949	4.953	-0.004	4.951			

CHECK LEVELS					BENCHMARK LIST		
SWO 4879(1)					NAVD 88 DATUM		
BM NO.	RUN 1	RUN 2	DIFF.	MEAN DIFF.	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
NGS G-149						870.540	870.540 PER NGS DATASHEET STANDARD BRASS CAP AT CHICAGO/ROCK ISLAND & PACIFIC RAILWAY STATION
TO	-4.941	-4.929	-0.012	-4.935			
BM1					865.603		CUT "X" ON CARWASH PAD - 44' RT. STA 58+20.86
TO	1.720	1.739	-0.019	1.730			
BM 2					867.331		CUT SQUARE SW COR. MAGNOLIA CREEK BRIDGE - 15' RT. STA 65+61.86
TO	-0.460	-0.472	0.012	-0.466			
BM 3					866.863		#6 BAR 30" LONG SET FLUSH - 64' LT. STA 72+36.69
TO	9.539	9.533	0.006	9.536			
BM 4					876.397		#6 BAR 30" LONG SET FLUSH - 117' LT. STA 79+12.03
TO	4.647	4.651	0.004	4.649			
7400					881.048		#4 BAR W/ ALUM CAP "LEMKE LAND SURVEYING" - 96' LT. STA 80+94.60
TO	12.721	12.720	0.001	-12.720			
BM 5					868.325		#6 BAR 30" LONG SET FLUSH - 78' RT. STA 85+85.46
TO	-7.393	-7.383	-0.010	-7.388			
BM 6					860.935		#6 BAR 30" LONG SET FLUSH - 66' RT. STA 93+22.86
TO	6.397	6.399	-0.002	6.398			
BM 7					867.331		#6 BAR 30" LONG SET FLUSH - 56' RT. STA 100+20.74
TO	1.492	1.490	0.002	1.491			

March 31st, 2015

SDS 12 OF 47

PLS	SS		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,RR		
SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5012			SURVEY DATA SHEET

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5013	5047

DESCRIPTION	REVISIONS	DATE

BM 36				906.622	CUT "X" ON HDWL- 46' LT. STA 301+70.12
TO	9.302	9.289	0.013	9.296	
BM 37				915.915	#6 BAR 30" LONG SET FLUSH - 62' RT. STA 309+41.15
TO	-3.690	-3.672	0.018	3.681	
7402				912.237	#4 BAR W/ ALUM CAP "LEMKE LAND SURVEYING" - 66' LT. STA 310+65.94
TO	-23.616	-23.615	0.001	-23.616	
BM 38				888.619	CUT "X" ON HDWL- 28' RT. STA 316+64.78
TO	-32.595	-32.592	-0.003	-32.594	
BM 39				856.024	CUT "X" ON HDWL- 29' RT. STA 324+22.00
TO	-0.595	-0.597	0.002	-0.596	
BM 40				855.426	#6 BAR 30" LONG SET FLUSH - 69' LT. STA 331+31.57
TO	-11.817	-11.811	-0.006	-11.814	
BM 41				843.610	CUT "X" ON HDWL- 34' LT. STA 338+74.51
TO	-14.006	-14.009	0.003	-14.008	
BM 42				829.601	#6 BAR 30" LONG SET FLUSH - 60' RT. STA 346+06.39
TO	1.403	1.406	-0.003	1.404	
BM 43				831.003	#6 BAR 30" LONG SET FLUSH - 67' LT. STA 352+67.33
TO	-2.526	-2.538	0.012	-2.532	
BM 44				828.469	CUT "X" ON HDWL- 35' LT. STA 359+85.51
TO	18.363	18.352	0.011	18.358	

BM 45				846.825	CUT "X" ON WINGWALL- 16' RT. STA 369+28.29
TO	-5.396	-5.407	0.011	-5.401	
BM 46				841.421	CUT "X" ON WINGWALL- 16' RT. STA 375+18.34
TO	-11.748	-11.743	-0.005	-11.746	
BM 47				829.674	CUT "X" ON ROCK OUTCROP- 104' RT. STA 381+83.04
TO	19.405	19.400	0.005	19.403	
BM 48				849.074	CUT "X" ON ROCK OUTCROP- 108' RT. STA 388+56.90
TO	33.029	33.019	0.010	33.024	
BM 49				882.096	CUT "X" ON HDWL- 23' RT. STA 395+97.18
TO	20.822	20.808	0.014	20.815	
BM 49A				902.910	#6 BAR 30" LONG SET FLUSH - 93' RT. STA 402+93.77
TO	2.089	2.101	0.012	2.095	
7403				905.006	#4 BAR W/ ALUM CAP "LEMKE LAND SURVEYING" - 46' LT. STA 403+35.88
TO	-0.436	-0.420	0.016	-0.428	
BM 50				904.576	#6 BAR 30" LONG SET FLUSH - 65' RT. STA 410+33.78
TO	-1.060	-1.055	-0.005	-1.058	
BM 51				903.516	CUT "X" ON HDWL- 23' RT. STA 417+04.57
TO	17.132	17.121	0.011	17.127	
BM 52				920.641	#6 BAR 30" LONG SET FLUSH - 69' LT. STA 424+13.51
TO	-11.297	-11.295	-0.002	-11.296	

CHECK LEVELS					BENCHMARK LIST		NAVD 88 DATUM	
SWO 4879(1)	BM NO.	RUN 1	RUN 2	DIFF.	MEAN DIFF.	ADJ. ELEV.	PUBLISHED ELEV.	BM DESCRIPTION
	NGS G-149						870.540	870.540 PER NGS DATASHEET STANDARD BRASS CAP AT CHICAGO/ROCK ISLAND & PACIFIC RAILWAY STATION
	TO	-4.941	-4.929	-0.012	-4.935			
	BM1					865.603		CUT "X" ON CARWASH PAD - 44' RT. STA 58+20.86
	TO	1.720	1.739	-0.019	1.730			
	BM 2					867.331		CUT SQUARE SW COR. MAGNOLIA CREEK BRIDGE - 15' RT. STA 65+61.86
	TO	-0.460	-0.472	0.012	-0.466			
	BM 3					866.863		#6 BAR 30" LONG SET FLUSH - 64' LT. STA 72+36.69
	TO	9.539	9.533	0.006	9.536			
	BM 4					876.397		#6 BAR 30" LONG SET FLUSH - 117' LT. STA 79+12.03
	TO	4.647	4.651	0.004	4.649			
	7400					881.048		#4 BAR W/ ALUM CAP "LEMKE LAND SURVEYING" - 96' LT. STA 80+94.60
	TO	12.721	12.720	0.001	-12.720			
	BM 5					868.325		#6 BAR 30" LONG SET FLUSH - 78' RT. STA 85+85.46
	TO	-7.393	-7.383	-0.010	-7.388			
	BM 6					860.935		#6 BAR 30" LONG SET FLUSH - 66' RT. STA 93+22.86
	TO	6.397	6.399	-0.002	6.398			
	BM 7					867.331		#6 BAR 30" LONG SET FLUSH - 56' RT. STA 100+20.74
	TO	1.492	1.490	0.002	1.491			

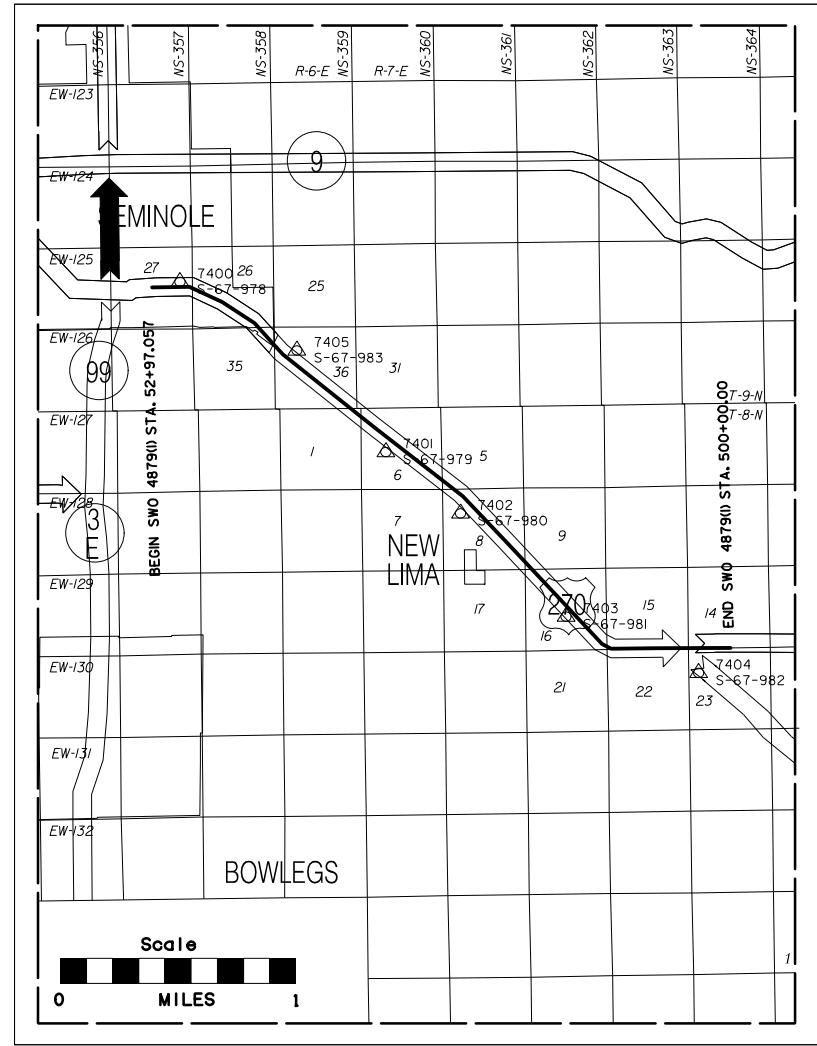
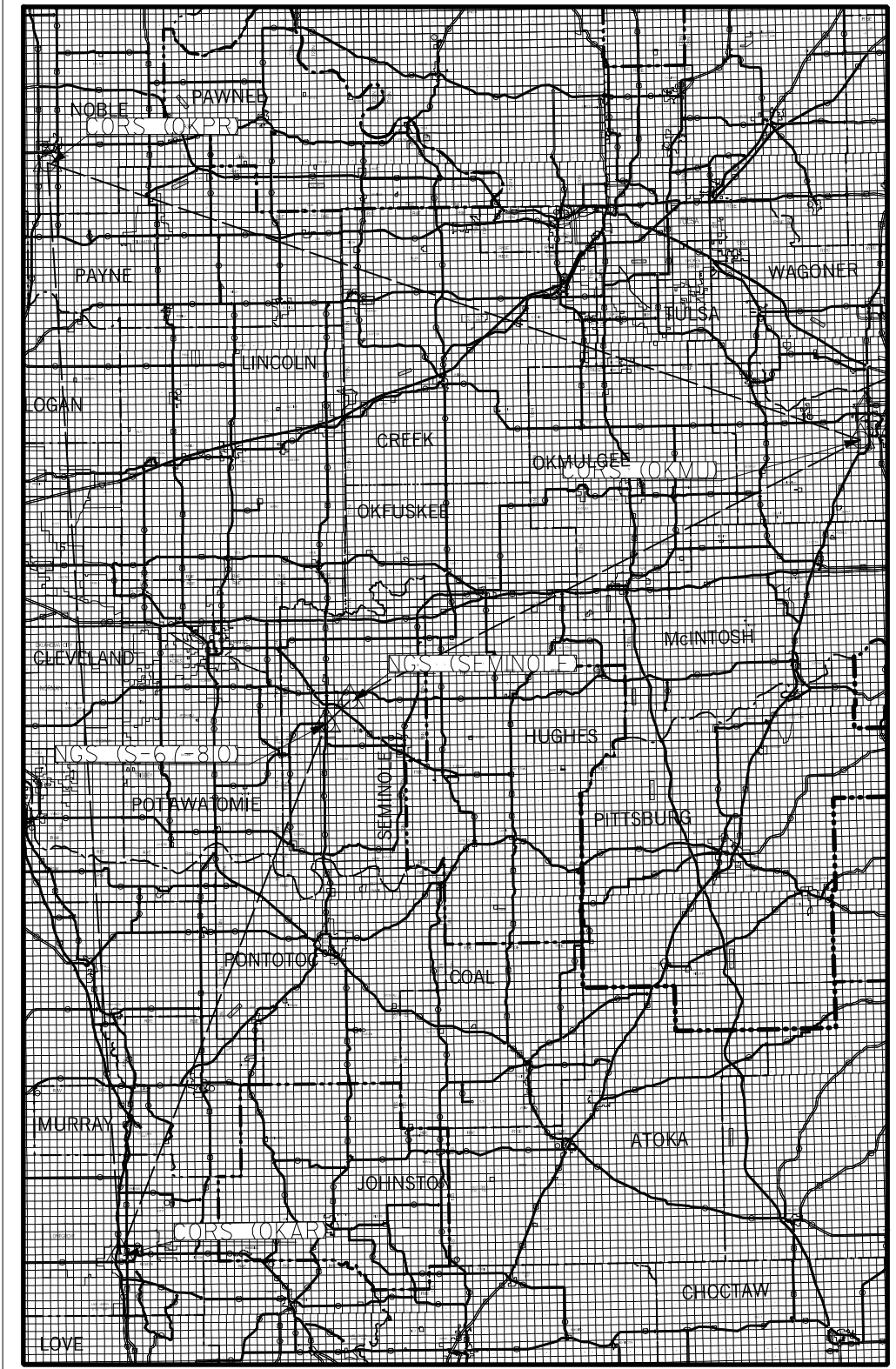
NGS U149				790.716	790.680	790.68 PER NGS DATASHEET STANDARD BRASS CAP AT FILTER WORKS PLANT IN NW COR OF WATER TANK BASE
TO	22.698	22.694	0.004	22.696		
NGS T149				813.410	813.410	813.41 PER NGS DATASHEET STANDARD BRASS CAP IN THE FACE OF THE WEST WALL OF COURTHOUSE
BM 62				830.459		CUT "X" ON HDWL- 21' RT. STA 489+16.93
TO	7.741	7.729	0.012	-7.735		
7404				838.194		#4 BAR W/ ALUM CAP "LEMKE LAND SURVEYING" - 86' RT. STA 490+10.89
TO	-1.345	-1.334	0.011	1.339		
BM 63				836.855		#6 BAR 30" LONG SET FLUSH - 93' RT. STA 496+32.30
TO	13.579	13.587	0.008	13.583		
BM 64				850.438		#6 BAR 30" LONG SET FLUSH - OFF ALIGNMENT

March 31st, 2015

PLS	SS		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,BB	SWO 4879 (1)	PROJECT NO. 21006 (07) SHEET NO. 5013

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5014	5047

DESCRIPTION	REVISIONS	DATE



Page 1 of 9

Project Information		Coordinate System	
Name:	H:\14018\115-Data\Final\Control\140423 Precise Egh Network.vce	Name:	US State Plane 1983 (2011)
Size:	1 MB	Datum:	NAD 1983 (2011)
Modified:	4/28/2014 9:28:48 AM (UTC-5)	Zone:	Oklahoma South 3502
Time zone:	Central Standard Time	Grid:	GEOD12A (Conus)
Reference number:		Vertical datum:	
Description:			

Network Adjustment Report

Adjustment Settings

Set-Up Errors
 GNSS
 Error in Height of Antenna: 0.005 ft
 Centering Error: 0.005 ft

GNSS Weighting
 Fixed Standard Errors
 Horizontal: 0.008 ft + 0.200 ppm
 Vertical: 0.015 ft + 1.000 ppm

Covariance Display
 Horizontal:
 Propagated Linear Error [E]: U.S.
 Constant Term [C]: 0.000 ft
 Scale on Linear Error [S]: 1.950
 Three-Dimensional
 Propagated Linear Error [E]: U.S.
 Constant Term [C]: 0.000 ft
 Scale on Linear Error [S]: 1.950

Adjustment Statistics

Number of Iterations for Successful Adjustment: 2
 Network Reference Factor: 0.99
 Chi Square Test (95%): Passed
 Precision Confidence Level: 95%
 Degrees of Freedom: 164

Post Processed Vector Statistics
 Reference Factor: 0.99
 Redundancy Number: 164.00
 A Priori Scale: 1.00

Control Coordinate Comparisons

Values shown are control coordinates minus adjusted coordinates

Point ID	Δ Northing (US survey feet)	Δ Easting (US survey feet)	Δ Elevation (US survey feet)	Δ Height (US survey feet)
100	1.490	1.332		9.456
101	-2.880	0.509		-2.032
102	7.800	0.430		4.523
103	2.709	0.342		2.224
110	-0.143	-0.108	0.471	2.085
120	4.285	4.054		-1.327

Control Point Constraints

4/28/2014

March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5014

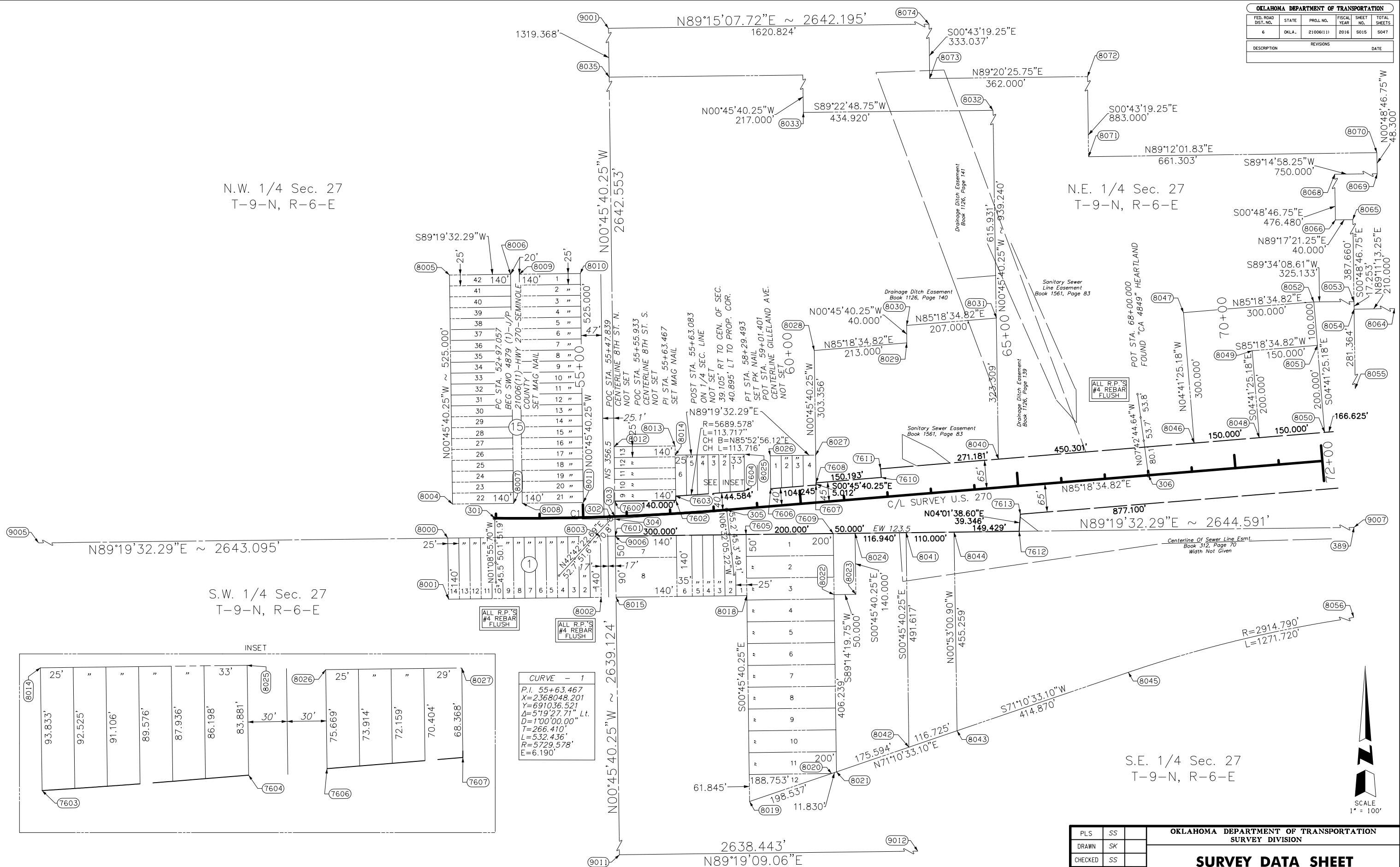
OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5015	5047
DESCRIPTION		REVISIONS		DATE	

N.W. 1/4 Sec. 27
T-9-N, R-6-E

N.E. 1/4 Sec. 27
T-9-N, R-6-E

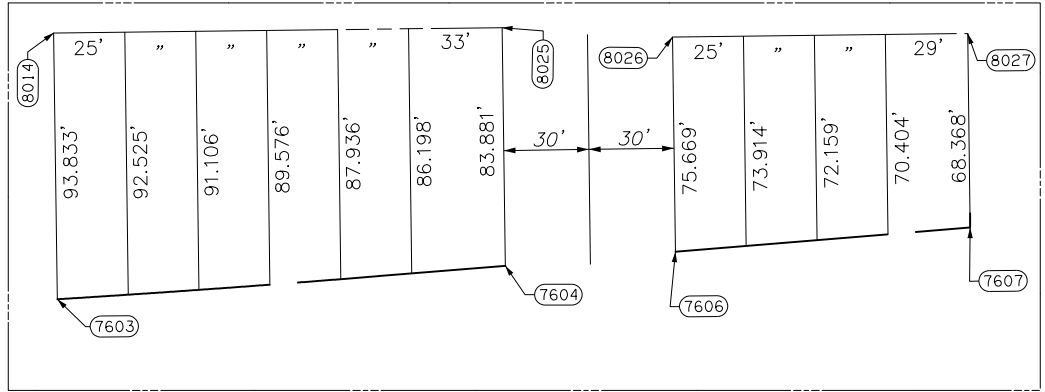
S.W. 1/4 Sec. 27
T-9-N, R-6-E

S.E. 1/4 Sec. 27
T-9-N, R-6-E

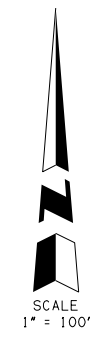


CURVE - 1

P.I.	55+63.467
X	=2368048.201
Y	=691036.521
Δ	=5'19"27.71" Lt.
D	=1'00"00.00"
T	=266.410'
L	=532.436'
R	=5729.578'
E	=6.190'



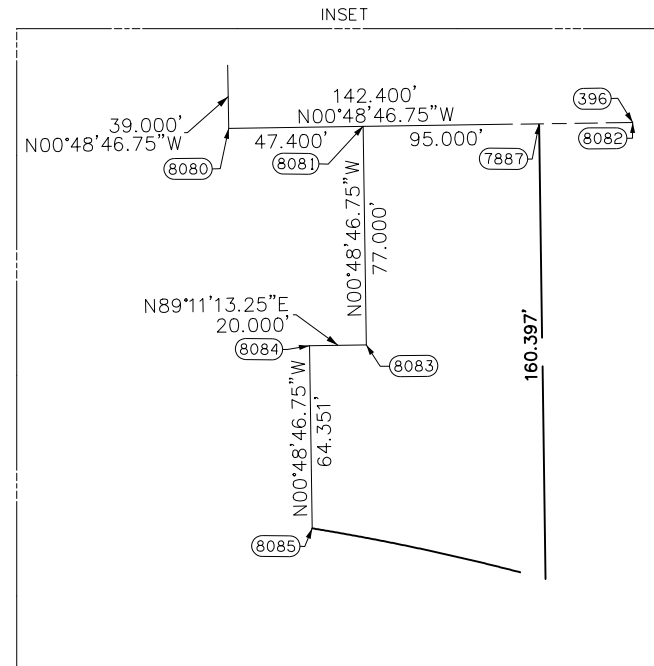
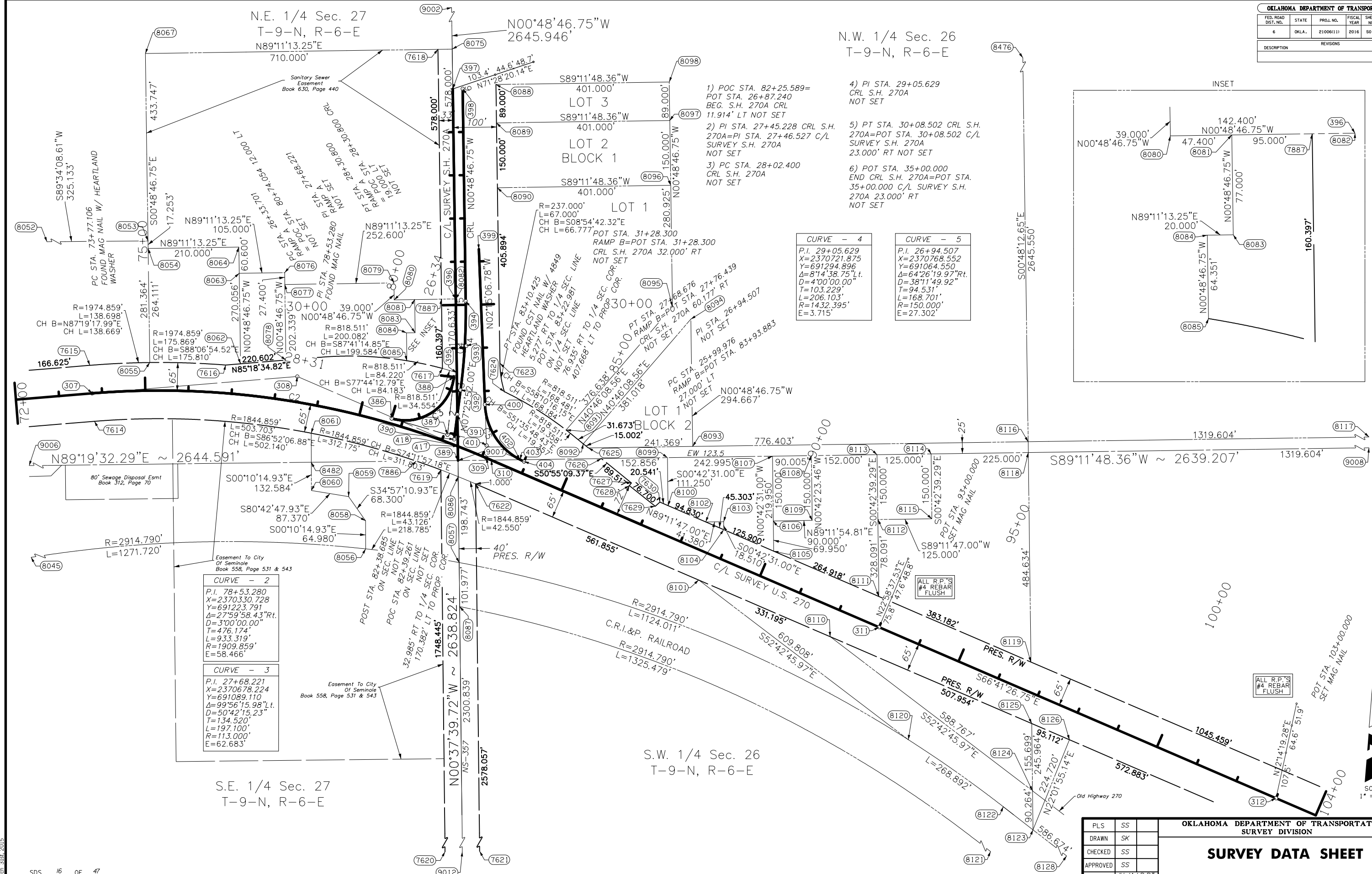
OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	SS		
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,RR	SWO 4879 (1)	PROJECT NO. 21006 (07) SHEET NO. 5015



March 31st, 2015

SDS 15 OF 47

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5016	5047
DESCRIPTION			REVISIONS	DATE	



CURVE - 4	
P.I.	29+05.629
X	2370721.875
Y	691294.896
Δ	81°38'37.75" Lt.
D	400'00.00"
T	103.229'
L	206.103'
R	1432.395'
E	3.715'

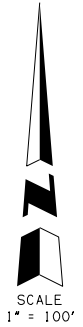
CURVE - 5	
P.I.	26+94.507
X	2370768.552
Y	691064.550
Δ	64°26'19.97" Rt.
D	38'11.49.92"
T	94.531'
L	168.701'
R	150.000'
E	27.302'

CURVE - 2	
P.I.	78+53.280
X	2370330.728
Y	691223.791
Δ	27°59'58.43" Rt.
D	300'00.00"
T	476.174'
L	933.319'
R	1909.859'
E	58.466'

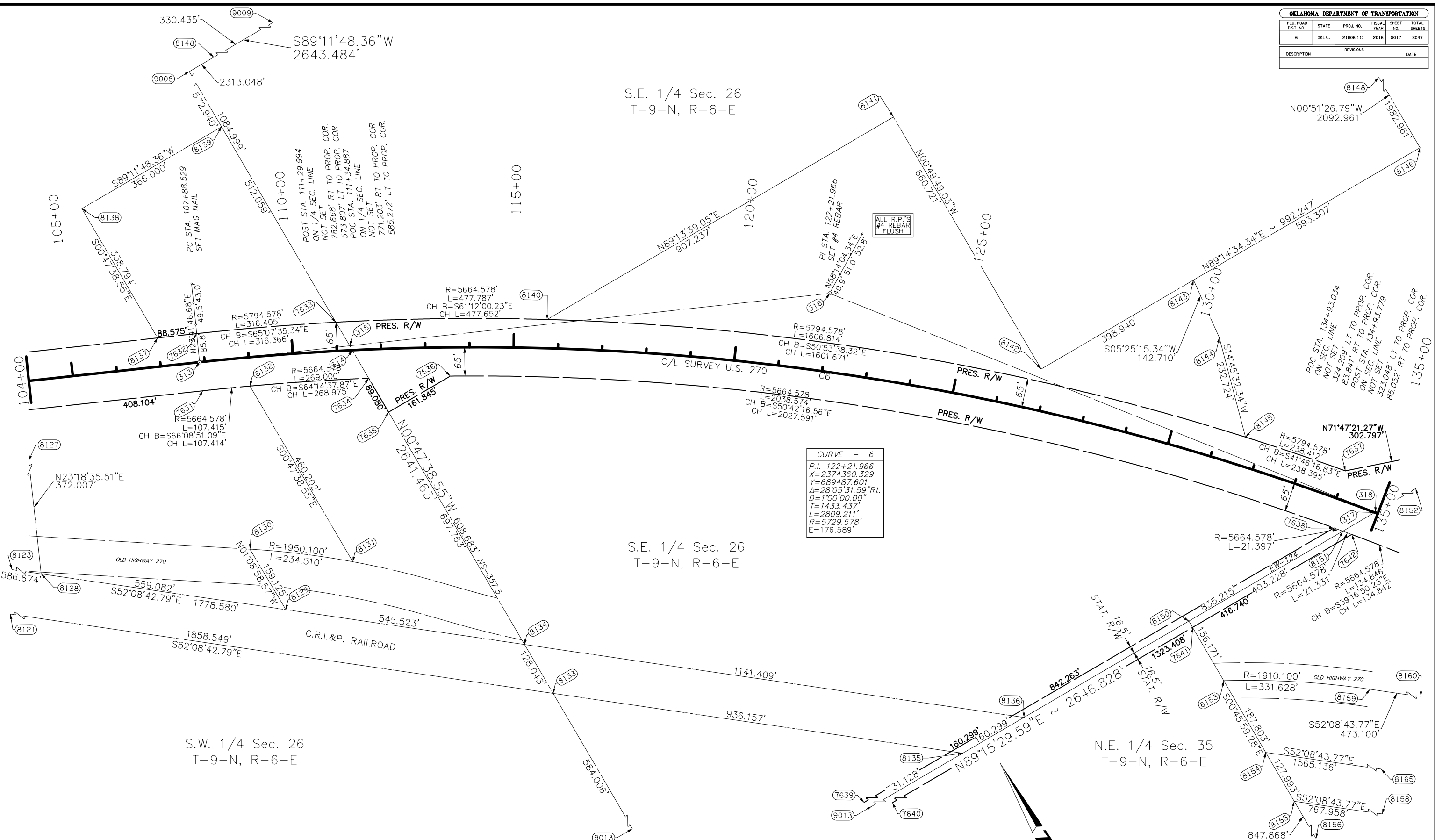
CURVE - 3	
P.I.	27+68.221
X	2370678.224
Y	691089.110
Δ	99°56'15.98" Lt.
D	50'42'15.23"
T	134.520'
L	197.100'
R	113.000'
E	62.683'

PLS	SS	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
DRAWN	SK	<p align="center">SURVEY DATA SHEET</p> <p>CREW: JH,JA,LB,RT,BS,PT,RR SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5016</p>	
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT,BS,PT,RR		

March, 31st, 2015



OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5017	5047
DESCRIPTION		REVISIONS		DATE	



CURVE - 6	
P.I.	122+21.966
X	2374.360.329
Y	689487.601
Δ	28°05'31.59" Rt.
D	1°00'00.00"
T	1433.437'
L	2809.211'
R	5729.578'
E	176.589'

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	SS		
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,RR	SWO 4879 (1)	PROJECT NO. 21006 (07) SHEET NO. 5017

March 31st, 2015

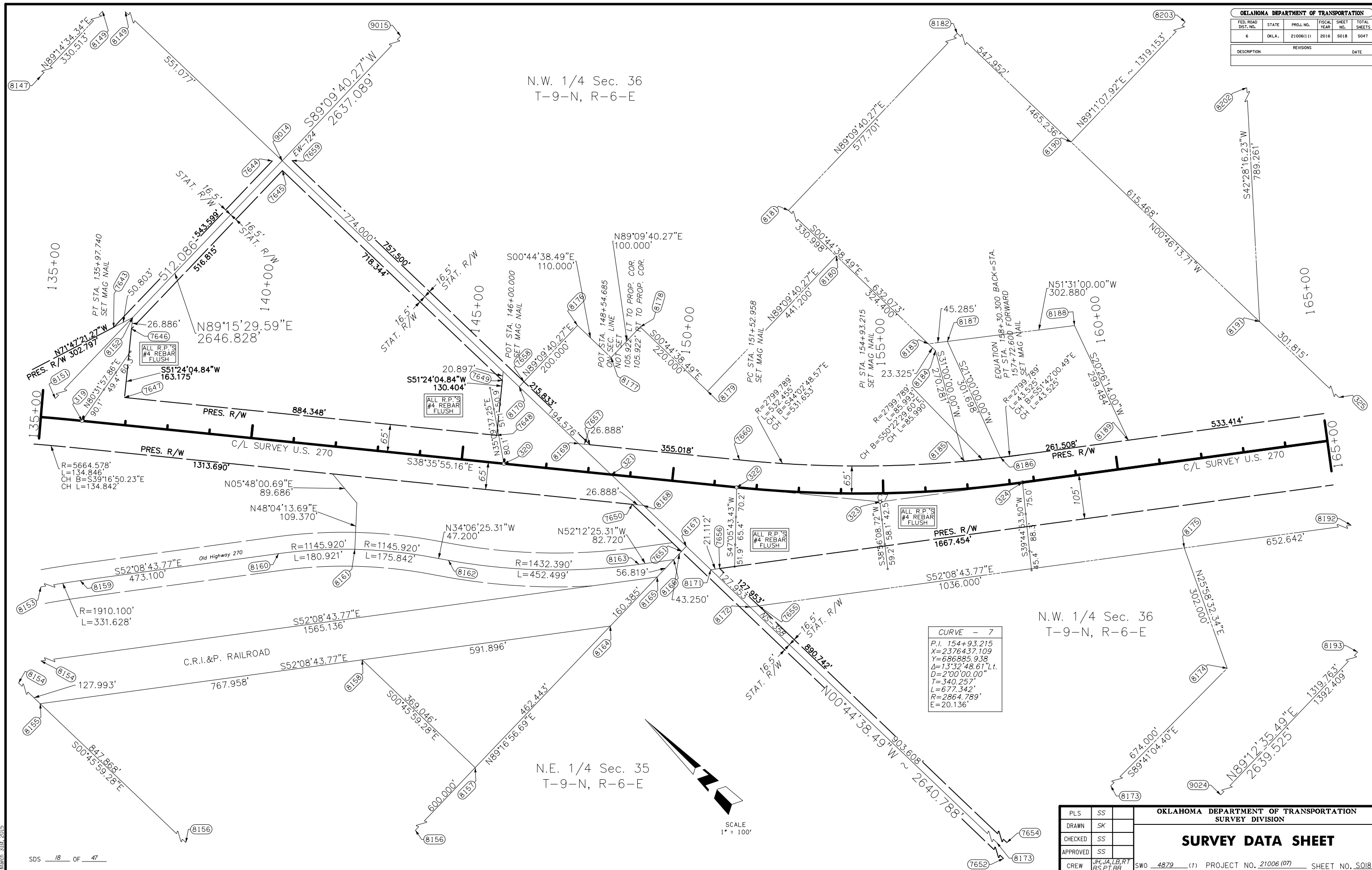
SDS 17 OF 47

SCALE 1" = 100'

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5018	5047
DESCRIPTION		REVISIONS		DATE	

N.W. 1/4 Sec. 36
T-9-N, R-6-E

N.E. 1/4 Sec. 35
T-9-N, R-6-E



CURVE - 7
 P.I. 154+93.215
 X=2376437.109
 Y=686885.938
 Δ=13°32'48.61" Lt.
 D=2'00"00.00"
 T=340.257'
 L=677.342'
 R=2864.789'
 E=20.136'

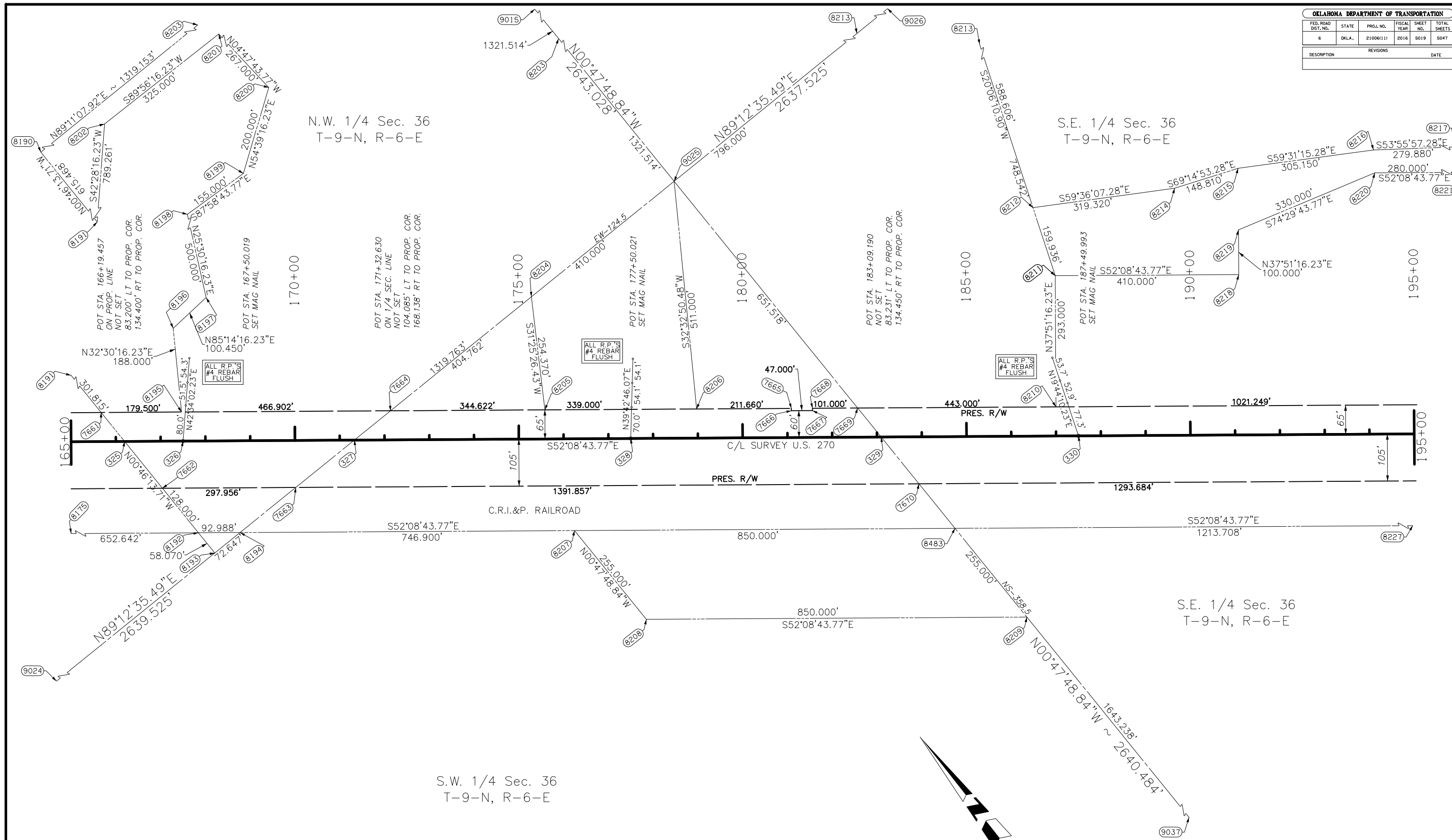
OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION					
SURVEY DATA SHEET					
PLS	SS				
DRAWN	SK				
CHECKED	SS				
APPROVED	SS				
CREW	JH,JA,LB,RT BS,PT,BB	SWO 4879 (1)	PROJECT NO. 21006 (07)	SHEET NO. 5018	

March 31st, 2015

SDS 18 OF 47

SCALE
1" = 100'

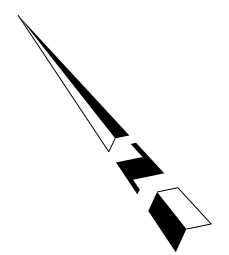
OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5019	5047
DESCRIPTION		REVISIONS		DATE	



N.W. 1/4 Sec. 36
T-9-N, R-6-E

S.E. 1/4 Sec. 36
T-9-N, R-6-E

S.W. 1/4 Sec. 36
T-9-N, R-6-E



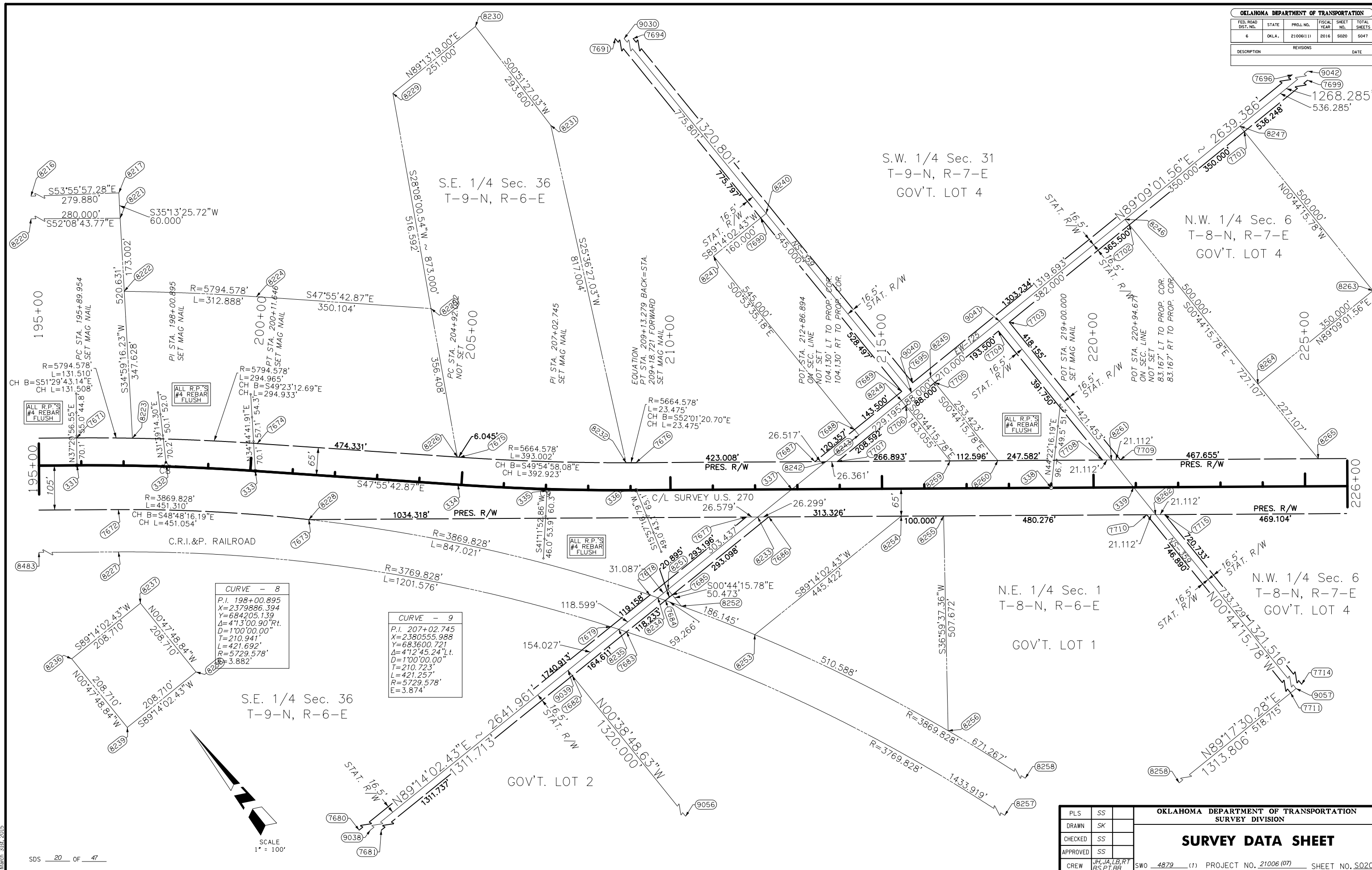
OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
SURVEY DATA SHEET	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH, JA, LB, RT BS, PT, RB

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5019

March 31st, 2015

SDS 19 OF 47

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5020	5047
DESCRIPTION		REVISIONS		DATE	



March 31st, 2015

SDS 20 OF 47

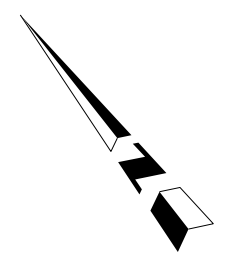
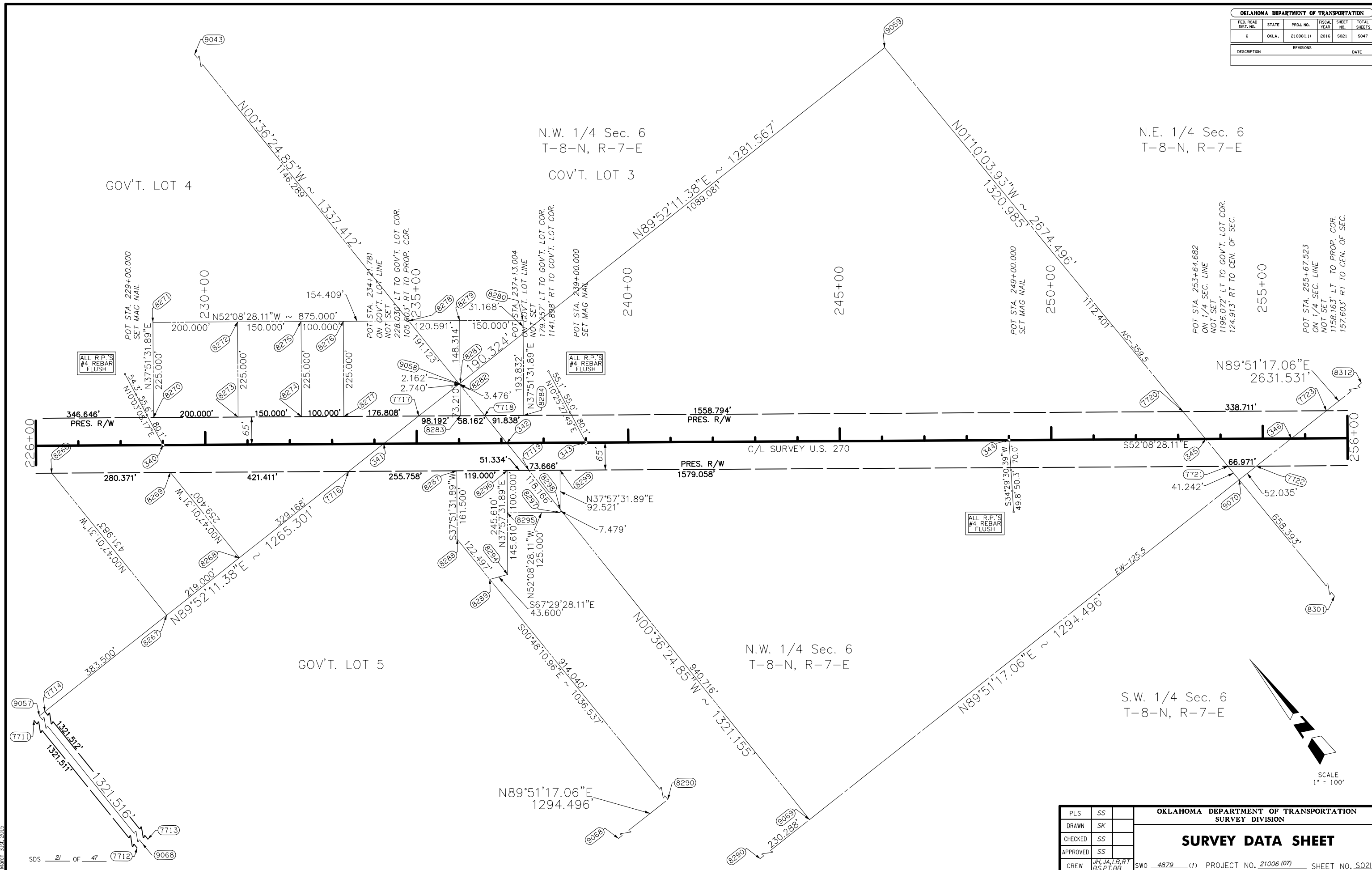
SCALE 1" = 100'

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH, JA, LB, RT, BS, PT, BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5020

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5021	5047
DESCRIPTION		REVISIONS		DATE	



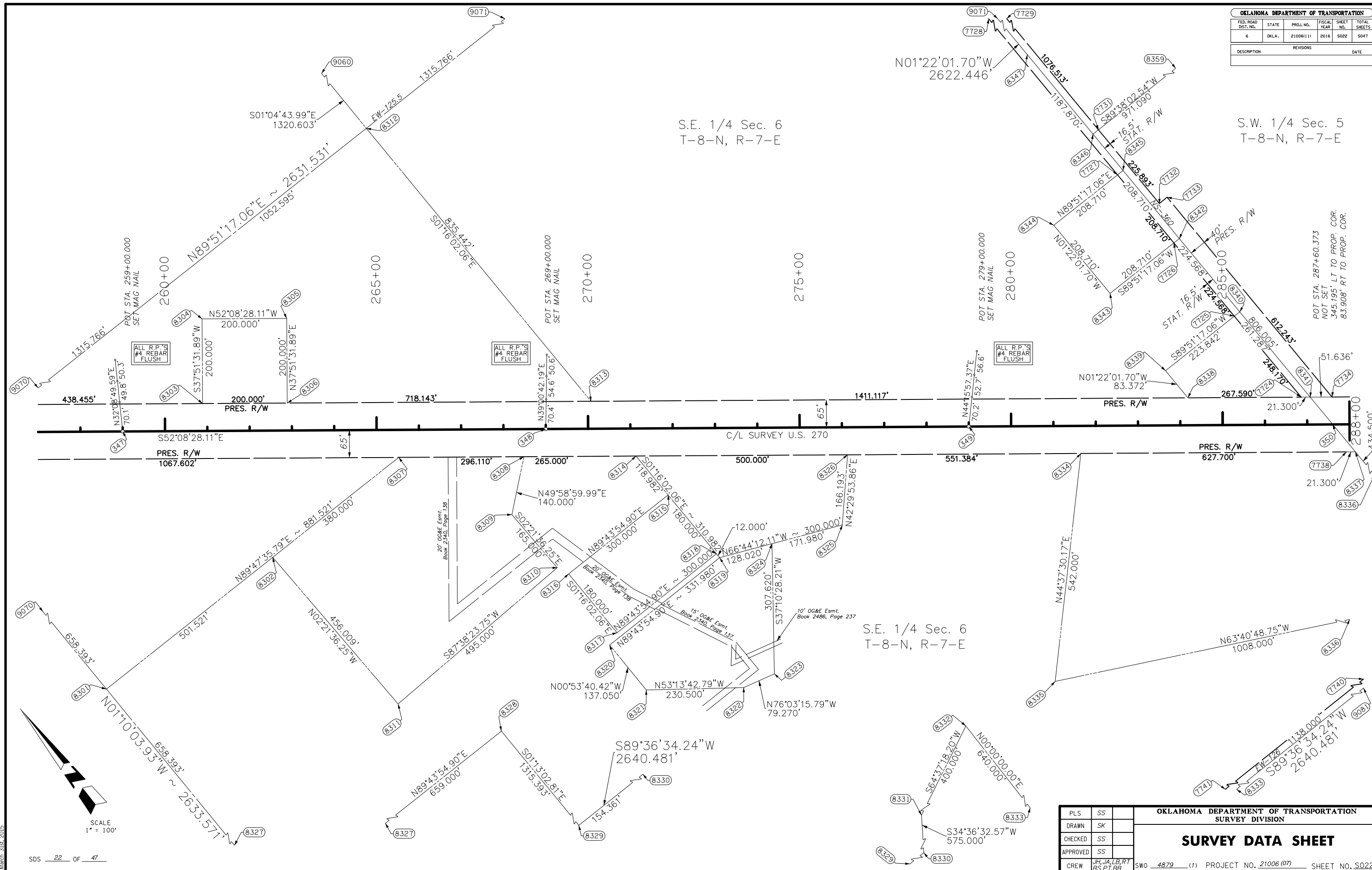
SCALE
1" = 100'

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
SURVEY DATA SHEET	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH, JA, LB, RT, BS, PT, BB

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5021

March 31st, 2015
SDS 21 OF 47

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5022	5047
DESCRIPTION		REVISIONS		DATE	



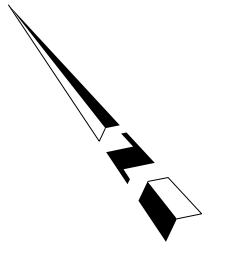
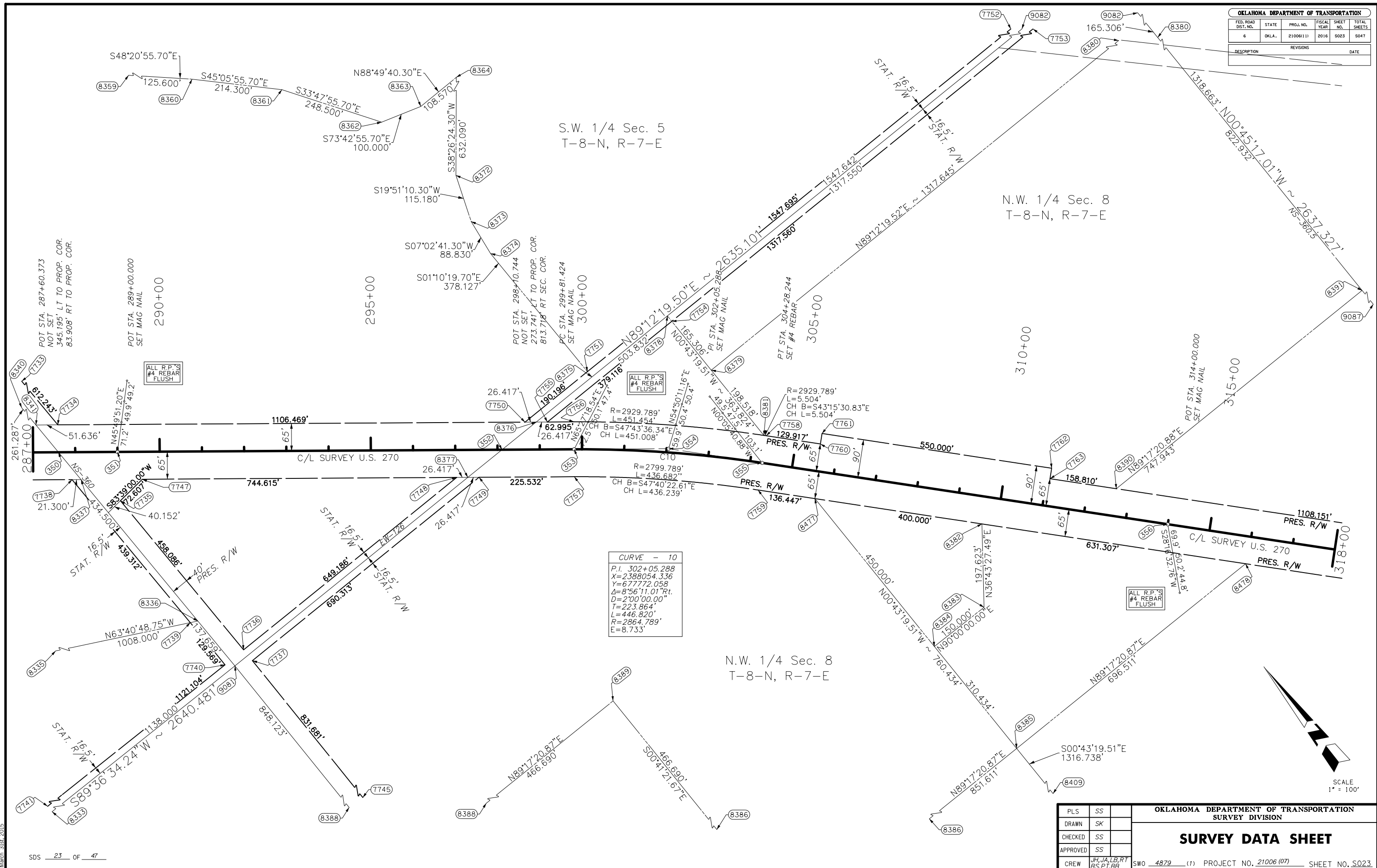
March 31st, 2015

SCALE
1" = 100'

SDS 22 OF 47

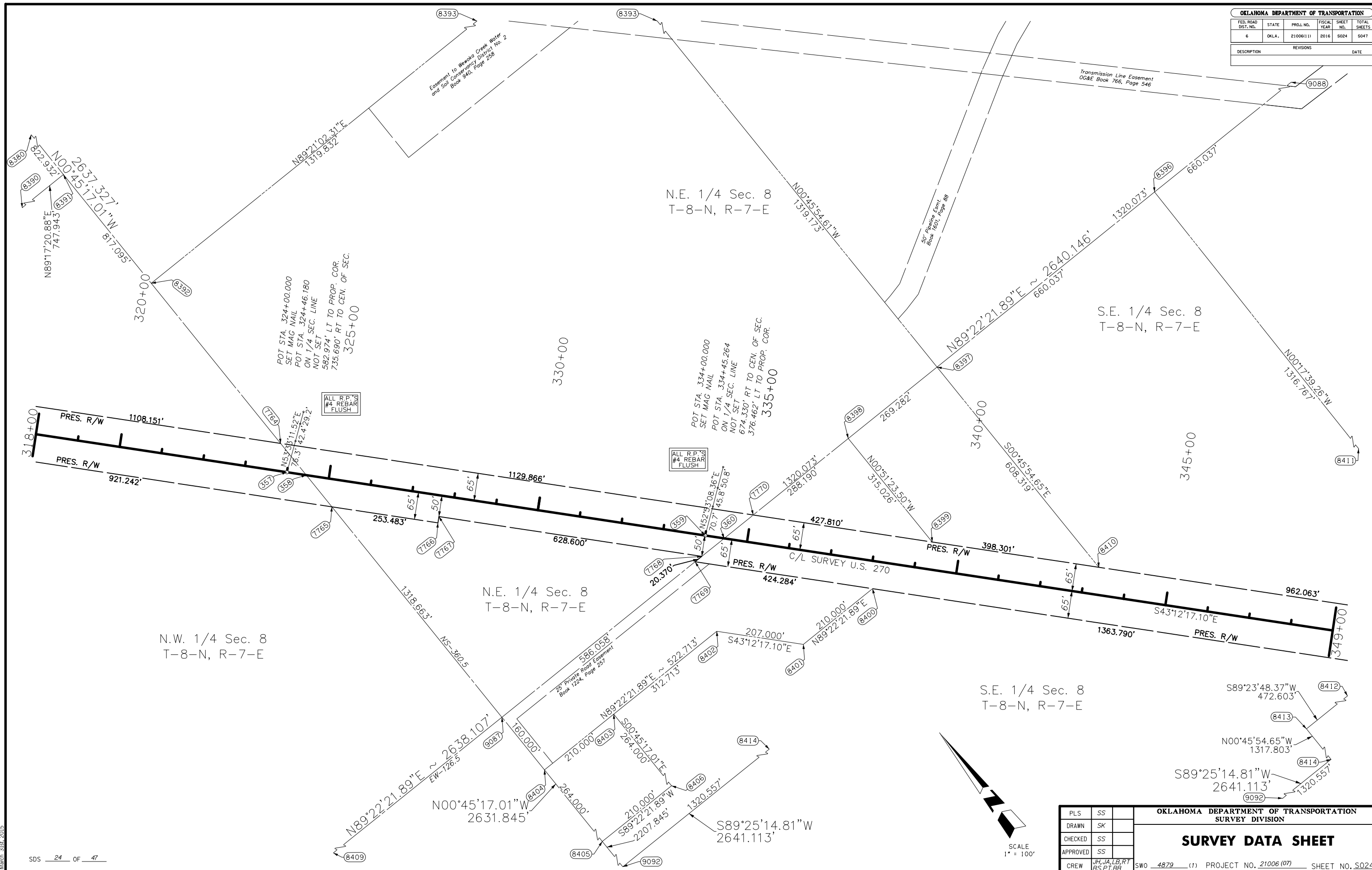
OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	SS		
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,RR	SWO 4879 (1)	PROJECT NO. 21006 (07) SHEET NO. S022

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5023	5047
DESCRIPTION		REVISIONS		DATE	



OKLAHOMA DEPARTMENT OF TRANSPORTATION					
SURVEY DIVISION					
SURVEY DATA SHEET					
PLS	SS				
DRAWN	SK				
CHECKED	SS				
APPROVED	SS				
CREW	JH, JA, LB, RT BS, PT, RB	SWO 4879 (1)	PROJECT NO. 21006 (07)	SHEET NO. S023	

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5024	5047
DESCRIPTION			REVISIONS	DATE	



March 31st, 2015

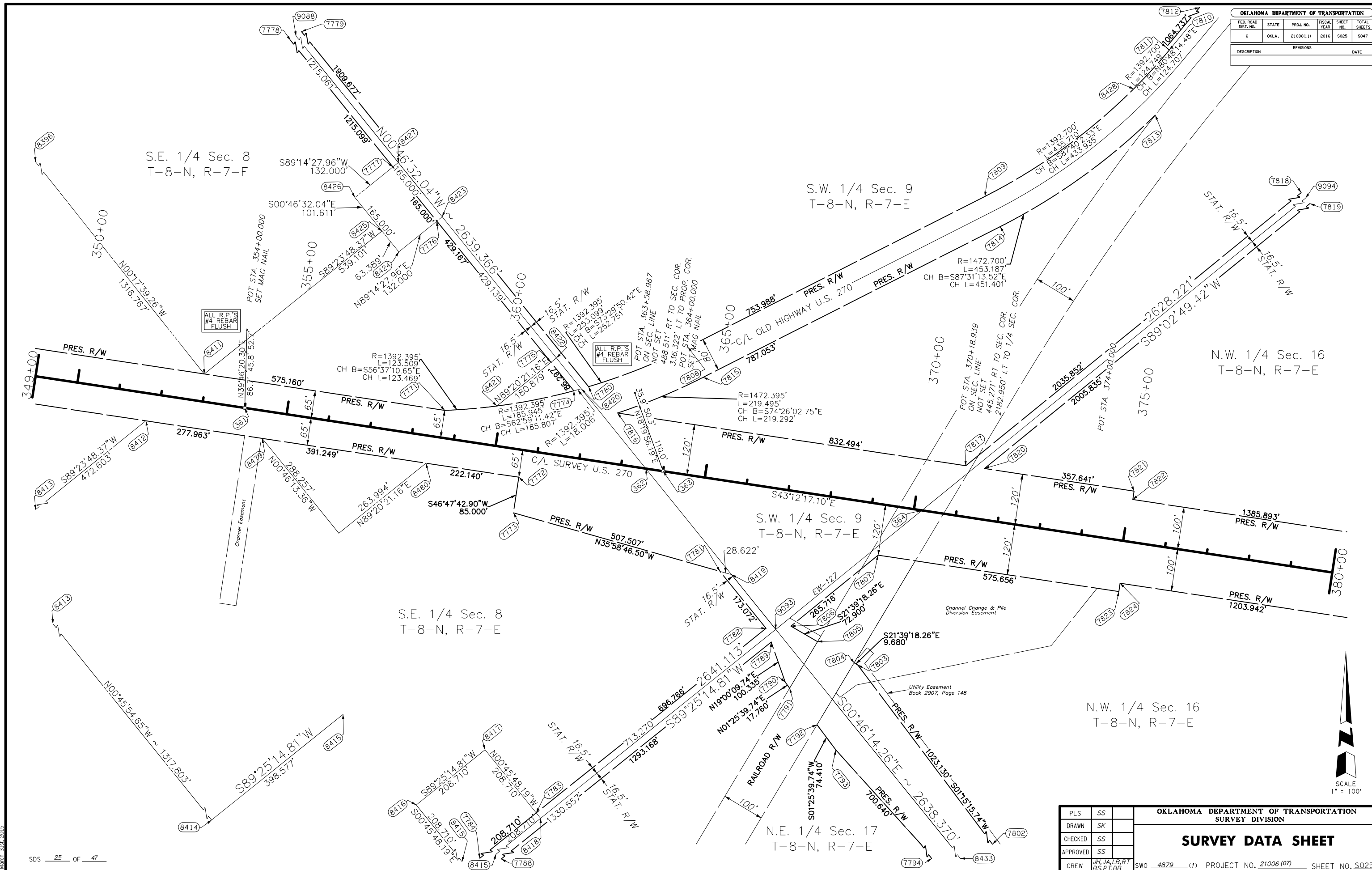
SDS 24 OF 47

SCALE
1" = 100'

PLS	SS	
DRAWN	SK	
CHECKED	SS	
APPROVED	SS	
CREW	JH,JA,LB,RT BS,PT,RR	

OKLAHOMA DEPARTMENT OF TRANSPORTATION
SURVEY DIVISION
SURVEY DATA SHEET
SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5024

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5025	5047
DESCRIPTION		REVISIONS		DATE	



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION					
SURVEY DATA SHEET					
PLS	SS				
DRAWN	SK				
CHECKED	SS				
APPROVED	SS				
CREW	JH,JA,LB,RT BS,PT,BB	SWO 4879 (1)	PROJECT NO. 21006 (07)	SHEET NO. 5025	



March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5026	5047
DESCRIPTION		REVISIONS		DATE	

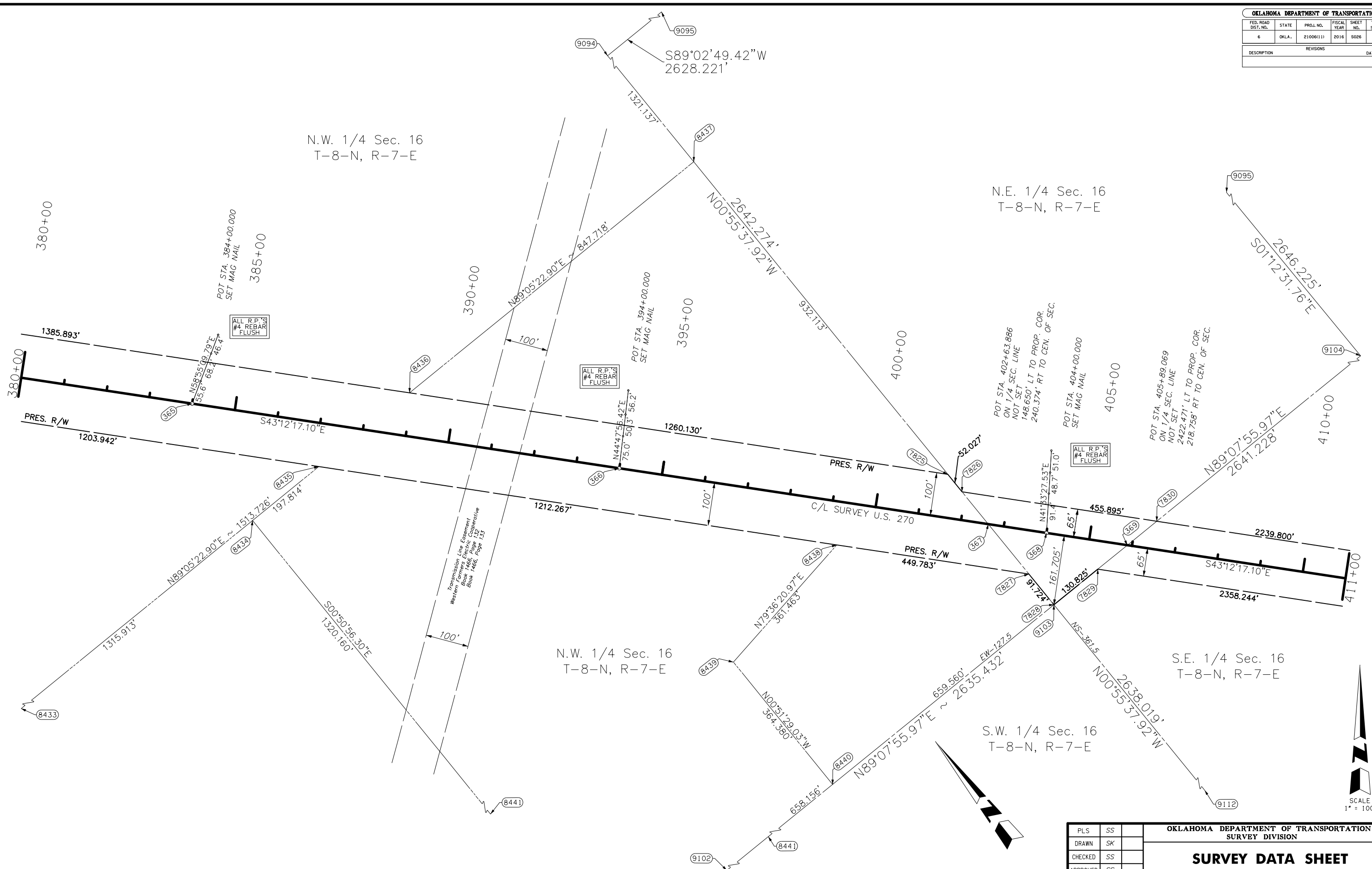
N.W. 1/4 Sec. 16
T-8-N, R-7-E

N.E. 1/4 Sec. 16
T-8-N, R-7-E

N.W. 1/4 Sec. 16
T-8-N, R-7-E

S.E. 1/4 Sec. 16
T-8-N, R-7-E

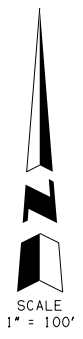
S.W. 1/4 Sec. 16
T-8-N, R-7-E



Transmission Line Equipment
Western Farmers Electric Cooperative
Book 1465, Page 152
Book 1466, Page 133

POT STA. 402+63.886
ON 1/4 SEC. LINE
NOT SET
148.650' LT TO PROP. COR.
240.374' RT TO CEN. OF SEC.

POT STA. 405+89.069
ON 1/4 SEC. LINE
NOT SET
242.471' LT TO PROP. COR.
218.758' RT TO CEN. OF SEC.

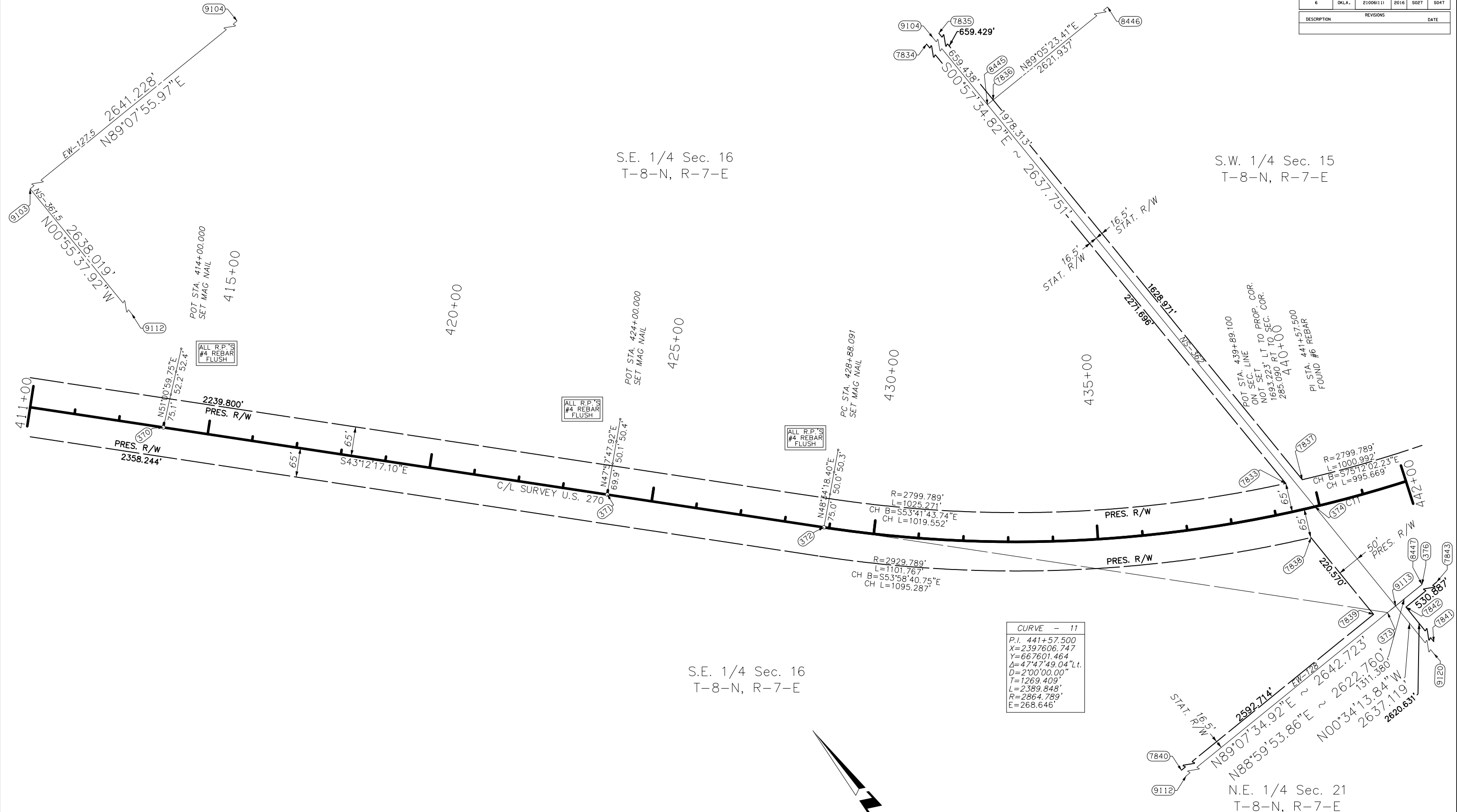


OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
SURVEY DATA SHEET	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,RR

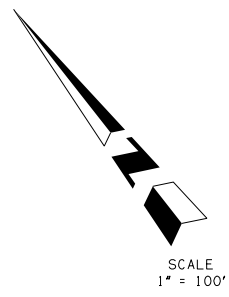
SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. S026

March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5027	5047
DESCRIPTION		REVISIONS		DATE	



CURVE - 11
 P.I. 441+57.500
 X=2397606.747
 Y=667601.464
 Δ=47°47'49.04" Lt.
 D=2'00'00.00"
 T=1269.409'
 L=2389.848'
 R=2864.789'
 E=268.646'



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH, JA, LB, RT, BS, PT, BB

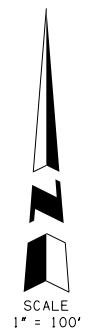
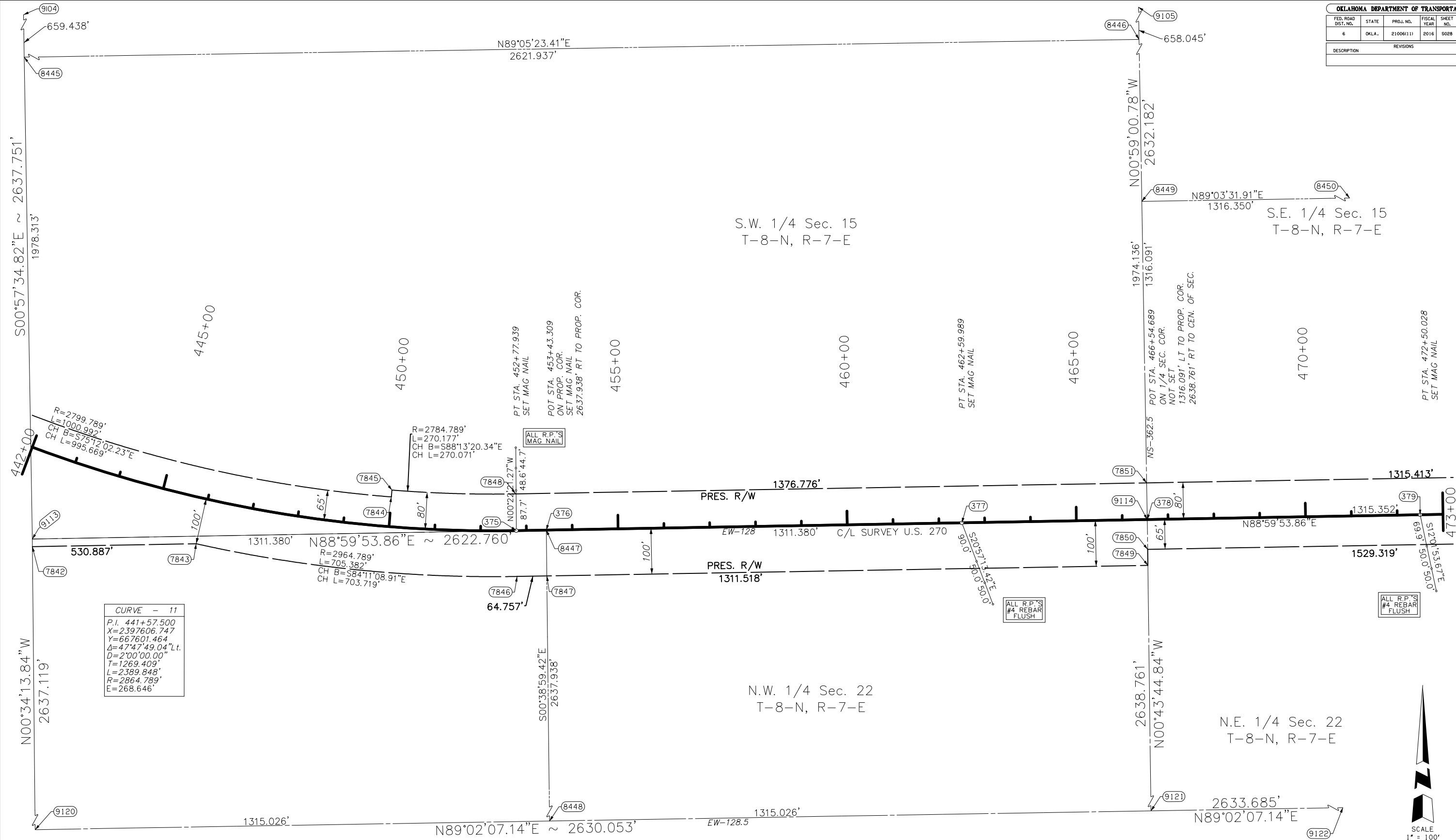
SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. S027

March 31st, 2015

SDS 27 OF 47

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5028	5047
DESCRIPTION		REVISIONS		DATE	

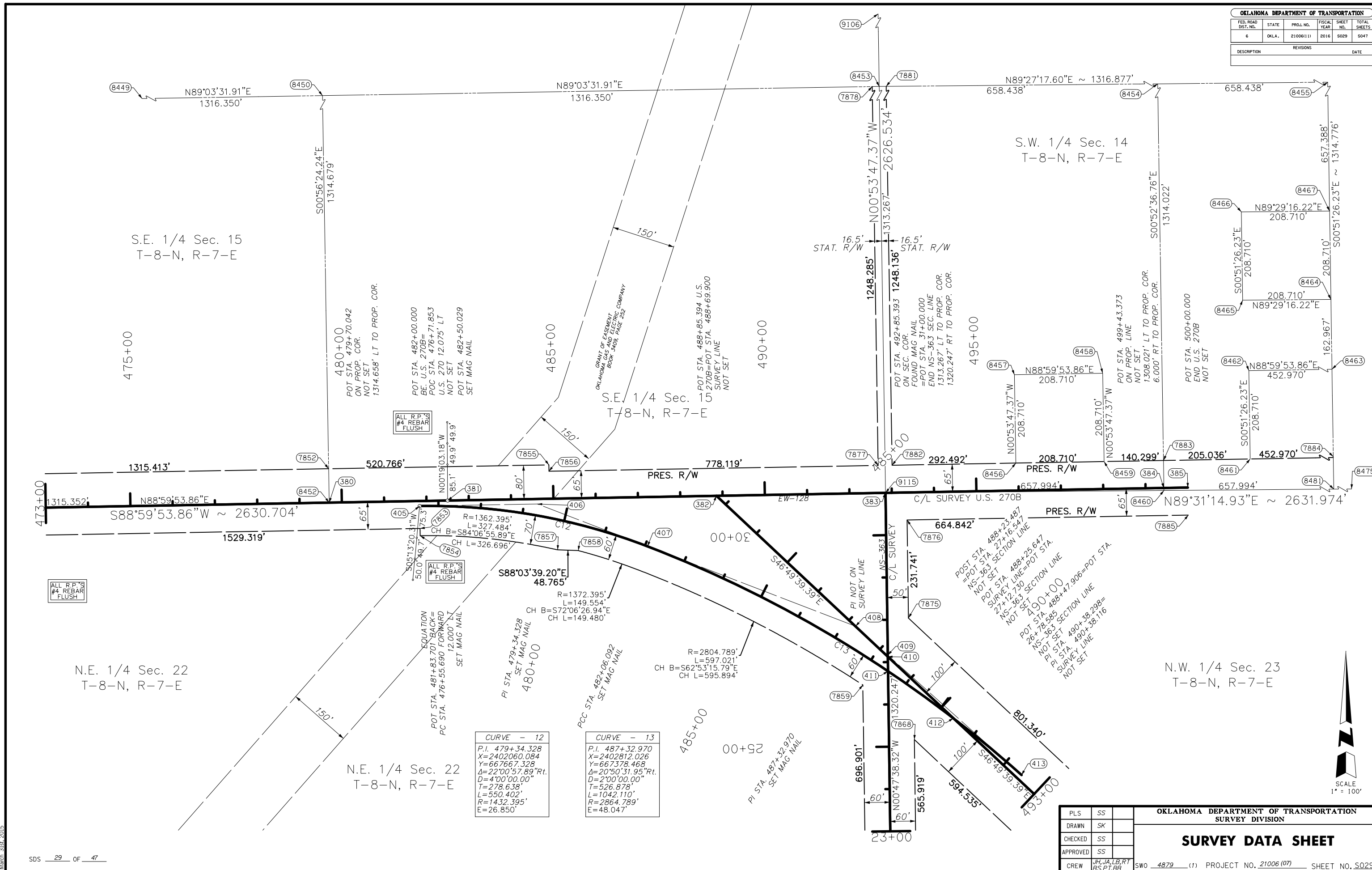


		OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS		
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,RR	SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. S028	

March 31st, 2015

SDS 28 OF 47

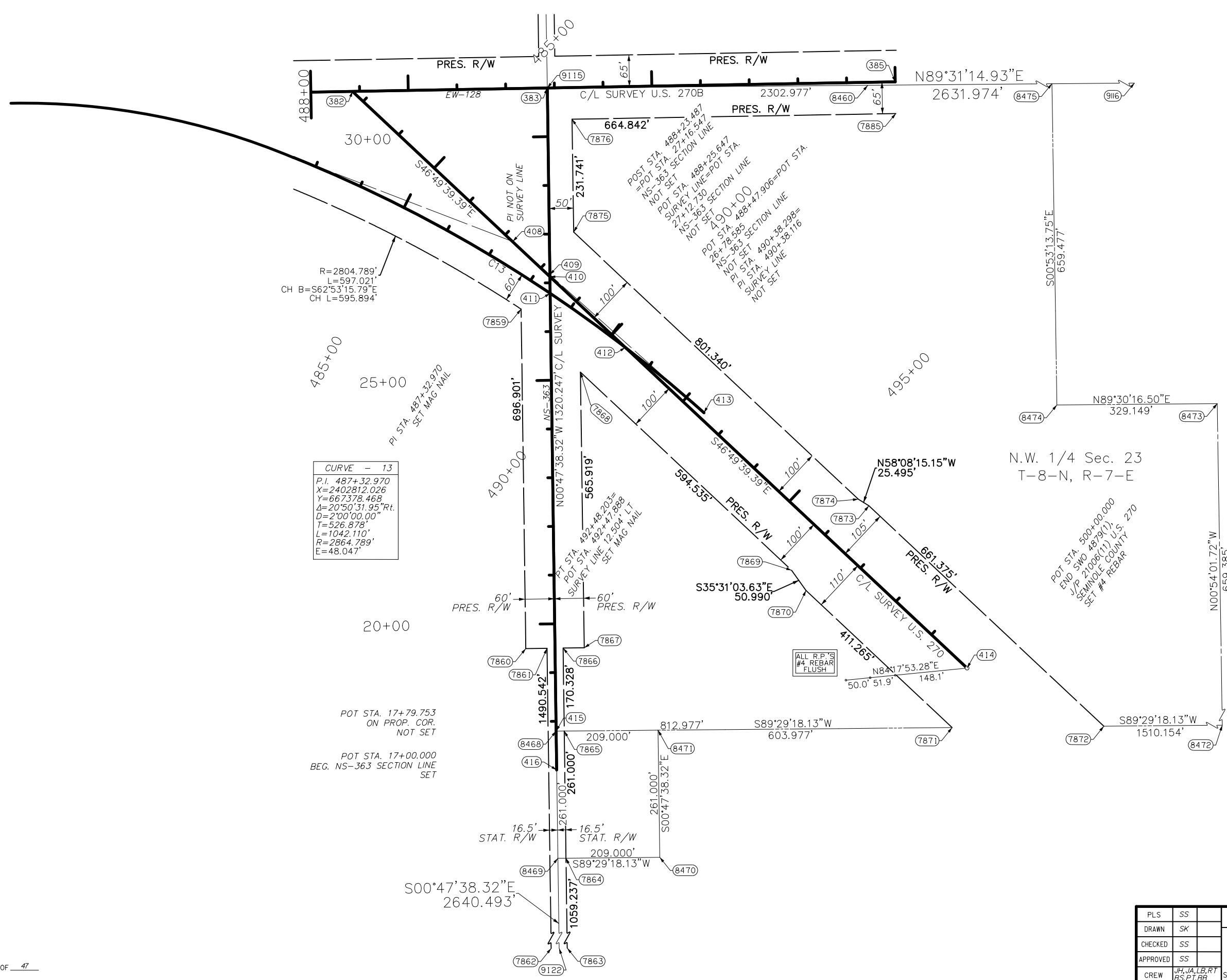
OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(1)	2016	5029	5047
DESCRIPTION		REVISIONS		DATE	



OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	SS		
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,RR	SWO 4879 (1)	PROJECT NO. 21006 (07) SHEET NO. S029

March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5030	5047
DESCRIPTION		REVISIONS		DATE	



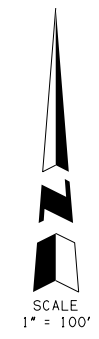
R=2804.789'
L=597.021'
CH B=S62°53'15.79\"E
CH L=595.894'

CURVE - 13	
P.I.	487+32.970
X	=2402812.026
Y	=667378.468
A	=20°50'31.95\"Rt.
D	=2°00'00.00"
T	=526.878'
L	=1042.110'
R	=2864.789'
E	=48.047'

ALL R.P.'S
#4 REBAR
FLUSH

N.W. 1/4 Sec. 23
T-8-N, R-7-E

POT STA. 500+00.000
END SWO 4879(1)
J/R 21006(11) U.S. 270
SET #4 REBAR



March 31st, 2015

SDS 30 OF 47

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,RR

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5030

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5031	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-209
 FOUND AND ACCEPTED ODOT BRASS MONUMENT AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-513
 FOUND AND ACCEPTED PK NAIL AS SHOWN ON CORNER RECORD FILED BY TIMOTHY G. POLLARD, PLS 1474. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

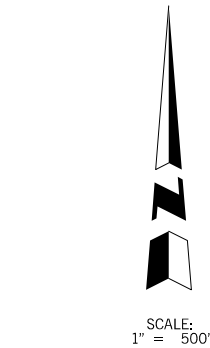
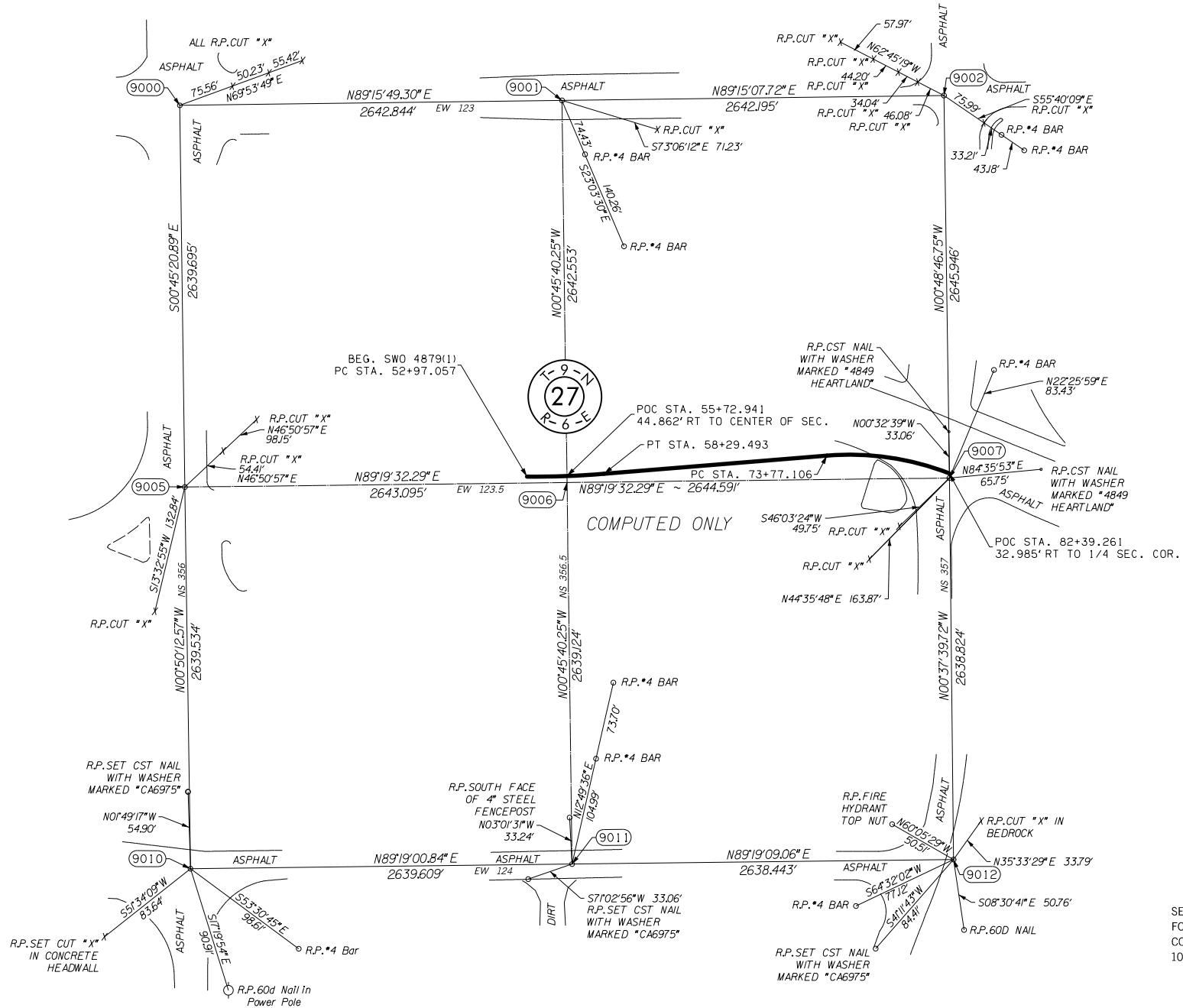
SECTION CORNER - O.D.O.T. S-67-515
 FOUND AND ACCEPTED CUT "X" IN CONCRETE AS SHOWN ON CORNER RECORD FILED BY ROBBY L. JOHNSON, PLS 1539 AND BY BRUCE IRA WILLIAMS, PLS 1280. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-499
 FOUND AND ACCEPTED #5 REBAR SHOWN AS FOUND ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-503
 FOUND AND ACCEPTED PK NAIL. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-500
 FOUND AND ACCEPTED RAILROAD SPIKE. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-501
 FOUND AND ACCEPTED RAILROAD SPIKE. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080 AND BY RODGER WHITED, PLS 1298.



NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LR,RT,BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5031

SECTION CORNER - O.D.O.T. S-67-515
 FOUND AND ACCEPTED CUT "X" IN CONCRETE AS SHOWN ON CORNER RECORD FILED BY ROBBY L. JOHNSON, PLS 1539 AND BY BRUCE IRA WILLIAMS, PLS 1280. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-509
 FOUND AND ACCEPTED #5 REBAR AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080 AND BY BRUCE IRA WILLIAMS, PLS 1280. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JAMES B. MARSHALL, PLS 113.

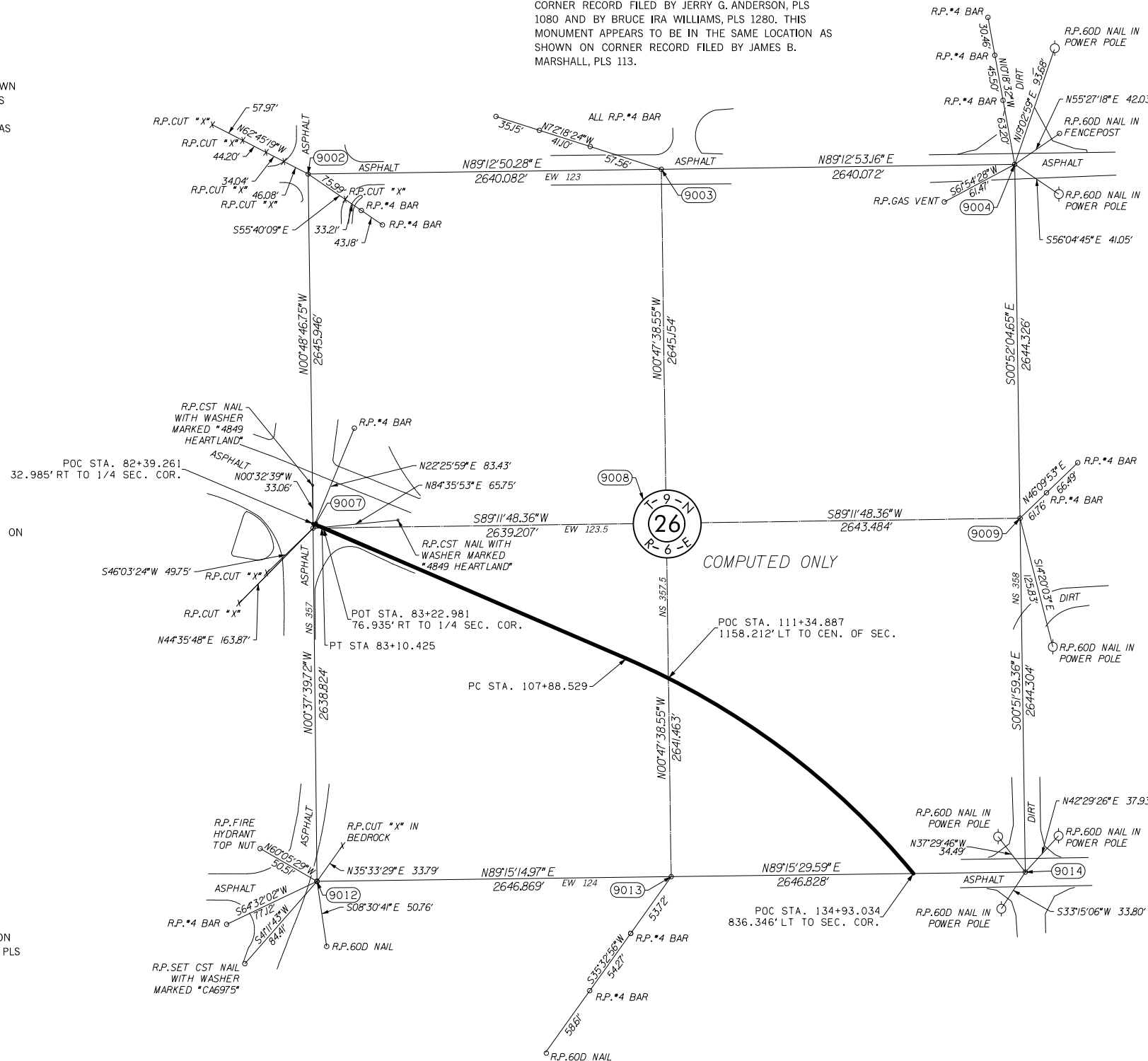
SECTION CORNER - O.D.O.T. S-67-508
 FOUND AND ACCEPTED #5 REBAR AS SHOWN ON CORNER RECORD FILED BY JAMES B. MARSHALL, PLS 113, JERRY G. ANDERSON, PLS 1080 AND BRUCE IRA WILLIAMS, PLS 1280.

SECTION CORNER - O.D.O.T. S-67-503
 FOUND AND ACCEPTED PK NAIL. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-507
 FOUND AND ACCEPTED #5 REBAR AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080. MONUMENT SHOWN ON CORNER RECORD FILED BY JAMES B. MARSHALL, PLS 113 WAS NOT FOUND.

SECTION CORNER - O.D.O.T. S-67-502
 FOUND AND ACCEPTED #5 REBAR AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-506
 FOUND DAMAGED NAIL AT THE LOCATION SHOWN ON CORNER RECORD FILED BY ROBBY L. JOHNSON, PLS 1539. REPLACED DAMAGED NAIL WITH #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT MATCHES THE LOCATION SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080 AND BY JAMES B. MARSHALL, PLS 113.



SECTION CORNER - O.D.O.T. S-67-505
 FOUND AND ACCEPTED #5 REBAR AS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SCALE:
 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT,BS,PT,BB

SURVEY DATA SHEET

SW0 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5032

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5033	5047
DESCRIPTION		REVISIONS		DATE	

SECTION CORNER - O.D.O.T. S-67-505
 FOUND AND ACCEPTED #5 REBAR AS SHOWN ON
 CORNER RECORD FILED BY JERRY G. ANDERSON, PLS
 1080.

SECTION CORNER - O.D.O.T. S-67-502
 FOUND AND ACCEPTED #5 REBAR AS SHOWN ON
 CORNER RECORD FILED BY JERRY G. ANDERSON, PLS
 1080.

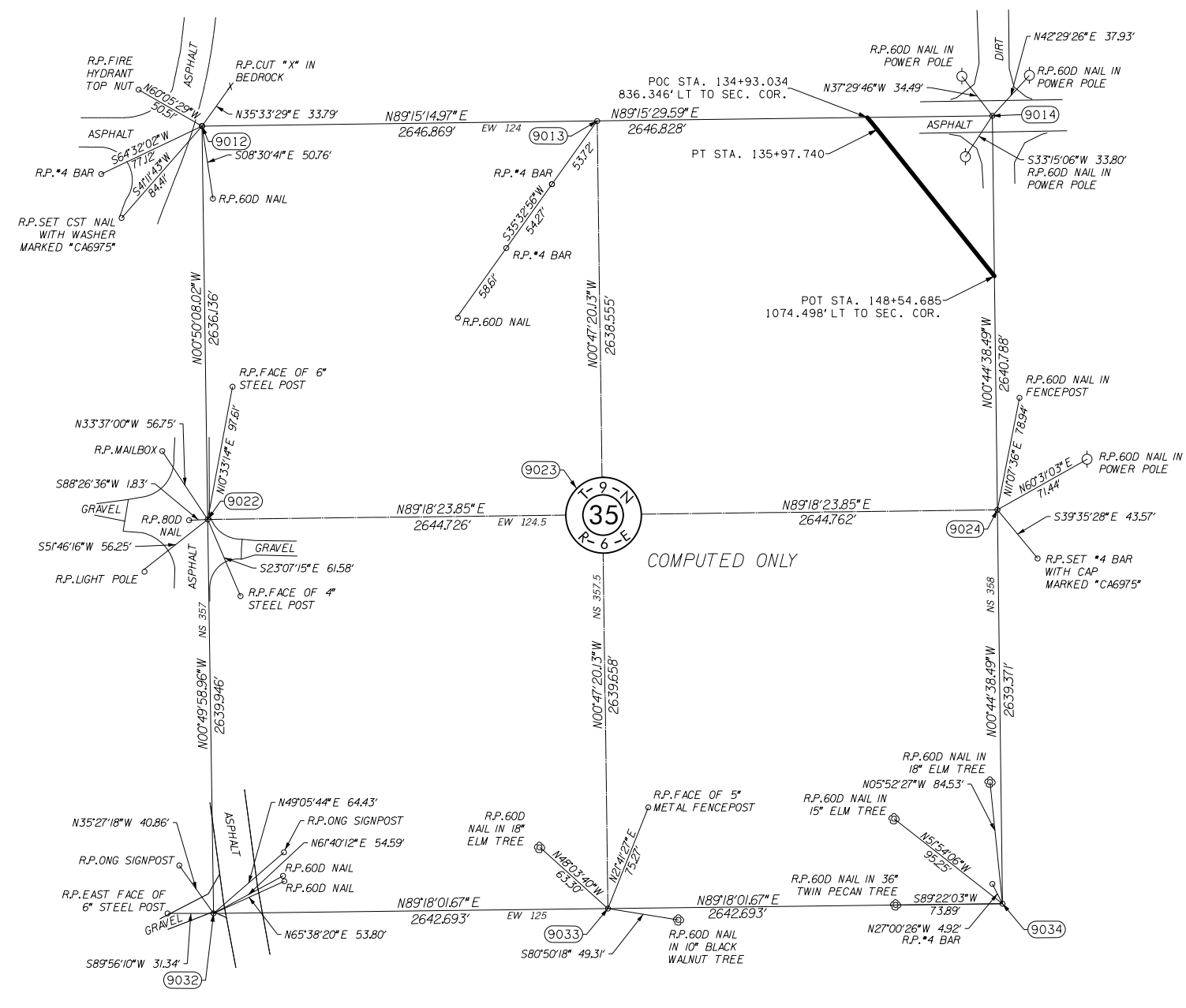
SECTION CORNER - O.D.O.T. S-67-506
 FOUND DAMAGED NAIL AT THE LOCATION SHOWN ON
 CORNER RECORD FILED BY ROBBY L. JOHNSON, PLS 1539.
 REPLACED DAMAGED NAIL WITH #4 REBAR WITH CAP
 STAMPED CA 6975. THIS MONUMENT MATCHES THE
 LOCATION SHOWN ON CORNER RECORD FILED BY
 G. ANDERSON, PLS 1080 AND BY JAMES B. MARSHALL, PLS
 113.

SECTION CORNER - O.D.O.T. S-67-910
 FOUND AND ACCEPTED RAILROAD SPIKE AS SHOWN ON
 CORNER RECORD FILED BY BILLY JACK WILLINGHAM, PLS
 754.

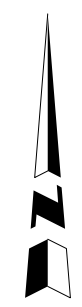
SECTION CORNER - O.D.O.T. S-67-911
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON
 CORNER RECORD FILED BY RODGER WHITED, PLS 1298.

SECTION CORNER - O.D.O.T. S-67-914
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON
 CORNER RECORD FILED BY BILLY JACK WILLINGHAM, PLS
 754.

SECTION CORNER - O.D.O.T. S-67-916
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS RE-ESTABLISHED USING SURROUNDING
 MONUMENTS AND CHECKS WELL WITH GLO DISTANCES.
 THERE WAS NO CORNER RECORD ON FILE.



SECTION CORNER - O.D.O.T. S-67-915
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS RE-ESTABLISHED USING PROPORTIONATE
 MEASUREMENT. THERE WAS NO CORNER RECORD ON
 FILE.



SCALE:
 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5033

March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION					
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5034	5047
DESCRIPTION		REVISIONS		DATE	

SECTION CORNER - O.D.O.T. S-67-506
 FOUND DAMAGED NAIL AT THE LOCATION SHOWN ON CORNER RECORD FILED BY ROBBY L. JOHNSON, PLS 1539. REPLACED DAMAGED NAIL WITH #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT MATCHES THE LOCATION SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080 AND BY JAMES B. MARSHALL, PLS 113.

SECTION CORNER - O.D.O.T. S-67-911
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY RODGER WHITED, PLS 1298.

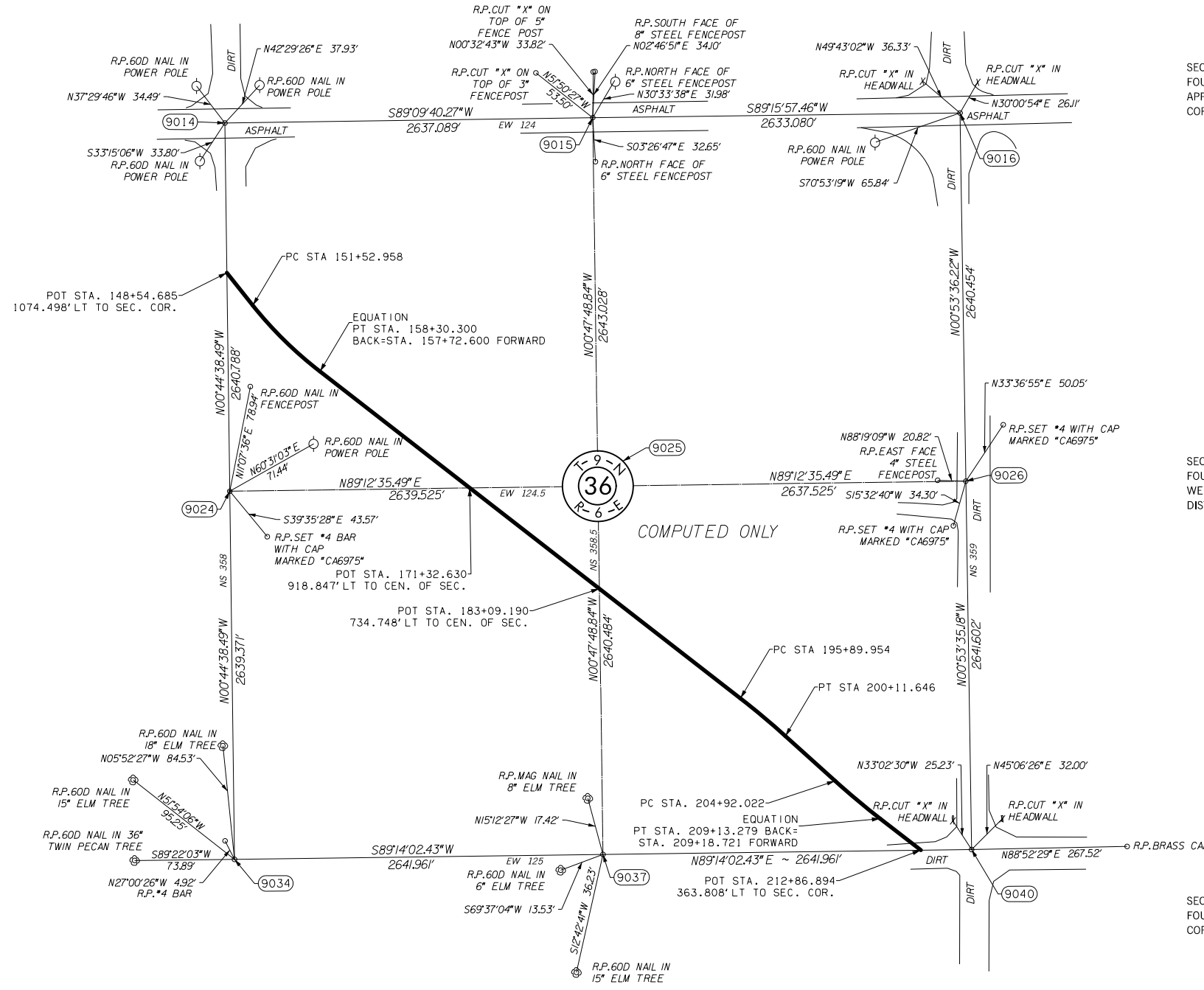
SECTION CORNER - O.D.O.T. S-67-916
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING SURROUNDING MONUMENTS AND CHECKS WELL WITH GLO DISTANCES. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-906
 SET MAG NAIL WITH WASHER STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED AT THE LOCATION SHOWN ON CORNER RECORD FILED BY ROBBY L. JOHNSON, PLS 1539 USING EXISTING REFERENCES AND SECTION DATA.

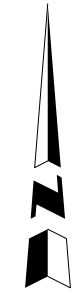
SECTION CORNER - O.D.O.T. S-67-907
 FOUND AND ACCEPTED 80D NAIL. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY RODGER WHITED, PLS 1298.

SECTION CORNER - O.D.O.T. S-67-912
 FOUND AND ACCEPTED #4 REBAR. THIS MONUMENT FITS WELL WITH SURROUNDING OCCUPATION LINES AND GLO DISTANCES. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-920
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON CORNER RECORD FILED BY JAMES B. MARSHALL, PLS 113.



SECTION CORNER - O.D.O.T. S-67-918
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONATE MEASUREMENT. THERE WAS NO CORNER RECORD ON FILE.



SCALE:
1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5034

March 31st, 2015

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5035	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-907
 FOUND AND ACCEPTED 80D NAIL. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY RODGER WHITED, PLS 1298.

SECTION CORNER - O.D.O.T. S-67-912
 FOUND AND ACCEPTED #4 REBAR. THIS MONUMENT FITS WELL WITH SURROUNDING OCCUPATION LINES AND GLO DISTANCES. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-920
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON CORNER RECORD FILED BY JAMES B. MARSHALL, PLS 113.

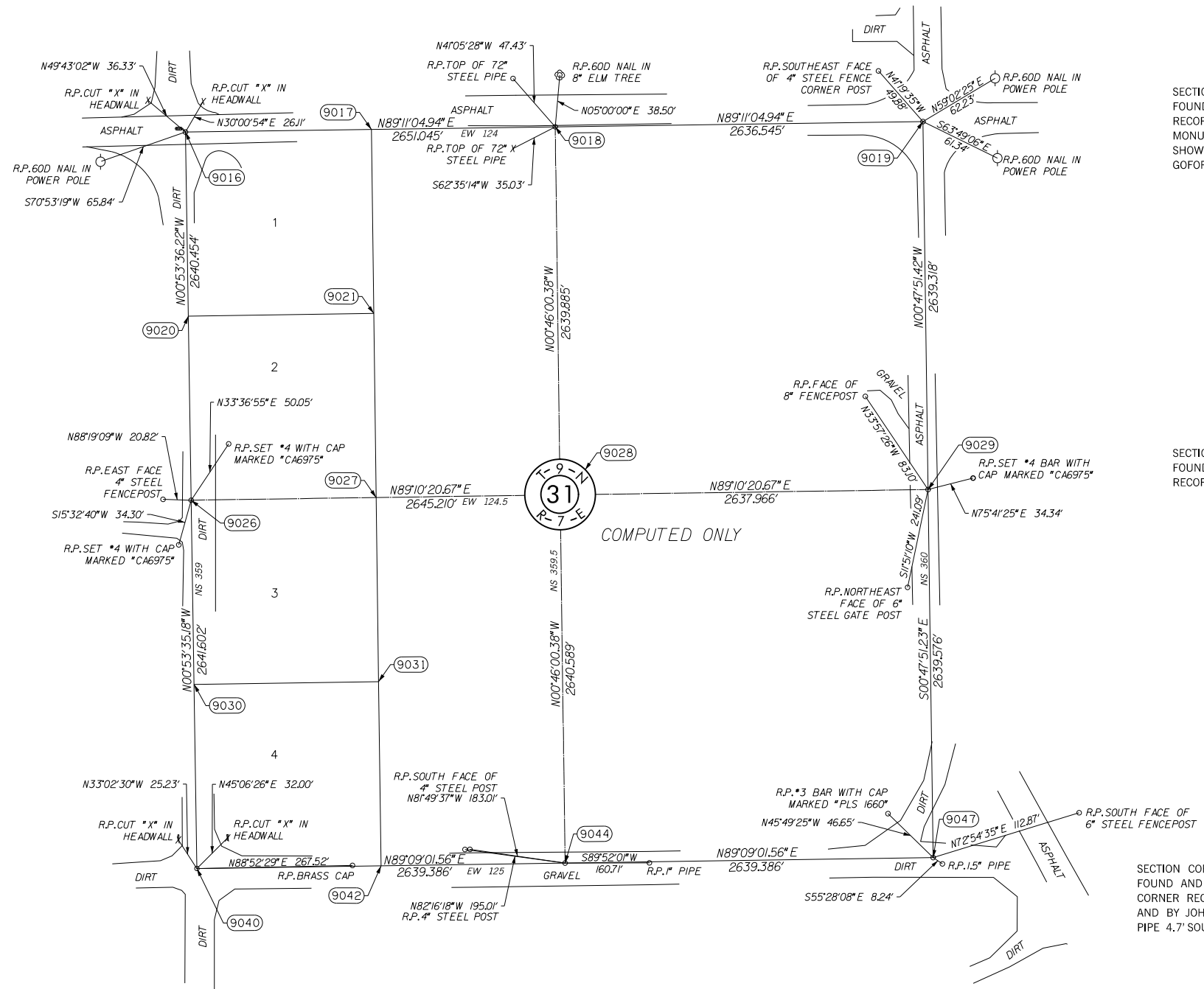
SECTION CORNER - O.D.O.T. S-67-908
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONAL MEASUREMENT. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-909
 FOUND AND ACCEPTED PK NAIL AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY BOBBY L. GOFORTH, PLS 340 AND BY JAMES B. MARSHALL, PLS 113.

SECTION CORNER - O.D.O.T. S-67-913
 FOUND AND ACCEPTED PK NAIL AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-924
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY RODGER WHITED, PLS 1298 AND BY JOHNNY LEE PACK, PLS 1252. ALSO FOUND 1-1/2' PIPE 4.7' SOUTH AND 6.9' EAST OF CORNER.

SECTION CORNER - O.D.O.T. S-67-922
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON CORNER RECORD FILED BY BILLY JACK WILLINGHAM, PLS 754 AND BY DAN W. ROGERS, PLS 1200.



SCALE: 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

PLS	SS	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	SK	SURVEY DATA SHEET
CHECKED	SS	
APPROVED	SS	
CREW	JH,JA,LB,RT,BS,PT,BB	

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5035

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5036	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-919
 FOUND BLM BRASS MONUMENT AS SHOWN ON THE 2007 DEPENDENT RESURVEY. THIS MONUMENT IS 0.5' NORTH OF THE CALCULATED POSITION FOR CORNER. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-917
 FOUND BLM BRASS MONUMENT AS CLOSING CORNER AS SHOWN ON THE 2007 DEPENDENT RESURVEY. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-921
 FOUND BLM BRASS MONUMENT AS CLOSING CORNER AS SHOWN ON THE 2007 DEPENDENT RESURVEY. THIS MONUMENT IS 1.3' NORTH OF THE CALCULATED POSITION FOR CORNER. THERE WAS NO CORNER RECORD ON FILE.

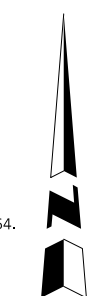
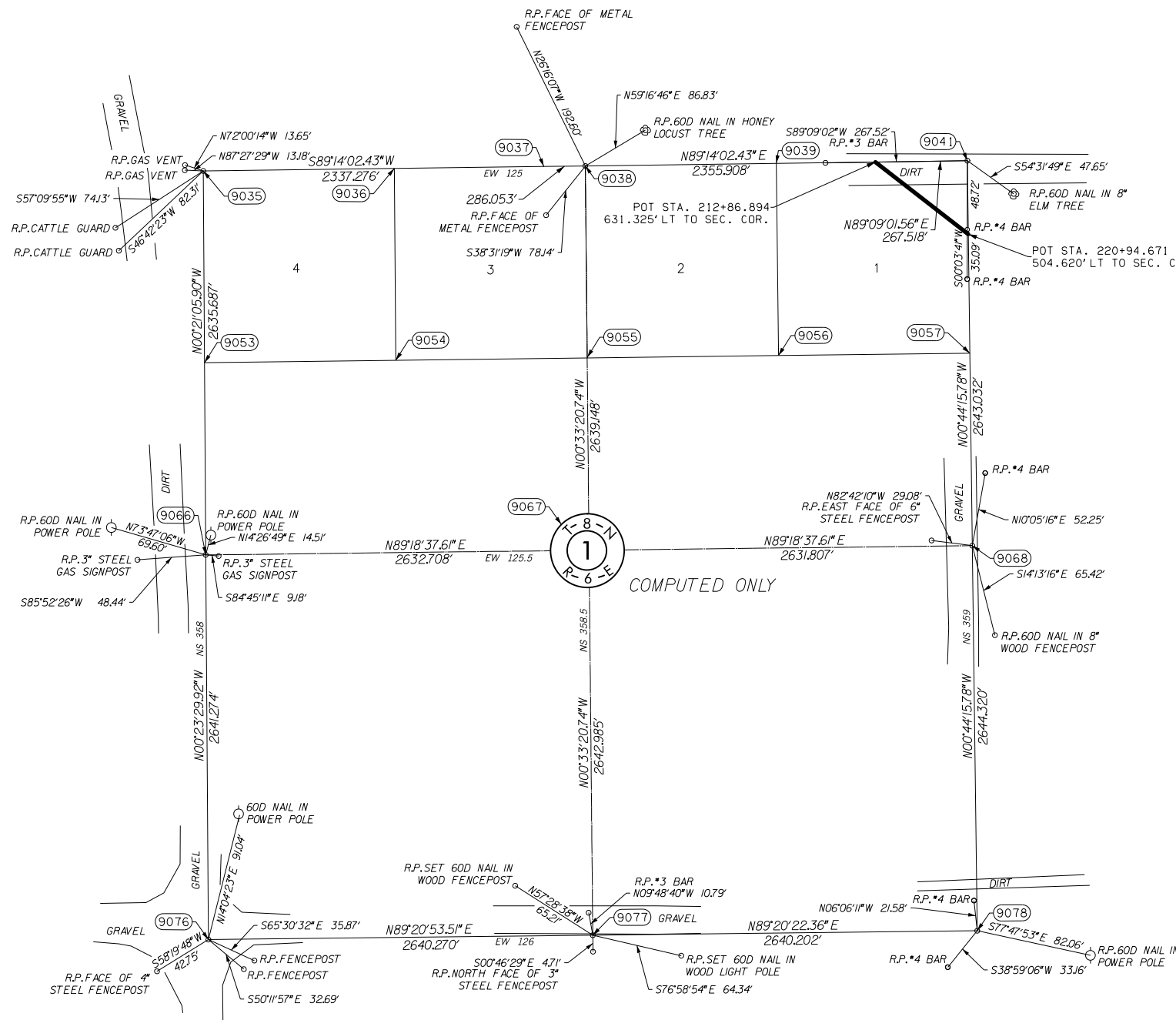
SECTION CORNER - O.D.O.T. S-67-928
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2007 DEPENDENT RESURVEY. THIS MONUMENT WAS SET AT THE LOCATION SHOWN ON CORNER RECORD FILED BY RODGER WHITED, PLS 1298.

SECTION CORNER - O.D.O.T. S-67-929
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED AT THE LOCATION SHOWN ON THE 2007 DEPENDENT RESURVEY. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-932
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2007 DEPENDENT RESURVEY. THIS MONUMENT WAS SET AT THE LOCATION SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080.

SECTION CORNER - O.D.O.T. S-67-934
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE DEPENDENT RESURVEY. THIS MONUMENT WAS SET AT THE LOCATION SHOWN ON CORNER RECORD FILED BY DAVID F. HEAVNER, PLS 964.

SECTION CORNER - O.D.O.T. S-67-933
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2007 DEPENDENT RESURVEY. MONUMENTS SHOWN ON CORNER RECORD FILED BY JERRY G. ANDERSON, PLS 1080 AND BY DAVID F. HEAVNER, PLS, 964 WERE NOT FOUND.



SCALE: 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

PLS	SS	OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION
DRAWN	SK	SURVEY DATA SHEET
CHECKED	SS	
APPROVED	SS	
CREW	JH,JA,LB,RT,BS,PT,BB	

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5036

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5037	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-921
 FOUND BLM BRASS MONUMENT AS CLOSING CORNER AS SHOWN ON THE 2007 DEPENDENT RESURVEY. THIS MONUMENT IS 1.3' NORTH OF THE CALCULATED POSITION FOR CORNER. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-923
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON CORNER RECORD FILED BY DAN W. ROGERS, PLS 1200. ALSO FOUND 1-1/2" PIPE 2.1' SOUTH AND 4.0' WEST OF CORNER.

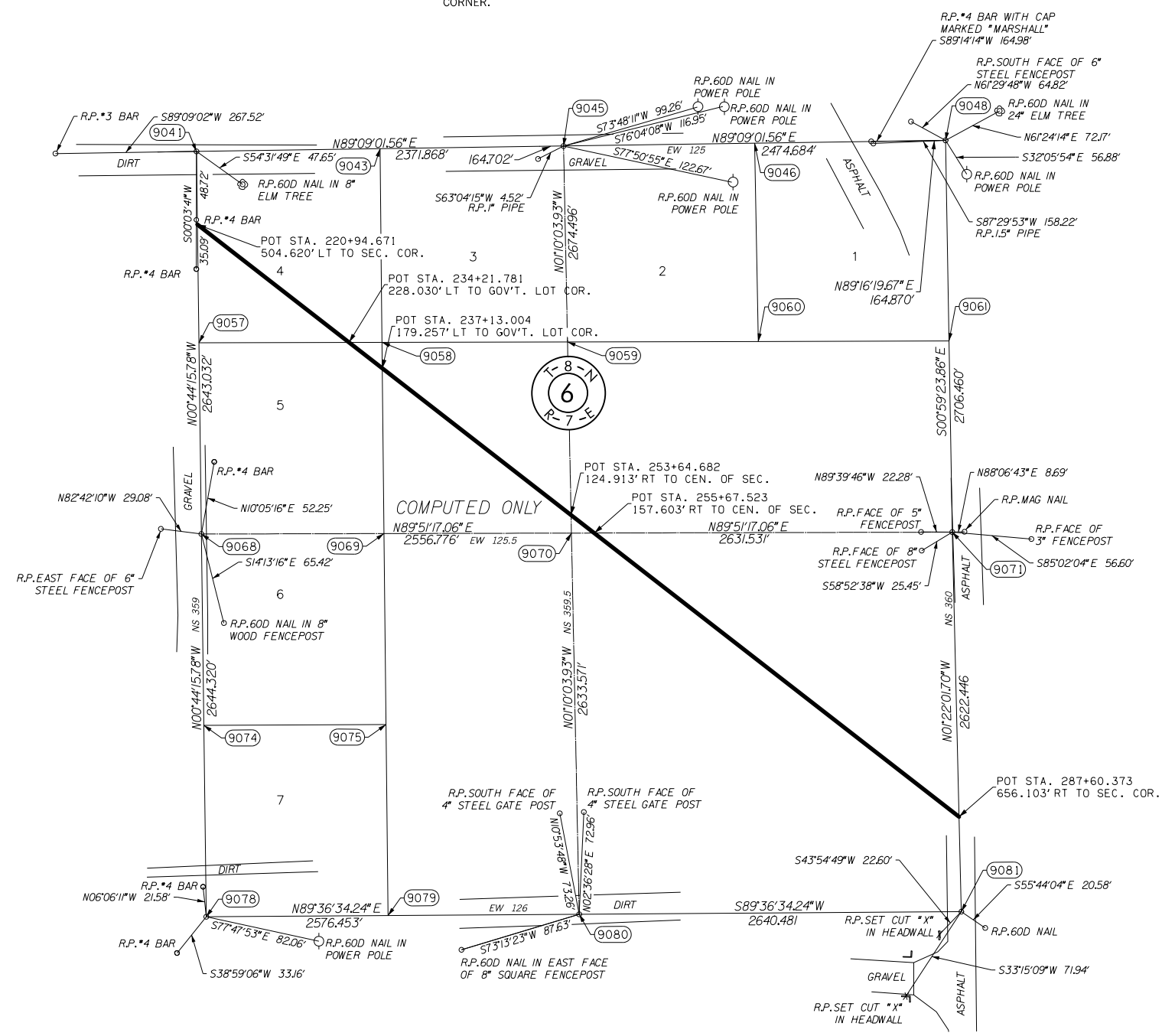
SECTION CORNER - O.D.O.T. S-67-925
 FOUND AND ACCEPTED #6 REBAR. THIS MONUMENT MATCHES THE LOCATION SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-929
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED AT THE LOCATION SHOWN ON THE 2007 DEPENDENT RESURVEY. THERE WAS NO CORNER RECORD ON FILE.

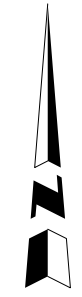
SECTION CORNER - O.D.O.T. S-67-930
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY FRANK MAGNER, PLS 1564. ALSO FOUND MAG NAIL 0.3' NORTH AND 8.7' EAST OF CORNER.

SECTION CORNER - O.D.O.T. S-67-934
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE DEPENDENT RESURVEY. THIS MONUMENT WAS SET AT THE LOCATION SHOWN ON CORNER RECORD FILED BY DAVID F. HEAVNER, PLS 964.

SECTION CORNER - O.D.O.T. S-67-936
 FOUND AND ACCEPTED RAILROAD SPIKE AS SHOWN ON CORNER RECORD FILED BY FRANK MAGNER, PLS 1564.



SECTION CORNER - O.D.O.T. S-67-935
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS CORNER WAS RE-ESTABLISHED USING PROPORTIONATE MEASUREMENT. THERE WAS NO CORNER RECORD ON FILE.



SCALE: 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT,BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5037

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5038	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-926
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONATE MEASUREMENT. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-925
 FOUND AND ACCEPTED #6 REBAR. THIS MONUMENTS MATCHES THE LOCATION SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

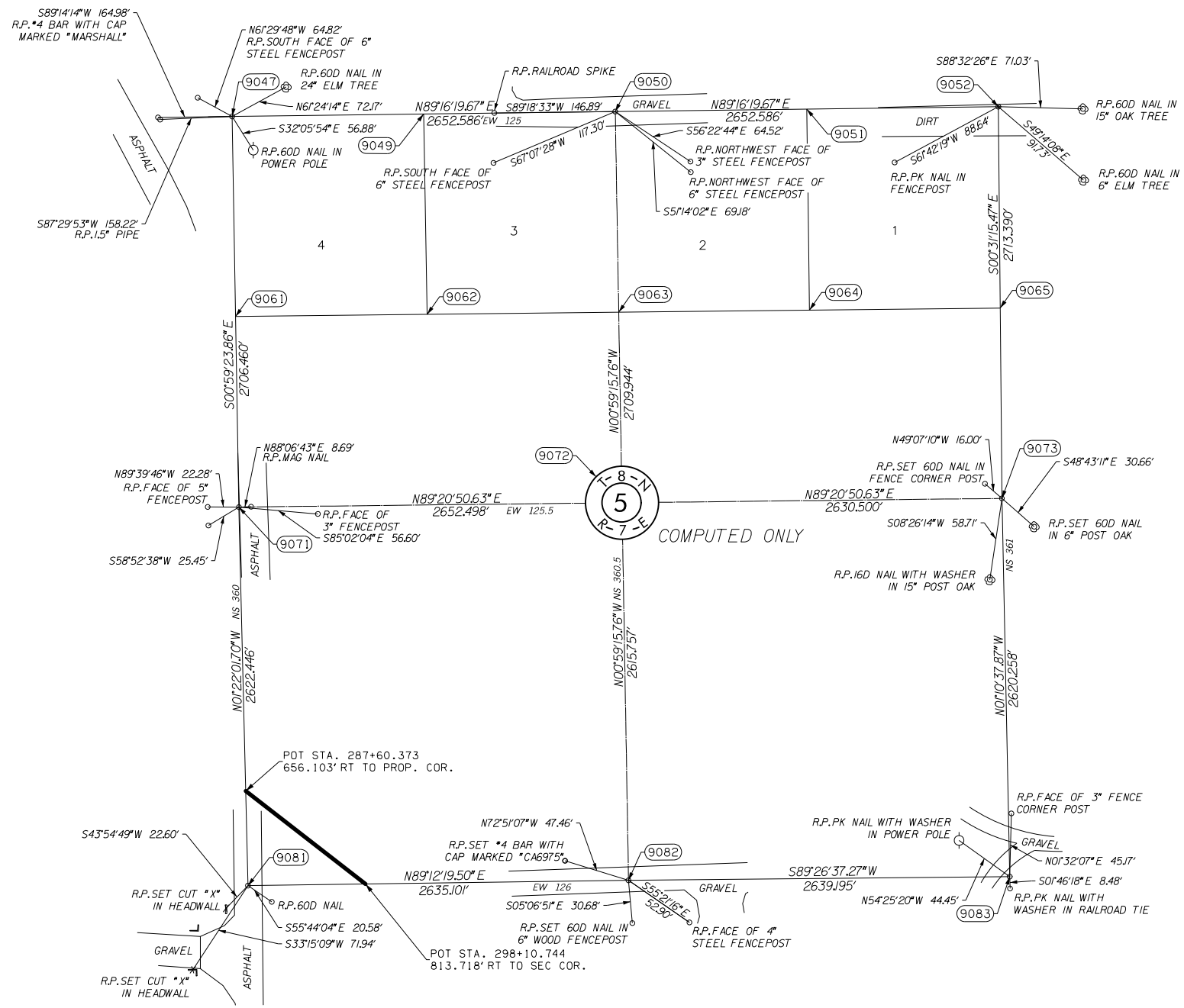
SECTION CORNER - O.D.O.T. S-67-927
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-930
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY FRANK MAGNER, PLS 1564. ALSO FOUND MAG NAIL 0.3' NORTH AND 8.7' EAST OF CORNER.

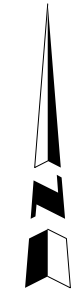
SECTION CORNER - O.D.O.T. S-67-931
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-936
 FOUND AND ACCEPTED RAILROAD SPIKE AS SHOWN ON CORNER RECORD FILED BY FRANK MAGNER, PLS 1564.

SECTION CORNER - O.D.O.T. S-67-938
 FOUND AND ACCEPTED 16P NAIL INSIDE 2-1/2" IRON PIPE AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.



SECTION CORNER - O.D.O.T. S-67-937
 FOUND AND ACCEPTED #3 REBAR. THIS MONUMENT FITS WELL WITH EXISTING OCCUPATION EVIDENCE. THERE WAS NO CORNER RECORD ON FILE.



SCALE: 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT,BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5038

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5039	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-937
 FOUND AND ACCEPTED #3 REBAR. THIS MONUMENT FITS WELL WITH EXISTING OCCUPATION EVIDENCE. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-936
 FOUND AND ACCEPTED RAILROAD SPIKE AS SHOWN ON CORNER RECORD FILED BY FRANK MAGNER, PLS 1564.

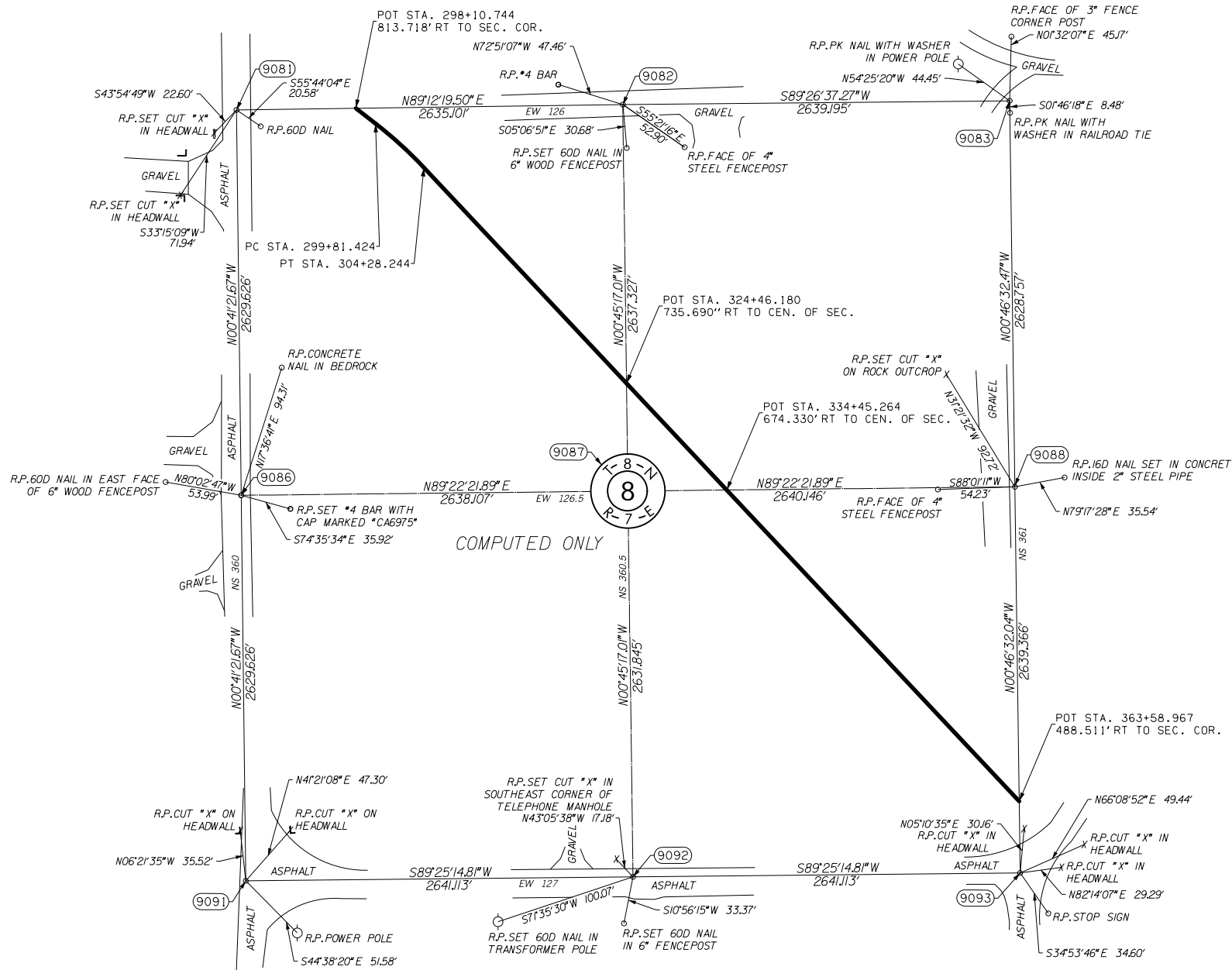
SECTION CORNER - O.D.O.T. S-67-938
 FOUND AND ACCEPTED 16P NAIL INSIDE 2-1/2" IRON PIPE AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-941
 SET MAG NAIL WITH WASHER STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONATE MEASUREMENT. THERE WAS NO CORNER RECORD ON FILE.

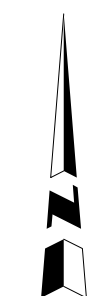
SECTION CORNER - O.D.O.T. S-67-942
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING SECTION INFORMATION PROVIDED BY MARSHALL SURVEYING AT THE LOCATION SHOWN ON CORNER RECORD FILED BY DAVID F. HEAVNER, PLS 964.

SECTION CORNER - O.D.O.T. S-67-944
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2005 DEPENDENT RESURVEY. THIS MONUMENT WAS SET AT THE POINT SHOWN ON CORNER RECORD FILED BY TERRY M. MARSHALL, PLS 1322.

SECTION CORNER - O.D.O.T. S-67-946
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON CORNER RECORD FILED BY RODGER WHITED, PLS 1298 AND BY KELLY K. SCHMIDT, PLS 1507.



COMPUTED ONLY



SCALE: 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

SECTION CORNER - O.D.O.T. S-67-945
 SET MAG NAIL WITH WASHER STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONAL MEASUREMENT AND FITS EXISTING OCCUPATION EVIDENCE. THERE WAS NO CORNER RECORD ON FILE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT,BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5039

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5040	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-939
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON
 CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-938
 FOUND AND ACCEPTED 16P NAIL INSIDE 2-1/2" IRON
 PIPE AS SHOWN ON CORNER RECORD FILED BY JOHNNY
 LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-940
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON
 CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

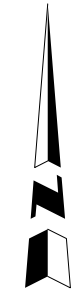
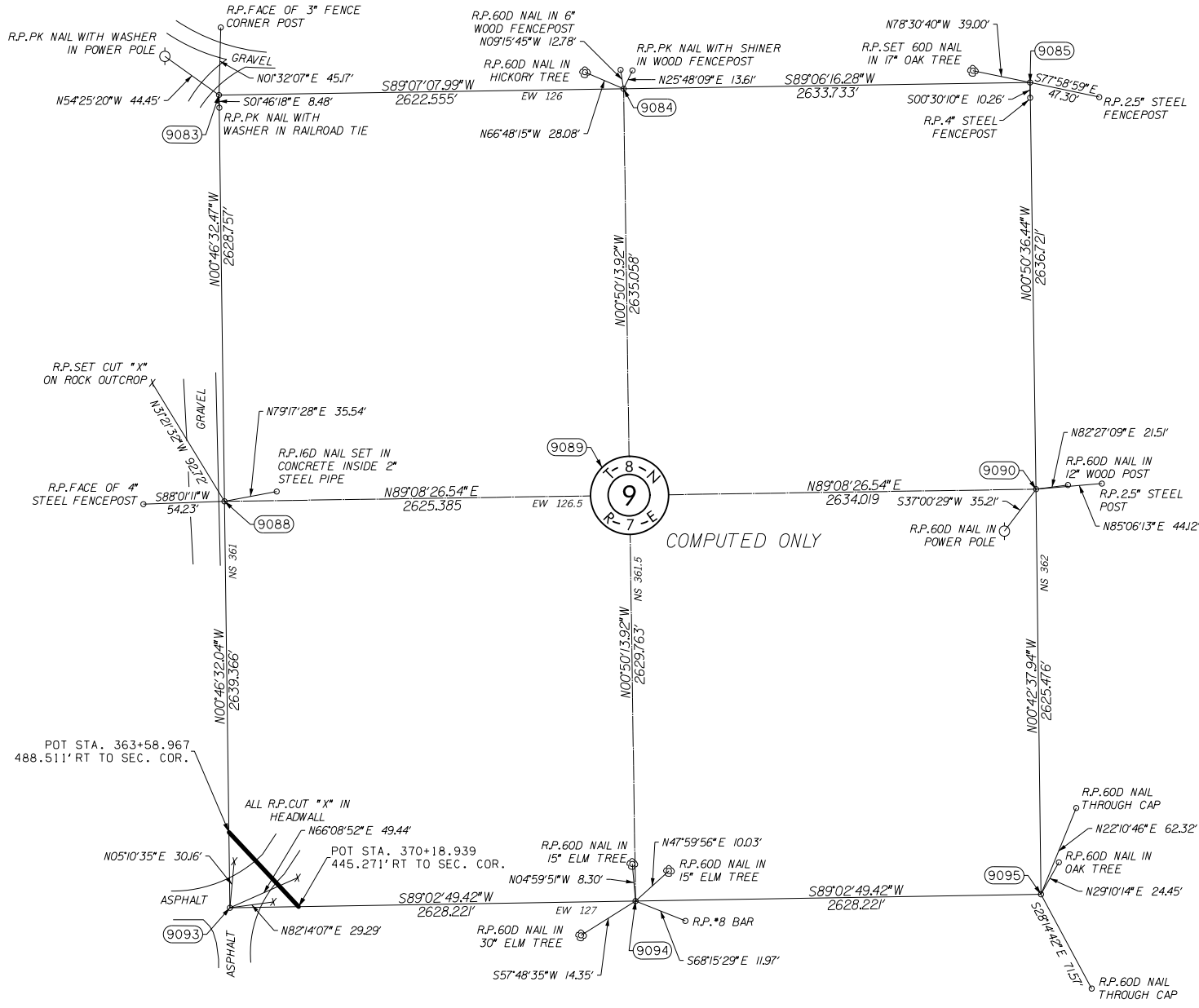
SECTION CORNER - O.D.O.T. S-67-942
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS RE-ESTABLISHED USING SECTION
 INFORMATION PROVIDED BY MARSHALL SURVEYING AT
 THE LOCATION SHOWN ON CORNER RECORD FILED BY
 DAVID F. HEAVNER, PLS 964.

SECTION CORNER - O.D.O.T. S-67-943
 FOUND AND ACCEPTED RAILROAD SPIKE AS SHOWN ON
 CORNER RECORD FILED BY DAVID F. HEAVNER, PLS 964.

SECTION CORNER - O.D.O.T. S-67-946
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON
 CORNER RECORD FILED BY RODGER WHITED, PLS 1298
 AND BY KELLY K. SCHMIDT, PLS 1507.

SECTION CORNER - O.D.O.T. S-67-948
 FOUND AND ACCEPTED BULL PRICK AS SHOWN ON
 CORNER RECORD FILED BY KELLY K. SCHMIDT, PLS 1507.

SECTION CORNER - O.D.O.T. S-67-947
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS RE-ESTABLISHED USING PROPORTIONAL
 MEASUREMENT. THERE WAS NO CORNER RECORD ON
 FILE.



SCALE:
 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
SURVEY DATA SHEET	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5040

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5041	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-945
 SET MAG NAIL WITH WASHER STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONAL MEASUREMENT AND FITS EXISTING OCCUPATION EVIDENCE. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-944
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2005 DEPENDENT RESURVEY. THIS MONUMENT WAS SET AT THE POINT SHOWN ON CORNER RECORD FILED BY TERRY M. MARSHALL, PLS 1322.

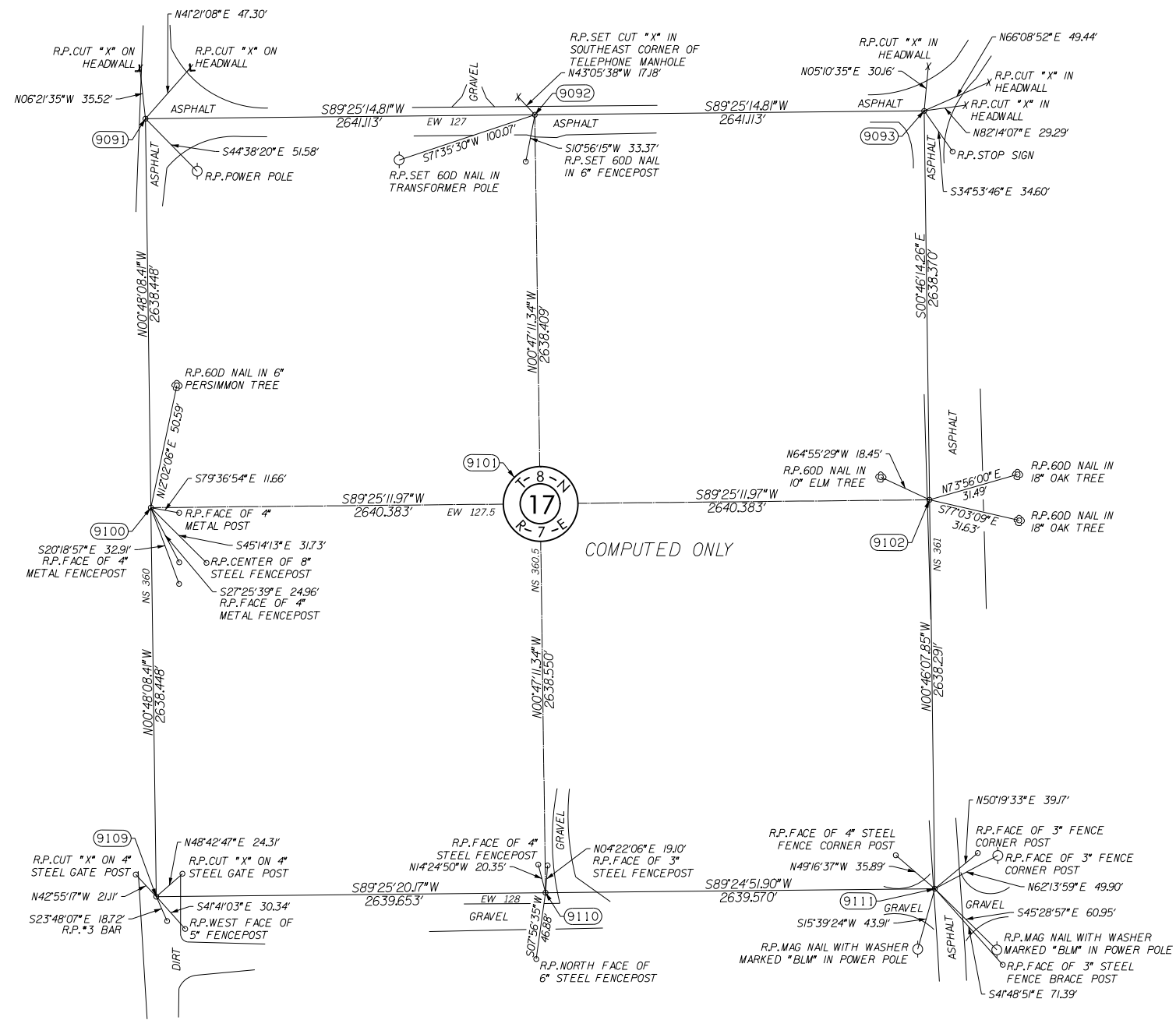
SECTION CORNER - O.D.O.T. S-67-946
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON CORNER RECORD FILED BY RODGER WHITED, PLS 1298 AND BY KELLY K. SCHMIDT, PLS 1507.

SECTION CORNER - O.D.O.T. S-67-953
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONAL MEASUREMENT. THERE WAS NO CORNER RECORD ON FILE.

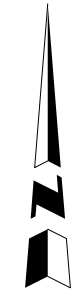
SECTION CORNER - O.D.O.T. S-67-954
 FOUND AND ACCEPTED RAILROAD SPIKE AS SHOWN ON CORNER RECORD FILED BY JESSE L. CARROLL, PLS 1071.

SECTION CORNER - O.D.O.T. S-67-958
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2005 DEPENDENT RESURVEY. ALSO FOUND #3 REBAR 17.1' SOUTH AND 7.6' EAST OF CORNER AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-960
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2005 DEPENDENT RESURVEY. THIS MONUMENT WAS SET AT THE LOCATION SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252 AND BY RODGER WHITED, PLS 1298.



SECTION CORNER - O.D.O.T. S-67-959
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2005 DEPENDENT RESURVEY. THERE WAS NO CORNER RECORD ON FILE.



SCALE:
 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT,BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5041

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5042	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-946
 FOUND AND ACCEPTED #3 REBAR AS SHOWN ON
 CORNER RECORD FILED BY RODGER WHITED, PLS 1298
 AND BY KELLY K. SCHMIDT, PLS 1507.

SECTION CORNER - O.D.O.T. S-67-947
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS RE-ESTABLISHED USING PROPORTIONAL
 MEASUREMENT. THERE WAS NO CORNER RECORD ON
 FILE.

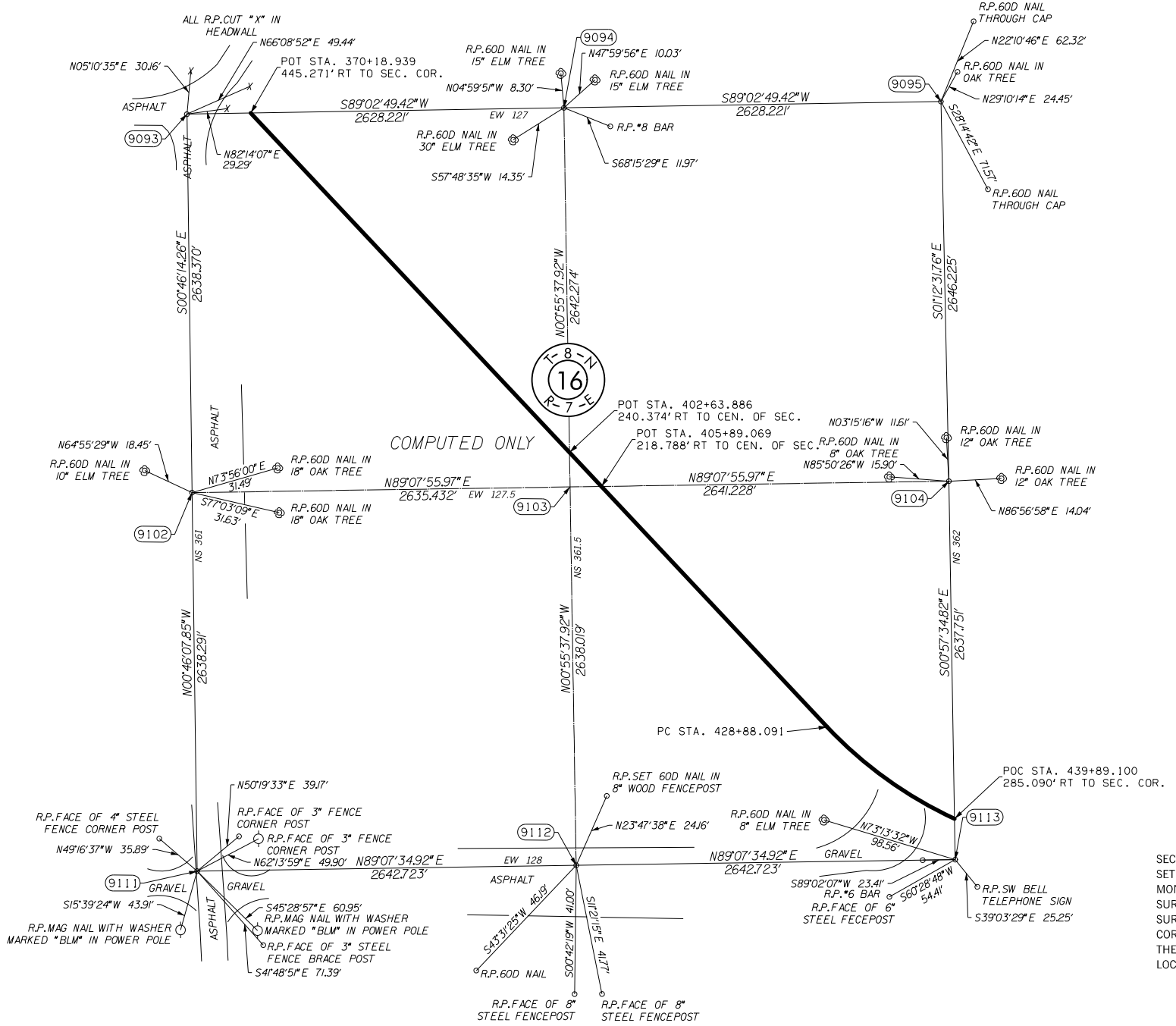
SECTION CORNER - O.D.O.T. S-67-948
 FOUND AND ACCEPTED BULL PRICK AS SHOWN ON
 CORNER RECORD FILED BY KELLY K. SCHMIDT, PLS 1507.

SECTION CORNER - O.D.O.T. S-67-954
 FOUND AND ACCEPTED RAILROAD SPIKE AS SHOWN ON
 CORNER RECORD FILED BY JESSE L. CARROLL, PLS 1071.

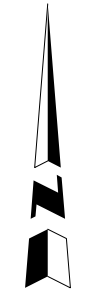
SECTION CORNER - O.D.O.T. S-67-955
 FOUND AND ACCEPTED ORIGINAL STONE. THERE WAS NO
 CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-960
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS
 SHOWN ON THE 2005 DEPENDENT RESURVEY. THIS
 MONUMENT WAS SET AT THE LOCATION SHOWN ON
 CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252
 AND BY RODGER WHITED, PLS 1298.

SECTION CORNER - O.D.O.T. S-67-962
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS RE-ESTABLISHED FROM SWO 1739(1)
 SURVEY. AFTER DISCUSSION WITH BEARING TREE LAND
 SURVEYING, IT WAS DETERMINED THAT THE DISTANCE TO
 CORNER FROM HIGHWAY PI WAS MISREAD. THEREFORE
 THE PREVIOUS CORNER WAS PULLED AND NEW
 LOCATION ACCEPTED.



SECTION CORNER - O.D.O.T. S-67-961
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS SET USING SURROUNDING MONUMENTS
 AND CHECKS WITH GLO DISTANCES AND EXISTING
 OCCUPATION LINES.



SCALE:
 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5042

March 31st, 2015

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5043	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-948
 FOUND AND ACCEPTED BULL PRICK AS SHOWN ON
 CORNER RECORD FILED BY KELLY K. SCHMIDT, PLS 1507.

SECTION CORNER - O.D.O.T. S-67-955
 FOUND AND ACCEPTED ORIGINAL STONE. THERE WAS NO
 CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-962
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS RE-ESTABLISHED FROM SWO 1739(1)
 SURVEY. AFTER DISCUSSION WITH BEARING TREE LAND
 SURVEYING, IT WAS DETERMINED THAT THE DISTANCE TO
 CORNER FROM HIGHWAY PI WAS MISREAD. THEREFORE
 THE PREVIOUS CORNER WAS PULLED AND NEW
 LOCATION ACCEPTED.

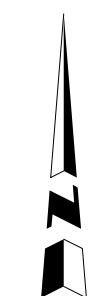
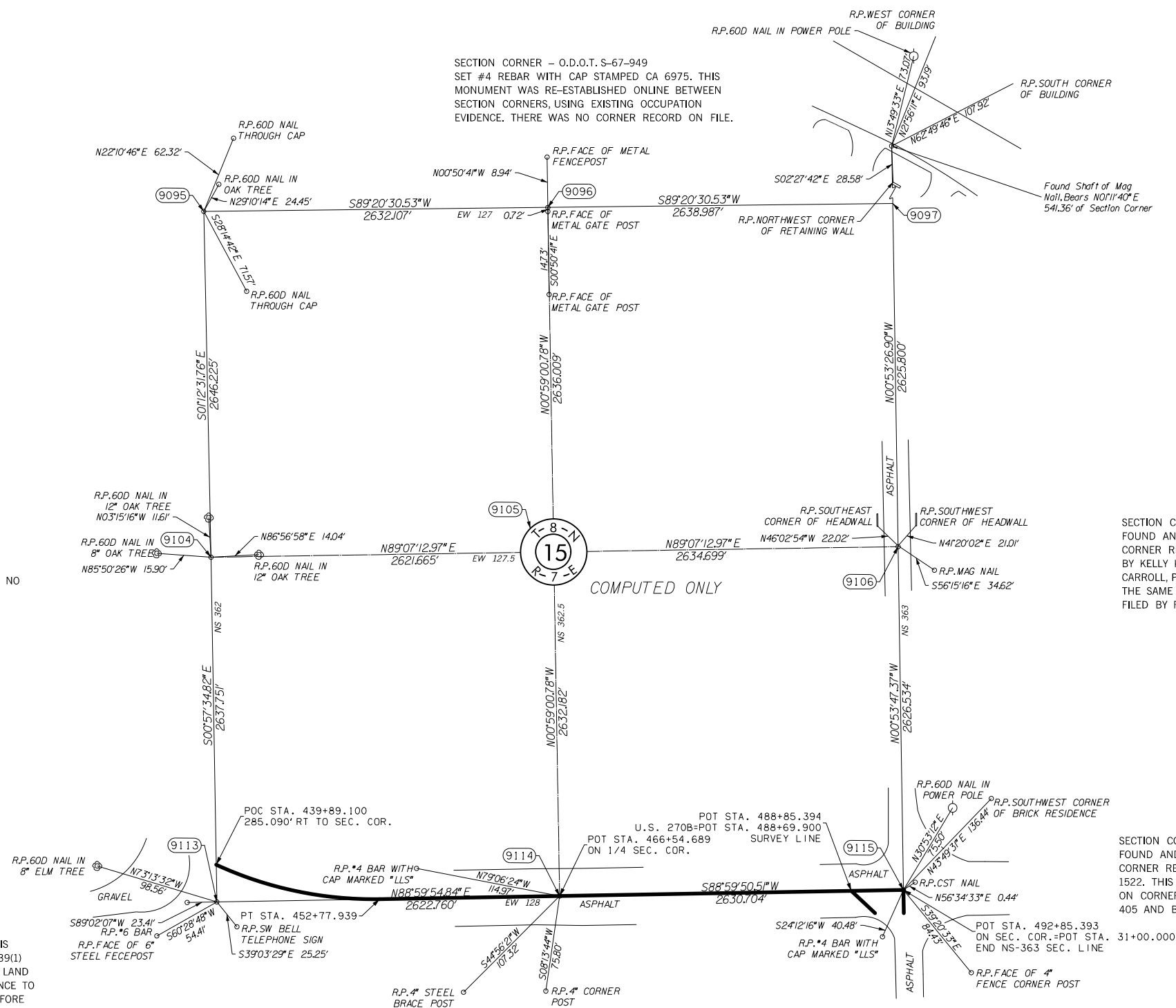
SECTION CORNER - O.D.O.T. S-67-949
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS
 MONUMENT WAS RE-ESTABLISHED ONLINE BETWEEN
 SECTION CORNERS, USING EXISTING OCCUPATION
 EVIDENCE. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-950
 FOUND AND ACCEPTED BROKE OFF MAG NAIL FOR
 WITNESS CORNER AS SHOWN ON CORNER RECORD
 FILED BY KELLY K. SCHMIDT, PLS 1507. WITNESS
 MONUMENTS SET BY FRED R. SMITH, JR., PLS 917 AND
 BY VIRGIL C. VAUGHN, PLS 405 WERE NOT FOUND.

SECTION CORNER - O.D.O.T. S-67-956
 FOUND AND ACCEPTED BROKE OFF MAG NAIL AS SHOWN ON
 CORNER RECORD FILED BY VIRGIL C. VAUGHN, PLS 405,
 BY KELLY K. SCHMIDT, PLS 1507 AND BY JACOB ROYCE
 CARROLL, PLS 1522. THIS MONUMENT APPEARS TO BE AT
 THE SAME LOCATION AS SHOWN ON CORNER RECORD
 FILED BY FRED R. SMITH, JR., PLS 917.

SECTION CORNER - O.D.O.T. S-67-964
 FOUND AND ACCEPTED MAG NAIL AS SHOWN ON
 CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS
 1522. THIS MONUMENT MATCHES THE LOCATION SHOWN
 ON CORNER RECORD FILED BY VIRGIL C. VAUGHN, PLS
 405 AND BY KELLY K. SCHMIDT, PLS 1507.

SECTION CORNER - O.D.O.T. S-67-963
 FOUND AND ACCEPTED MAG NAIL. THIS MONUMENT
 APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON
 CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS
 1522.



SCALE:
 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5043

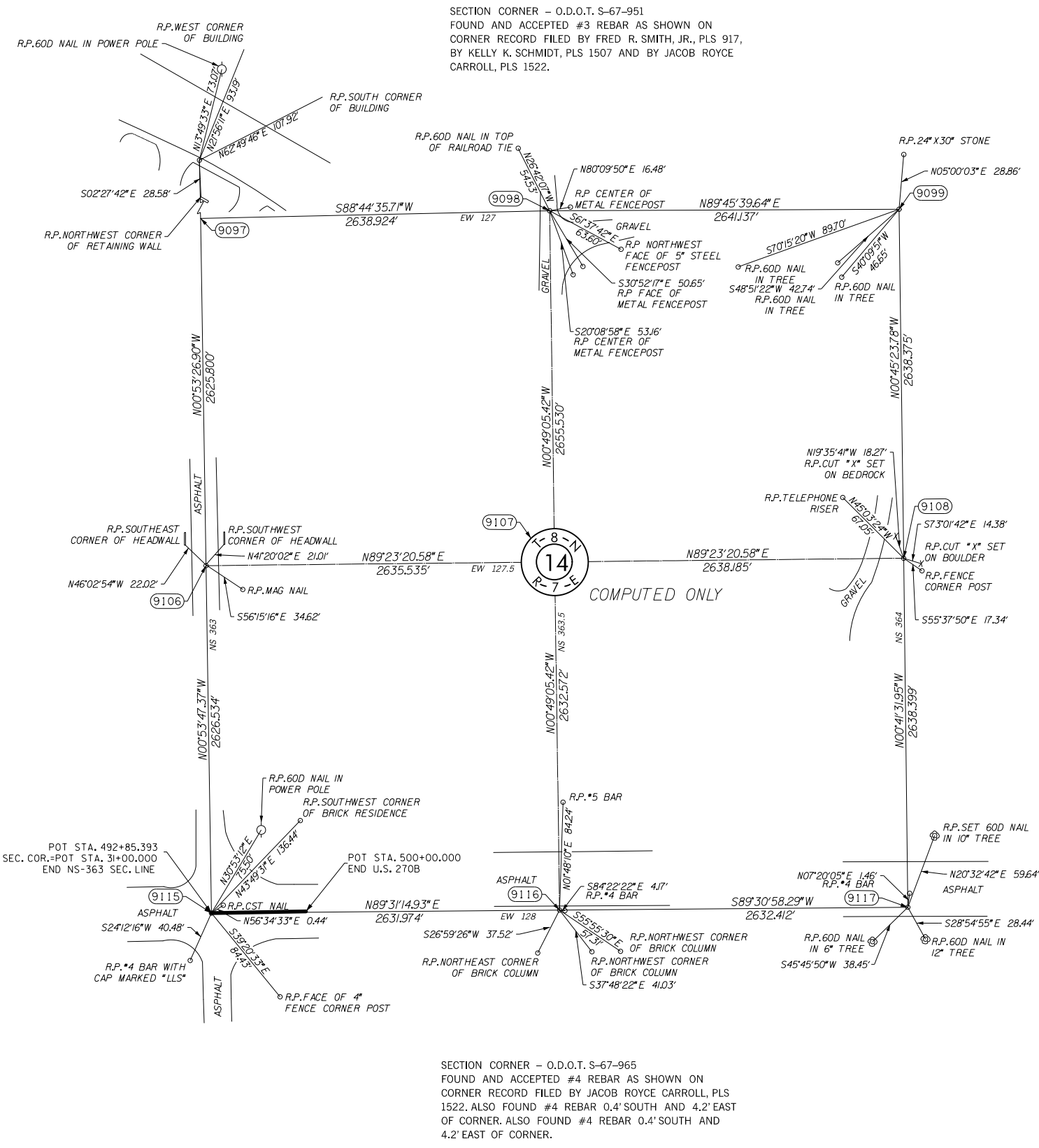
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5044	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-950
 FOUND AND ACCEPTED BROKE OFF MAG NAIL FOR WITNESS CORNER AS SHOWN ON CORNER RECORD FILED BY KELLY K. SCHMIDT, PLS 1507. WITNESS MONUMENTS SET BY FRED R. SMITH, JR., PLS 917 AND BY VIRGIL C. VAUGHN, PLS 405 WERE NOT FOUND.

SECTION CORNER - O.D.O.T. S-67-956
 FOUND AND ACCEPTED MAG NAIL AS SHOWN ON CORNER RECORD FILED BY VIRGIL C. VAUGHN, PLS 405, BY KELLY K. SCHMIDT, PLS 1507 AND BY JACOB ROYCE CARROLL, PLS 1522. THIS MONUMENT APPEARS TO BE AT THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY FRED R. SMITH, JR., PLS 917.

SECTION CORNER - O.D.O.T. S-67-964
 FOUND AND ACCEPTED MAG NAIL AS SHOWN ON CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS 1522. THIS MONUMENT MATCHES THE LOCATION SHOWN ON CORNER RECORD FILED BY VIRGIL C. VAUGHN, PLS 405 AND BY KELLY K. SCHMIDT, PLS 1507.

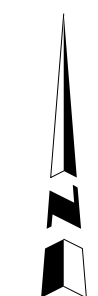


SECTION CORNER - O.D.O.T. S-67-952
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY TIMOTHY G. POLLARD, PLS 1474.

SECTION CORNER - O.D.O.T. S-67-957
 FOUND AND ACCEPTED 2" PIPE. THIS MONUMENT FITS WELL WITH EXISTING OCCUPATION EVIDENCE AND GLO DISTANCES. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-966
 FOUND HOLE IN PAVEMENT AT THE LOCATION SHOWN ON CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS 1522. SET MAG NAIL WITH WASHER STAMPED CA 6975 IN HOLE. THE MONUMENT SHOWN ON CORNER RECORD FILED BY FRED R. SMITH, JR., PLS 917 WAS NOT FOUND.

SECTION CORNER - O.D.O.T. S-67-965
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS 1522. ALSO FOUND #4 REBAR 0.4' SOUTH AND 4.2' EAST OF CORNER. ALSO FOUND #4 REBAR 0.4' SOUTH AND 4.2' EAST OF CORNER.



SCALE: 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT,BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5044

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5045	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-960
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2005 DEPENDENT RESURVEY. THIS MONUMENT WAS SET AT THE LOCATION SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252 AND BY RODGER WHITED, PLS 1298.

SECTION CORNER - O.D.O.T. S-67-967
 FOUND AND ACCEPTED BLM BRASS MONUMENT AS SHOWN ON THE 2005 DEPENDENT RESURVEY. THERE WAS NO CORNER RECORD ON FILE.

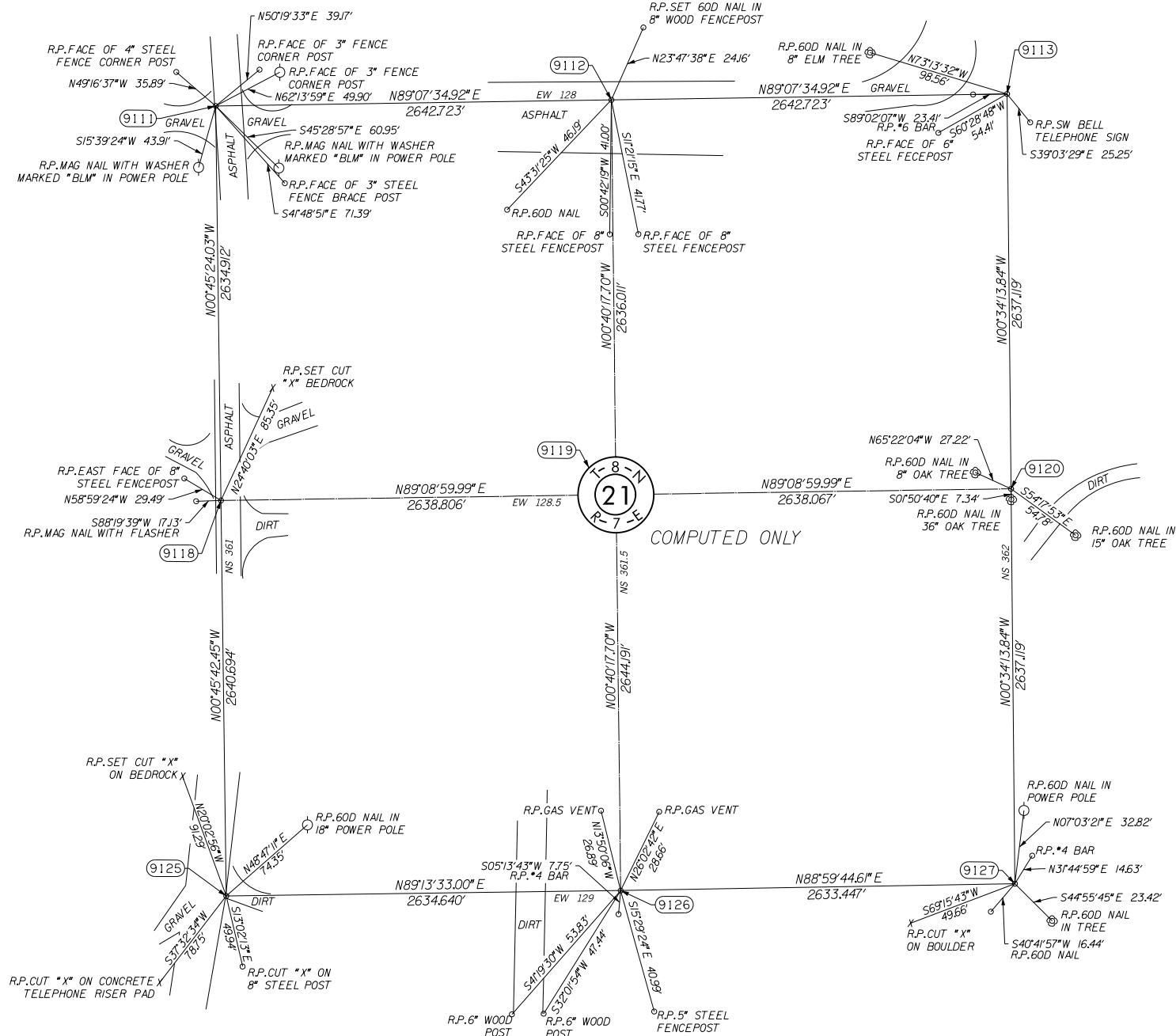
SECTION CORNER - O.D.O.T. S-67-971
 FOUND AND ACCEPTED BLM BRASS CAP AS SHOWN ON THE 2005 DEPENDENT RESURVEY. THIS MONUMENT MATCHES THE LOCATION SHOWN ON CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS 1522 AND BY JOHNNY LEE PACK, PLS 1252.

SECTION CORNER - O.D.O.T. S-67-961
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS SET USING SURROUNDING MONUMENTS AND CHECKS WITH GLO DISTANCES AND EXISTING OCCUPATION LINES.

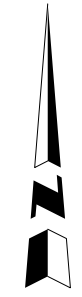
SECTION CORNER - O.D.O.T. S-67-962
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED FROM SWO 1739(1) SURVEY. AFTER DISCUSSION WITH BEARING TREE LAND SURVEYING, IT WAS DETERMINED THAT THE DISTANCE TO CORNER FROM HIGHWAY PI WAS MISREAD. THEREFORE THE PREVIOUS CORNER WAS PULLED AND NEW LOCATION ACCEPTED.

SECTION CORNER - O.D.O.T. S-67-968
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONATE MEASUREMENT. THERE WAS NO CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-973
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252 AND BY JACOB ROYCE CARROLL, PLS 1522. ALSO FOUND #4 REBAR 12.4' NORTH AND 7.7' EAST OF CORNER.



SECTION CORNER - O.D.O.T. S-67-972
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252 AND BY JACOB ROYCE CARROLL, PLS 1522. ALSO FOUND #4 REBAR 7.7' SOUTH AND 0.8' WEST OF CORNER.



SCALE: 1" = 500'

NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT,BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006(07) SHEET NO. 5045

March 31st, 2015

SECTION CORNER - O.D.O.T. S-67-962
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED FROM SWO 1739(1) SURVEY. AFTER DISCUSSION WITH BEARING TREE LAND SURVEYING, IT WAS DETERMINED THAT THE DISTANCE TO CORNER FROM HIGHWAY PI WAS MISREAD. THEREFORE THE PREVIOUS CORNER WAS PULLED AND NEW LOCATION ACCEPTED.

SECTION CORNER - O.D.O.T. S-67-968
 SET #4 REBAR WITH CAP STAMPED CA 6975. THIS MONUMENT WAS RE-ESTABLISHED USING PROPORTIONATE MEASUREMENT. THERE WAS NO CORNER RECORD ON FILE.

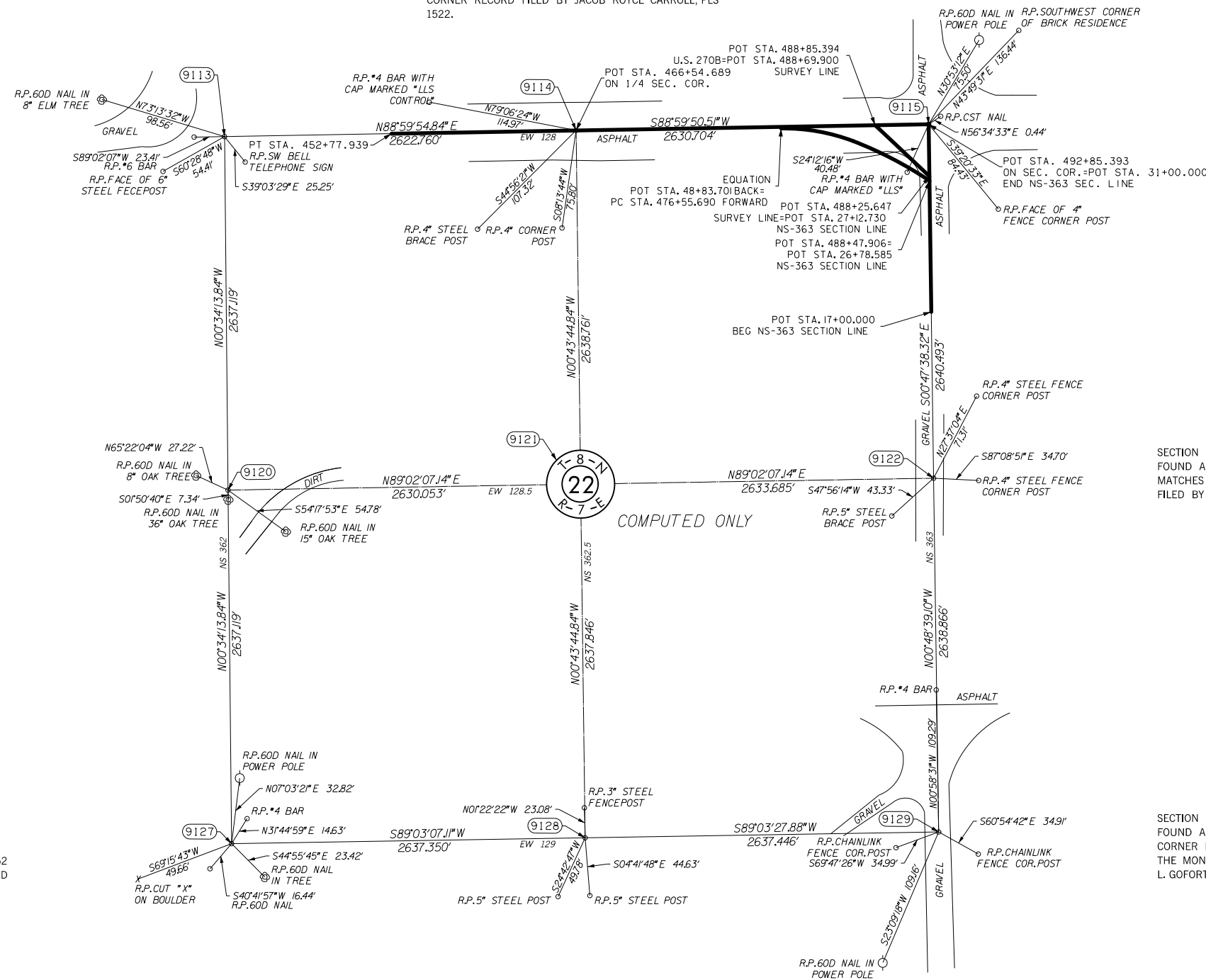
SECTION CORNER - O.D.O.T. S-67-973
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY JOHNNY LEE PACK, PLS 1252 AND BY JACOB ROYCE CARROLL, PLS 1522. ALSO FOUND #4 REBAR 12.4' NORTH AND 7.7' EAST OF CORNER.

SECTION CORNER - O.D.O.T. S-67-963
 FOUND AND ACCEPTED MAG NAIL. THIS MONUMENT APPEARS TO BE IN THE SAME LOCATION AS SHOWN ON CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS 1522.

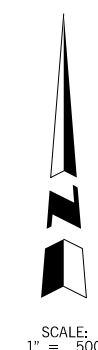
SECTION CORNER - O.D.O.T. S-67-964
 FOUND AND ACCEPTED MAG NAIL AS SHOWN ON CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS 1522. THIS MONUMENT MATCHES THE LOCATION SHOWN ON CORNER RECORD FILED BY VIRGIL C. VAUGHN, PLS 405 AND BY KELLY K. SCHMIDT, PLS 1507.

SECTION CORNER - O.D.O.T. S-67-969
 FOUND AND ACCEPTED #4 REBAR. THIS MONUMENT MATCHES THE LOCATION SHOWN ON CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS 1522.

SECTION CORNER - O.D.O.T. S-67-975
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY DAVID F. HEAVNER, PLS 964. THE MONUMENT ON CORNER RECORD FILED BY BOBBY L. GOFORTH, PLS 340 WAS NOT FOUND.



SECTION CORNER - O.D.O.T. S-67-974
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS 1522.



NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

March 31st, 2015

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION			
SURVEY DATA SHEET			
PLS	SS		
DRAWN	SK		
CHECKED	SS		
APPROVED	SS		
CREW	JH,JA,LB,RT BS,PT,BB	SWO 4879 (1)	PROJECT NO. 21006(07) SHEET NO. 5046

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(11)	2016	5047	5047

DESCRIPTION	REVISIONS	DATE

SECTION CORNER - O.D.O.T. S-67-965
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON
 CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS
 1522. ALSO FOUND #4 REBAR 0.4' SOUTH AND 4.2' EAST
 OF CORNER.

SECTION CORNER - O.D.O.T. S-67-964
 FOUND AND ACCEPTED MAG NAIL AS SHOWN ON
 CORNER RECORD FILED BY JACOB ROYCE CARROLL, PLS
 1522. THIS MONUMENT MATCHES THE LOCATION SHOWN
 ON CORNER RECORD FILED BY VIRGIL C. VAUGHN, PLS
 405 AND BY KELLY K. SCHMIDT, PLS 1507.

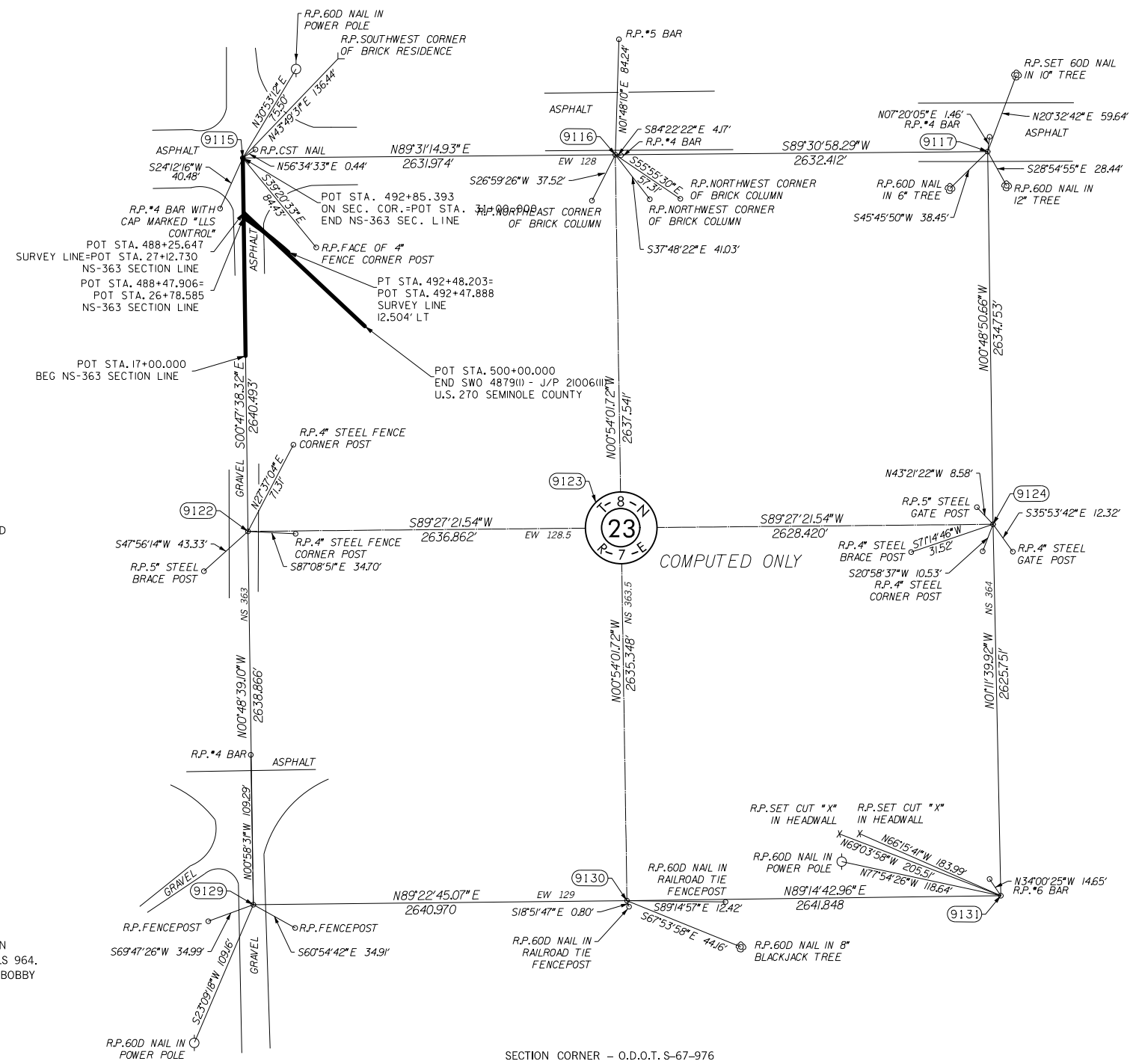
SECTION CORNER - O.D.O.T. S-67-969
 FOUND AND ACCEPTED #4 REBAR. THIS MONUMENT
 MATCHES THE LOCATION SHOWN ON CORNER RECORD
 FILED BY JACOB ROYCE CARROLL, PLS 1522.

SECTION CORNER - O.D.O.T. S-67-975
 FOUND AND ACCEPTED #4 REBAR AS SHOWN ON
 CORNER RECORD FILED BY DAVID F. HEAVNER, PLS 964.
 THE MONUMENT ON CORNER RECORD FILED BY BOBBY
 L. GOFORTH, PLS 340 WAS NOT FOUND.

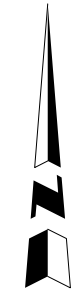
SECTION CORNER - O.D.O.T. S-67-966
 FOUND HOLE IN PAVEMENT AT THE LOCATION SHOWN
 ON CORNER RECORD FILED BY JACOB ROYCE CARROLL,
 PLS 1522. SET MAG NAIL WITH WASHER STAMPED CA
 6975 IN HOLE. THE MONUMENT SHOWN ON CORNER
 RECORD FILED BY FRED R. SMITH, JR., PLS 917 WAS NOT
 FOUND.

SECTION CORNER - O.D.O.T. S-67-970
 FOUND AND ACCEPTED #3 REBAR. THIS MONUMENT FITS
 EXISTING OCCUPATION EVIDENCE. THERE WAS NO
 CORNER RECORD ON FILE.

SECTION CORNER - O.D.O.T. S-67-977
 FOUND AND ACCEPTED #5 REBAR AS SHOWN ON
 CORNER RECORD FILED BY DAVID F. HEAVNER, PLS 964.
 ALSO FOUND #6 REBAR 12.1' NORTH AND 8.2' WEST OF
 CORNER.



SECTION CORNER - O.D.O.T. S-67-976
 FOUND AND ACCEPTED #4 REBAR. THIS MONUMENT FITS
 EXISTING OCCUPATION EVIDENCE. THERE WAS NO
 CORNER RECORD ON FILE.



SCALE:
 1" = 500'

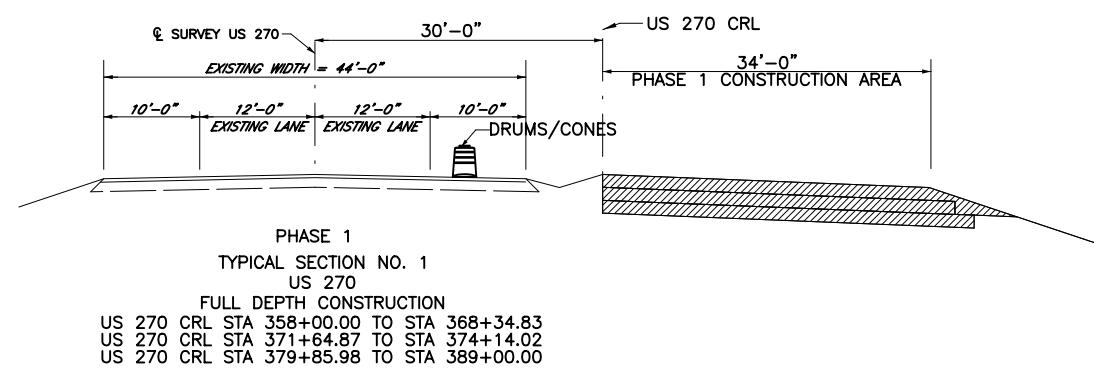
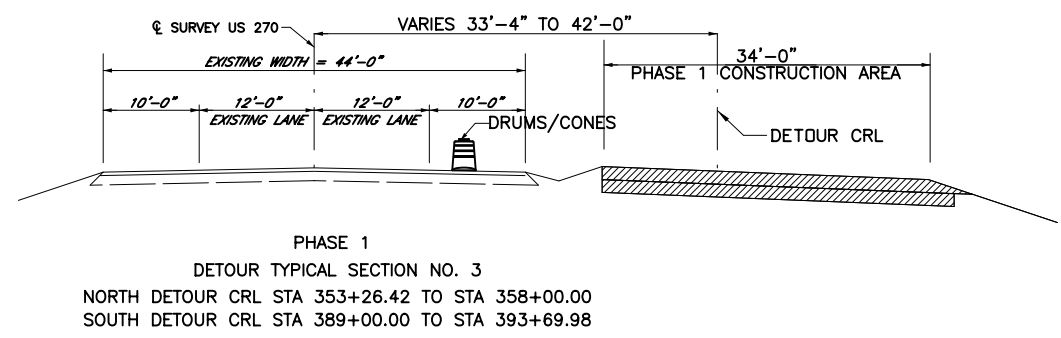
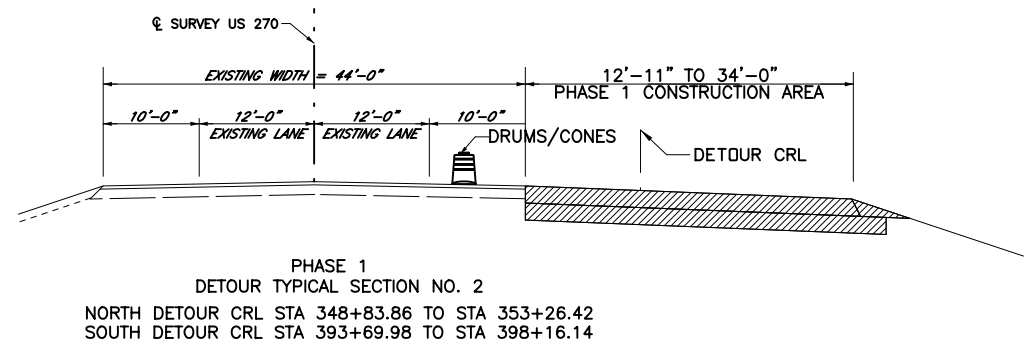
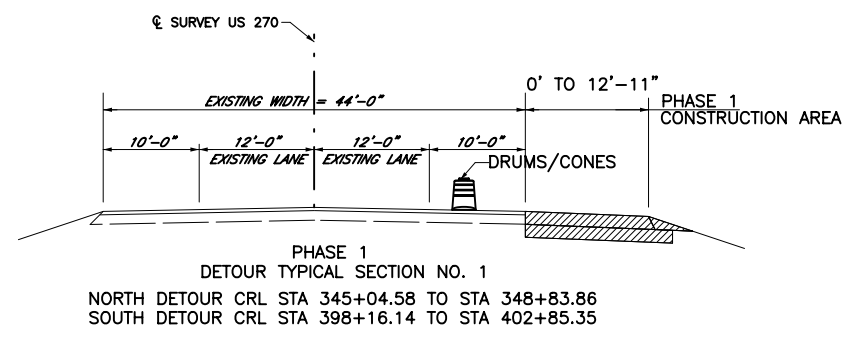
NOTE: REFERENCES SHOWN ARE NOT TO SCALE.

OKLAHOMA DEPARTMENT OF TRANSPORTATION SURVEY DIVISION	
PLS	SS
DRAWN	SK
CHECKED	SS
APPROVED	SS
CREW	JH,JA,LB,RT BS,PT,BB

SURVEY DATA SHEET

SWO 4879 (1) PROJECT NO. 21006 (07) SHEET NO. 5047

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T001	44
REVISIONS					
NO.	DATE	DESCRIPTION			



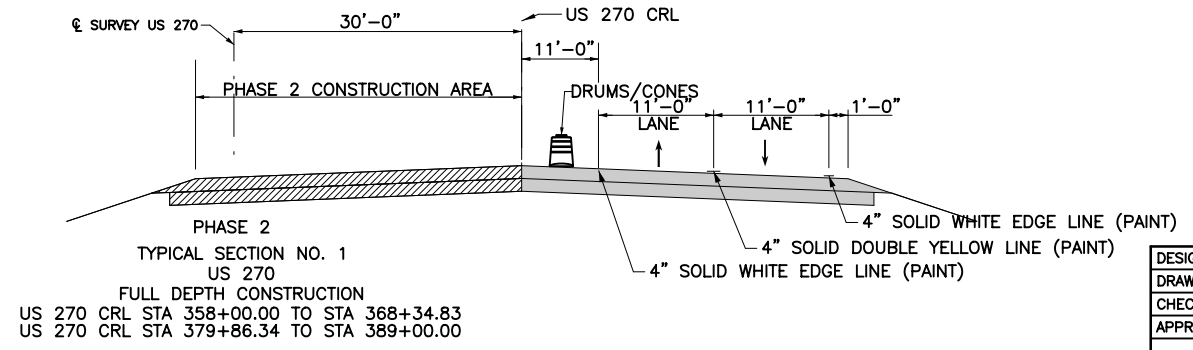
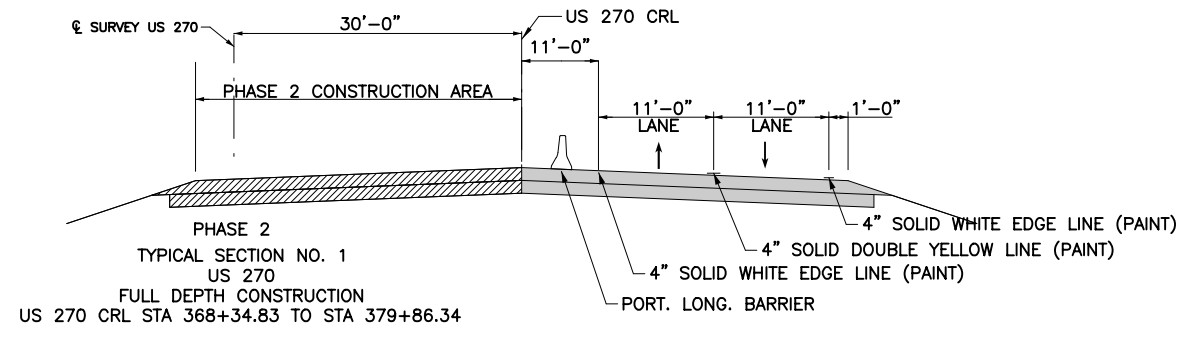
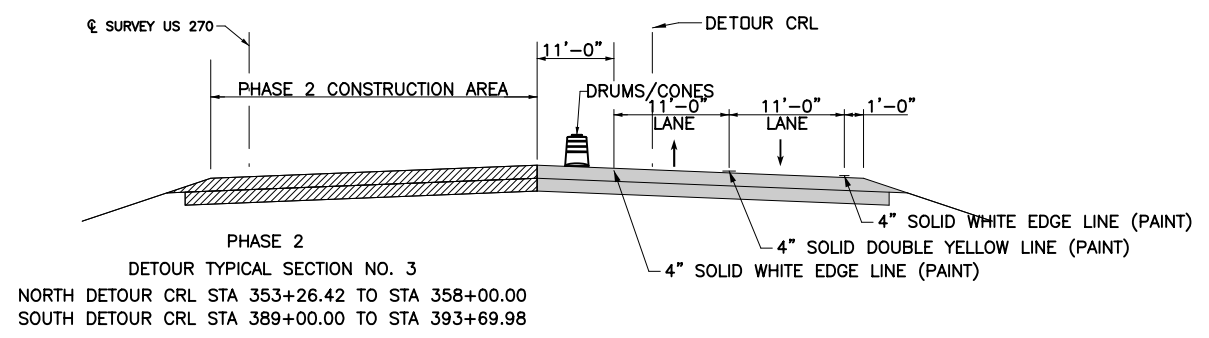
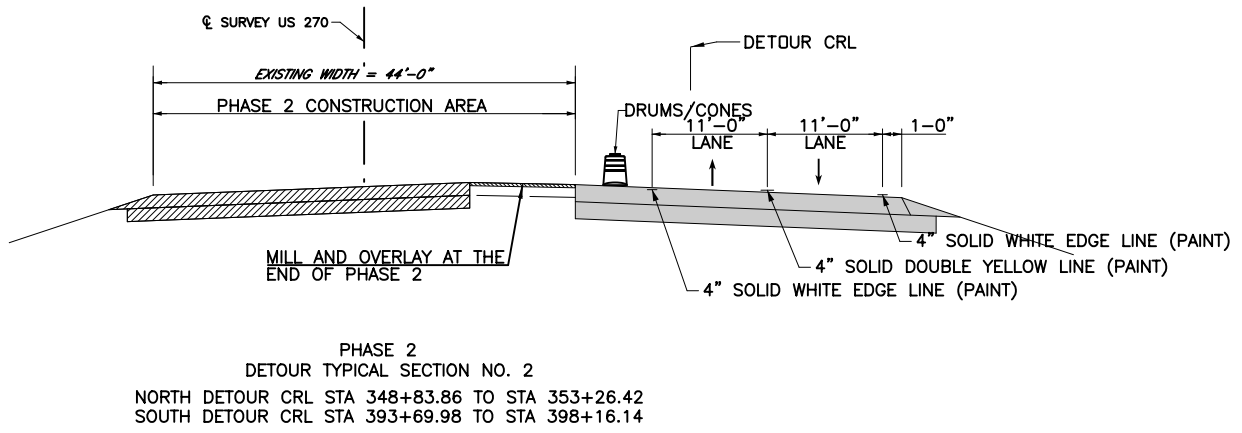
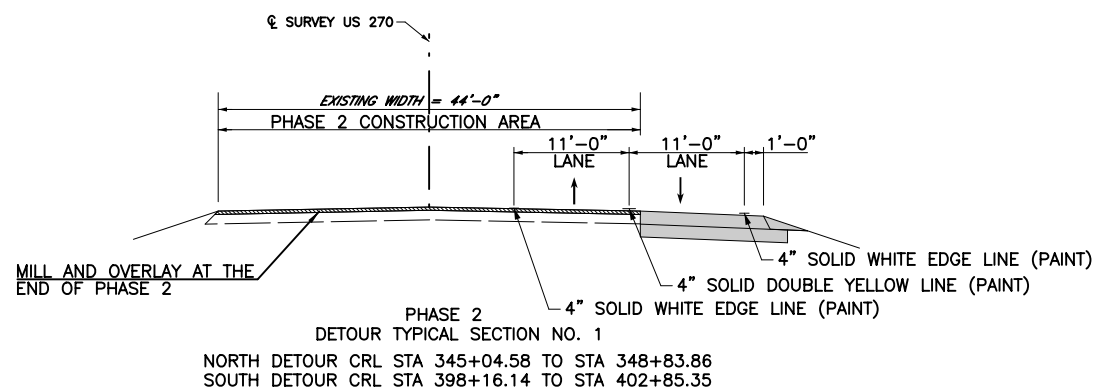
LEGEND

	EXISTING PAVEMENT
	PERMANENT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION PREVIOUS PHASE

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18	TCP TYPICAL SECTIONS	
CHECKED: CPY 06/18	PHASE 1	
APPRVD: CPY 06/18	SHEET 1 OF 2	
STATE JOB PIECE NO: 21006(07)		SHEET NO. T001

2:48:25 PM
 6/5/2018
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.03 TCF/21006TCPYP01.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T002	44
REVISIONS					
NO.	DATE	DESCRIPTION			



LEGEND

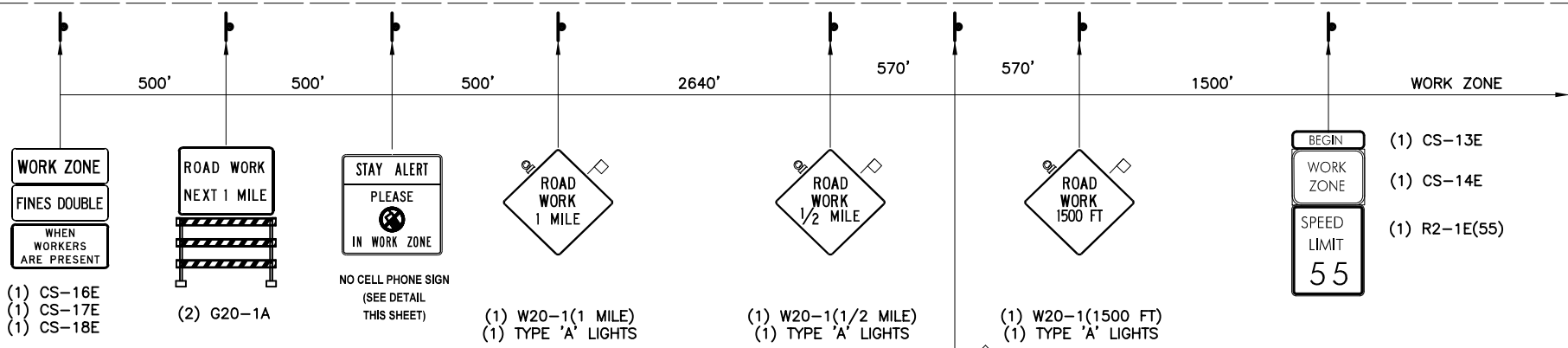
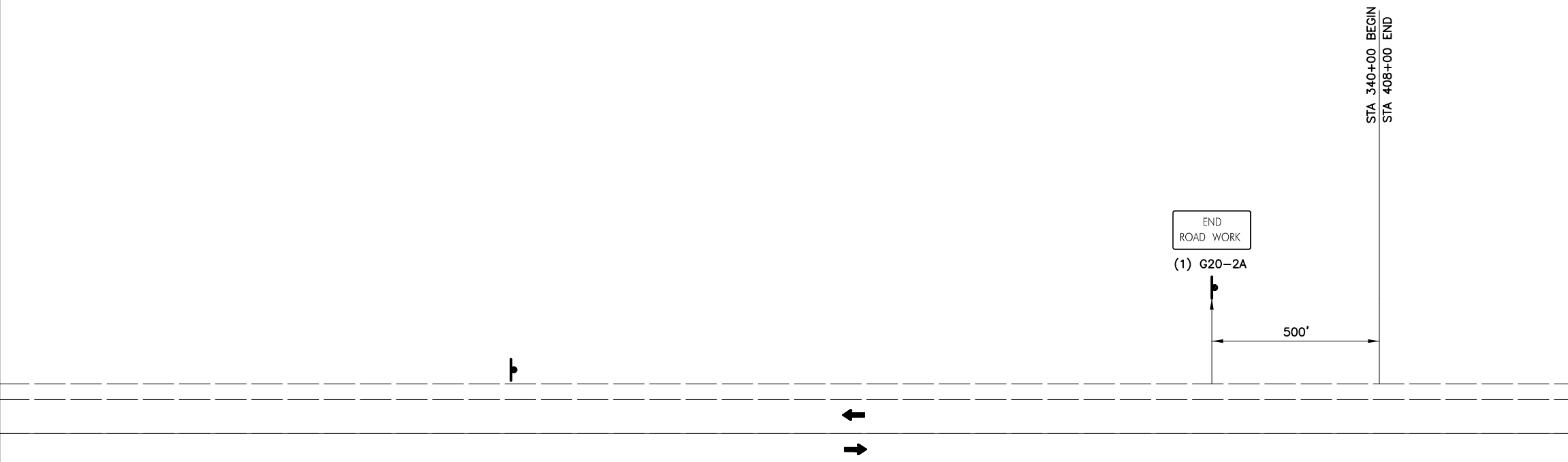
	EXISTING PAVEMENT
	PERMANENT CONSTRUCTION THIS PHASE
	COMPLETED CONSTRUCTION PREVIOUS PHASE

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18	TCP TYPICAL SECTIONS PHASE 2	
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
		SHEET 2 OF 2 SHEET NO. T002
STATE JOB PIECE NO: 21006(07)		

2:48:28 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 6/5/2018
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.03 TCP/21006TCPYP02.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T003	44
REVISIONS					
NO.	DATE	DESCRIPTION			

6/5/2018 2:48:32 PM cpyb&w_ANSIB.tbl p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.03 TCP\21006TCPAW.dgn



NO CELL PHONE SIGN

- LEGEND, SYMBOL & BORDER - BLACK NON-REFLECTIVE
BACKGROUND - YELLOW REFLECTIVE
- ▲ LEGEND, SYMBOL & BORDER - BLACK NON-REFLECTIVE
BACKGROUND - ORANGE REFLECTIVE

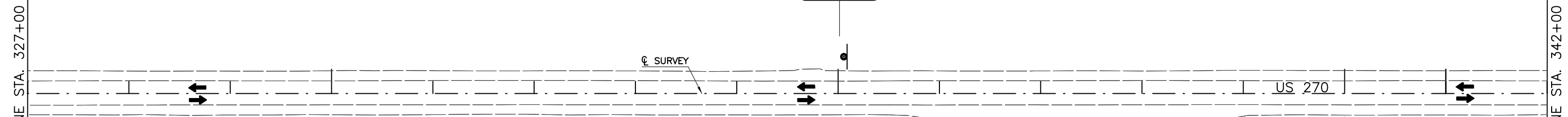
DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18	TCP - ADVANCE WARNING SIGN	
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
STATE JOB PIECE NO: 21006(07)		SHEET 1 OF 1 SHEET NO. T003



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T004	44
REVISIONS					
NO.	DATE	DESCRIPTION			

SE/4, SEC. 8
T-8-N, R-7-E

END ROAD WORK (1) G20-2A

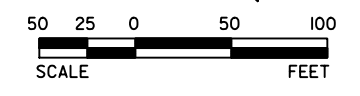
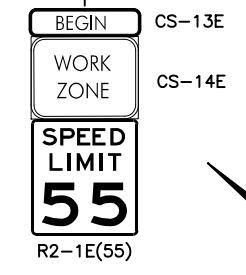


LEGEND

- SIGN
- DRUMS
- ▲ CHANNELIZER CONES
- ▨ CONSTRUCTION THIS PHASE
- ▩ MILL AND OVERLAY COMPLETED AFTER THIS PHASE WITH USE OF FLAGGER AT CONTRACTOR'S DISCRETION
- ▧ CONSTRUCTION PREVIOUS PHASE
- ▤ PAVEMENT REMOVAL
- ➔ DIRECTION OF TRAFFIC FLOW
- ▬ IMPACT ATTENUATOR
- ▬ PORTABLE LONGITUDINAL BARRIER

NOTES:

1. TRAFFIC CONTROL BASED ON 55 MPH THROUGH CONSTRUCTION ZONE.
2. SEE CONSTRUCTION SEQUENCE TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.
3. CONSTRUCTION SEQUENCING AS SHOWN IS FOR ESTIMATING PURPOSES. CONTRACTOR SHALL SUBMIT A DETAILED WORK PLAN TO THE ENGINEER AT THE PRE-WORK CONFERENCE.
4. TEMPORARY DRAINAGE SHALL BE MAINTAINED AT ALL EXISTING DRAINAGE STRUCTURES DURING ALL PHASES OF CONSTRUCTION.

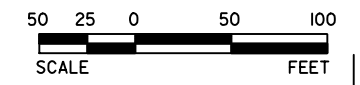
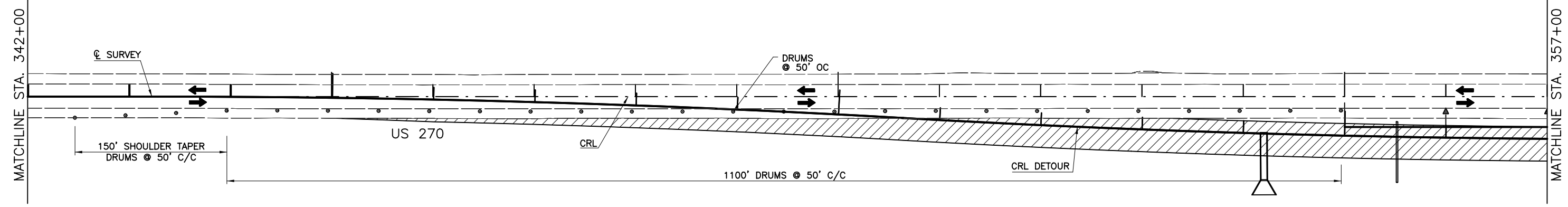


SE/4, SEC. 8
T-8-N, R-7-E

1 4" SOLID DOUBLE YELLOW LANE LINE (PAINT)

2 4" SOLID WHITE EDGE LINE (PAINT)

SE/4, SEC. 8
T-8-N, R-7-E



SE/4, SEC. 8
T-8-N, R-7-E

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18	TRAFFIC CONTROL PLAN PHASE 1	
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
CP&Y		SHEET 1 OF 6 SHEET NO. T004
STATE JOB PIECE NO: 21006(07)		

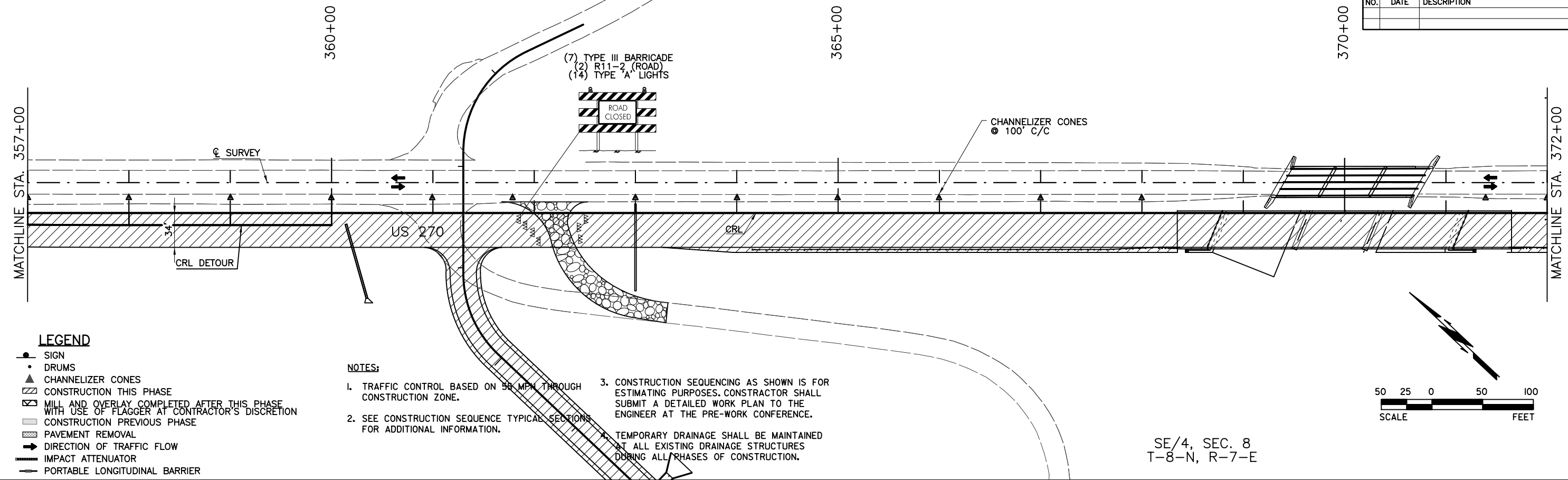
2:48:38 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.03 TCF/21006TCP01.dgn

SE/4 SEC. 8
T-8-N, R-7-E

SW/4, SEC. 9
T-8-N, R-7-E

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T005	44

REVISIONS		
NO.	DATE	DESCRIPTION



LEGEND

- SIGN
- DRUMS
- ▲ CHANNELIZER CONES
- ▨ CONSTRUCTION THIS PHASE
- ▩ MILL AND OVERLAY COMPLETED AFTER THIS PHASE WITH USE OF FLAGGER AT CONTRACTOR'S DISCRETION
- ▧ CONSTRUCTION PREVIOUS PHASE
- ▤ PAVEMENT REMOVAL
- ➔ DIRECTION OF TRAFFIC FLOW
- ▬ IMPACT ATTENUATOR
- ▬ PORTABLE LONGITUDINAL BARRIER

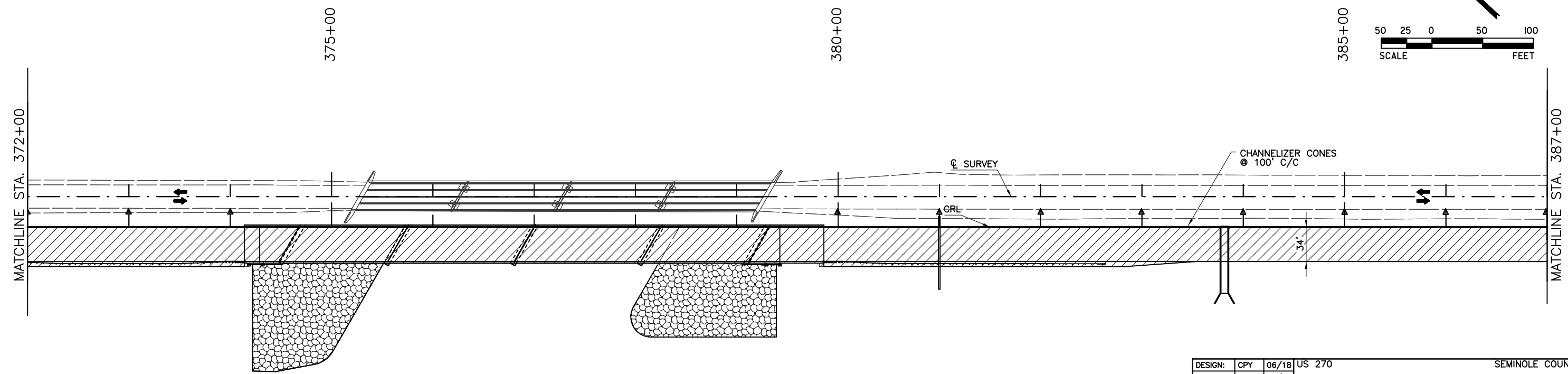
NOTES:

1. TRAFFIC CONTROL BASED ON 55 MPH THROUGH CONSTRUCTION ZONE.
2. SEE CONSTRUCTION SEQUENCE TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.
3. CONSTRUCTION SEQUENCING AS SHOWN IS FOR ESTIMATING PURPOSES. CONTRACTOR SHALL SUBMIT A DETAILED WORK PLAN TO THE ENGINEER AT THE PRE-WORK CONFERENCE.
4. TEMPORARY DRAINAGE SHALL BE MAINTAINED AT ALL EXISTING DRAINAGE STRUCTURES DURING ALL PHASES OF CONSTRUCTION.

- 1 4" SOLID DOUBLE YELLOW LANE LINE (PAINT)
- 2 4" SOLID WHITE EDGE LINE (PAINT)

NW/4, SEC. 16
T-8-N, R-7-E

NW/4, SEC. 16
T-8-N, R-7-E



NW/4, SEC. 16
T-8-N, R-7-E

NW/4, SEC. 16
T-8-N, R-7-E

DESIGN:	CPY	06/18	US 270
DRAWN:	CPY	06/18	
CHECKED:	CPY	06/18	
APPRVD:	CPY	06/18	

**TRAFFIC CONTROL PLAN
PHASE 1**

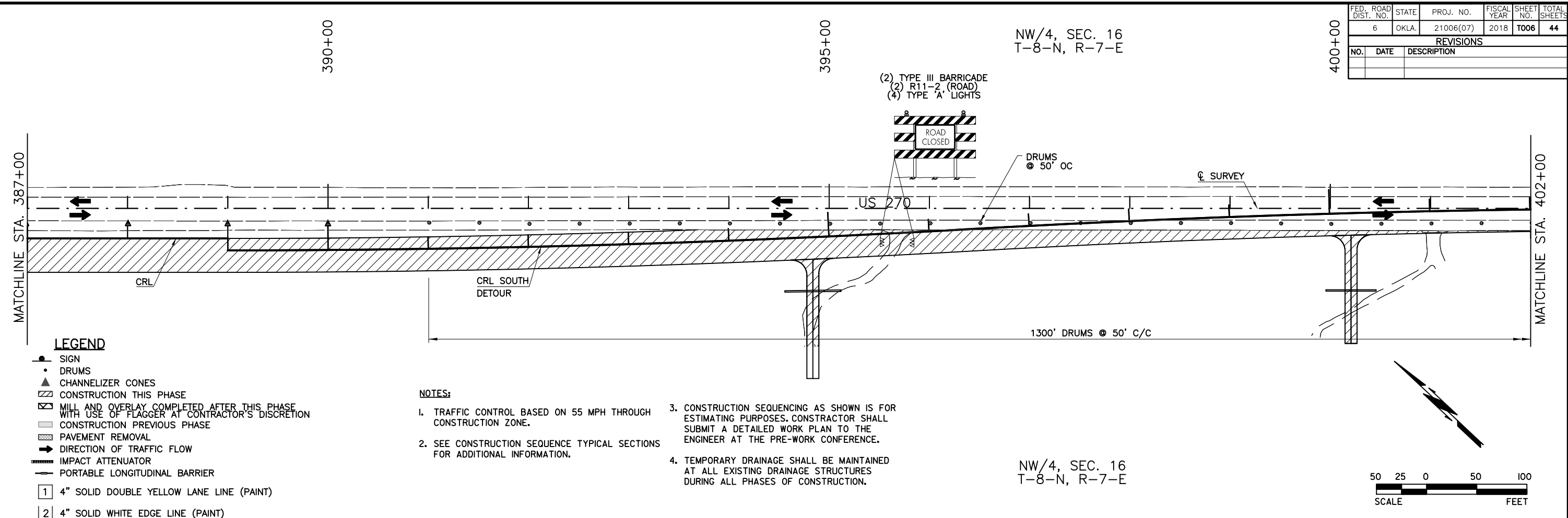


STATE JOB PIECE NO: 21006(07)

SEMINOLE COUNTY
SHEET 2 OF 6
SHEET NO. T005

6/5/2018 2:48:43 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.03 TCF/21006TCP02.dgn

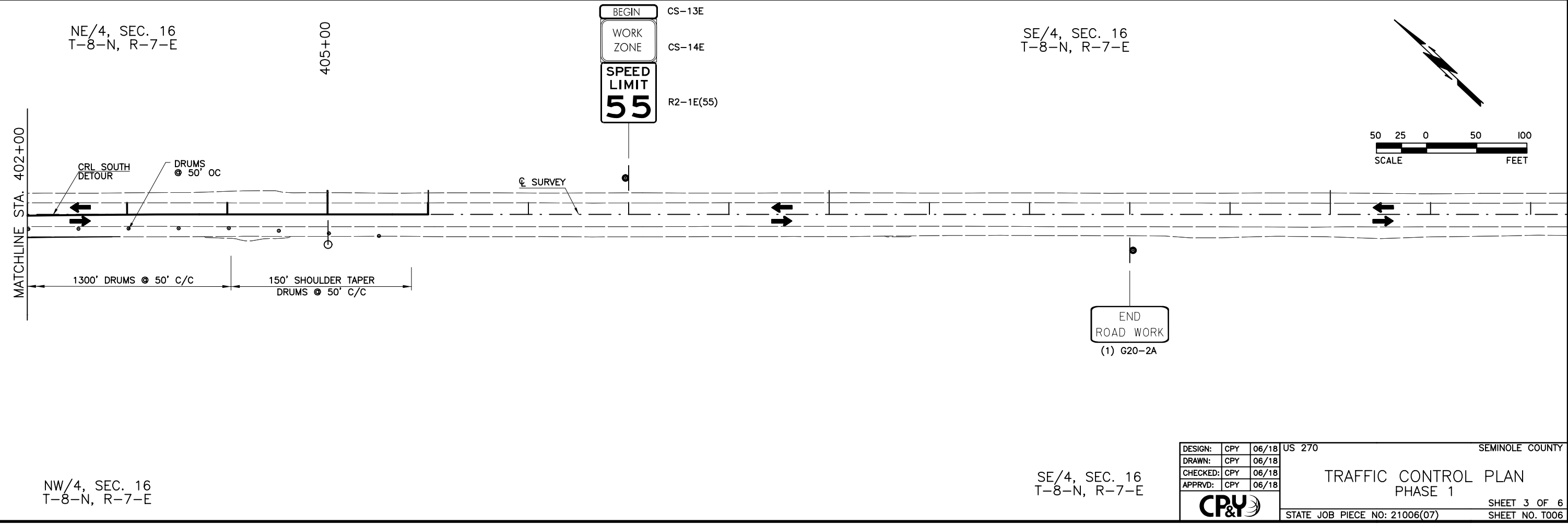
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T006	44
REVISIONS					
NO.	DATE	DESCRIPTION			



- LEGEND**
- SIGN
 - DRUMS
 - ▲ CHANNELIZER CONES
 - ▨ CONSTRUCTION THIS PHASE
 - ▩ MILL AND OVERLAY COMPLETED AFTER THIS PHASE WITH USE OF FLAGGER AT CONTRACTOR'S DISCRETION
 - ▧ CONSTRUCTION PREVIOUS PHASE
 - ▤ PAVEMENT REMOVAL
 - ➔ DIRECTION OF TRAFFIC FLOW
 - ▬ IMPACT ATTENUATOR
 - ▬ PORTABLE LONGITUDINAL BARRIER
 - 1 4" SOLID DOUBLE YELLOW LANE LINE (PAINT)
 - 2 4" SOLID WHITE EDGE LINE (PAINT)

- NOTES:**
- TRAFFIC CONTROL BASED ON 55 MPH THROUGH CONSTRUCTION ZONE.
 - SEE CONSTRUCTION SEQUENCE TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.
 - CONSTRUCTION SEQUENCING AS SHOWN IS FOR ESTIMATING PURPOSES. CONTRACTOR SHALL SUBMIT A DETAILED WORK PLAN TO THE ENGINEER AT THE PRE-WORK CONFERENCE.
 - TEMPORARY DRAINAGE SHALL BE MAINTAINED AT ALL EXISTING DRAINAGE STRUCTURES DURING ALL PHASES OF CONSTRUCTION.

NW/4, SEC. 16
T-8-N, R-7-E



NE/4, SEC. 16
T-8-N, R-7-E

405+00

MATCHLINE STA. 402+00

1300' DRUMS @ 50' C/C

150' SHOULDER TAPER
DRUMS @ 50' C/C

- BEGIN CS-13E
- WORK ZONE CS-14E
- SPEED LIMIT 55 R2-1E(55)

SE/4, SEC. 16
T-8-N, R-7-E

END
ROAD WORK
(1) G20-2A

NW/4, SEC. 16
T-8-N, R-7-E

SE/4, SEC. 16
T-8-N, R-7-E

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18	TRAFFIC CONTROL PLAN PHASE 1	
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
CP&Y		STATE JOB PIECE NO: 21006(07)
		SHEET 3 OF 6 SHEET NO. T006

2:48:48 PM 6/5/2018
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.03 TCF\21006TCP03.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T007	44
REVISIONS					
NO.	DATE	DESCRIPTION			

SE/4, SEC. 8
T-8-N, R-7-E

(1) G20-2A
END
ROAD WORK

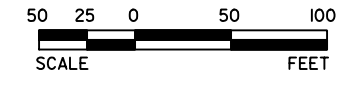
BEGIN (1) CS-13E
WORK ZONE (1) CS-14E
SPEED LIMIT 55 (1) R2-1E(55)

LEGEND

- SIGN
- DRUMS
- CHANNELIZER CONES
- CONSTRUCTION THIS PHASE
- MILL AND OVERLAY COMPLETED AFTER THIS PHASE WITH USE OF FLAGGER AT CONTRACTOR'S DISCRETION
- CONSTRUCTION PREVIOUS PHASE
- PAVEMENT REMOVAL
- DIRECTION OF TRAFFIC FLOW
- IMPACT ATTENUATOR
- PORTABLE LONGITUDINAL BARRIER
- 4" SOLID DOUBLE YELLOW LANE LINE (PAINT)
- 4" SOLID WHITE EDGE LINE (PAINT)

NOTES:

1. TRAFFIC CONTROL BASED ON 55 MPH THROUGH CONSTRUCTION ZONE.
2. SEE CONSTRUCTION SEQUENCE TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.
3. CONSTRUCTION SEQUENCING AS SHOWN IS FOR ESTIMATING PURPOSES. CONTRACTOR SHALL SUBMIT A DETAILED WORK PLAN TO THE ENGINEER AT THE PRE-WORK CONFERENCE.
4. TEMPORARY DRAINAGE SHALL BE MAINTAINED AT ALL EXISTING DRAINAGE STRUCTURES DURING ALL PHASES OF CONSTRUCTION.



SE/4, SEC. 8
T-8-N, R-7-E

SE/4, SEC. 8
T-8-N, R-7-E

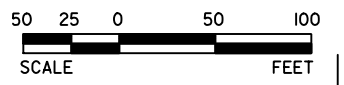
(1) W1-4E(L)

(2) TYPE III BARRICADE
(2) R11-2, (ROAD)
(4) TYPE 'A' LIGHTS

(2) TYPE III BARRICADE
(2) R11-2, (ROAD)
(4) TYPE 'A' LIGHTS

(2) TYPE III BARRICADE
(2) R11-2, (ROAD)
(4) TYPE 'A' LIGHTS

(2) TYPE III BARRICADE
(2) R11-2, (ROAD)
(4) TYPE 'A' LIGHTS



(1) W1-4E(R)

DESIGN:	CPY	06/18	US 270
DRAWN:	CPY	06/18	
CHECKED:	CPY	06/18	
APPRVD:	CPY	06/18	

TRAFFIC CONTROL PLAN
PHASE 2

STATE JOB PIECE NO: 21006(07)

SEMINOLE COUNTY
SHEET 4 OF 6
SHEET NO. T007

SE/4, SEC. 8
T-8-N, R-7-E

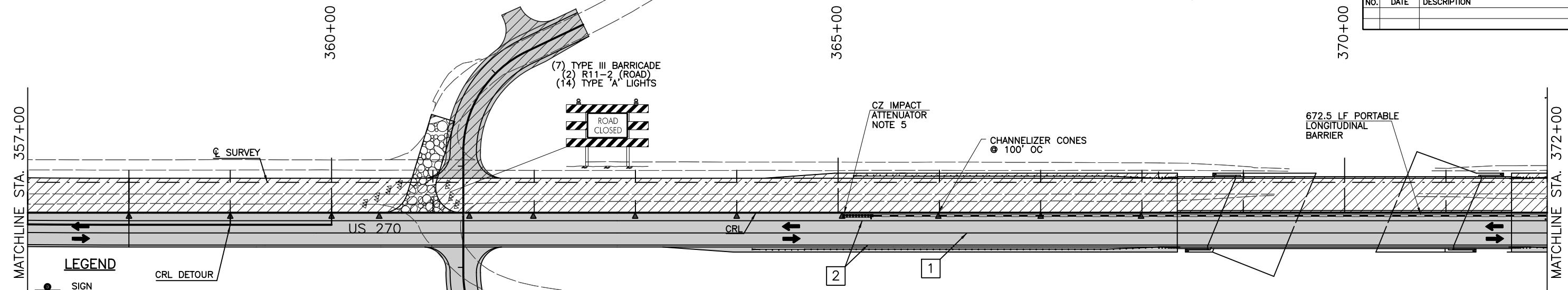
6/5/2018 2:48:52 PM
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.03 TCF\21006TCP04.dgn

SE/4 SEC. 8
T-8-N, R-7-E

SW/4, SEC. 9
T-8-N, R-7-E

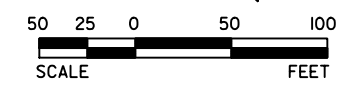
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T008	44

REVISIONS		
NO.	DATE	DESCRIPTION



- LEGEND**
- SIGN
 - DRUMS
 - ▲ CHANNELIZER CONES
 - ▨ CONSTRUCTION THIS PHASE
 - ▩ MILL AND OVERLAY COMPLETED AFTER THIS PHASE WITH USE OF FLAGGER AT CONTRACTOR'S DISCRETION
 - ▧ CONSTRUCTION PREVIOUS PHASE
 - ▤ PAVEMENT REMOVAL
 - ➔ DIRECTION OF TRAFFIC FLOW
 - ▬ IMPACT ATTENUATOR
 - ▬ PORTABLE LONGITUDINAL BARRIER
 - 1 4" SOLID DOUBLE YELLOW LANE LINE (PAINT)
 - 2 4" SOLID WHITE EDGE LINE (PAINT)

- NOTES:**
- TRAFFIC CONTROL BASED ON 55 MPH THROUGH CONSTRUCTION ZONE.
 - SEE CONSTRUCTION SEQUENCE TYPICAL SECTIONS FOR ADDITIONAL INFORMATION.
 - CONSTRUCTION SEQUENCING AS SHOWN IS FOR ESTIMATING PURPOSES. CONTRACTOR SHALL SUBMIT A DETAILED WORK PLAN TO THE ENGINEER AT THE PRE-WORK CONFERENCE.
 - TEMPORARY DRAINAGE SHALL BE MAINTAINED AT ALL EXISTING DRAINAGE STRUCTURES DURING ALL PHASES OF CONSTRUCTION.
 - IF CLEAR ZONE CAN BE MET BY FLARING MEDIAN BARRIER, THE CONST. ZONE IMPACT ATTENUATORS MAY BE OMITTED. SEE FLAIR RATE TABLE AND CLEAR ZONE FOR CONSTRUCTION AREAS ON ODOT STD. TCS2-1.

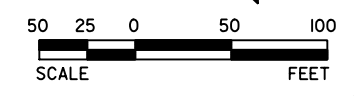
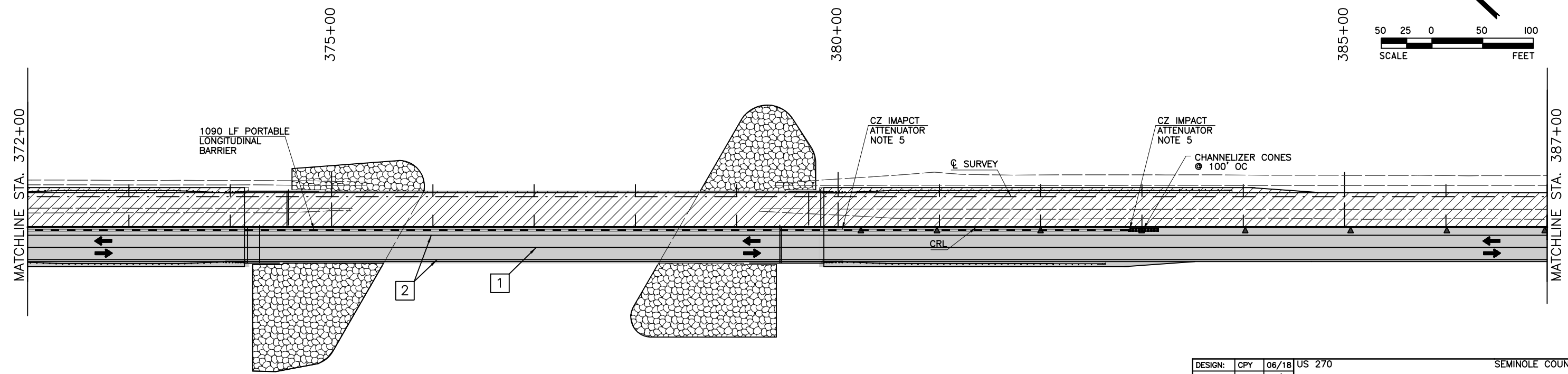


SE/4, SEC. 8
T-8-N, R-7-E

2 4" SOLID WHITE EDGE LINE (PAINT)

NW/4, SEC. 16
T-8-N, R-7-E

NW/4, SEC. 16
T-8-N, R-7-E



NW/4, SEC. 16
T-8-N, R-7-E

NW/4, SEC. 16
T-8-N, R-7-E

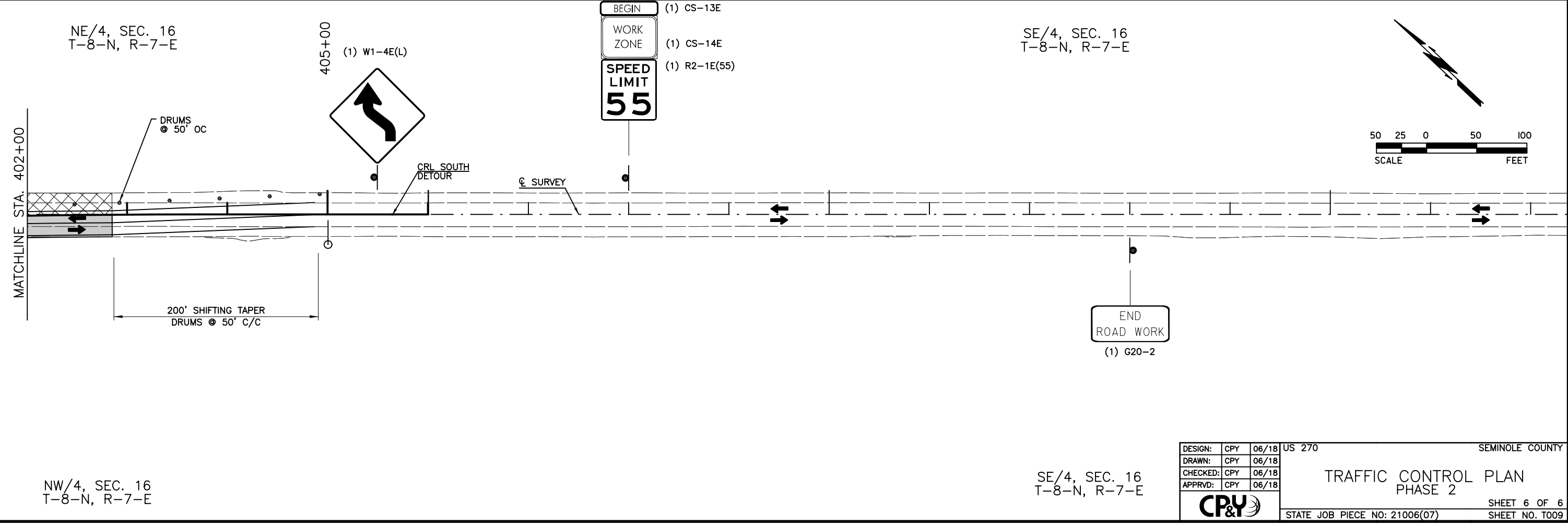
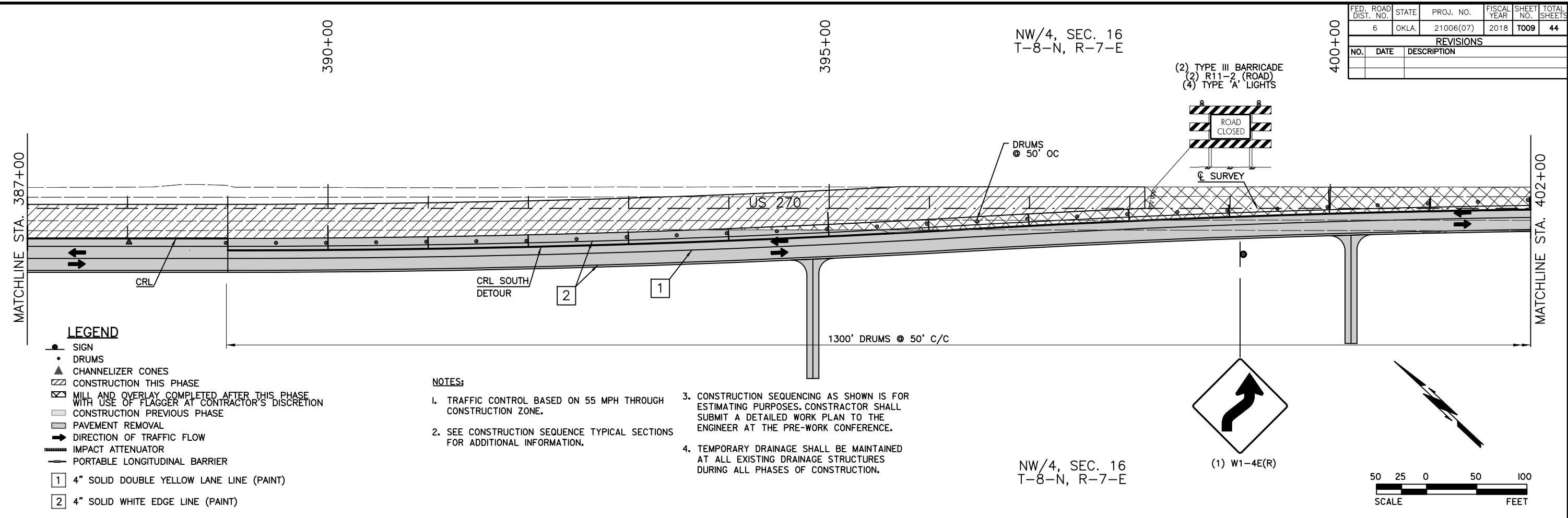
DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		

**TRAFFIC CONTROL PLAN
PHASE 2**

STATE JOB PIECE NO: 21006(07) SHEET 5 OF 6
SHEET NO. T008

6/5/2018 2:48:57 PM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.03 TCF\21006TCP05.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T009	44
REVISIONS					
NO.	DATE	DESCRIPTION			



2:49:02 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.03 TCF\21006TCP06.dgn

DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
CP&Y		
TRAFFIC CONTROL PLAN PHASE 2		SHEET 6 OF 6
STATE JOB PIECE NO: 21006(07)		SHEET NO. T009

NW/4, SEC. 16
T-8-N, R-7-E

SE/4, SEC. 16
T-8-N, R-7-E

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T010	44
REVISIONS					
NO.	DATE	DESCRIPTION			

SE/4, SEC. 8
T-8-N, R-7-E

345+00

350+00

355+00

MATCHLINE STA. 357+00

4" DOUBLE YELLOW 2600 LF
12" YELLOW GORE STRIPING (TYP) 1700 LF
4" WHITE LANE LINE (TYP) 1300 LF
4" WHITE LANE LINE (TYP) 1300 LF

☉ SURVEY

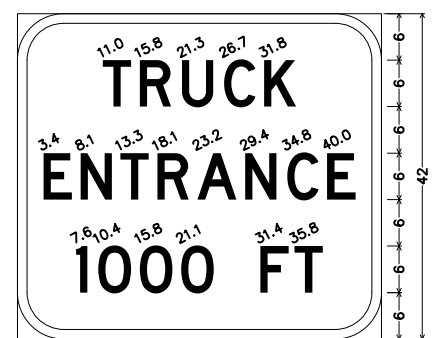
US 270

SPECIAL SIGN NO. 1



3.0" Radius, 1.0" Border, White on Green;
[NEW LIMA] D; Standard Arrow Custom 12.0" X 8.1" O;

SPECIAL SIGN NO. 3



6.0" Radius, 1.3" Border, Black on White;
[TRUCK] White D; [ENTRANCE] White D;
[1000 FT] White D;

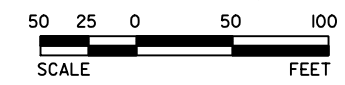
SPECIAL SIGN NO. 2



3.0" Radius, 1.0" Border, White on Green;
[NEW LIMA] D; Standard Arrow Custom 12.0" X 8.1" O;



(1) W2-1E



SE/4, SEC. 8
T-8-N, R-7-E

NEW LIMA SIGN
SPECIAL SIGN NO. 1

SE/4 SEC. 8
T-8-N, R-7-E

360+00

365+00

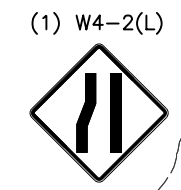
370+00

MATCHLINE STA. 372+00

MATCHLINE STA. 357+00

12" YELLOW GORE STRIPING (TYP) 540 LF
4" DOUBLE YELLOW 3000 LF

TRAFFIC STRIPE (ARROWS)
3 EA



(1) W4-2(L)

(1) R2-1E(65)



NEW LIMA SIGN
SPECIAL SIGN NO. 1

(1) R3-7(R)



SW/4, SEC. 9
T-8-N, R-7-E

(1) W2-1E



OM-3L
(REMOVE)



(1) W11-10E
SPECIAL SIGN NO. 3



OM-3R
(REMOVE)



4" WHITE LANE LINE (TYP) 3000 LF
4" WHITE DASH (TYP) 250 LF
4" WHITE DASH (TYP) 500 LF

US 270

NARROW BRIDGE
(REMOVE)

KEEP OUR LAND GRAND SIGN
(REMOVE)

TRAFFIC STRIPE (SYMBOLS, WORDS, ETC.)
3 EA

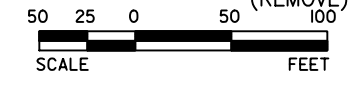
4" WHITE LANE LINE (TYP) 300 LF

4" WHITE DASH (TYP) 1400 LF

OM-3R
(REMOVE)



NW/4, SEC. 16
T-8-N, R-7-E



(1) R2-1E(65)

NOTE:
ALL STRIPING SHALL MEET AND MATCH
EXISTING STRIPING AT STRIPING LIMITS.

SE/4, SEC. 8
T-8-N, R-7-E

DESIGN:	CPY	06/18	US 270
DRAWN:	CPY	06/18	
CHECKED:	CPY	06/18	
APPRVD:	CPY	06/18	



SIGNING AND STRIPING

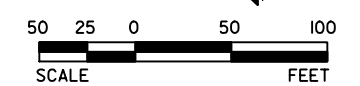
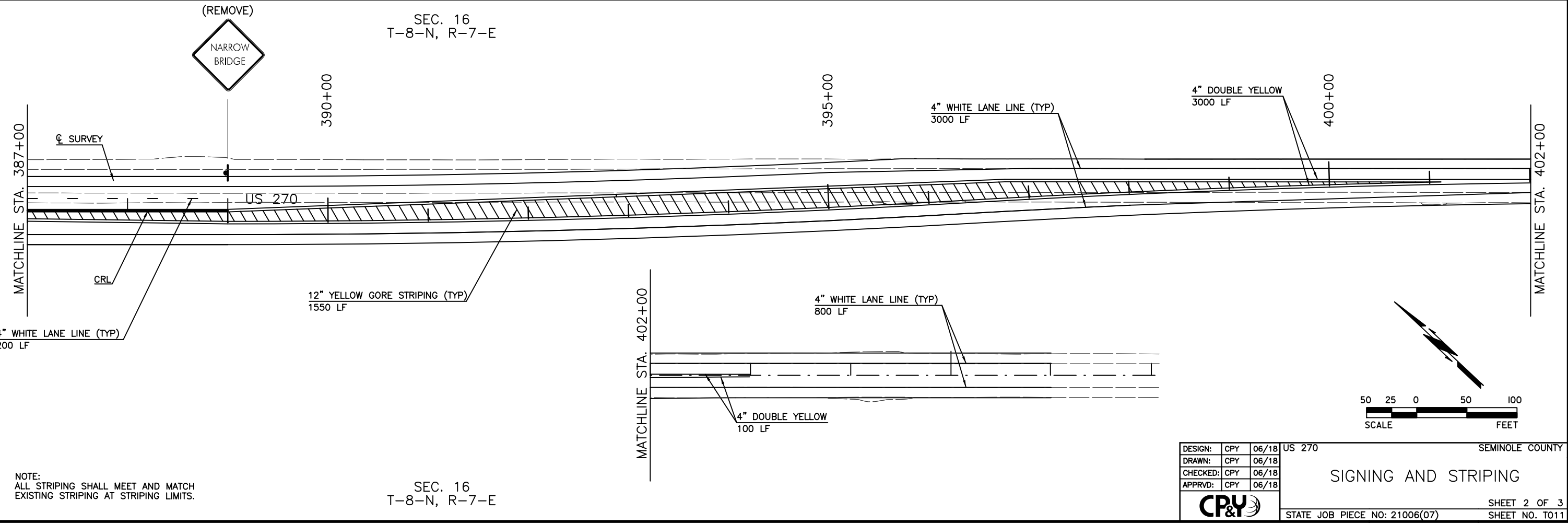
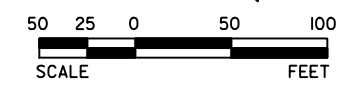
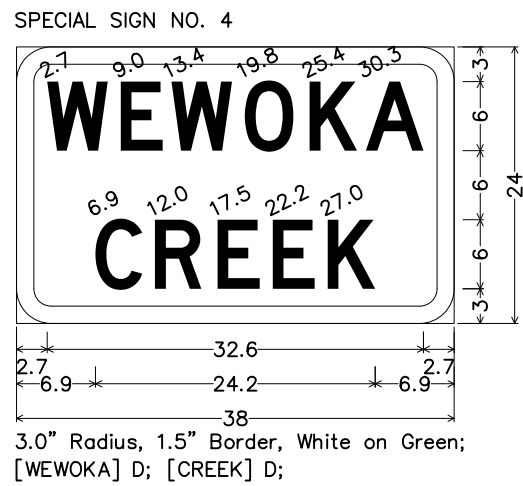
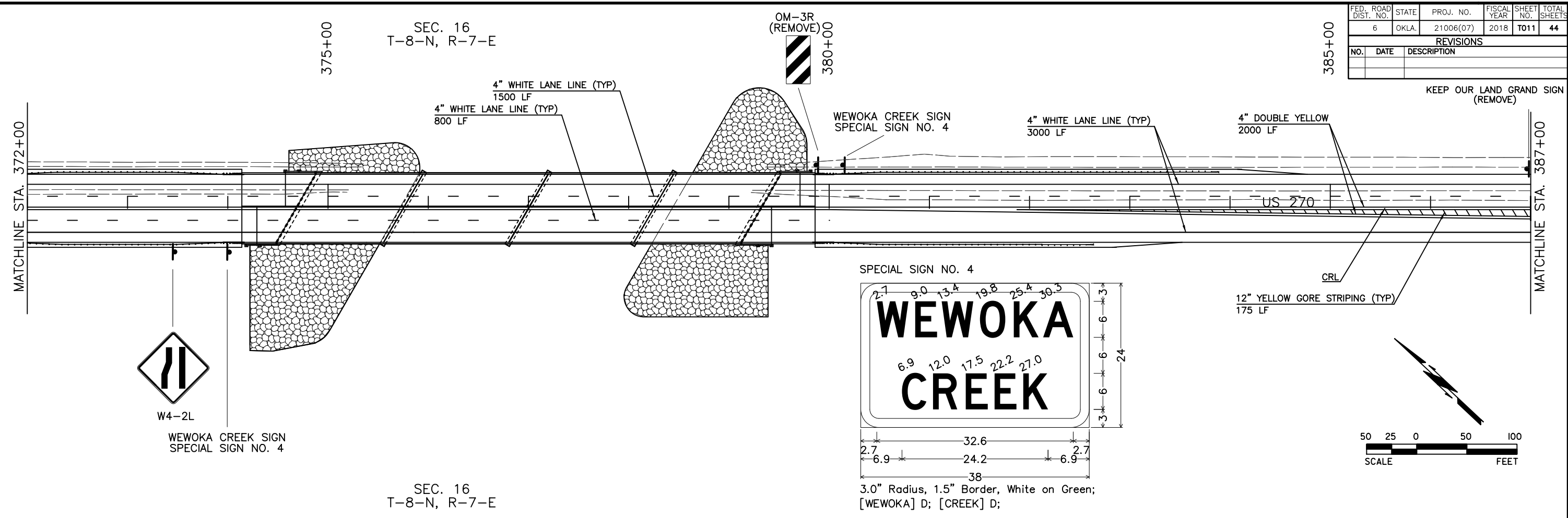
STATE JOB PIECE NO: 21006(07)

SHEET 1 OF 3
SHEET NO. T010

bmcanelly 6/5/2018 2:49:07 PM
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.xx Signing and Striping\21006S&S01.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T011	44
REVISIONS					
NO.	DATE	DESCRIPTION			

KEEP OUR LAND GRAND SIGN
(REMOVE)



DESIGN: CPY 06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY 06/18		
CHECKED: CPY 06/18		
APPRVD: CPY 06/18		
CP&Y		SIGNING AND STRIPING
STATE JOB PIECE NO: 21006(07)		SHEET 2 OF 3 SHEET NO. T011

NOTE:
ALL STRIPING SHALL MEET AND MATCH
EXISTING STRIPING AT STRIPING LIMITS.

2:49:12 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.30 Cut Sheets\8.3.xx Signing and Striping\21006S&S02.dgn

SEC. 8
T-8-N, R-7-E

SEC. 9
T-8-N, R-7-E

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	T012	44
REVISIONS					
NO.	DATE	DESCRIPTION			

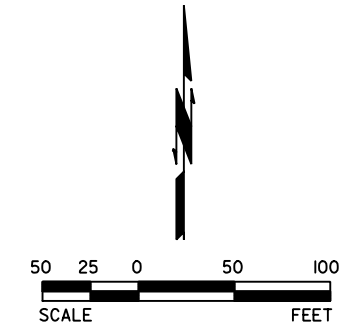


4" DOUBLE YELLOW
210 LF

24" STOP BAR

24" STOP BAR

4" DOUBLE YELLOW
620 LF



2:49:17 PM
 cpyb&w_ANSIB.tbl
 6/5/2018
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.30 Cut Sheets/8.3.xx Signing and Striping/21006S&S03.dgn

NOTE:
ALL STRIPING SHALL MEET AND MATCH
EXISTING STRIPING AT STRIPING LIMITS.

SEC. 17
T-8-N, R-7-E

SEC. 16
T-8-N, R-7-E

DESIGN: CPY	06/18	US 270	SEMINOLE COUNTY
DRAWN: CPY	06/18		
CHECKED: CPY	06/18		
APPRVD: CPY	06/18		

CP&Y

SIGNING AND STRIPING

STATE JOB PIECE NO: 21006(07)

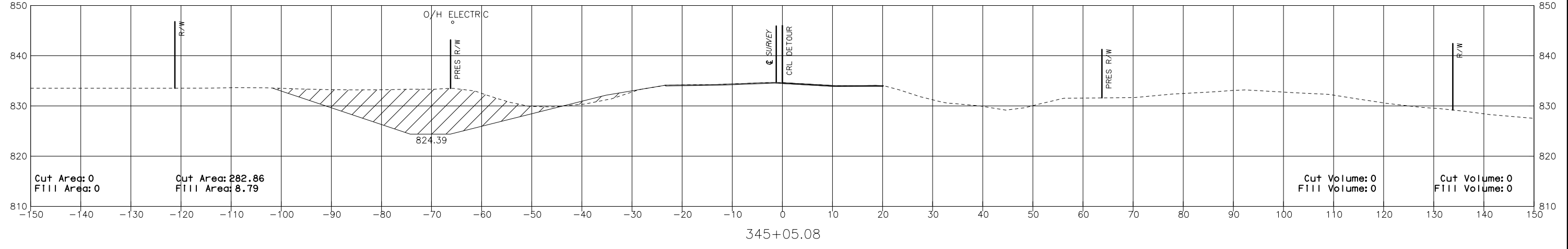
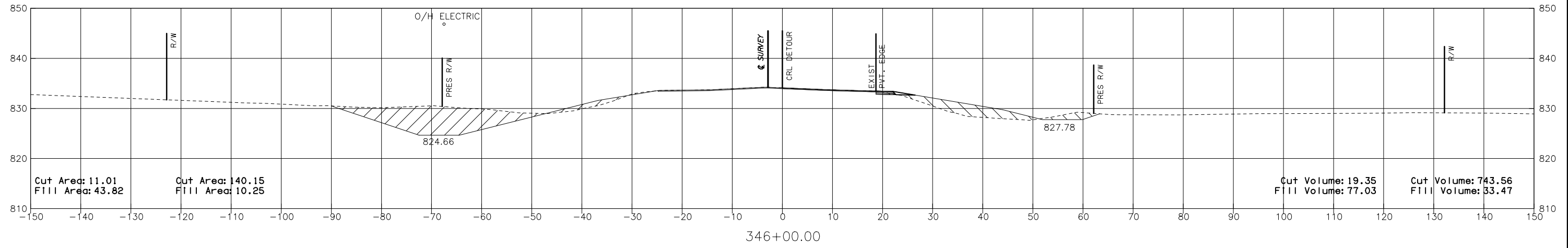
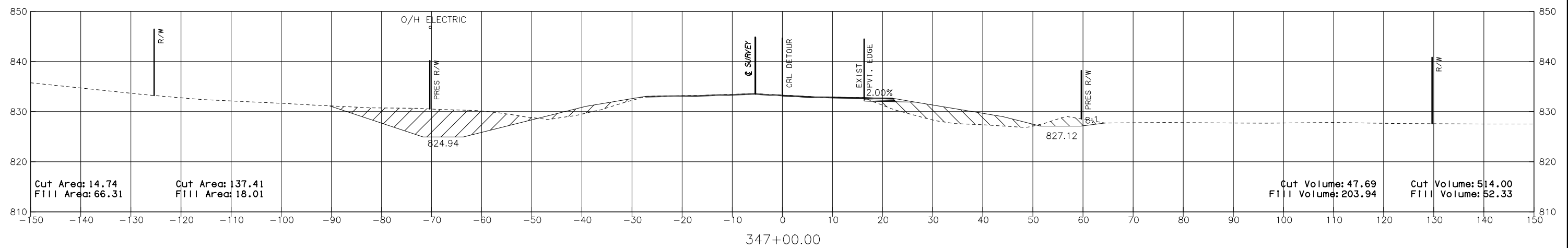
SHEET 3 OF 3
SHEET NO. T012

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X001	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



BEGIN INCIDENTAL CONSTRUCTION

CROSS SECTION BASED ON CRL NORTH DETOUR

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018	US 270 NORTH DETOUR	
CHECKED: CPY 2018	CROSS SECTIONS	
APPRVD: CPY 2018	STATE JOB PIECE NO: 21006(07) SHEET NO. X001	

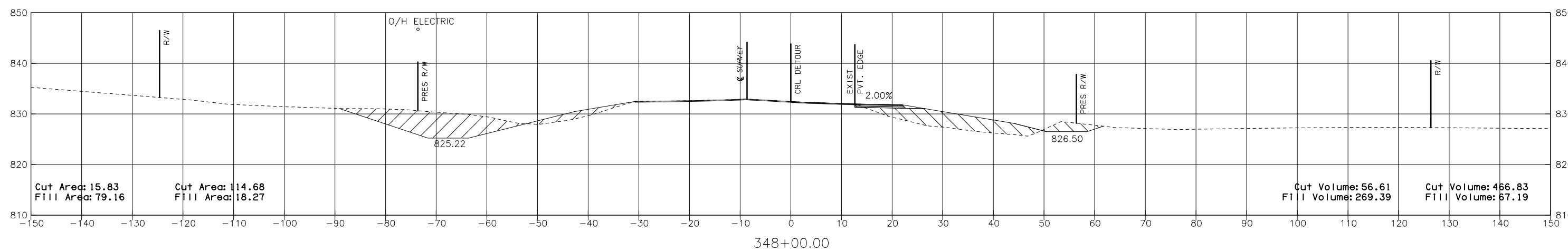
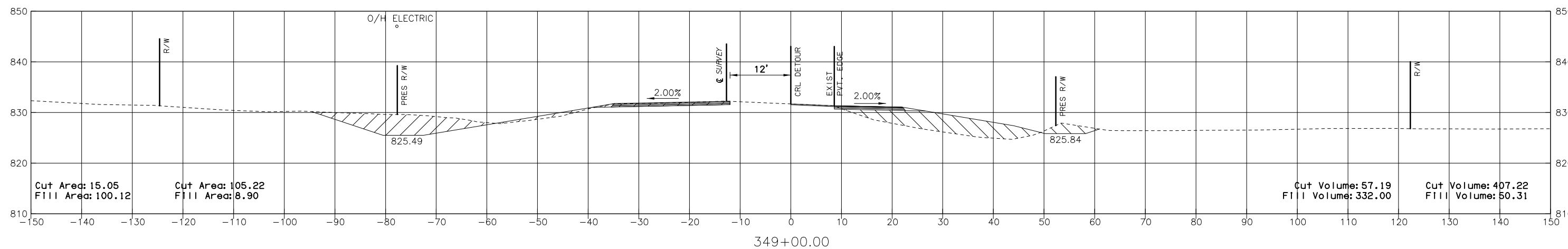
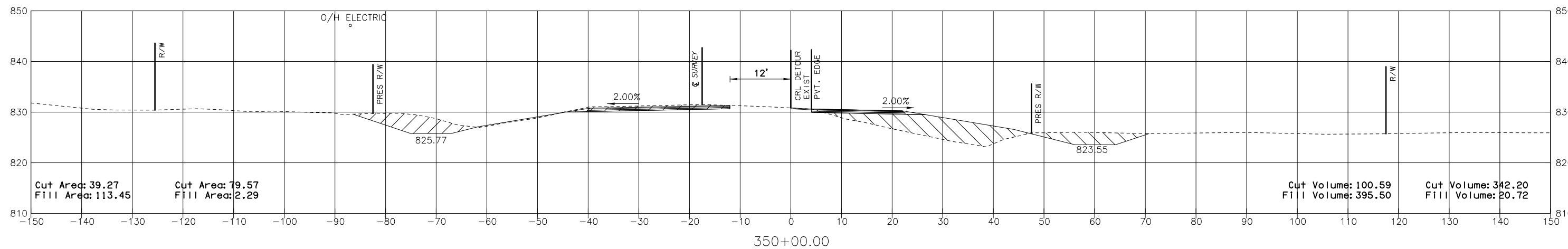
6/5/2018 2:49:22 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.10 Base Files\21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X002	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



CROSS SECTION BASED ON CRL NORTH DETOUR

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018	US 270 NORTH DETOUR CROSS SECTIONS	
CHECKED: CPY 2018		
APPRVD: CPY 2018		
		STATE JOB PIECE NO: 21006(07) SHEET NO. X002

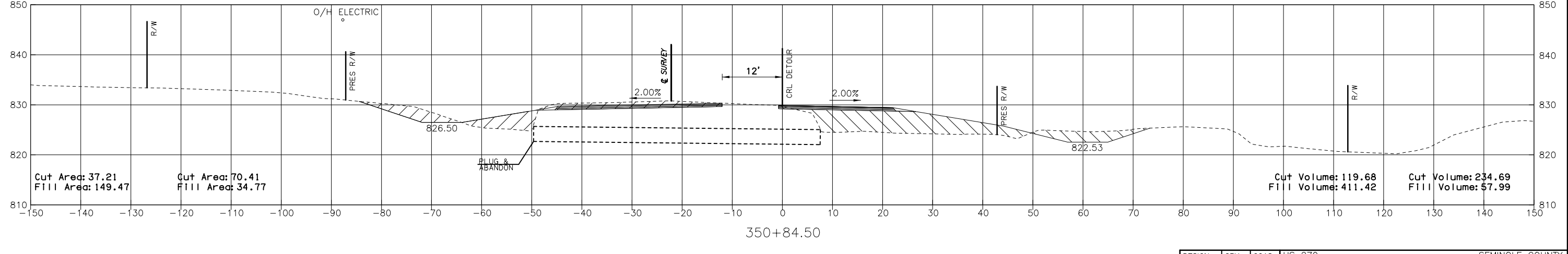
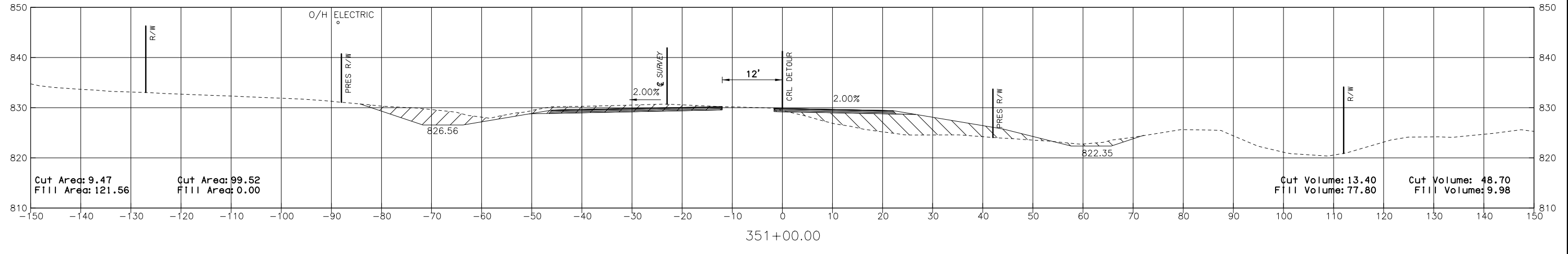
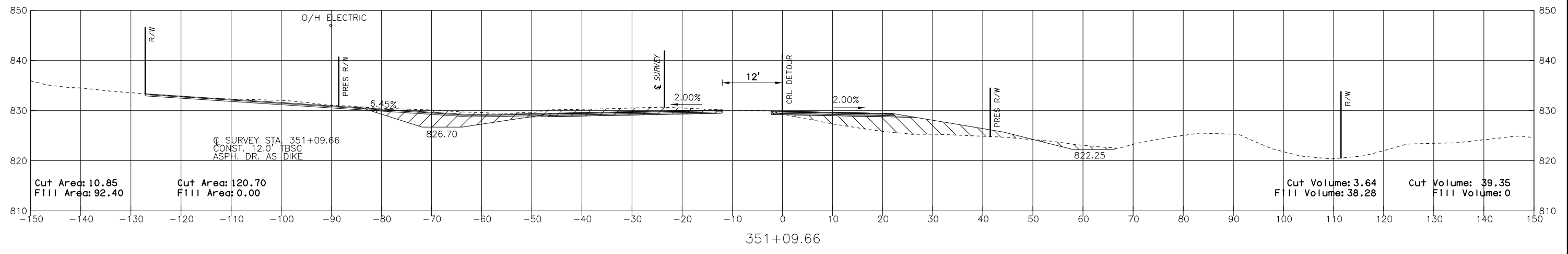
6/5/2018 2:49:23 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X003	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



CROSS SECTION BASED ON CRL NORTH DETOUR

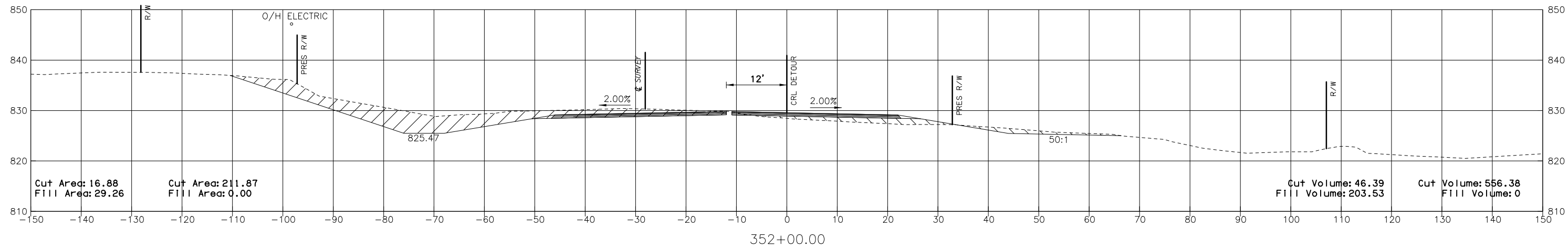
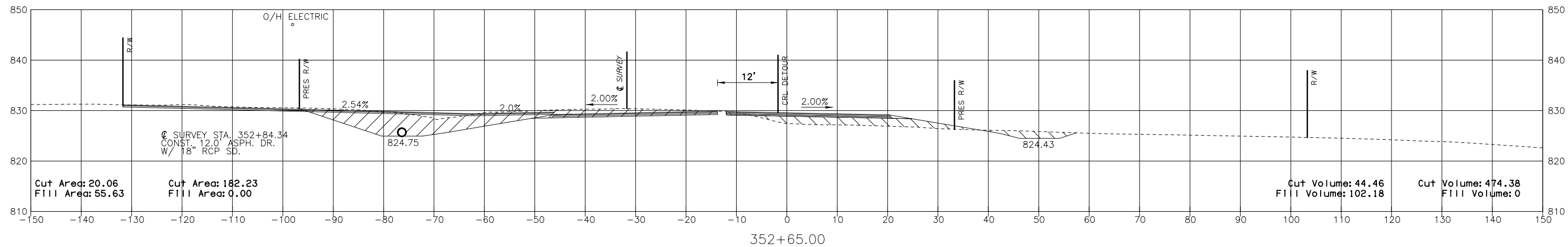
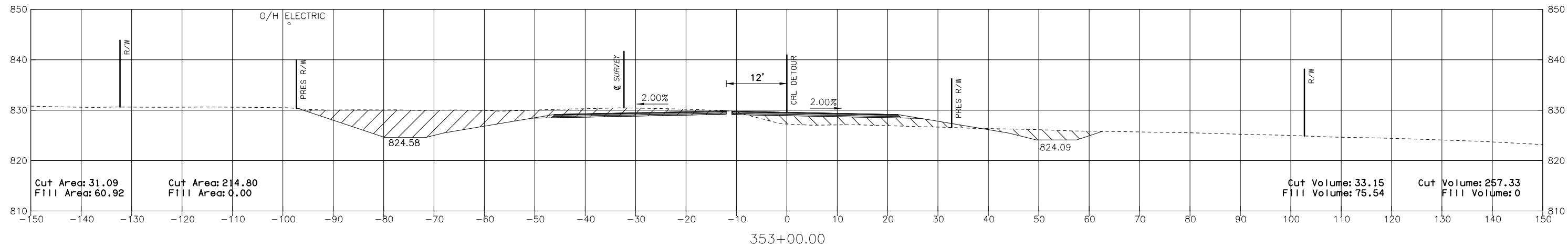
DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018	US 270 NORTH DETOUR CROSS SECTIONS	
CHECKED: CPY 2018		
APPRVD: CPY 2018		
		STATE JOB PIECE NO: 21006(07) SHEET NO. X003

6/5/2018 2:49:25 PM cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X004	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)

VOLUMES (CY)



CROSS SECTION BASED ON CRL NORTH DETOUR

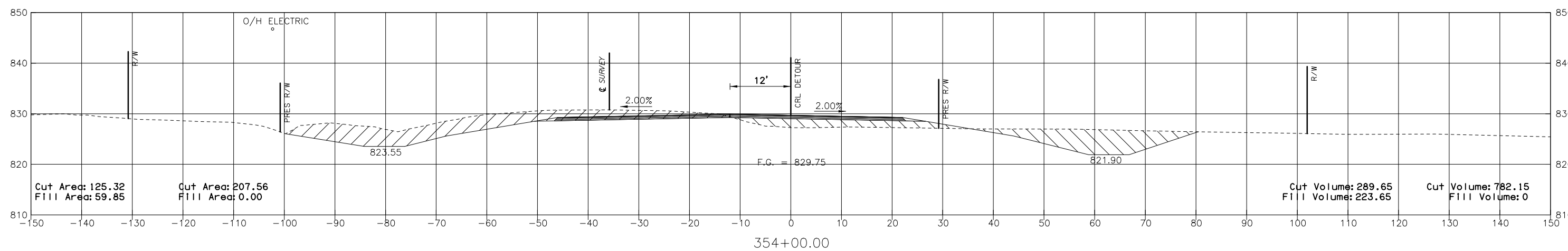
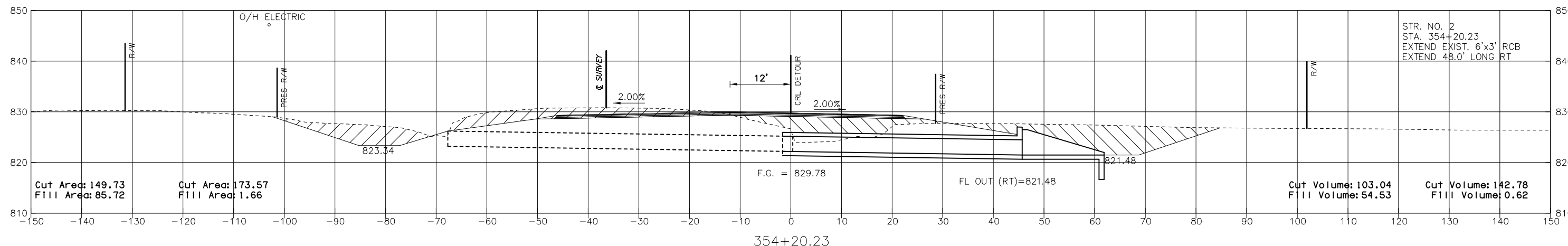
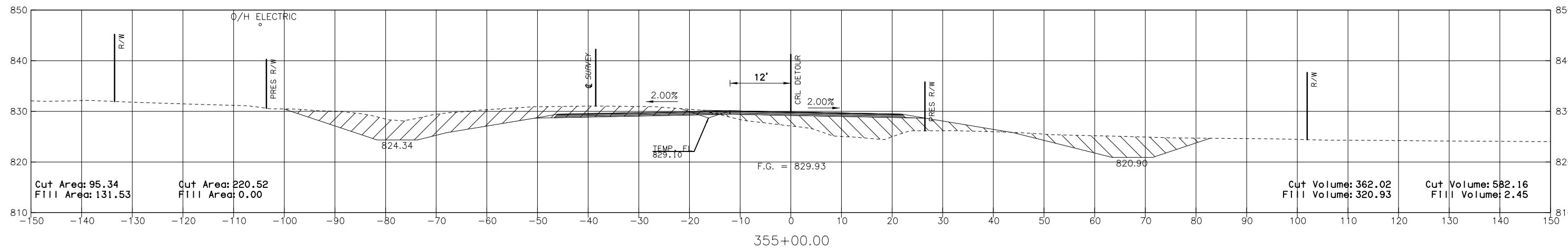
DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
CP&Y		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X004

6/5/2018 2:49:26 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X005	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)

VOLUMES (CY)



CROSS SECTION BASED ON CRL NORTH DETOUR

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		

US 270 NORTH DETOUR
CROSS SECTIONS

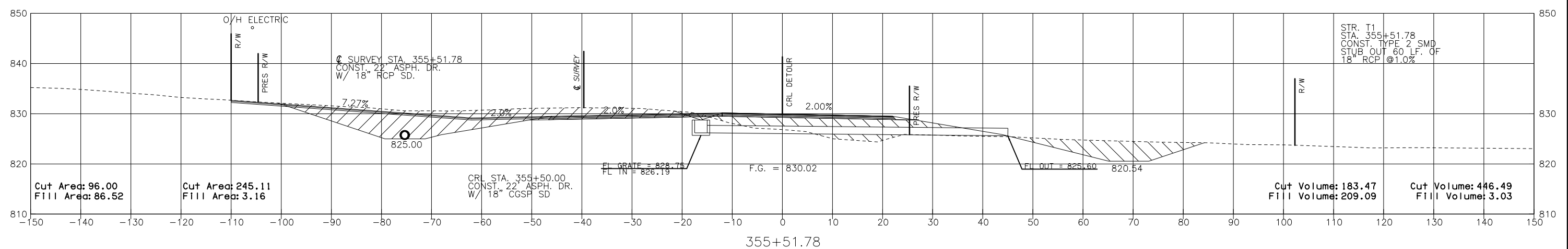
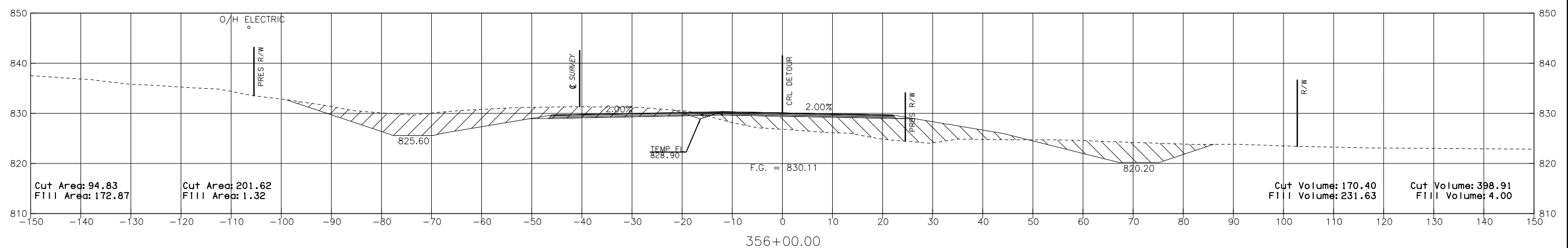
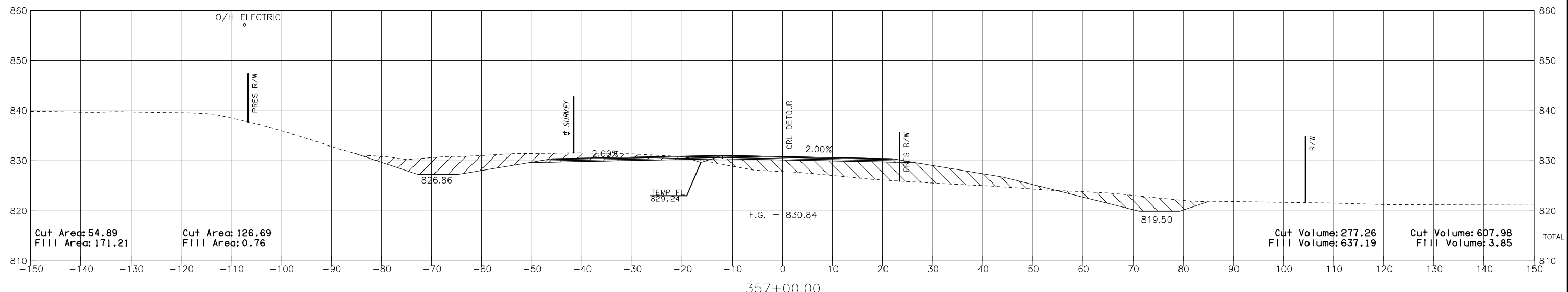
STATE JOB PIECE NO: 21006(07) SHEET NO. X005

6/5/2018
 bmcanelly
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X006	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)

VOLUMES (CY)



CROSS SECTION BASED ON CRL NORTH DETOUR

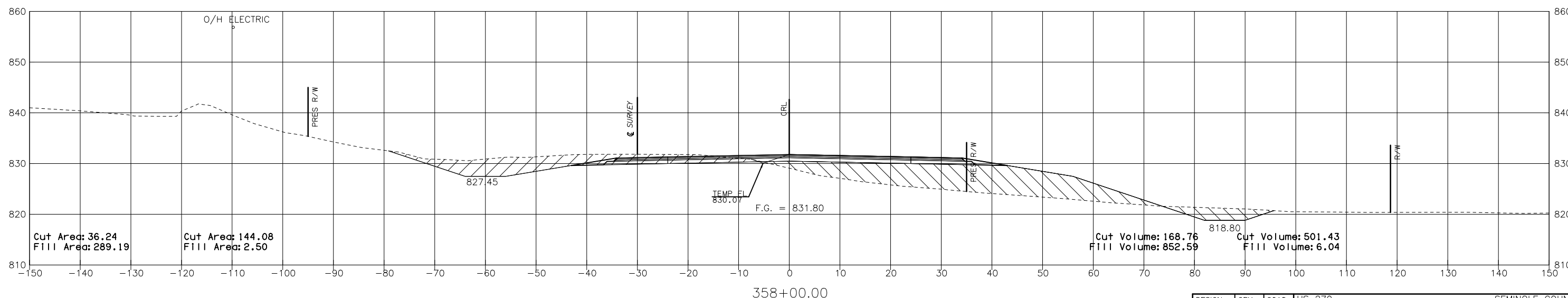
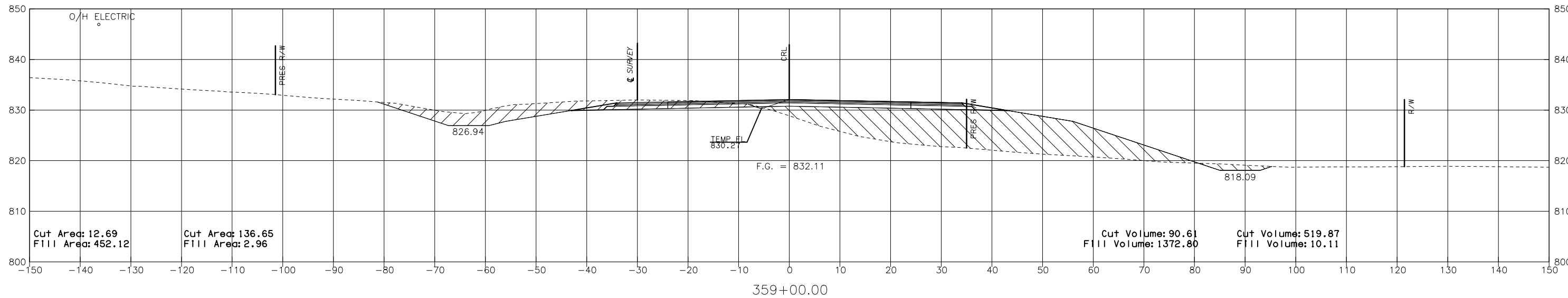
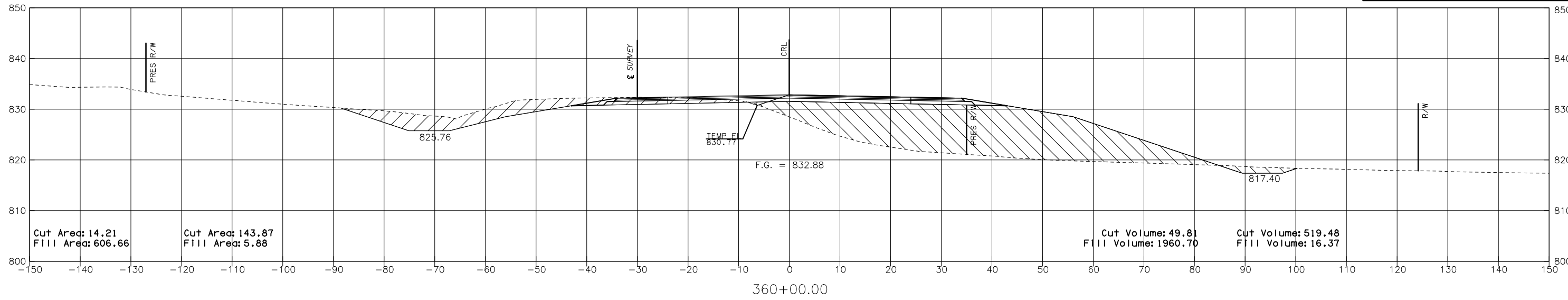
DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
CP&Y		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X006

6/5/2018
 bmcanelly
 cpy&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X007	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)

VOLUMES (CY)



END INCIDENTAL CONSTRUCTION
BEGIN PROJECT

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		

US 270 MAINLINE
CROSS SECTIONS

STATE JOB PIECE NO: 21006(07) SHEET NO. X007

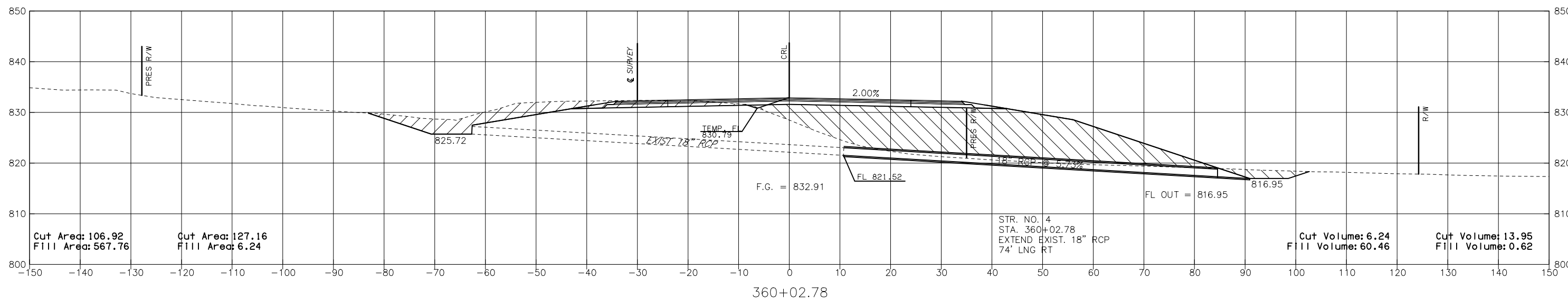
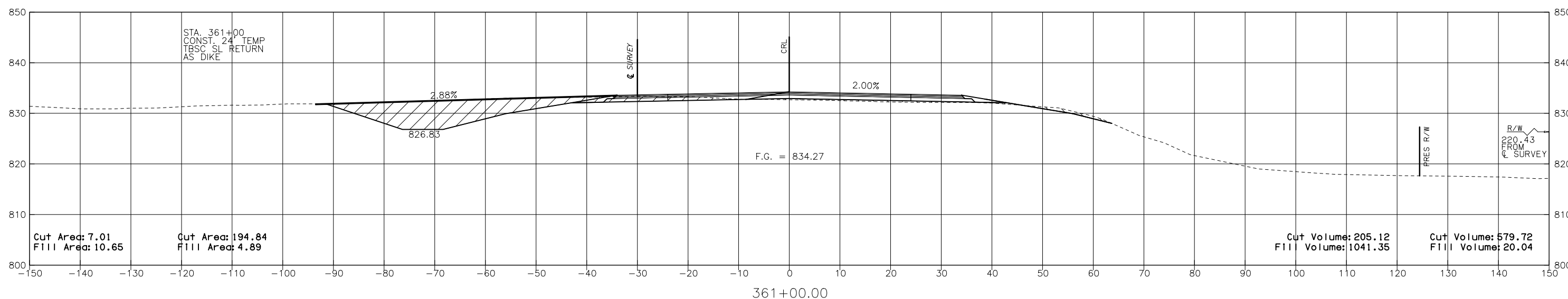
6/5/2018 2:49:29 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X008	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



6/5/2018 2:49:30 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.10 Base Files\21006XSEC.dgn

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		

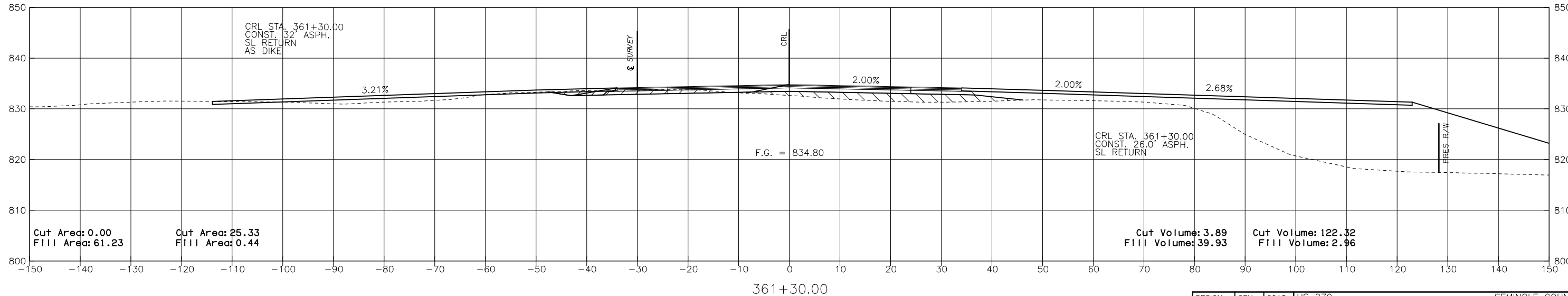
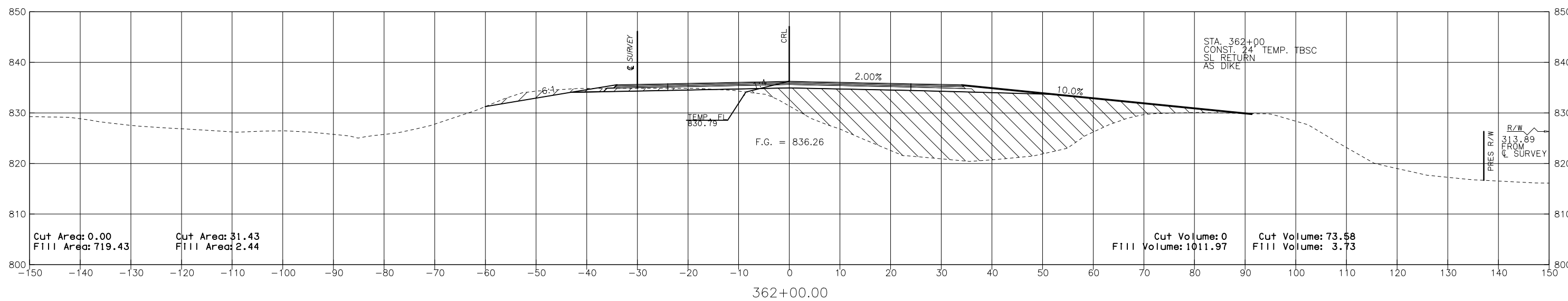
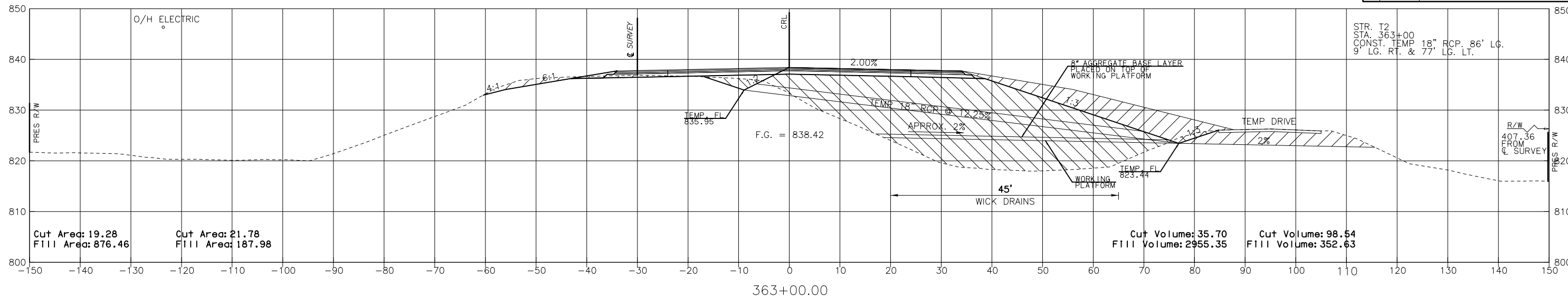
US 270 MAINLINE
CROSS SECTIONS

STATE JOB PIECE NO: 21006(07) SHEET NO. X008

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X009	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)

VOLUMES (CY)



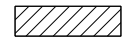
DESIGN:	CPY	2018	US 270	SEMINOLE COUNTY
DRAWN:	CPY	2018	US 270 MAINLINE CROSS SECTIONS	
CHECKED:	CPY	2018		
APPRVD:	CPY	2018		
CP&Y			STATE JOB PIECE NO: 21006(07)	SHEET NO. X009

6/5/2018 2:49:31 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

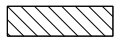
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X010	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

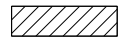
END AREAS (SF)

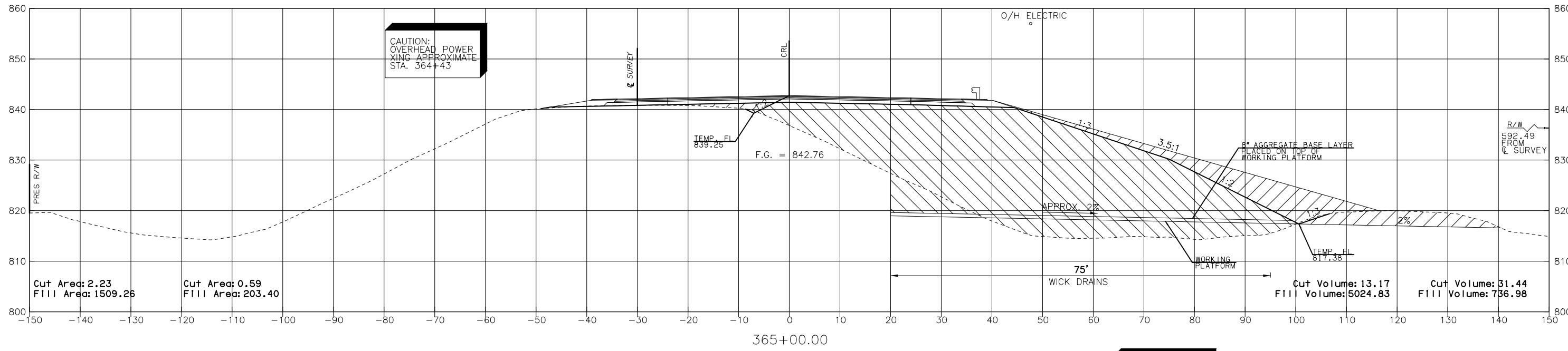
PHASE 1 

PHASE 2 

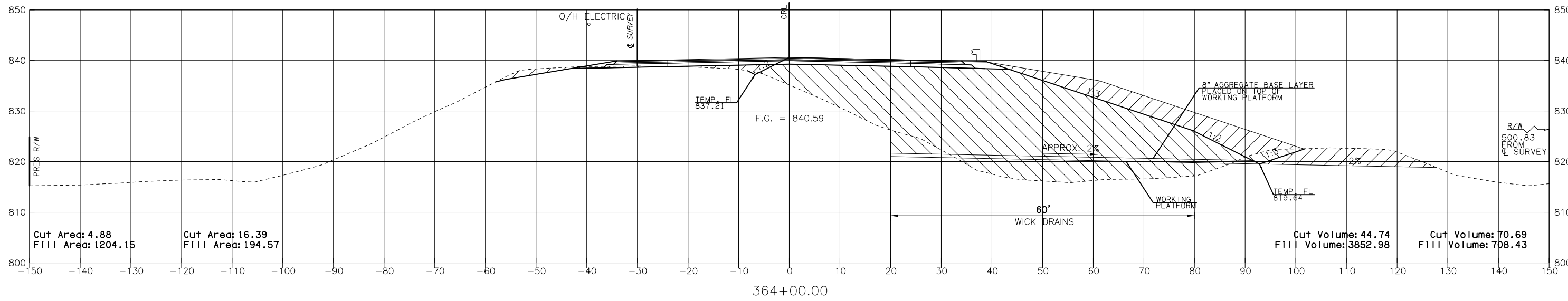
VOLUMES (CY)

PHASE 1 

PHASE 2 



CAUTION: OVERHEAD POWER XING APPROXIMATE STA. 364+43 23' CLEARANCE



DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
US 270 MAINLINE CROSS SECTIONS		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X010

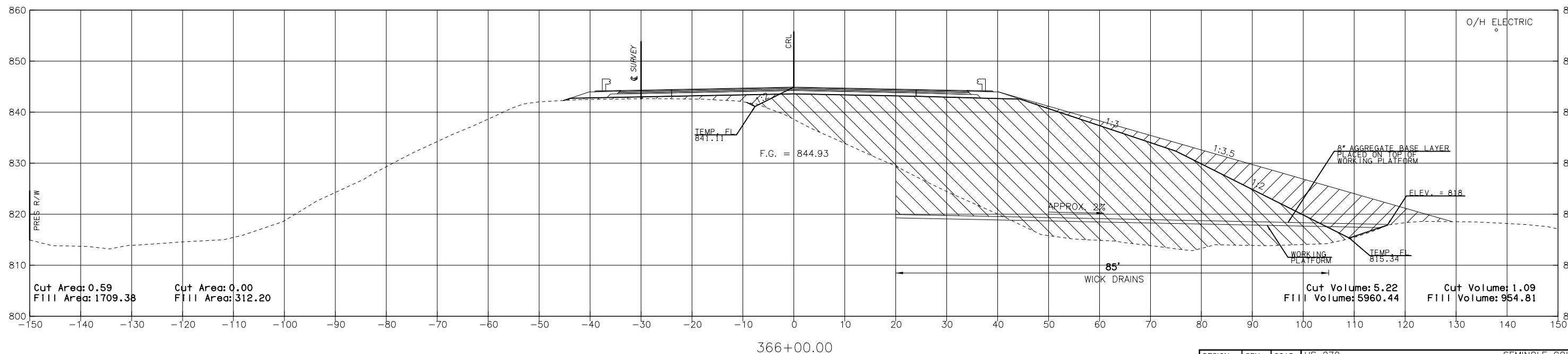
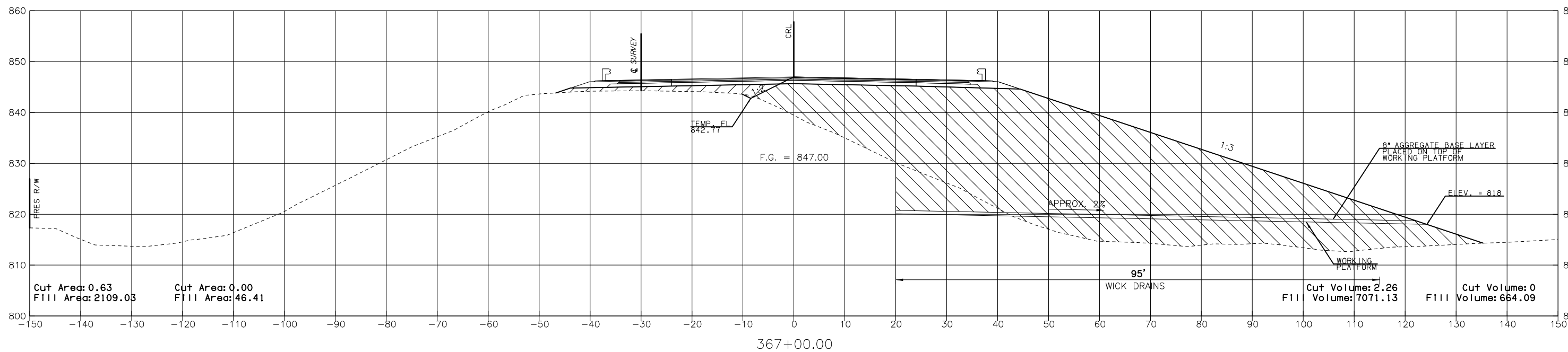
6/5/2018 2:49:32 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.10 Base Files\21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X011	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



6/5/2018 2:49:33 PM cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		

US 270 MAINLINE
CROSS SECTIONS

STATE JOB PIECE NO: 21006(07) SHEET NO. X011



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X012	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

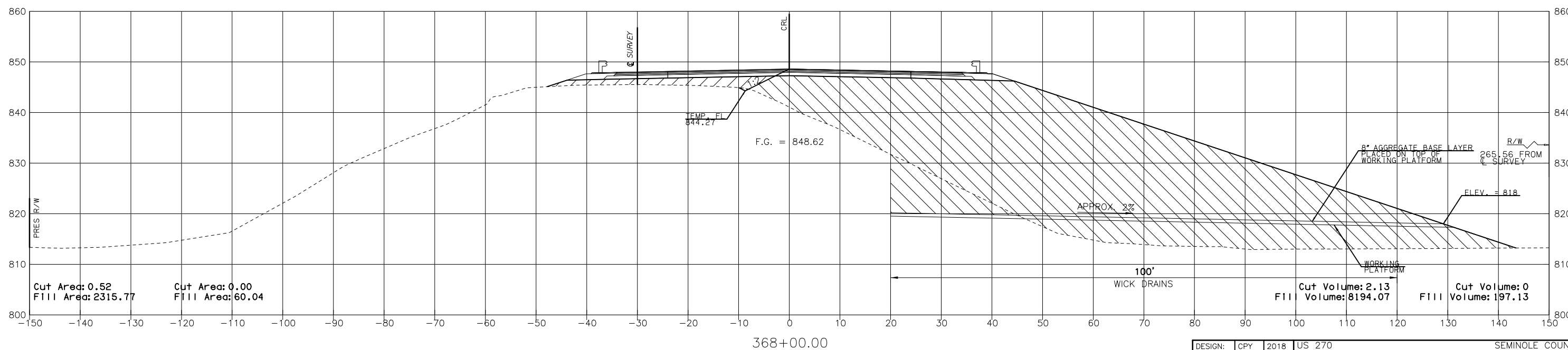
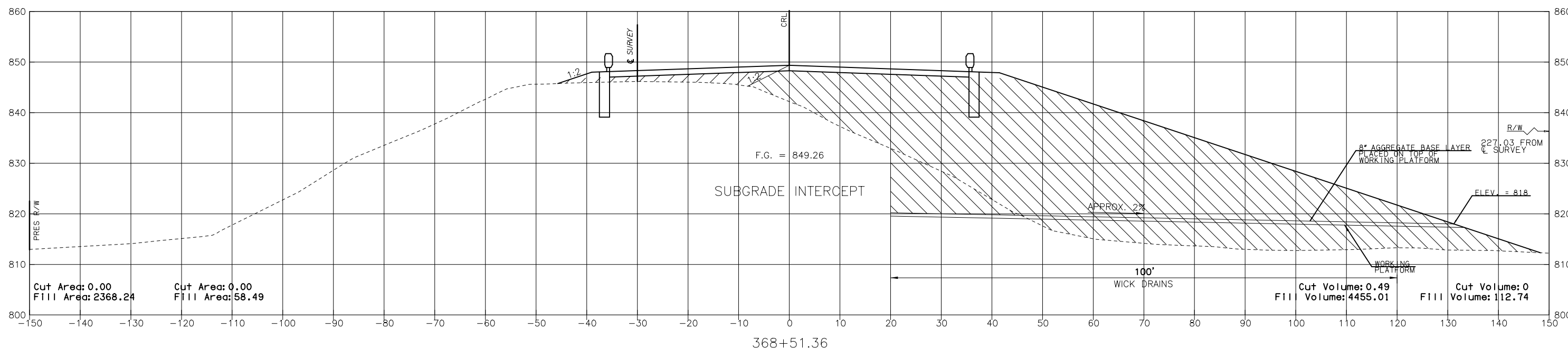
END AREAS (SF)



VOLUMES (CY)



BEGIN BRIDGE 'A' STA 368+78.43



6/5/2018 2:49:34 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
US 270 MAINLINE CROSS SECTIONS		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X012

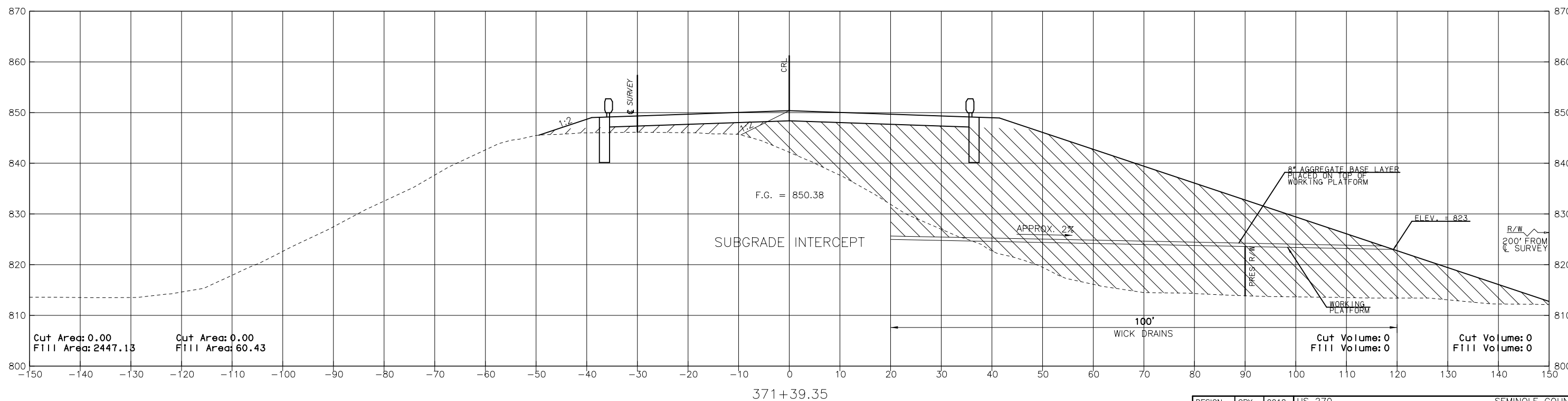
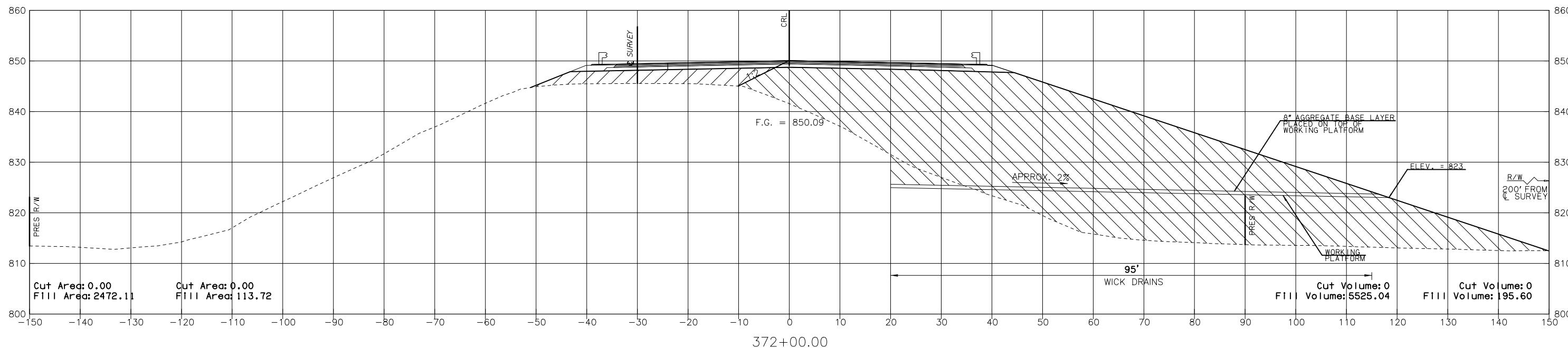


FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X013	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



END BRIDGE 'A' STA 371+21.27

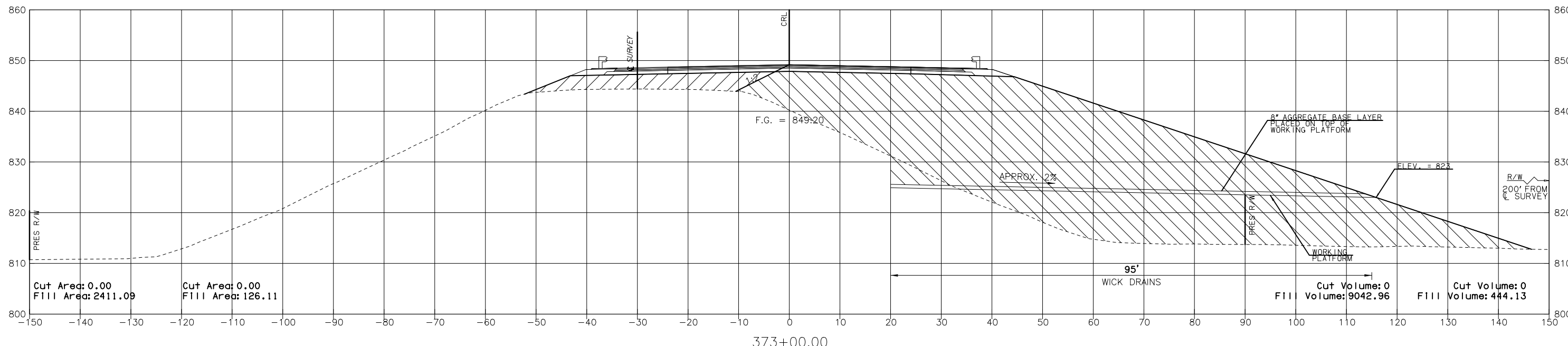
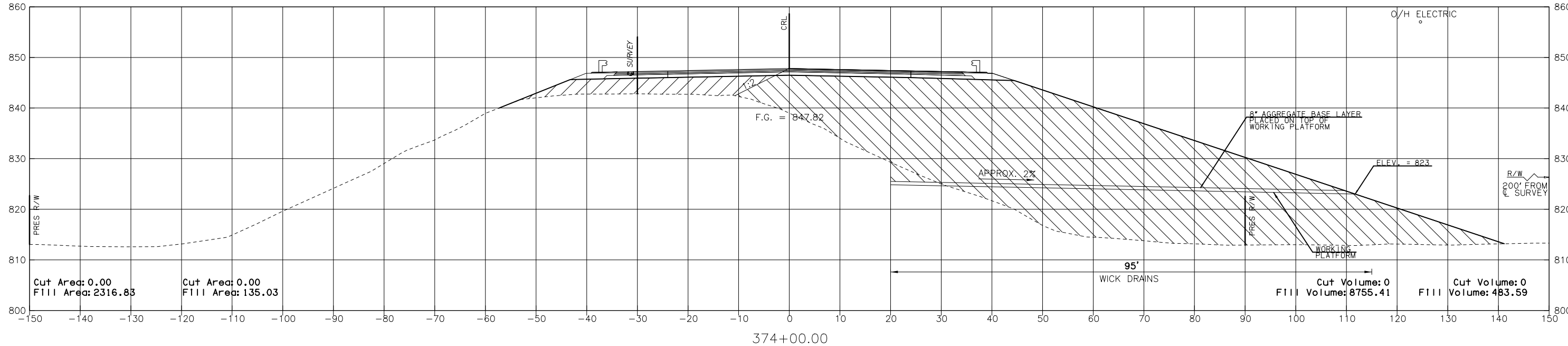
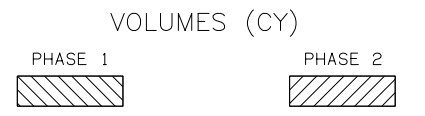
DESIGN:	CPY	2018	US 270	SEMINOLE COUNTY
DRAWN:	CPY	2018		
CHECKED:	CPY	2018		
APPRVD:	CPY	2018		

US 270 MAINLINE
CROSS SECTIONS

STATE JOB PIECE NO: 21006(07) SHEET NO. X013

6/5/2018 2:49:35 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X014	X030
REVISIONS					
NO.	DATE	DESCRIPTION			



6/5/2018 2:49:36 PM
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

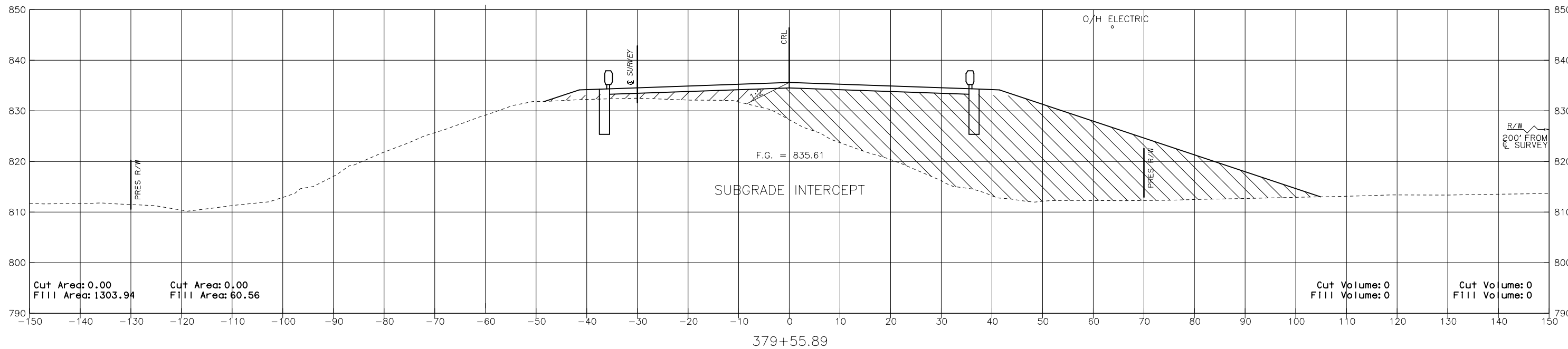
DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018	US 270 MAINLINE CROSS SECTIONS	
CHECKED: CPY 2018		
APPRVD: CPY 2018		
		STATE JOB PIECE NO: 21006(07) SHEET NO. X014

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X015	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

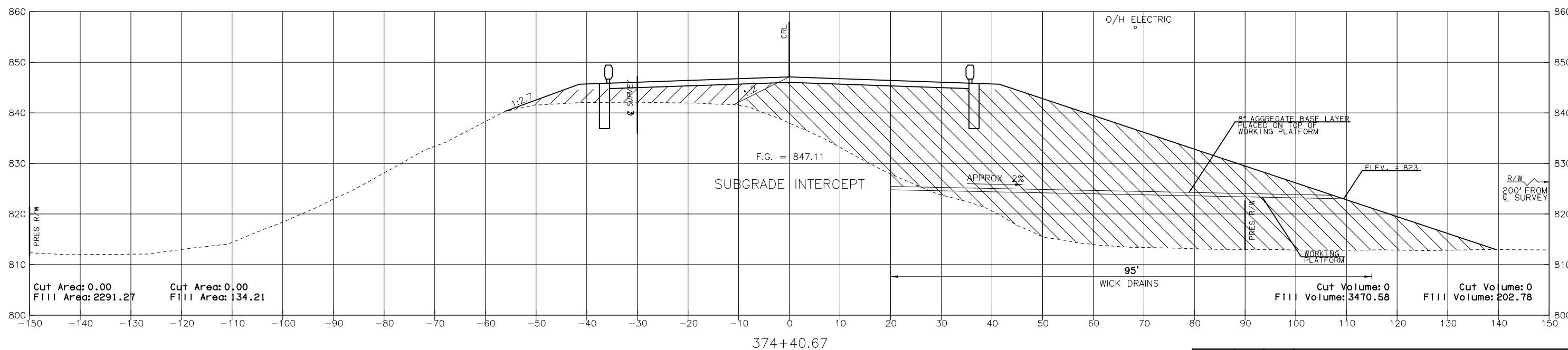
END AREAS (SF)



VOLUMES (CY)



END BRIDGE 'B' STA 379+32.00
 BEGIN BRIDGE 'B' STA 374+68.00



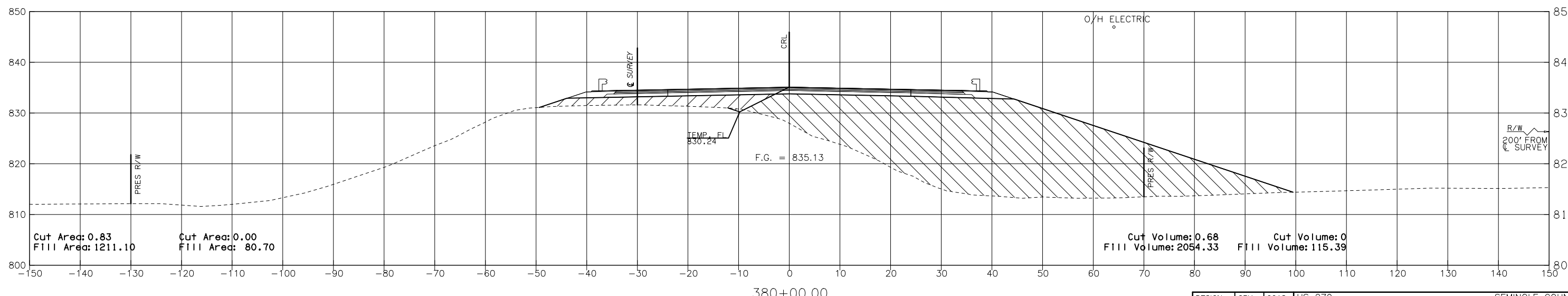
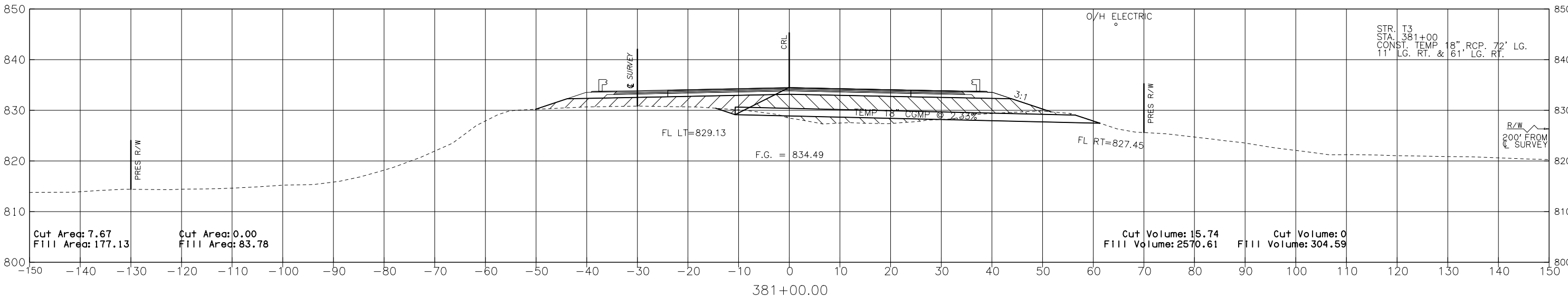
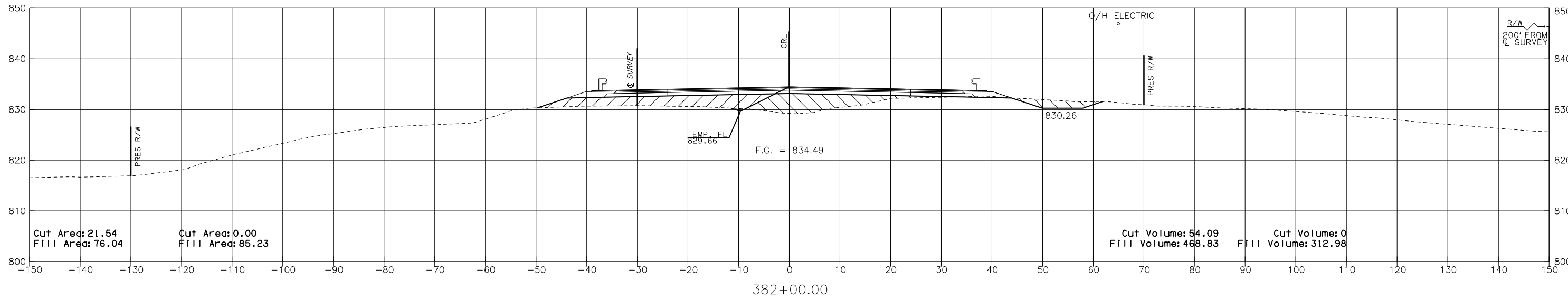
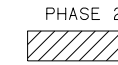
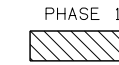
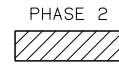
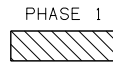
DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
US 270 MAINLINE CROSS SECTIONS		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X015

6/5/2018 2:49:37 PM
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

END AREAS (SF)

VOLUMES (CY)

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X016	X030
REVISIONS					
NO.	DATE	DESCRIPTION			



STR. T3
 STA. 381+00
 CONST. TEMP 18" RCP, 72' LG.
 11' LG. RT. & 61' LG. RT.

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
US 270 MAINLINE CROSS SECTIONS		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X016

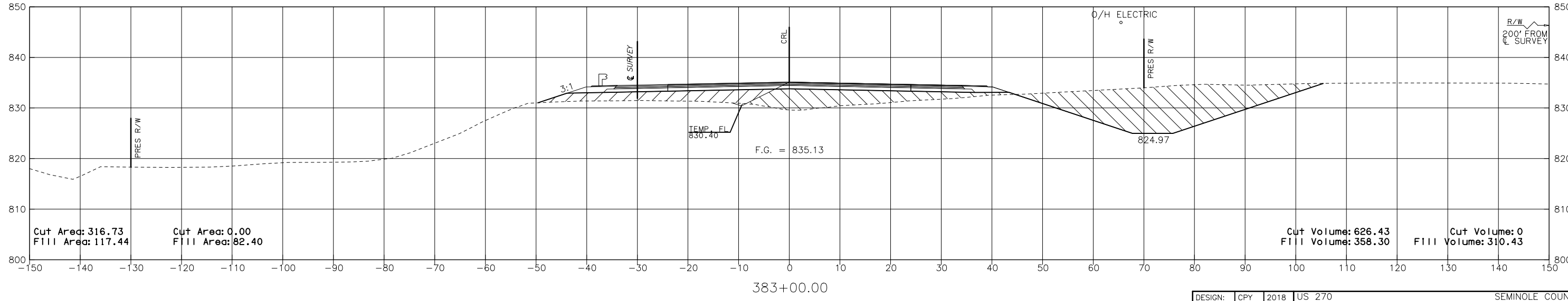
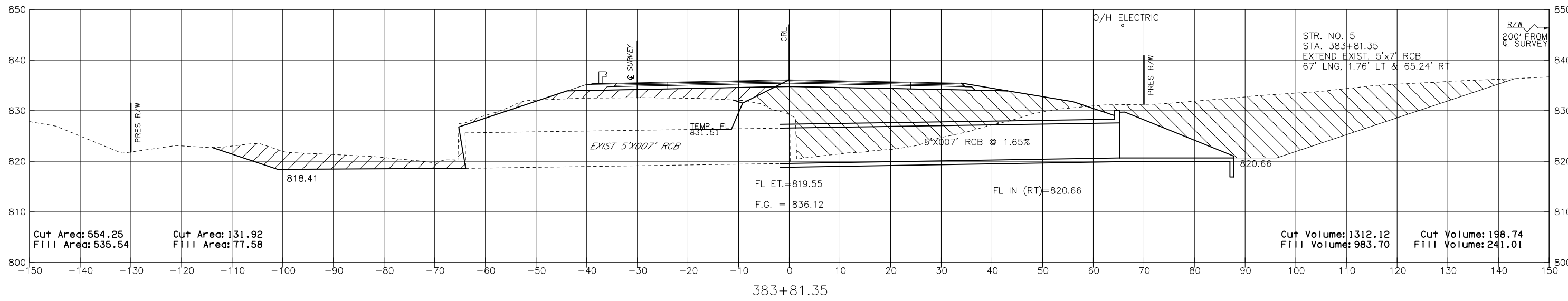
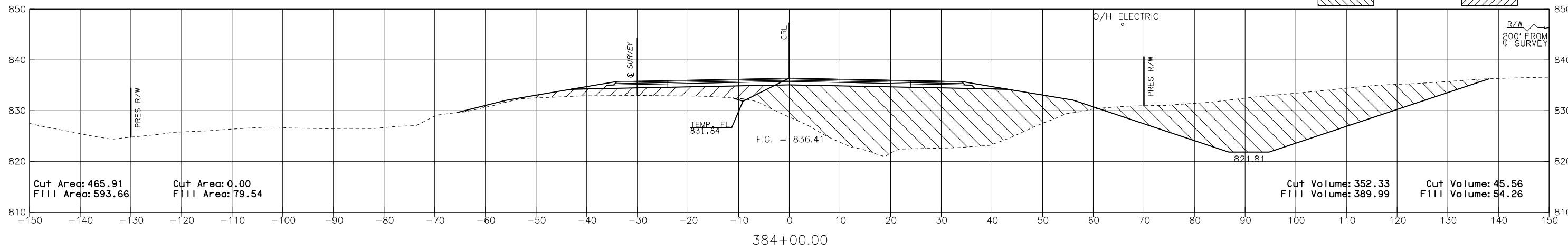
6/5/2018 2:49:38 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X017	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



6/5/2018 2:49:39 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

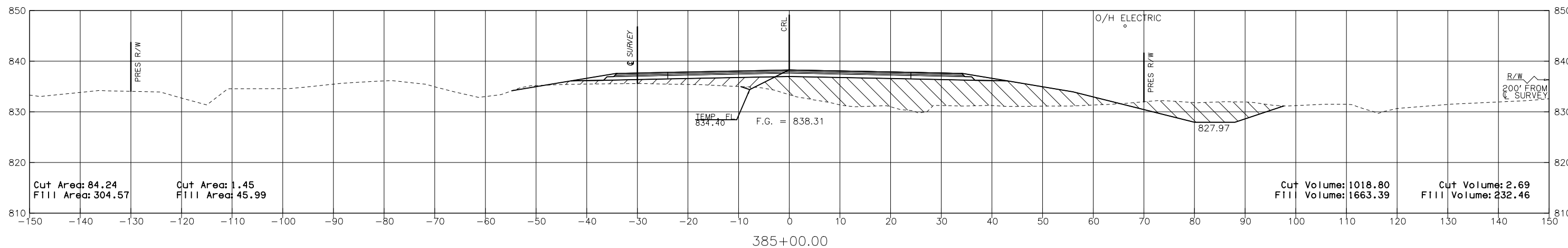
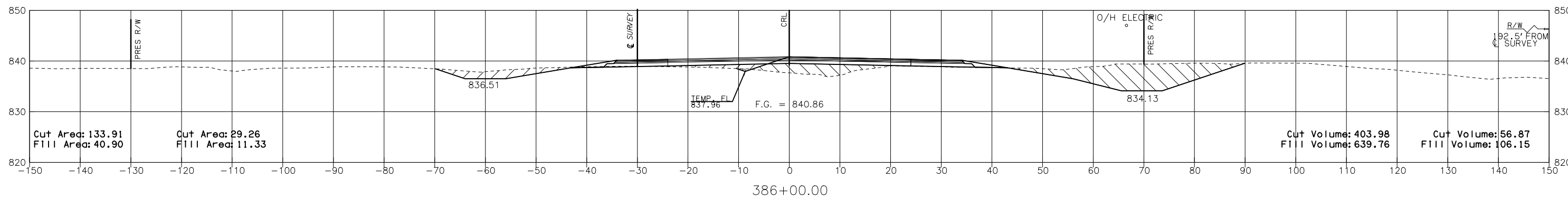
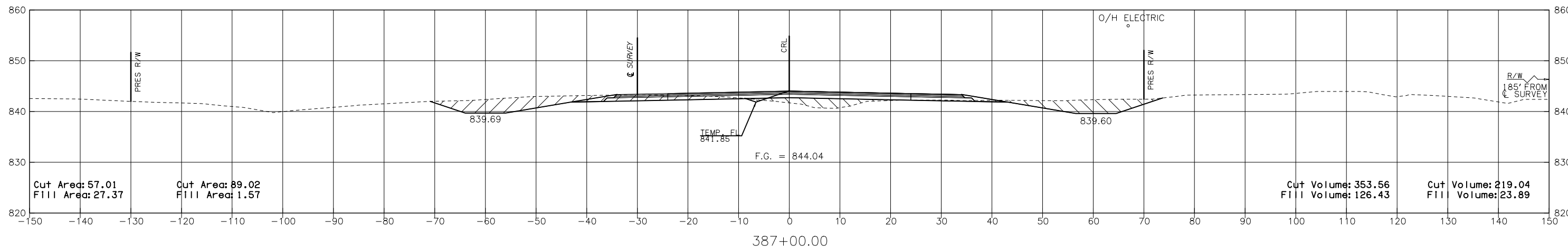
DESIGN:	CPY	2018	US 270	SEMINOLE COUNTY
DRAWN:	CPY	2018		
CHECKED:	CPY	2018		
APPRVD:	CPY	2018		
CP&Y			US 270 MAINLINE CROSS SECTIONS	
			STATE JOB PIECE NO: 21006(07)	SHEET NO. X017

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X018	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



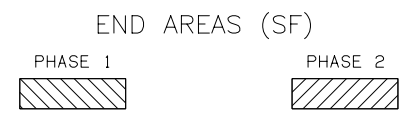
6/5/2018 2:49:40 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		

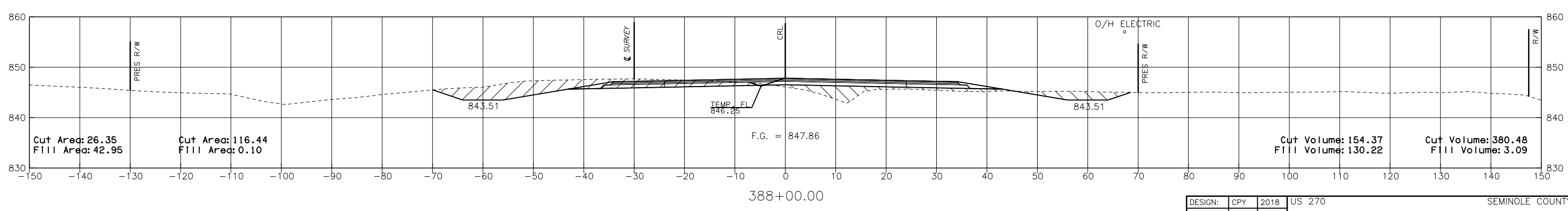
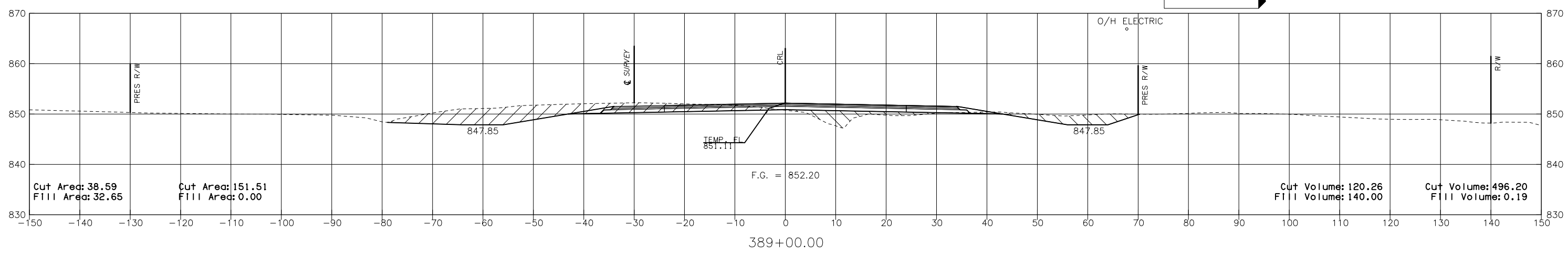
US 270 MAINLINE
CROSS SECTIONS

STATE JOB PIECE NO: 21006(07) SHEET NO. X018

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X019	X030
REVISIONS					
NO.	DATE	DESCRIPTION			



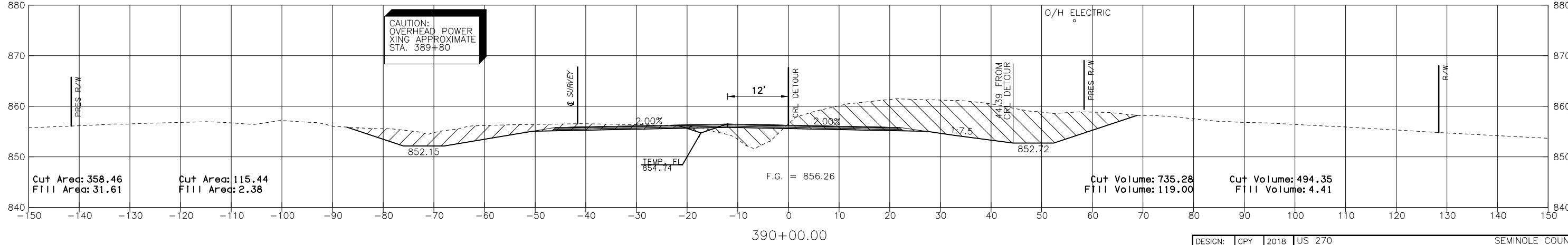
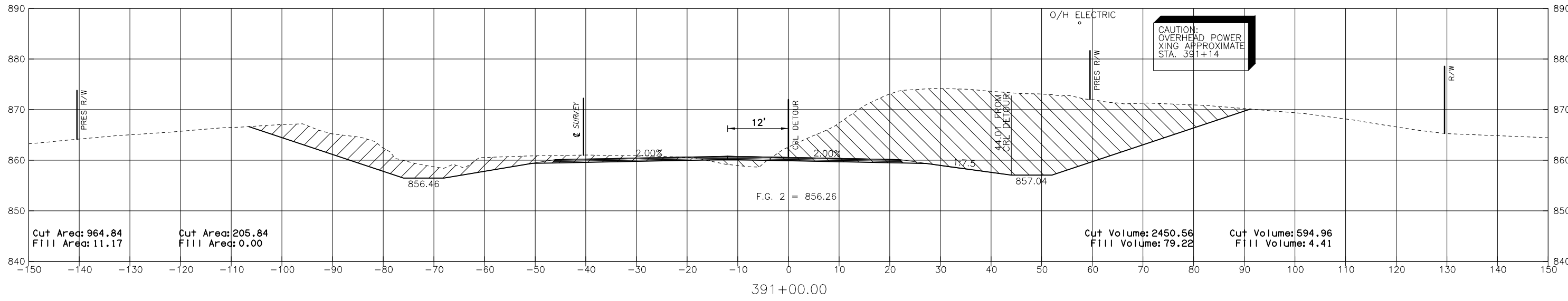
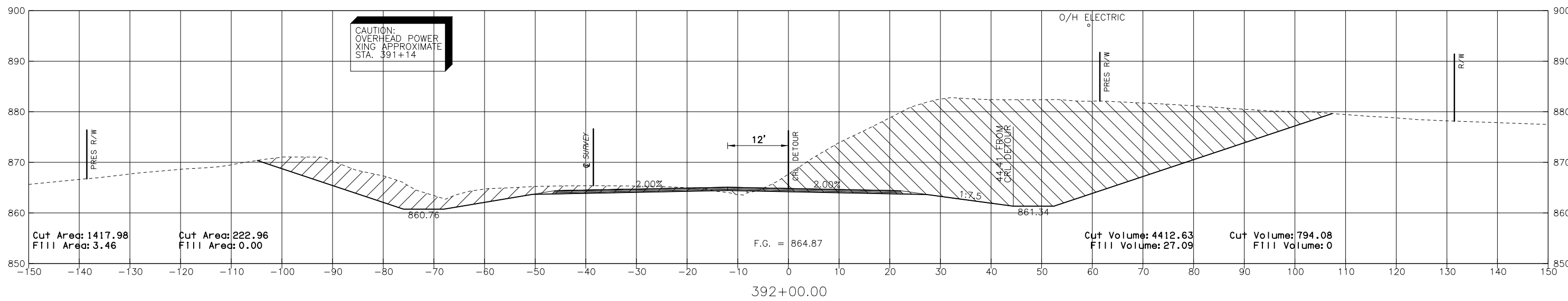
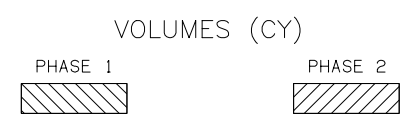
CAUTION:
OVERHEAD POWER
XING APPROXIMATE
STA. 389+80



DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
US 270 MAINLINE CROSS SECTIONS		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X019

6/5/2018 2:49:41 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X020	X030
REVISIONS					
NO.	DATE	DESCRIPTION			



CROSS SECTION BASED ON CRL SOUTH DETOUR

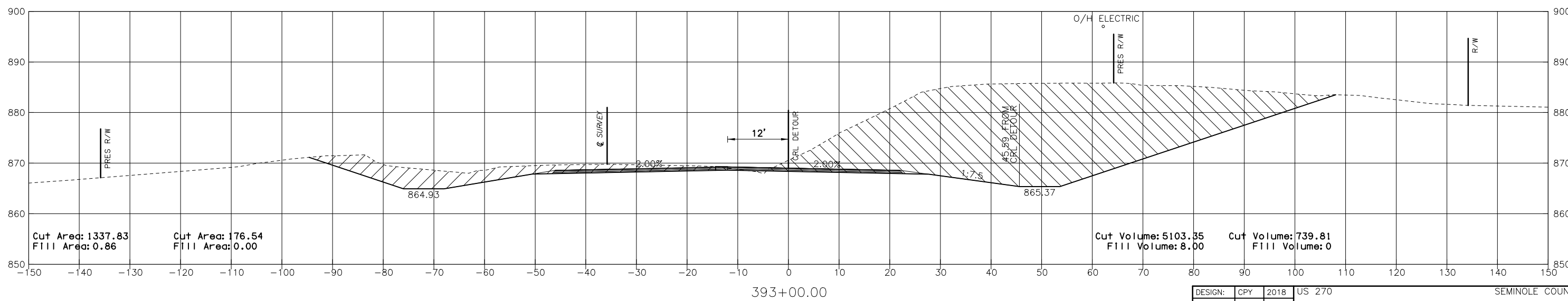
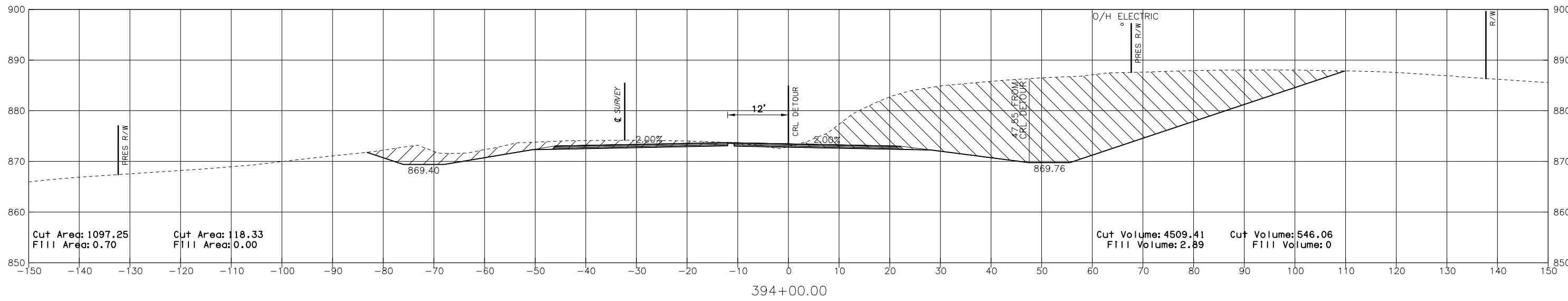
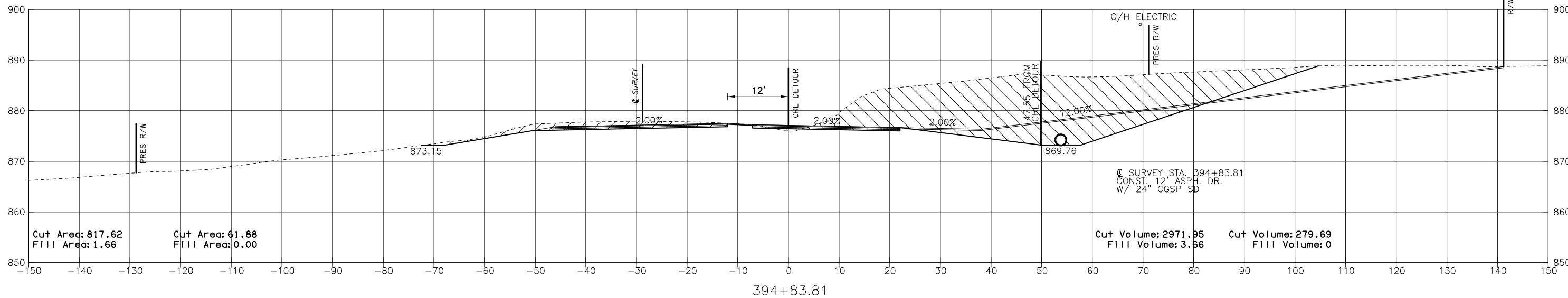
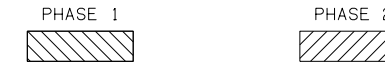
DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
CP&Y		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X020

6/5/2018 2:49:42 PM cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

END AREAS (SF)

VOLUMES (CY)

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X021	X030
REVISIONS					
NO.	DATE	DESCRIPTION			



CROSS SECTION BASED ON CRL SOUTH DETOUR

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		

US 270 SOUTH DETOUR
CROSS SECTIONS

STATE JOB PIECE NO: 21006(07) SHEET NO. X021

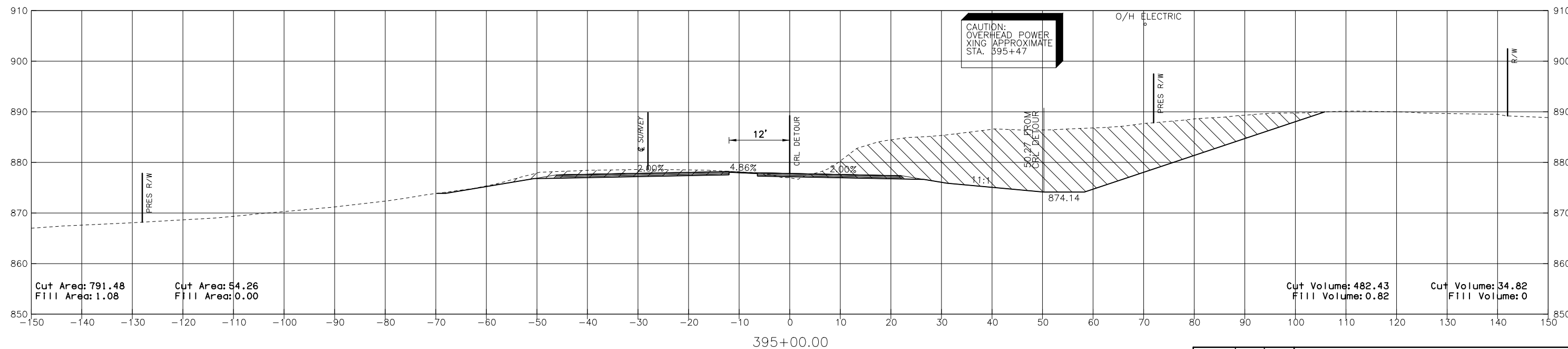
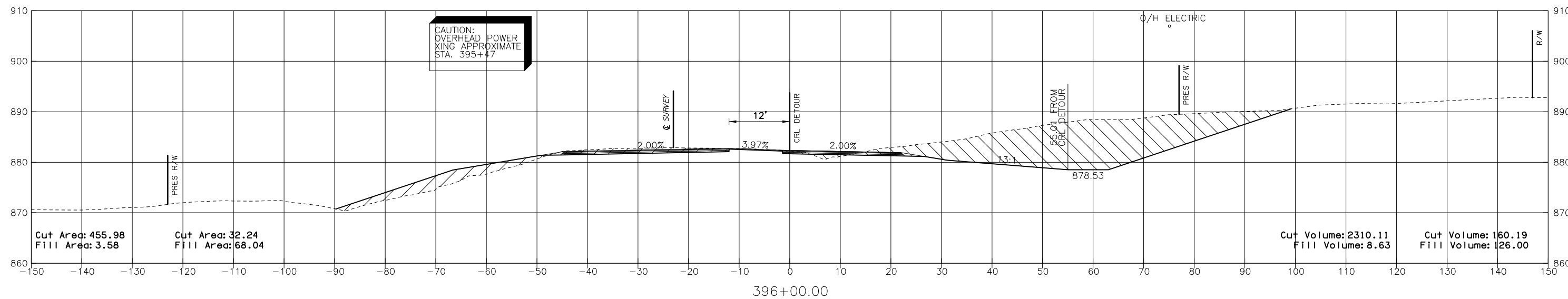
6/5/2018 2:49:43 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X022	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)



VOLUMES (CY)



CROSS SECTION BASED ON CRL SOUTH DETOUR

DESIGN:	CPY	2018	US 270	SEMINOLE COUNTY
DRAWN:	CPY	2018		
CHECKED:	CPY	2018		
APPRVD:	CPY	2018		

US 270 SOUTH DETOUR CROSS SECTIONS

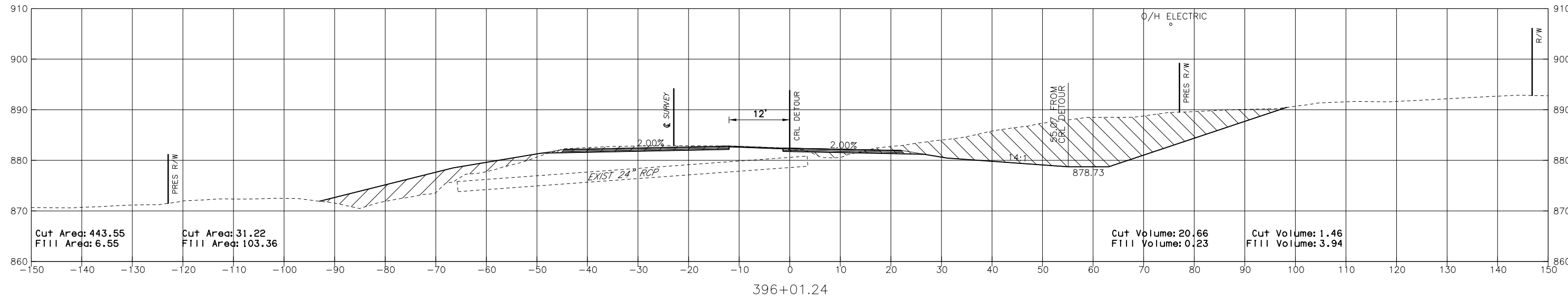
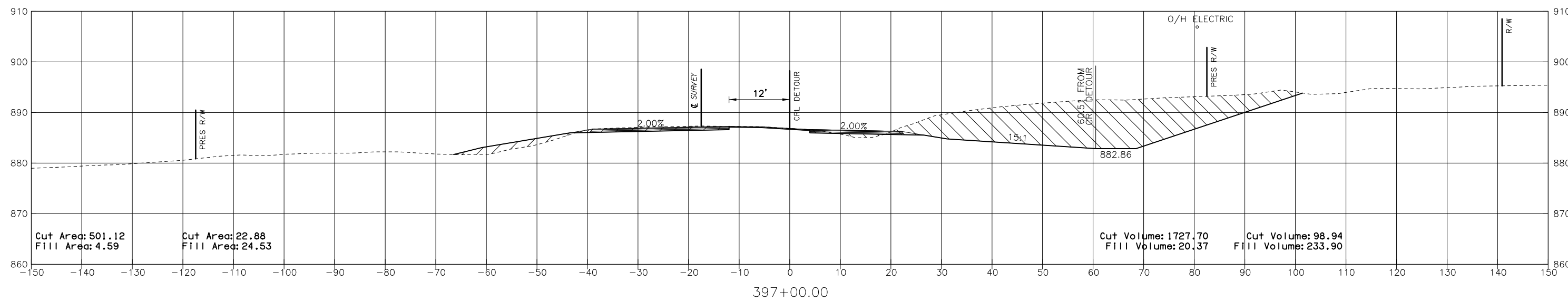
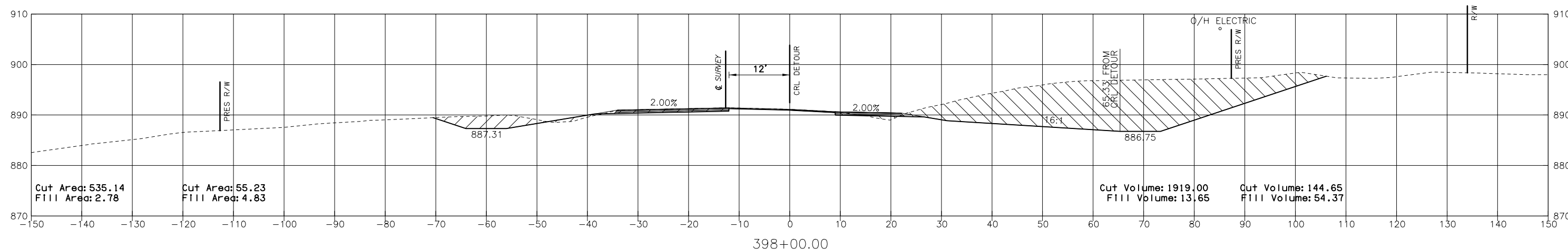
STATE JOB PIECE NO: 21006(07) SHEET NO. X022

6/5/2018 2:49:44 PM
 bmcanelly cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X023	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)

VOLUMES (CY)



CROSS SECTION BASED ON CRL SOUTH DETOUR

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		

US 270 SOUTH DETOUR CROSS SECTIONS

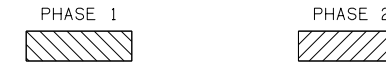
STATE JOB PIECE NO: 21006(07) SHEET NO. X023

6/5/2018 2:49:45 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.10 Base Files\21006XSEC.dgn

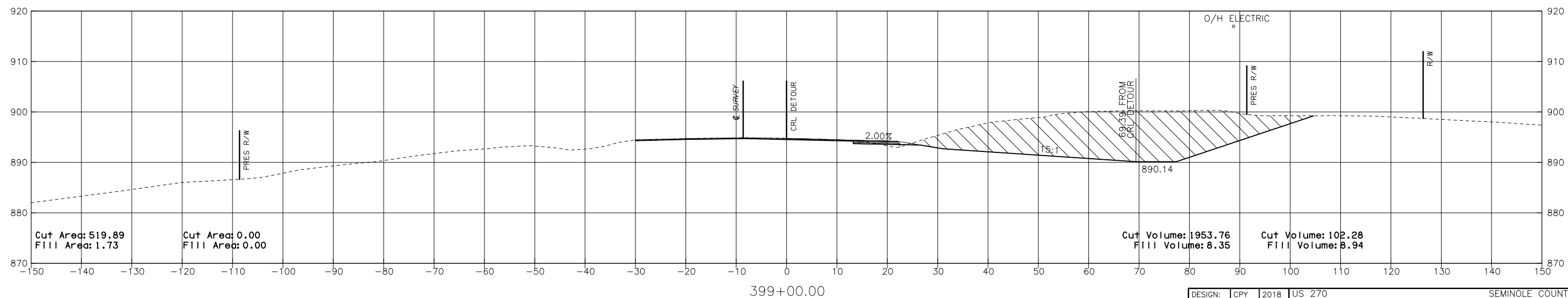
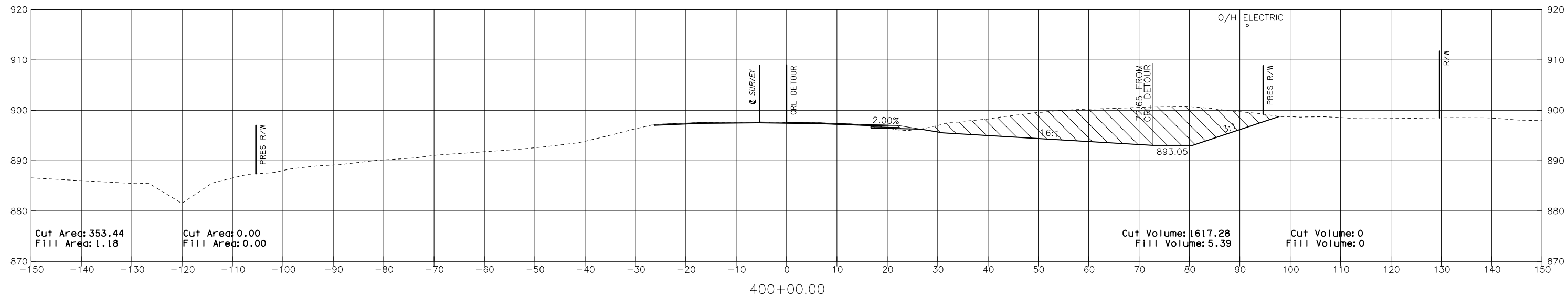
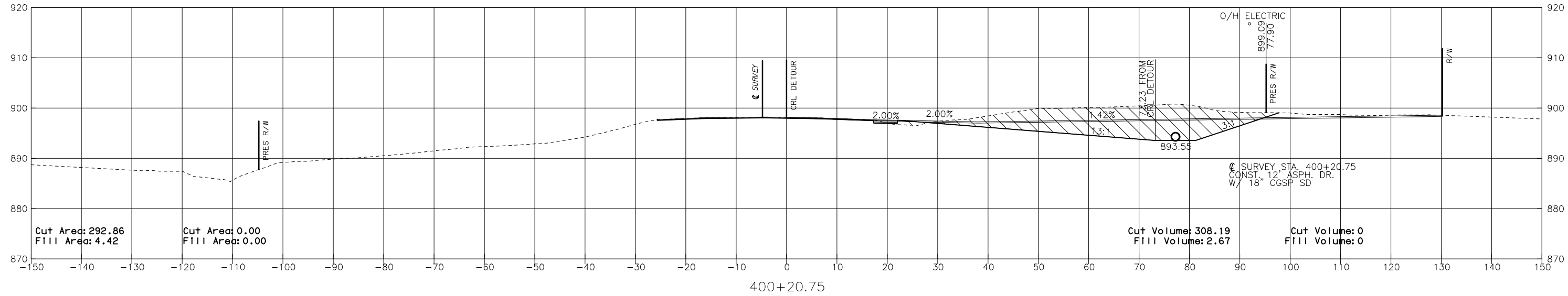
END AREAS (SF)



VOLUMES (CY)



FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X024	X030
REVISIONS					
NO.	DATE	DESCRIPTION			



CROSS SECTION BASED ON CRL SOUTH DETOUR

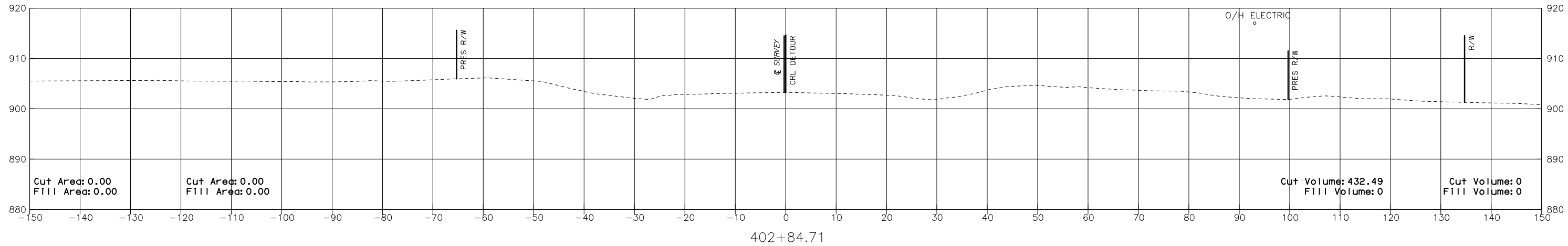
DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
CP&Y		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X024

6/5/2018 2:49:46 PM cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

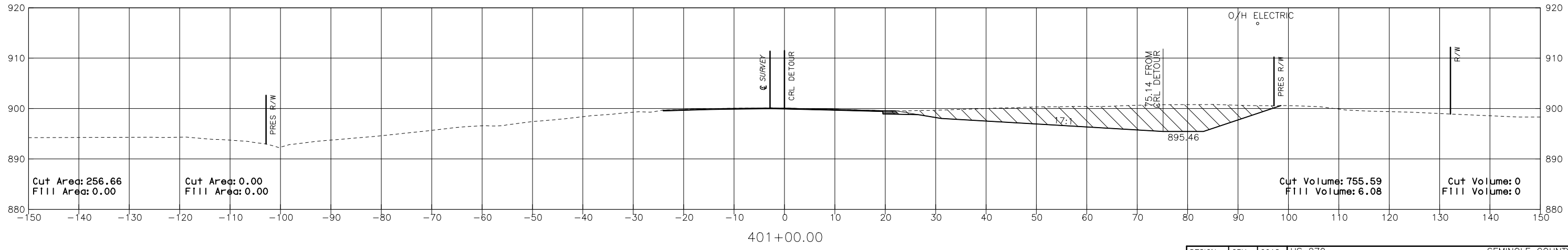
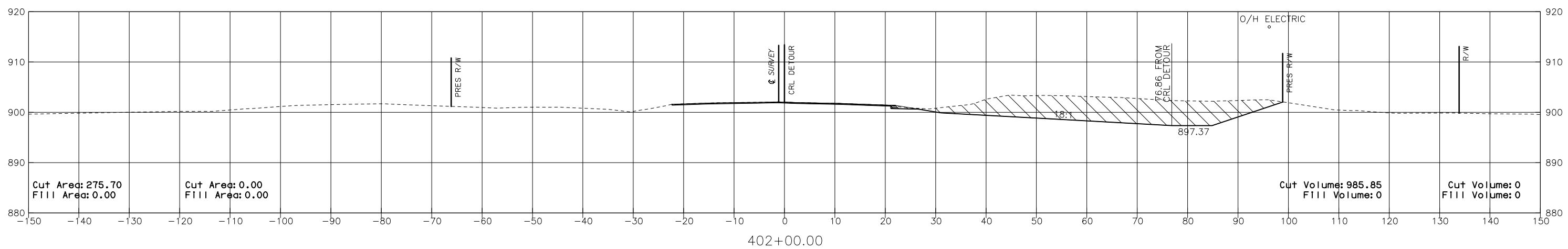
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X025	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)

VOLUMES (CY)



END INCIDENTAL CONSTRUCTION



CROSS SECTION BASED ON CRL SOUTH DETOUR

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
		US 270 SOUTH DETOUR CROSS SECTIONS
STATE JOB PIECE NO: 21006(07)		SHEET NO. X025

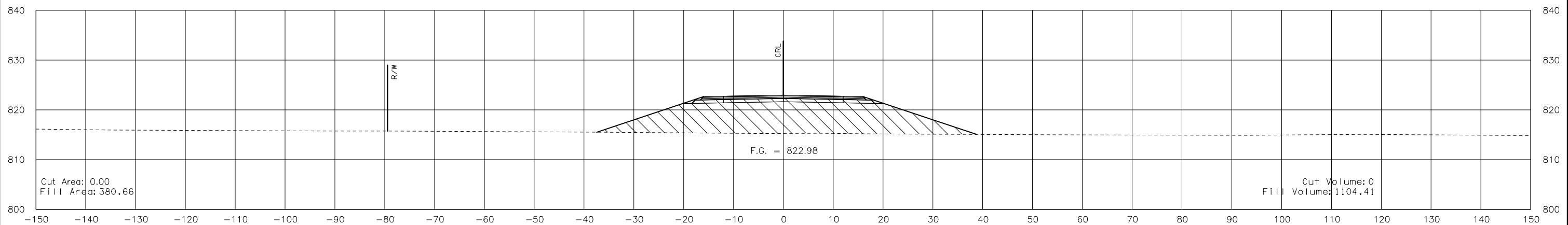
6/5/2018 2:49:47 PM
 cpyb&w_ANSIB.tbl
 cpypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X026	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

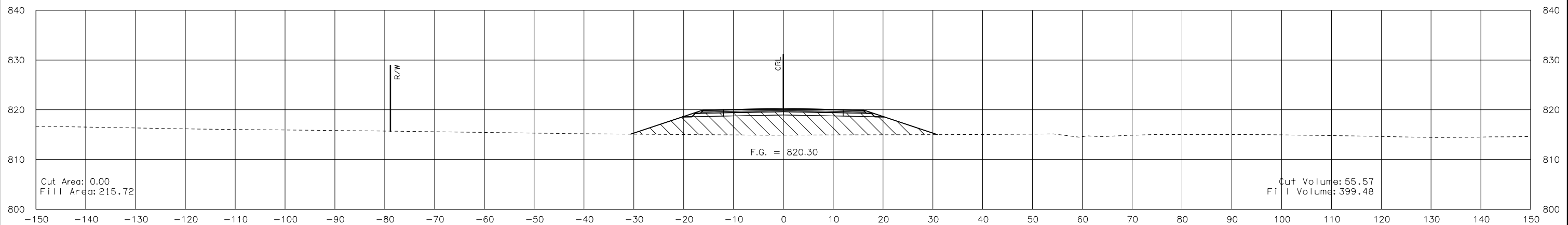
END AREAS (SF)



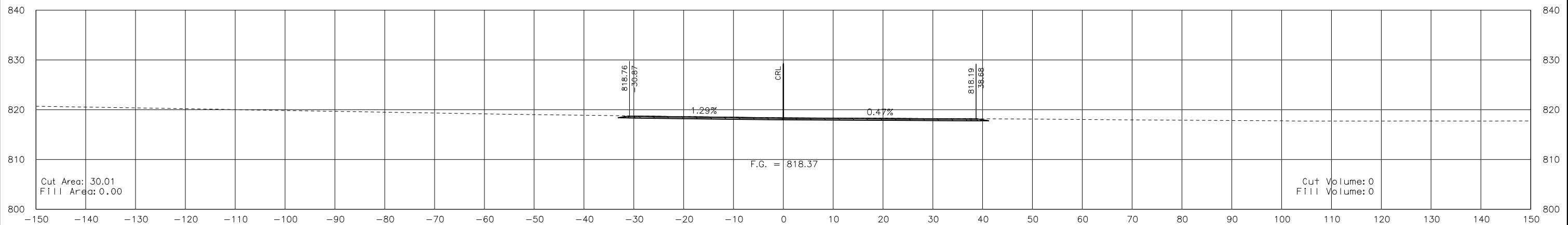
VOLUMES (CY)



12+00.00



11+00.00

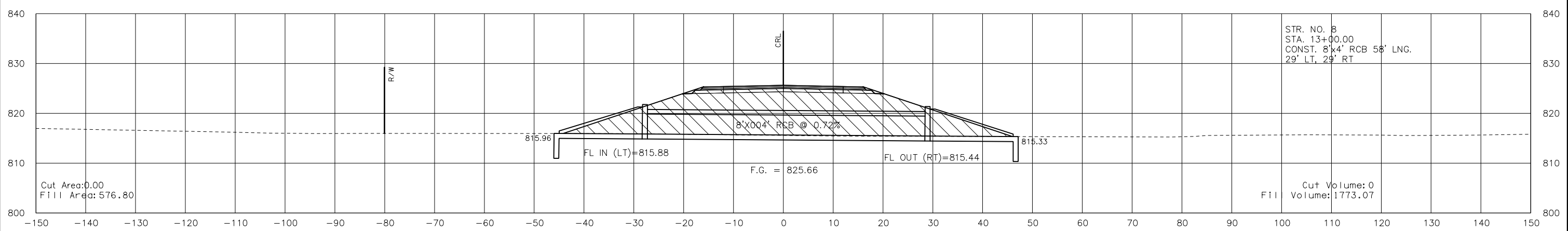
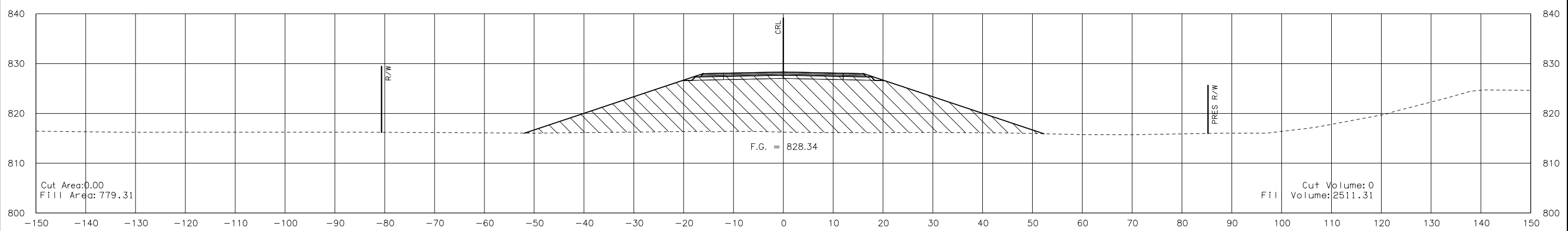
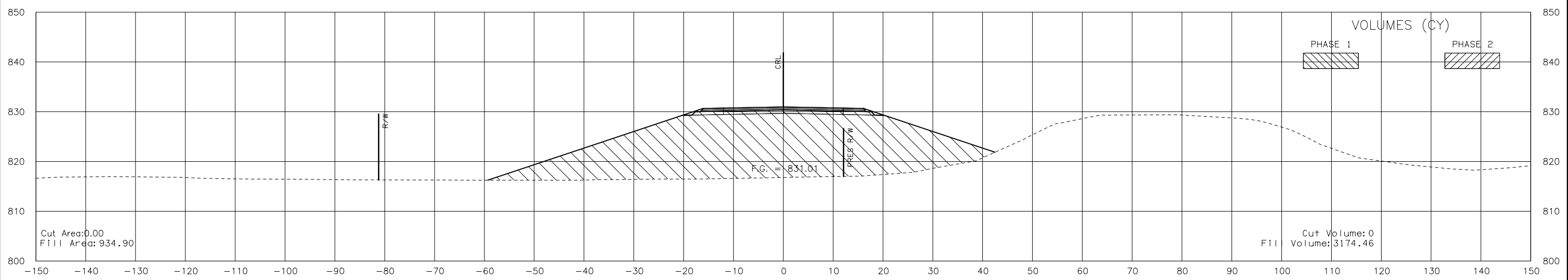


10+00.00

bmcanelly
 cpyb&w_ANSIB.tbl
 6/5/2018 2:49:48 PM
 cpypdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.10 Base Files\21006XSEC.dgn

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
NS 3610 CROSS SECTIONS		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X026

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X027	X030
REVISIONS					
NO.	DATE	DESCRIPTION			



6/5/2018 2:49:49 PM
 cpyb&w_ANSIB.tbl
 cpybdf_ANSIB.plt
 p:\Active Projects\ODOT13446.00\8.00 Plans and Drawings\8.10 Base Files\21006XSEC.dgn

DESIGN: CPY	2018	US 270	SEMINOLE COUNTY
DRAWN: CPY	2018		
CHECKED: CPY	2018		
APPRVD: CPY	2018		

NS 3610
CROSS SECTIONS

STATE JOB PIECE NO: 21006(07) SHEET NO. X027

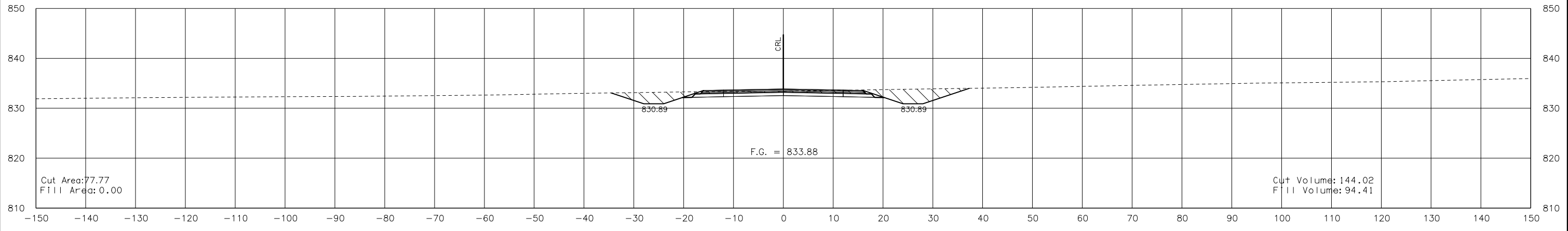
FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X028	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

END AREAS (SF)

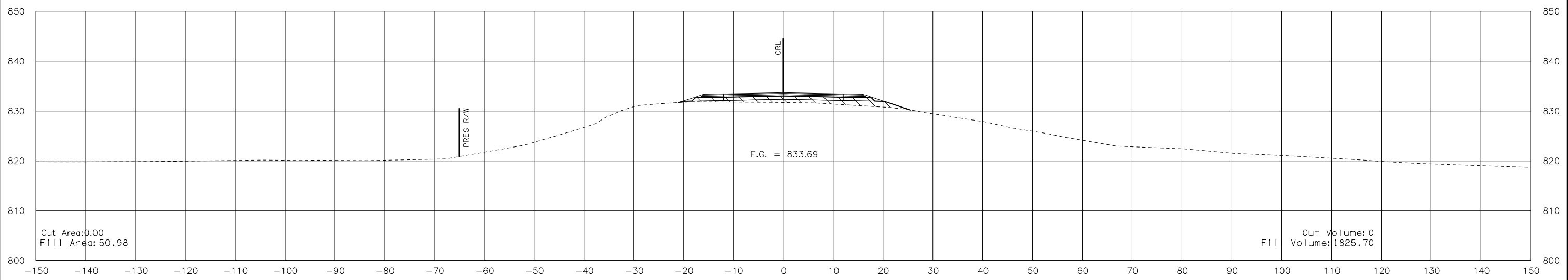
PHASE 1  PHASE 2 

VOLUMES (CY)

PHASE 1  PHASE 2 



17+00.00

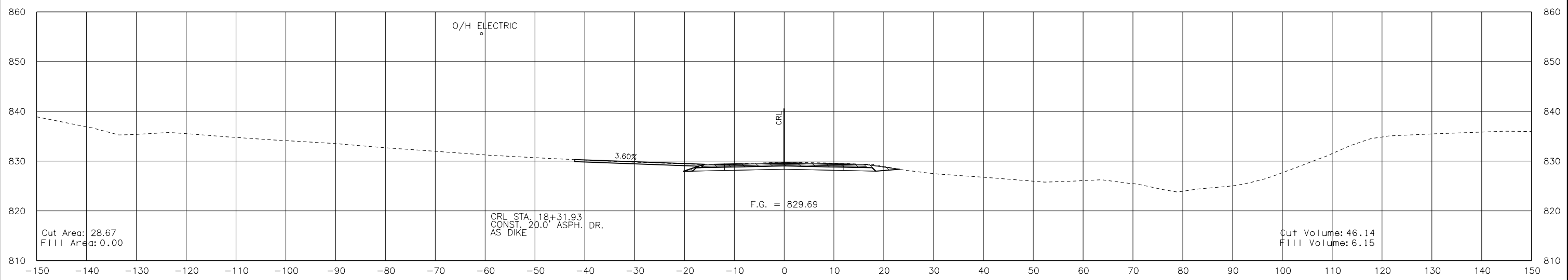


16+00.00

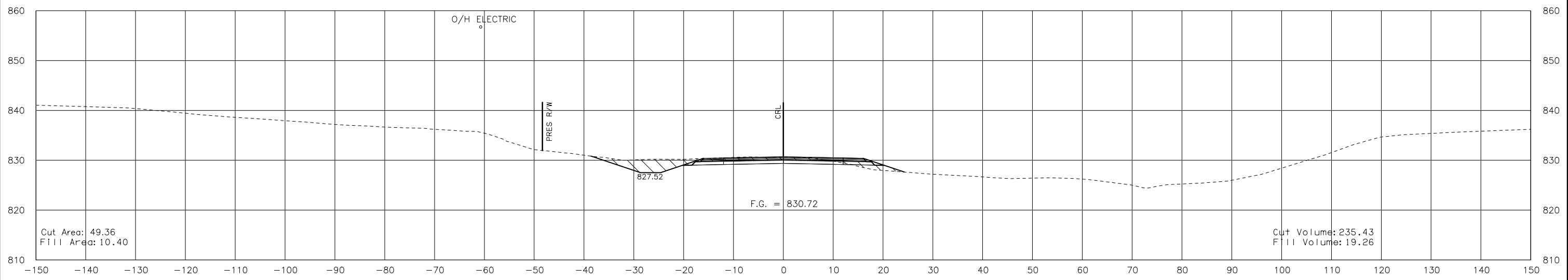
6/5/2018 2:49:50 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
NS 3610 CROSS SECTIONS		
		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X028

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X029	X030
REVISIONS					
NO.	DATE	DESCRIPTION			




18+31.93



18+00.00

6/5/2018 2:49:51 PM
 cpyb&w_ANSIB.tbl
 cpyb&w_ANSIB.tbl
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
NS 3610 CROSS SECTIONS		
		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X029

FED. ROAD DIST. NO.	STATE	PROJ. NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
6	OKLA.	21006(07)	2018	X030	X030
REVISIONS					
NO.	DATE	DESCRIPTION			

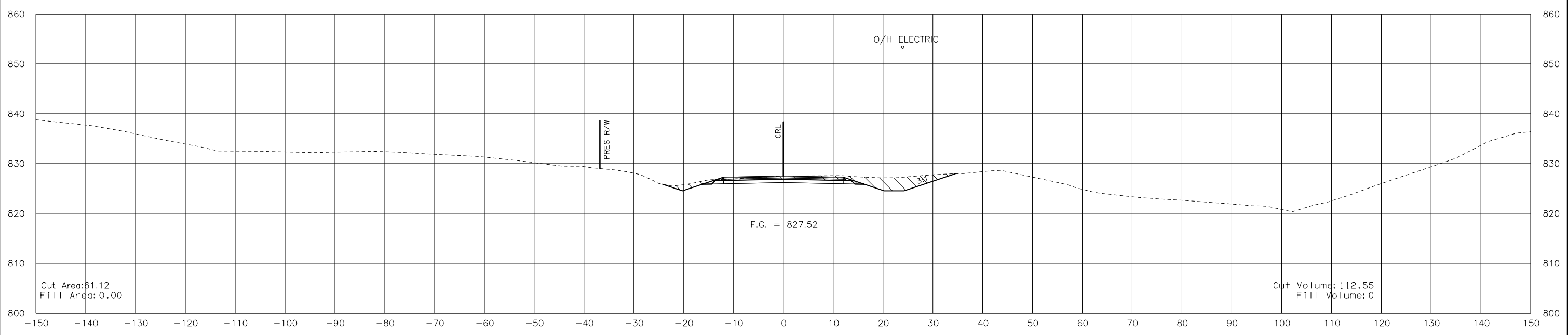
END AREAS (SF)

PHASE 1  PHASE 2 


VOLUMES (CY)

PHASE 1  PHASE 2 

6/5/2018 2:49:52 PM
 cpyb&w_ANSIB.tbl
 cypdf_ANSIB.plt
 pw:/Active Projects/ODOT13446.00/8.00 Plans and Drawings/8.10 Base Files/21006XSEC.dgn



18+99.62

DESIGN: CPY 2018	US 270	SEMINOLE COUNTY
DRAWN: CPY 2018		
CHECKED: CPY 2018		
APPRVD: CPY 2018		
NS 3610 CROSS SECTIONS		
		
STATE JOB PIECE NO: 21006(07)		SHEET NO. X030