

STATE OF OKLAHOMA DEPARTMENT OF TRANSPORTATION

PLAN OF PROPOSED INTERSTATE HIGHWAY

FEDERAL AID PROJECT NO. J3-3788(09) INTERCHANGE

US-75 / I-44 INTERCHANGE

TULSA COUNTY

CONTROL SECTION NO. 44-72-18, 75-72-18

STATE JOB NO. 33788(09)

BRIDGE "O" LOCATION NO. 7218 0982XR, EXISTING NBIS NO. 15838, NEW NBIS NO. XXXXX

BRIDGE "U" LOCATION NO. 72N3934E064001

BRIDGE 'R1' LOCATION NO. 7278 0253NX, EXISTING NBIS NO. 20294; NEW NBI NO. XXXXX

BRIDGE 'R2' LOCATION NO. 7278 0253SX, EXISTING NBIS NO. 12827; NEW NBI NO. XXXXX

BRIDGE 'S' LOCATION NO. 7278 0253SXR, EXISTING NBIS NO. 15769; NEW NBI NO. XXXXX

BRIDGE 'T1' LOCATION NO. 7278 0269NX, EXISTING NBIS NO. 20580; NEW NBI NO. XXXXX

BRIDGE 'T2' LOCATION NO. 7278 0269SX, EXISTING NBIS NO. 20326; NEW NBI NO. XXXXX

FOR INDEX OF SHEETS SEE SHEET 0002

Table listing bridge and frontage details: BRIDGE 'O', RAMP N-E, BRIDGE 'R1', I-44 WB, BRIDGE 'R2', I-44 EB, BRIDGE 'S', I-44 EB RAMP, BRIDGE 'T1', I-44 WB, BRIDGE 'T2', I-44 EB, BRIDGE 'U', FRONTAGE. Includes BEGIN STA., LENGTH, and END STA. for each.

DESIGN DATA table with columns for I-44 and US-75. Rows include ADT 2020, ADT 2040, DHV (2-WAY), K (DHV/ADT), D, T (% DHV), T (% ADT), T^3 (% ADT), V, 20 YR RIGID ESALS, and 20 YR FLEX ESALS.

SCALES: 1" = 50' PLAN, 1" = 50' PROFILE HOR., 1" = 5' VER., 1" = 1,500' LAYOUT MAP

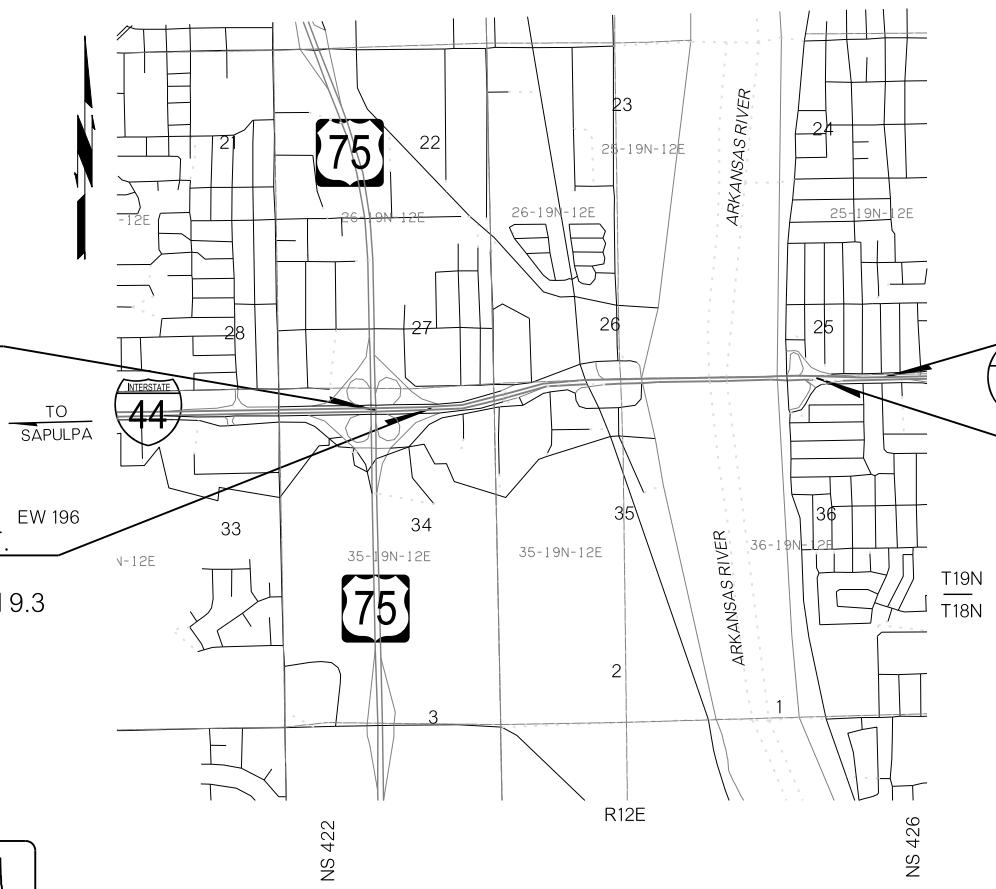
- CONVENTIONAL SYMBOLS: PROPOSED ROAD, RAILROADS, RANGE & TOWNSHIP SECTION LINES, QUARTER SECTION LINES, FENCES, GROUND LINE, EXISTING ROADS, BASE LINE, GRADE LINES, TELEPHONE & TELEGRAPH, POWER LINES, BUILDINGS, OILWELL, DRAINAGE STRUCTURES - IN PLACE, DRAINAGE STRUCTURES - NEW, RIGHT-OF-WAY LINES - EXISTING, RIGHT-OF-WAY LINES - NEW, CONTROLLED ACCESS, RIGHT-OF-WAY FENCE.

CRL I-44 STA. 305+00.00 BEGIN INCIDENTAL CONST.

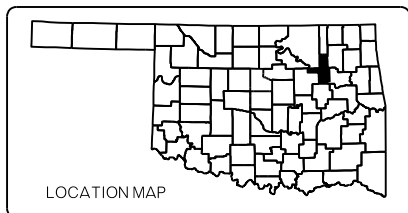
CRL I-44 STA. 310+00.00 END INCIDENTAL CONST. & BEGIN PROJECT CONTROL SUB-SECTION 9.3

CRL I-44 STA. 355+08.86 END PROJECT & BEGIN INCIDENTAL CONST.

CRL I-44 STA. 360+50.00 END INCIDENTAL CONST.



FOR SURVEY CONTROL DATA, SEE SURVEY DATA SHEETS



NOTE: PROJECT LENGTH BASED UPON I-44 CRL STATIONING

Summary table: ROADWAY LENGTH 1,786.21 FT. 0.338 MI., BRIDGE LENGTH 2,722.65 FT. 0.516 MI., PROJECT LENGTH 0.854 MI.

EQUATIONS: NONE, EXCEPTIONS: NONE

PREPARED AND SUBMITTED BY: POE & ASSOCIATES INC., WILLIAM E. WEATHERFORD, P.E., OKLA. REG. NO. 13834. Includes professional engineer seal.

APPROVAL AND PROJECT INFORMATION: OKLAHOMA DEPARTMENT OF TRANSPORTATION, FEDERAL HIGHWAY ADMINISTRATION, DATE APPROVED, BY CHIEF ENGINEER, BY DIVISION ADMINISTRATOR, SWO 5443(3), PROJECT NO. J3-3788(09), COUNTY TULSA, HIGHWAY I-44 / US-75, SHEET NO. 0001.


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B006	GENERAL PLAN AND ELEVATION (BRIDGE R1)
B007	GENERAL PLAN AND ELEVATION (BRIDGE R2)
B008	BRIDGE CONSTRUCTION SEQUENCE
B009	GENERAL PLAN AND ELEVATION (BRIDGE S)
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X057	- X107 CROSS SECTIONS - SKELLY DR.
X108	- X126 CROSS SECTIONS - FRONTAGE RD.
X127	- X135 CROSS SECTIONS - 51ST ST

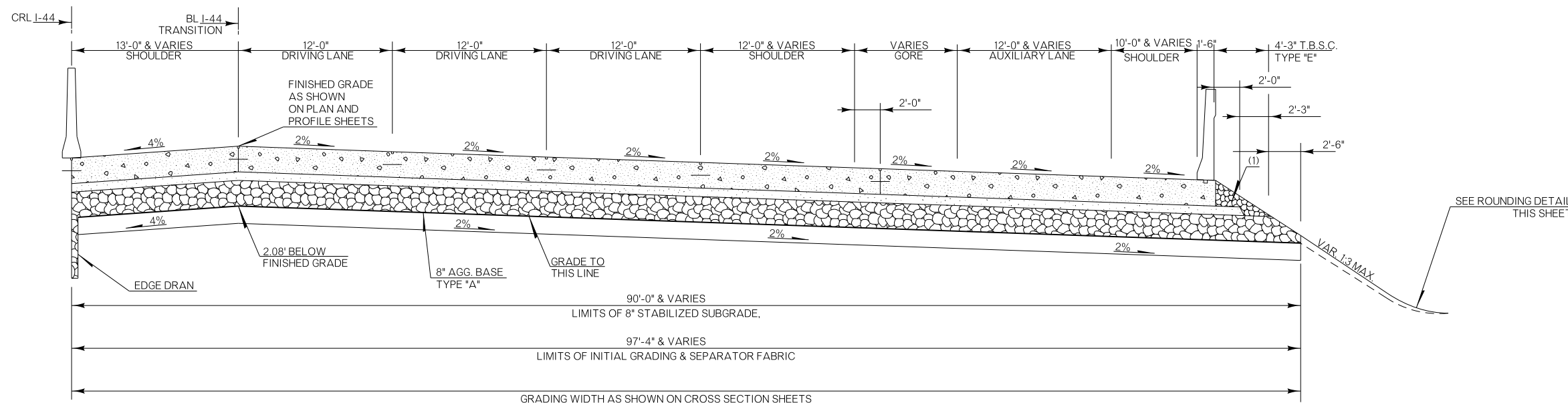
THE FOLLOWING ODOT STANDARDS WILL BE REQUIRED

ROADWAY TRAFFIC CONTROL TRAFFIC LIGHTING TRAFFIC SIGNAL TRAFFIC SIGNING TRAFFIC SAFETY BRIDGE

TO BE ADDED AT A LATER DATE

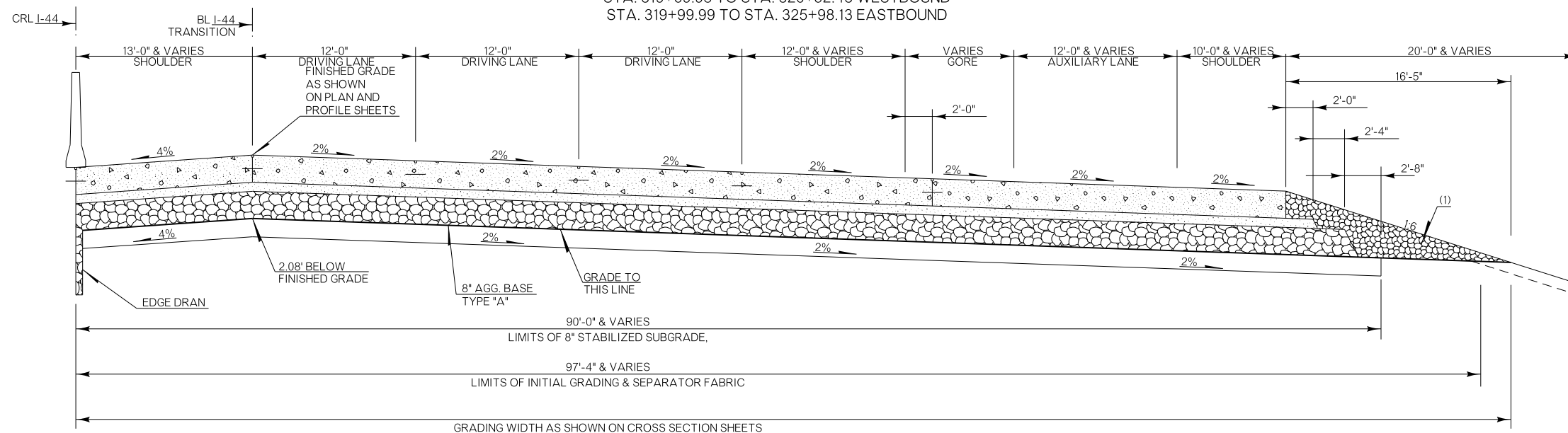
DESIGN			OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN			
CHECKED			
APPROVED			
SQUAD			
COUNTY - TULSA			HIGHWAY - US75/144 STATE JOB NO. - JP33788(09) SHEET NO. 0002

INDEX OF SHEETS AND STANDARDS



TYPICAL SECTION NO. 1: HALF SECTION I-44

STA. 310+00.00 TO STA. 316+64.51 WESTBOUND
 STA. 310+00.00 TO STA. 316+74.51 EASTBOUND
 STA. 319+69.98 TO STA. 320+52.43 WESTBOUND
 STA. 319+99.99 TO STA. 325+98.13 EASTBOUND



TYPICAL SECTION NO. 2: HALF SECTION I-44

STA. 320+14.80 TO STA. 325+98.13 WESTBOUND

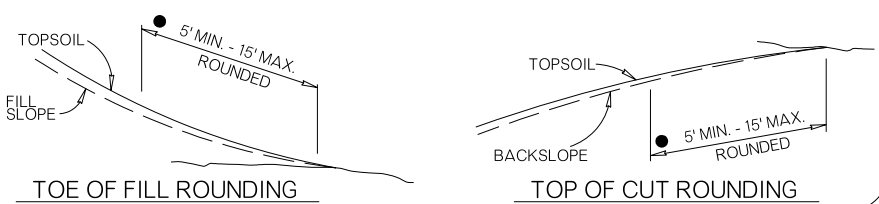
PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
 SECTION USED FOR COST ESTIMATING PURPOSES.

- (1) BACKFILL NOTE:
 TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN TBSG TYPE E.
- (2) TOPSOIL NOTE:
 THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

 THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.
- (3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

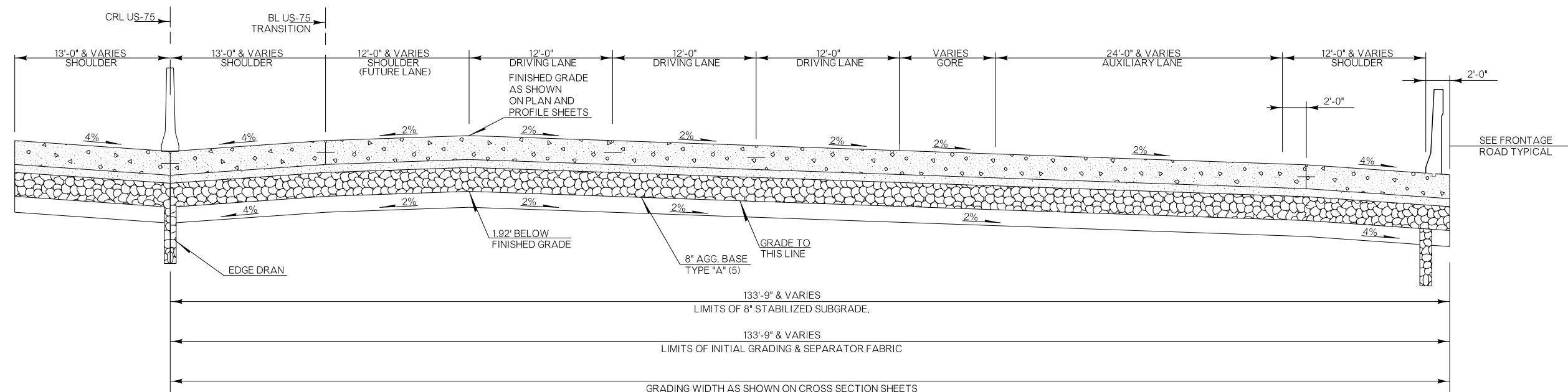
ROUNDING DETAIL

- INTERSECTION OF CUT AND/OR FILL SLOPES WITH GROUND LINE TO BE ROUNDED AS PART OF FINISHING OPERATIONS. ROUNDED SHALL BE 5' MINIMUM FOR SMALLER CUTS AND FILLS TO 15' MAXIMUM FOR LARGER CUTS AND FILLS OR AS DESIGNATED BY THE ENGINEER. COST OF ROUNDED TO BE INCLUDED IN PRICE BID FOR OTHER ITEMS OF WORK.



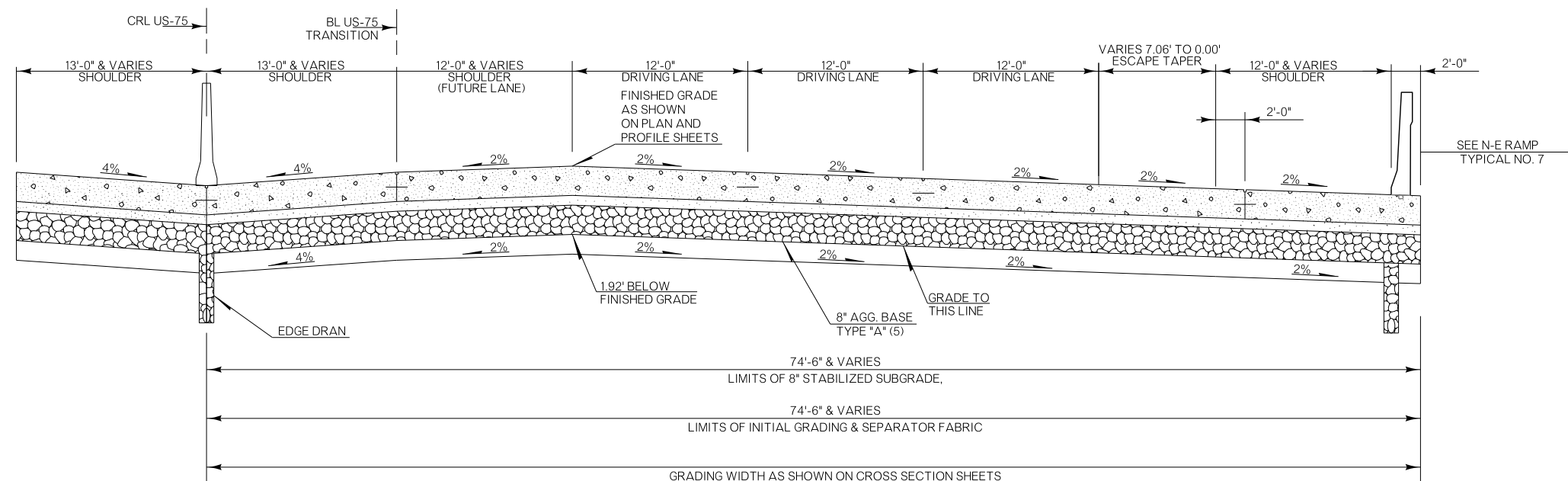
DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN		
CHECKED		
APPROVED		
SQUAD		
COUNTY	TULSA	HIGHWAY US75/I44 STATE JOB NO. JP33788(09) SHEET NO. 0003

TYPICAL SECTION (1)



TYPICAL SECTION NO. 3 : US-75
STA. 547+00.00 TO STA. 556+23.25

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.



TYPICAL SECTION NO. 4 : US-75
STA. 556+23.25 TO STA. 561+25.00

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.

(1) BACKFILL NOTE:
TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN TBSC TYPE E.

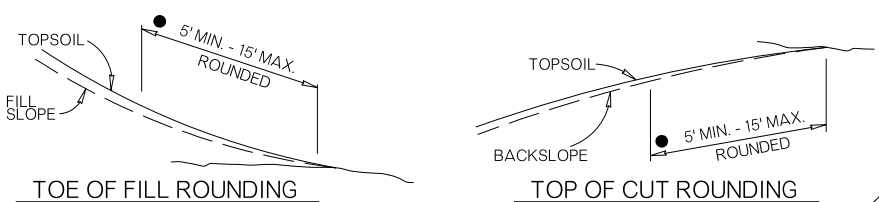
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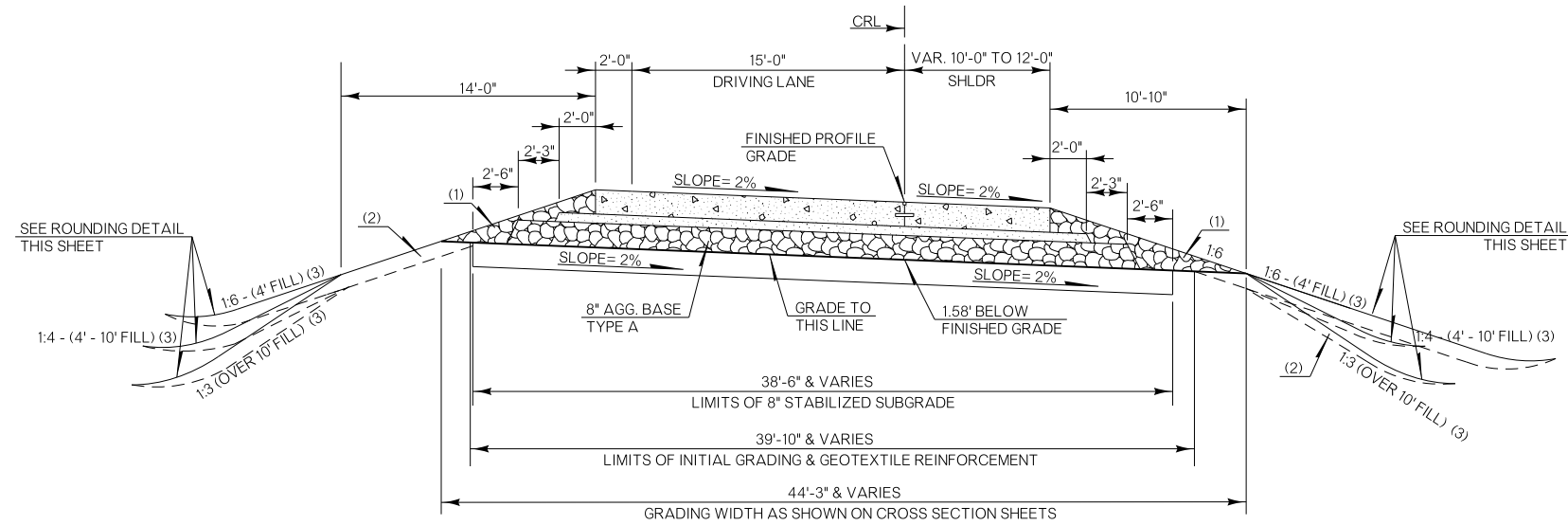
(3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

ROUNDING DETAIL

- INTERSECTION OF CUT AND/OR FILL SLOPES WITH GROUND LINE TO BE ROUNDED AS PART OF FINISHING OPERATIONS. ROUNDED SHALL BE 5' MINIMUM FOR SMALLER CUTS AND FILLS TO 15' MAXIMUM FOR LARGER CUTS AND FILLS OR AS DESIGNATED BY THE ENGINEER. COST OF ROUNDED TO BE INCLUDED IN PRICE BID FOR OTHER ITEMS OF WORK.

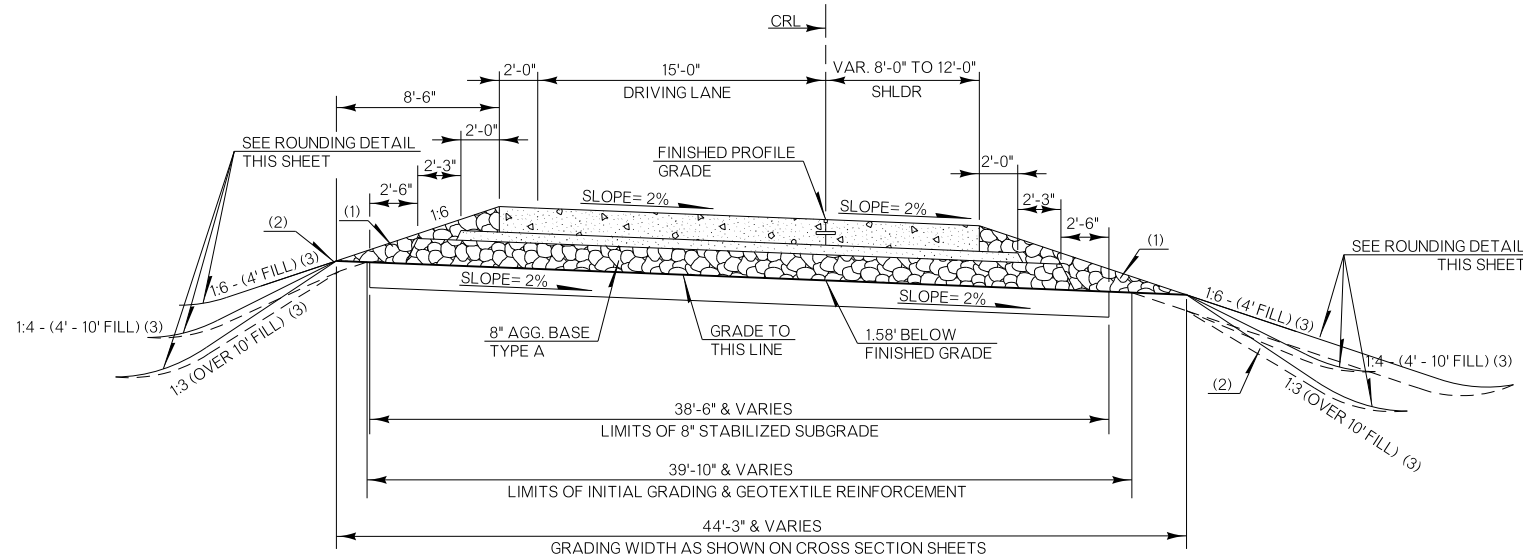


DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES					
DRAWN		TYPICAL SECTION (2)					
CHECKED							
APPROVED							
SQUAD							
COUNTY	TULSA	HIGHWAY	US75/I44	STATE JOB NO.	JP33788(09)	SHEET NO.	0004



TYPICAL SECTION NO. 5 : I-44 WESTBOUND OFF RAMP
STA. 18+34.06 TO STA. 20+57.08

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.

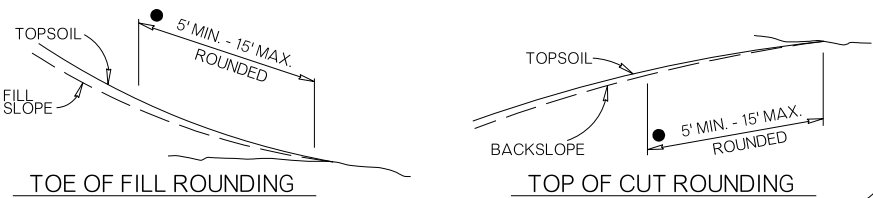


TYPICAL SECTION NO. 6 : I-44 EASTBOUND ON RAMP
STA. 116+20.01 TO STA. 117+32.03

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.

ROUNDING DETAIL

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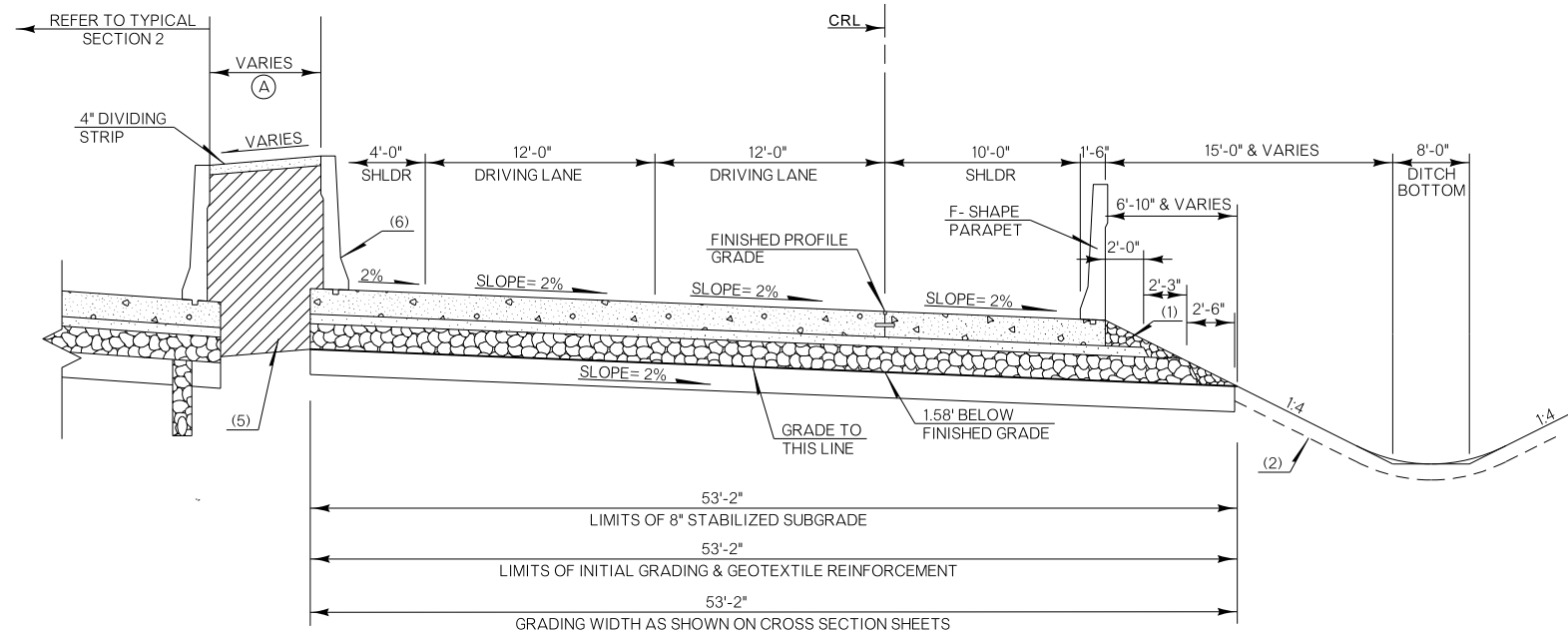
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(3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

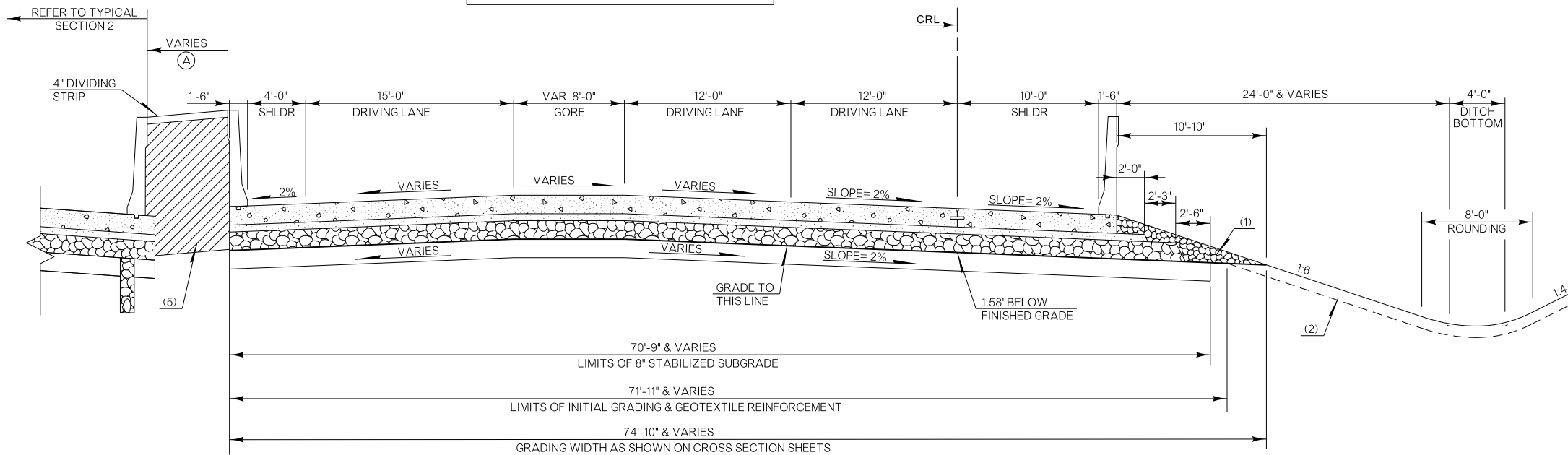
(4) PRIME COAT ON TOP OF AGGREGATE BASE.

DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN		TYPICAL SECTION (3)
CHECKED		
APPROVED		
SQUAD		
COUNTY	TULSA	HIGHWAY US75/I44 STATE JOB NO. JP33788(09) SHEET NO. 0005



TYPICAL SECTION NO. 7 : RAMP FROM NORTHBOUND US-75 TO EASTBOUND I-44
STA. 156+22.56 TO STA. 157+45.89

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.



TYPICAL SECTION NO. 8 : RAMP FROM NORTHBOUND US-75 TO EASTBOUND I-44
STA. 158+30.49 TO STA. 161+43.63

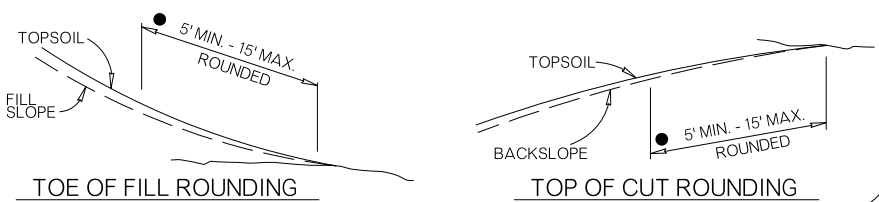
PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.

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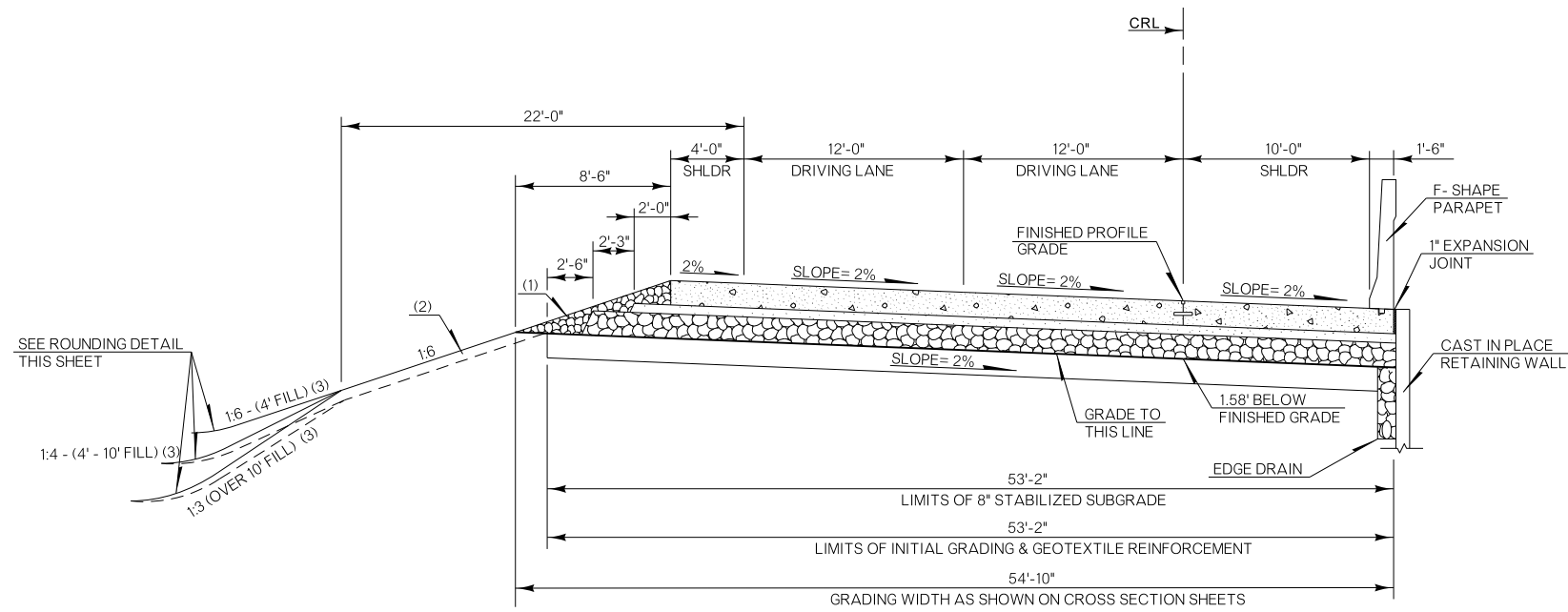
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- (3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.
- (4) PRIME COAT ON TOP OF AGGREGATE BASE.
- (5) TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN UNCLASSIFIED BORROW.

ROUNDING DETAIL

- INTERSECTION OF CUT AND/OR FILL SLOPES WITH GROUND LINE TO BE ROUNDED AS PART OF FINISHING OPERATIONS. ROUNDED SHALL BE 5' MINIMUM FOR SMALLER CUTS AND FILLS TO 15' MAXIMUM FOR LARGER CUTS AND FILLS OR AS DESIGNATED BY THE ENGINEER. COST OF ROUNDED TO BE INCLUDED IN PRICE BID FOR OTHER ITEMS OF WORK.

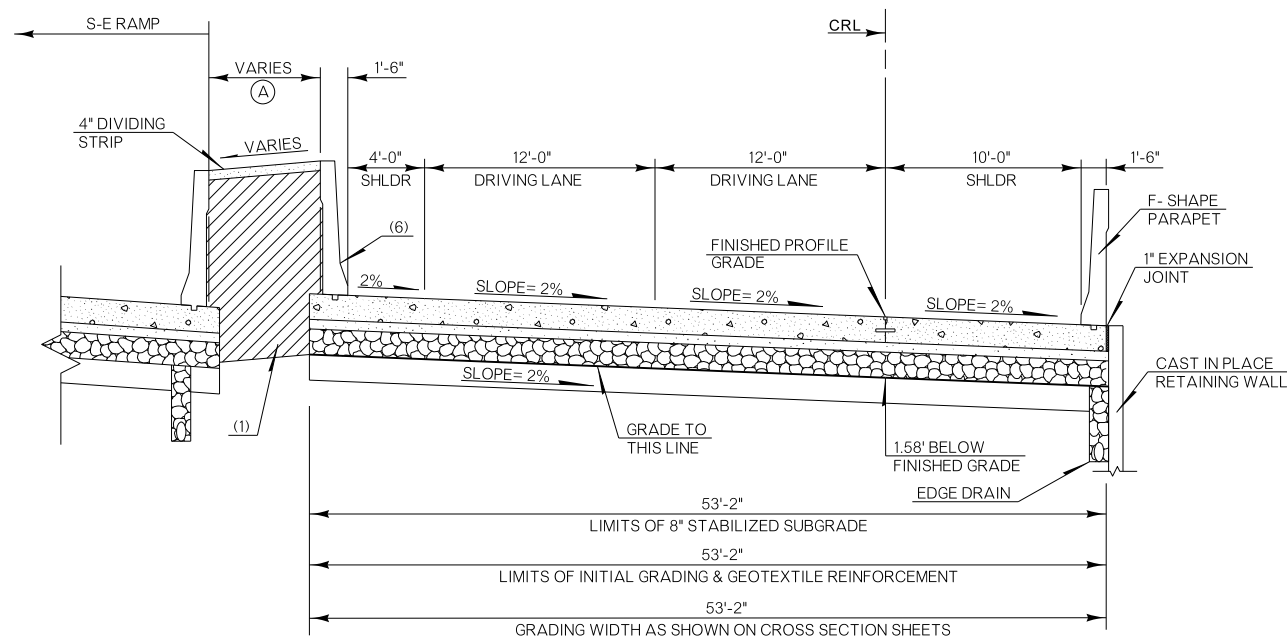


DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN		TYPICAL SECTION (4)
CHECKED		
APPROVED		
SQUAD		
COUNTY	TULSA	



TYPICAL SECTION NO. 9 : RAMP FROM NORTHBOUND US-75 TO EASTBOUND I-44
STA. 166+97.38 TO STA. 172+86.08

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.



TYPICAL SECTION NO. 10 : RAMP FROM NORTHBOUND US-75 TO EASTBOUND I-44
STA. 172+86.08 TO STA. 182+64.93

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.

(1) TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN UNCLASSIFIED BORROW.

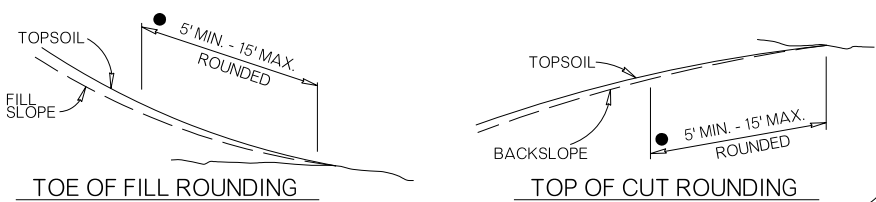
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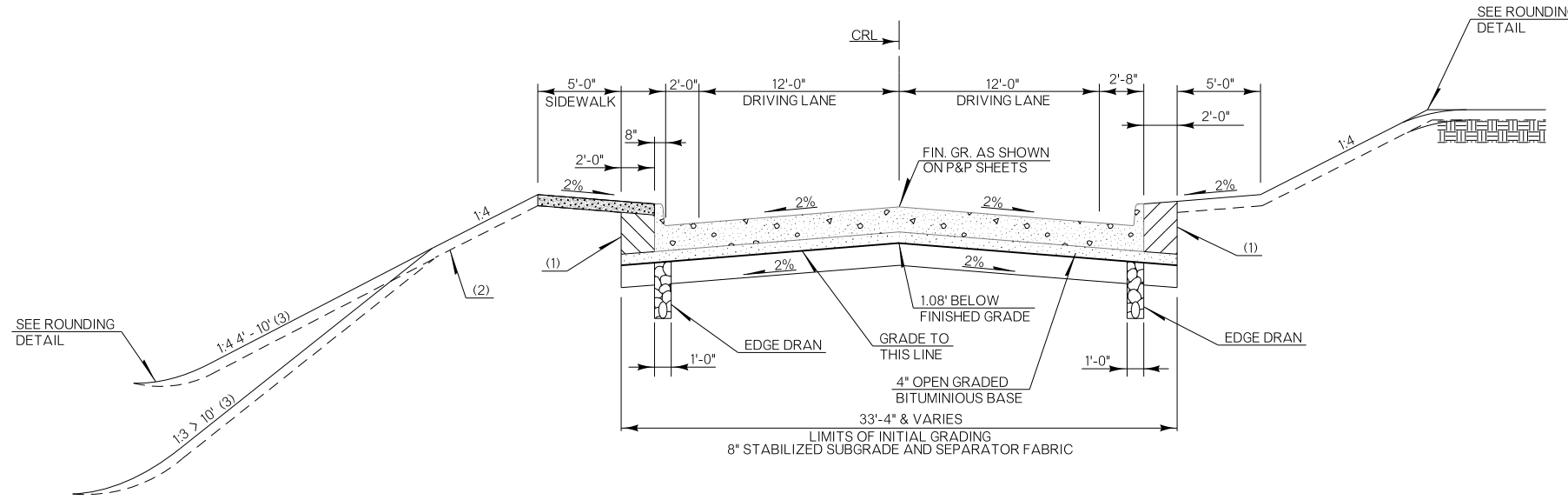
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DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN		
CHECKED		
APPROVED		
SQUAD		
COUNTY	TULSA	HIGHWAY US75/I44 STATE JOB NO. JP33788(09) SHEET NO. 0007

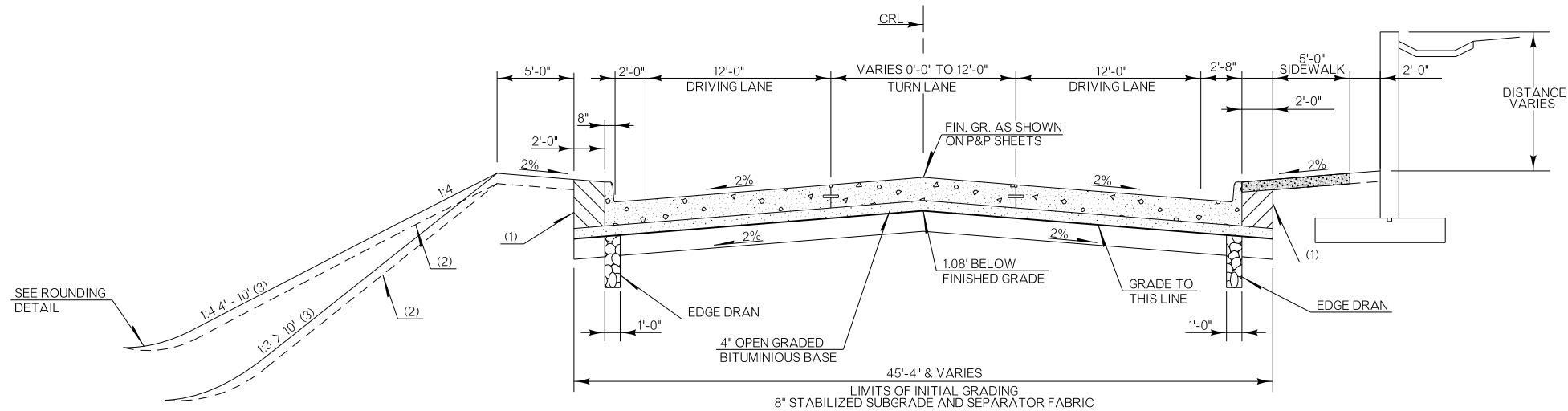
TYPICAL SECTION (5)



TYPICAL SECTION NO. 11 : 51ST STREET

STA. 552+01.26 TO STA. 556+70.90
STA. 558+39.08 TO STA. 566+40.88

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.



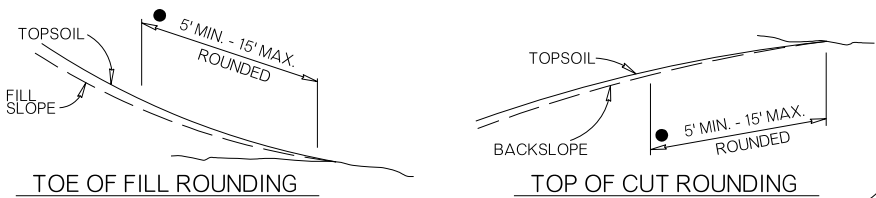
TYPICAL SECTION NO. 12 : FRONTAGE ROAD

STA. 155+85.71 TO STA. 163+30.00

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
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ROUNDING DETAIL

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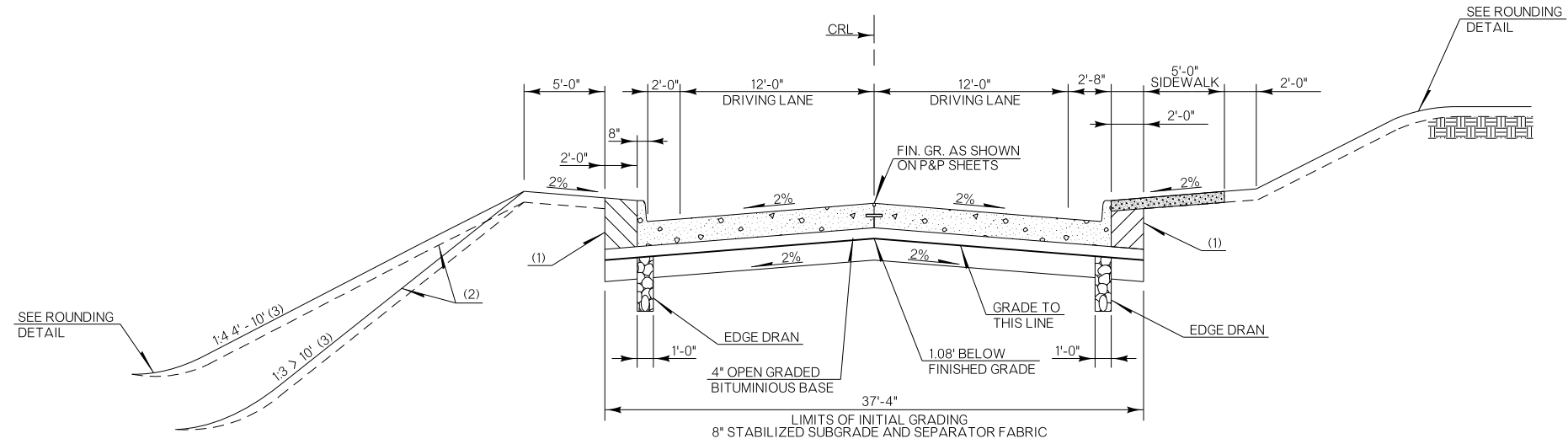
(1) TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN UNCLASSIFIED BORROW.

(2) THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

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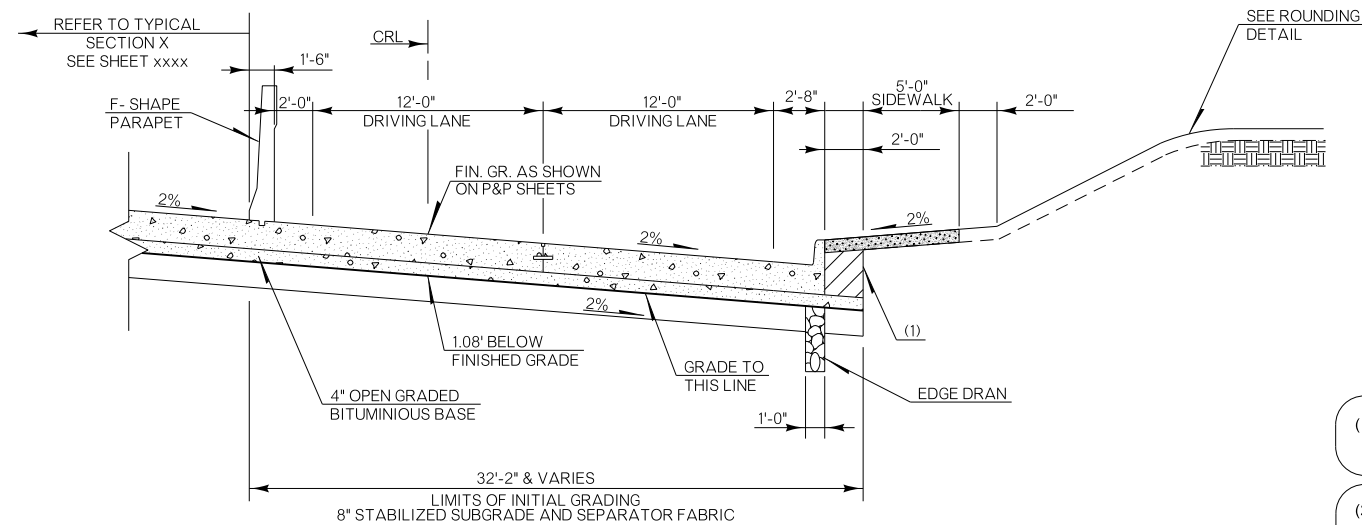
DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES					
DRAWN		TYPICAL SECTION (6)					
CHECKED							
APPROVED							
SQUAD							
COUNTY	TULSA	HIGHWAY	US75/I44	STATE JOB NO.	JP33788(09)	SHEET NO.	0008



TYPICAL SECTION NO. 13 : FRONTAGE ROAD

STA. 163+30.00 TO STA. 165+00.00
STA. 184+25.92 TO STA. 192+66.60

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.



TYPICAL SECTION NO. 14 : FRONTAGE ROAD

STA. 165+00.00 TO STA. 184+25.92

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
SECTION USED FOR COST ESTIMATING PURPOSES.

(1) TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN UNCLASSIFIED BORROW.

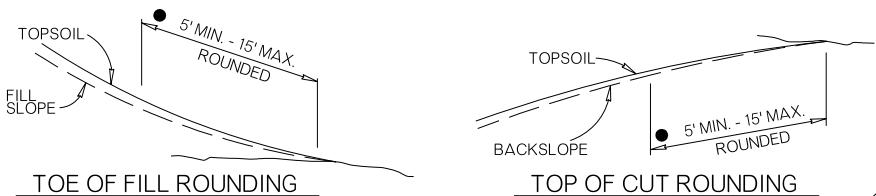
(2) THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.

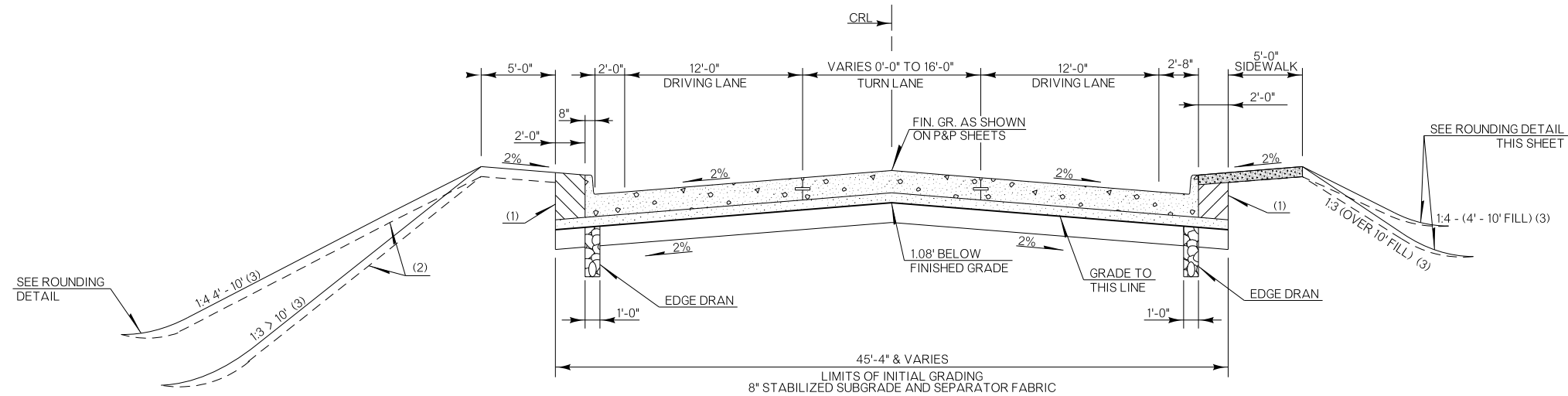
(3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

ROUNDING DETAIL

- INTERSECTION OF CUT AND/OR FILL SLOPES WITH GROUND LINE TO BE ROUNDED AS PART OF FINISHING OPERATIONS. ROUNDED SHALL BE 5' MINIMUM FOR SMALLER CUTS AND FILLS TO 15' MAXIMUM FOR LARGER CUTS AND FILLS OR AS DESIGNATED BY THE ENGINEER. COST OF ROUNDED TO BE INCLUDED IN PRICE BID FOR OTHER ITEMS OF WORK.



DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN		
CHECKED		
APPROVED		
SQUAD		
TULSA		TYPICAL SECTION (7)
HIGHWAY US75/I44		STATE JOB NO. JP33788(09)
		SHEET NO. 0009

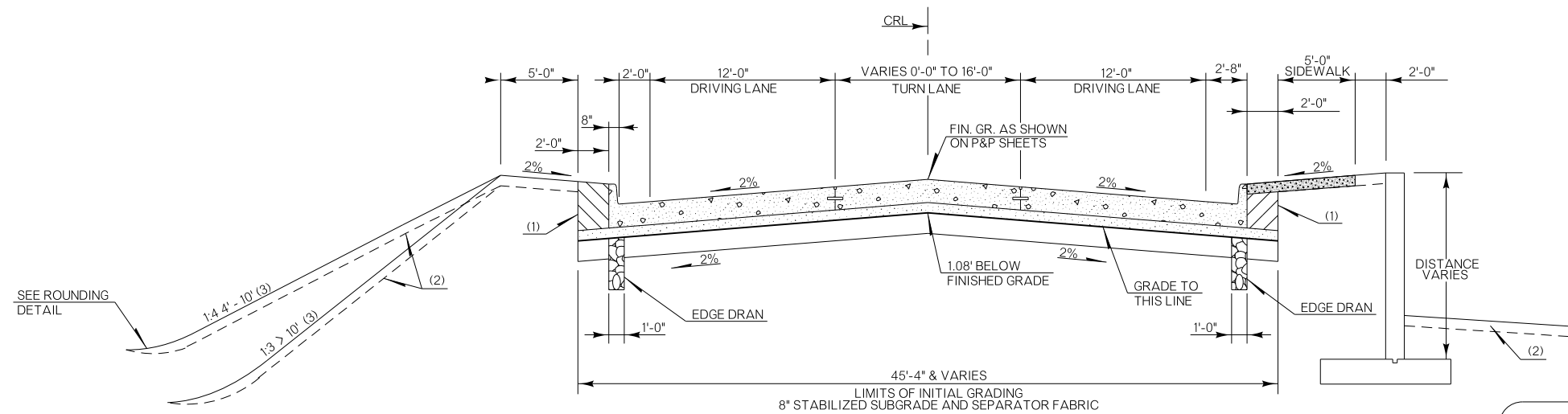


TYPICAL SECTION NO. 15 : SKELLY DRIVE

STA. 586+00.00 TO STA. 599+50.00
 STA. 603+10.00 TO STA. 617+93.09 *
 STA. 623+25.37 TO STA. 625+81.45 *
 STA. 627+58.91 TO STA. 640+66.55 *

* SIDEWALK OMITTED FROM STA. 617+35.92 TO STA. 638+39.00

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
 SECTION USED FOR COST ESTIMATING PURPOSES.



TYPICAL SECTION NO. 15 : SKELLY DRIVE

STA. 599+50.00 TO STA. 603+10.00
 STA. 617+93.09 TO STA. 623+25.37

PAVEMENT DESIGN HAS NOT BEEN COMPLETED.
 SECTION USED FOR COST ESTIMATING PURPOSES.

(1) TO BE BACKFILLED AND COMPACTED AS PART OF THE FINISHING OPERATIONS. QUANTITY IS MEASURED IN UNCLASSIFIED BORROW.

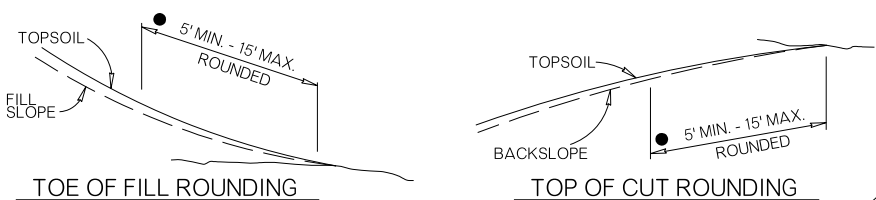
(2) THE CONTRACTOR SHALL STRIP ALL OF THE AVAILABLE TOPSOIL, STOCKPILE IT, AND PLACE IT BACK ON THE SECTION IN ACCORDANCE WITH SECTION 205 OF THE STANDARD SPECIFICATIONS. RESERVED TOPSOIL SHALL BE SPREAD FIRST ON THE COMPLETED SLOPES OF THE CUT SECTIONS AND THE REMAINDER ON COMPLETED FILL SLOPES OR OTHER PRIORITY AREAS LOCATED BY THE ENGINEER. ALL ADDITIONAL COSTS ASSOCIATED WITH OPERATIONS SHALL BE INCLUDED IN THE PAY ITEM FOR SALVAGED TOPSOIL, LUMP SUM.

THE GRADING LINE AS SHOWN ON THE TYPICAL AND CROSS SECTIONS IS TO THE TOP OF THE TOPSOIL. EARTHWORK QUANTITIES WERE NOT ADJUSTED FOR SALVAGE AND THE TOPSOIL QUANTITY IS INCLUDED IN THE MASS LINE BALANCE.

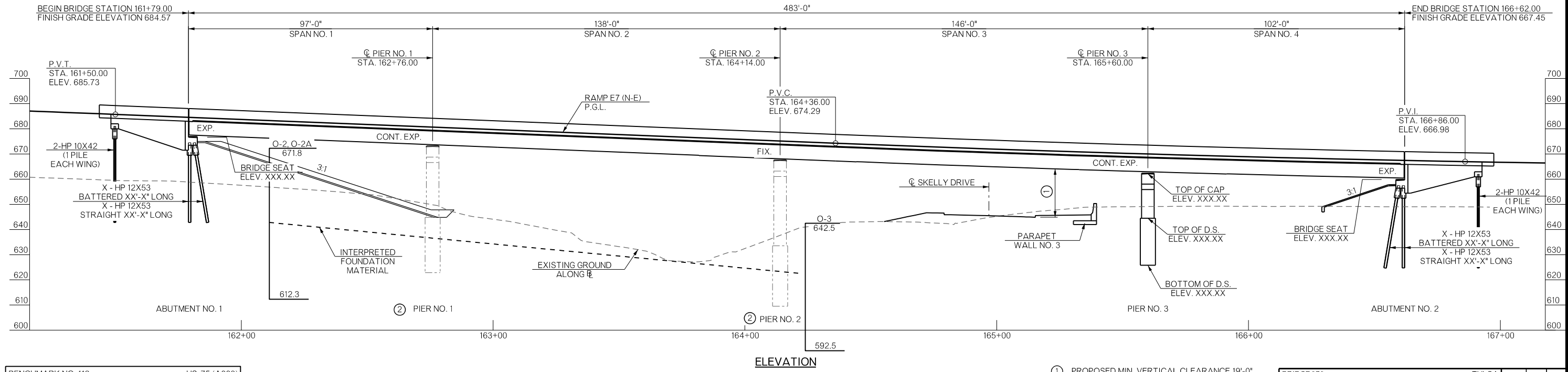
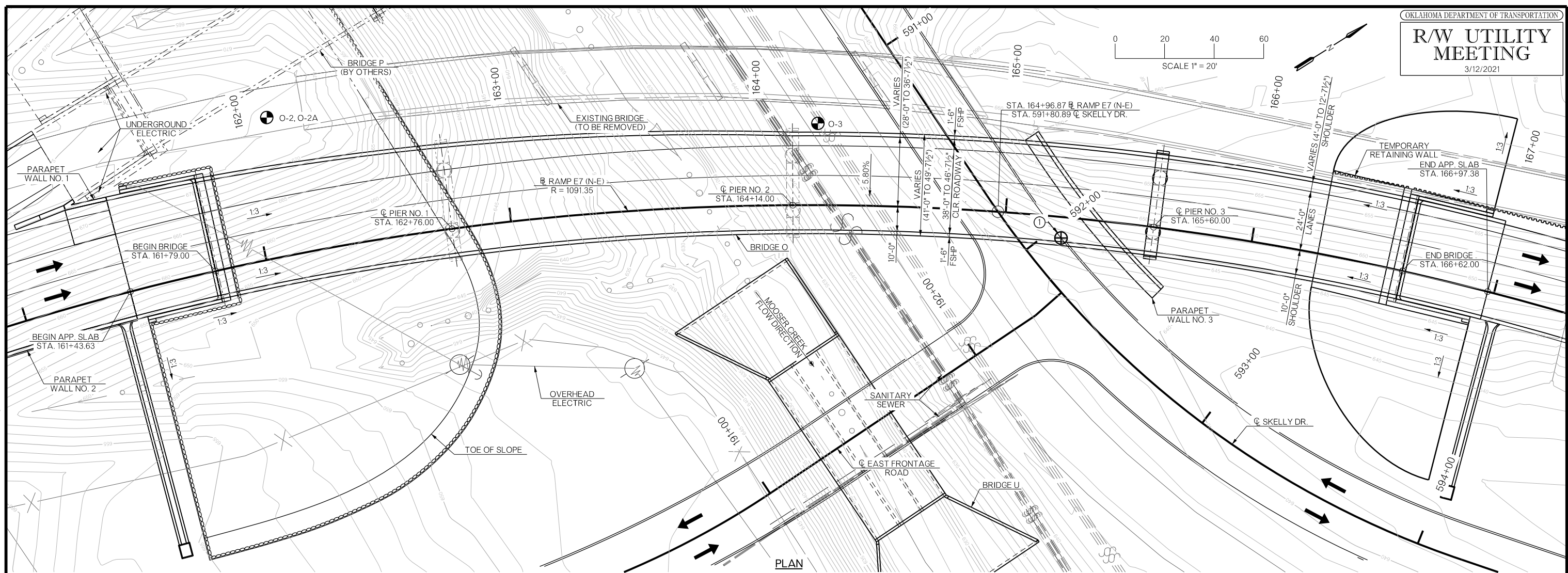
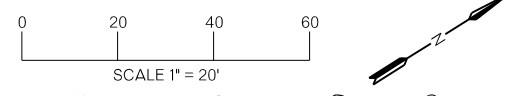
(3) DISTANCE MEASURED VERTICALLY FROM EDGE OF FINISHED GRADE SHOULDER.

ROUNDING DETAIL

● INTERSECTION OF CUT AND/OR FILL SLOPES WITH GROUND LINE TO BE ROUNDED AS PART OF FINISHING OPERATIONS. ROUNDED SHALL BE 5' MINIMUM FOR SMALLER CUTS AND FILLS TO 15' MAXIMUM FOR LARGER CUTS AND FILLS OR AS DESIGNATED BY THE ENGINEER. COST OF ROUNDED TO BE INCLUDED IN PRICE BID FOR OTHER ITEMS OF WORK.



DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN		TYPICAL SECTION (8)
CHECKED		
APPROVED		
SQUAD		
COUNTY	TULSA	
HIGHWAY US75/I44 STATE JOB NO. JP33788(09) SHEET NO. 0010		



BENCHMARK NO. 112 CUT BOX AT WEST EDGE OF CONCRETE BASE FOR LIGHT POLE OFFSET 78.24' LT. STA. 252+33.25	US-75 (A003) EL. 674.92
BENCHMARK NO. 1270 CUT X AT CENTER OF HEADWALL OFFSET 103.87' RT. STA. 121+96.83	I-44 (A001) EL. 649.57

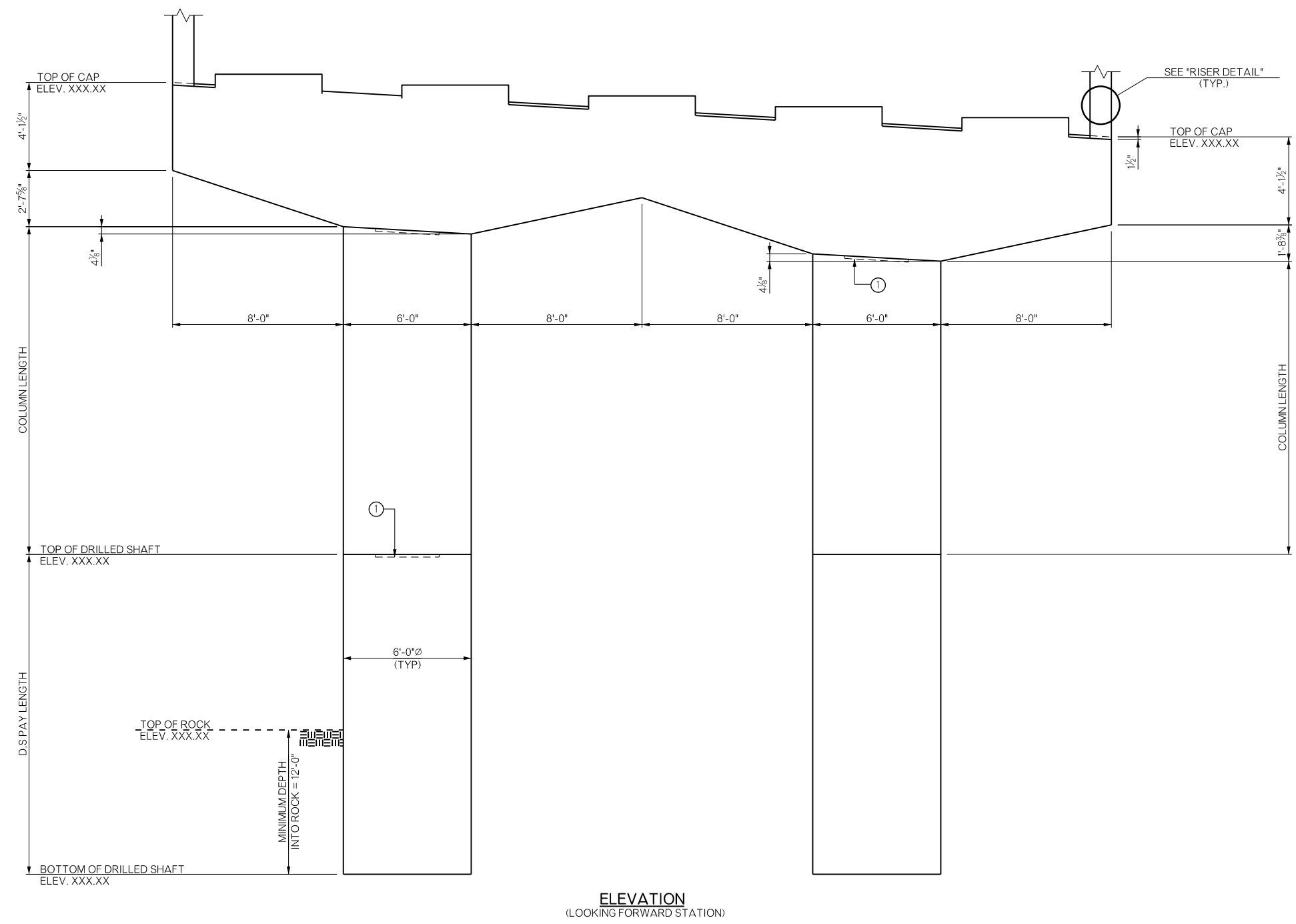
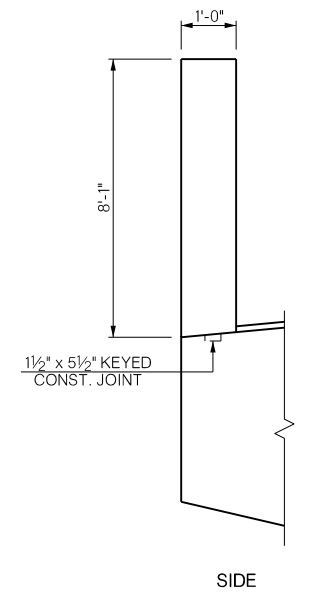
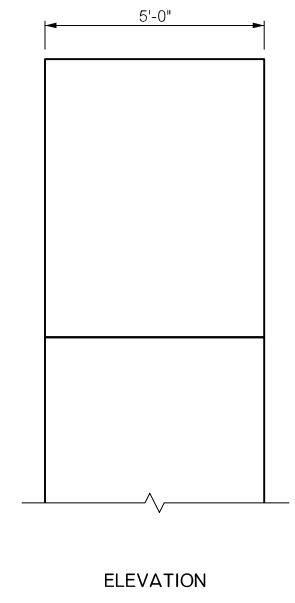
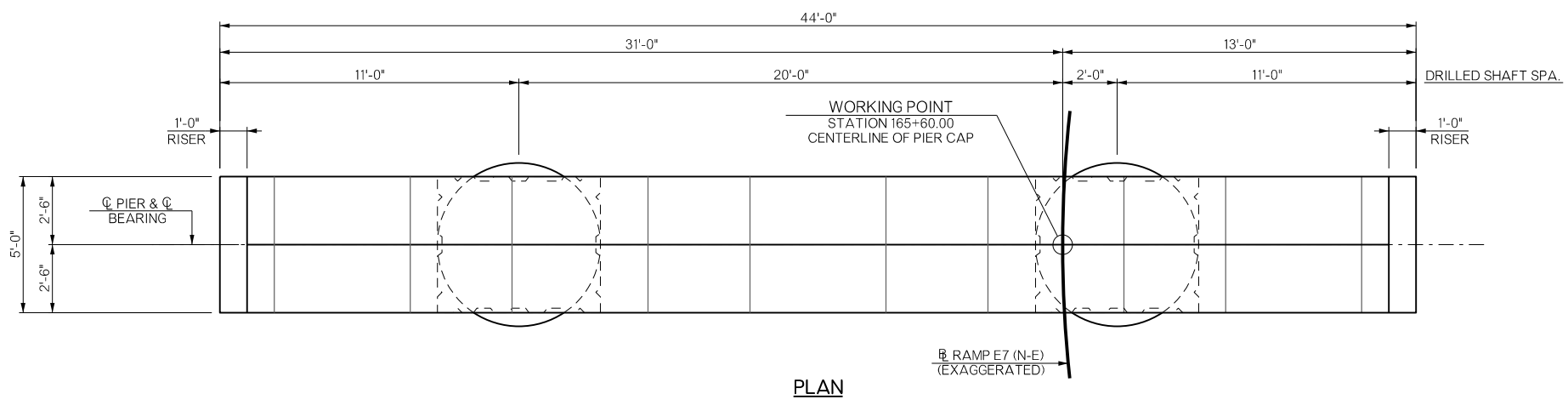
FOR FOUNDATION REPORT, SEE SHEET
 NOS. XXX-XXX AND JP 33788(04)

NOTES:
 FOR DESIGN DATA, HYDRAULIC DATA, VERTICAL PROFILE DATA,
 & FOUNDATION DATA SEE SHEET NO. B002.
 TOP OF CAP ELEVATION IS TAKEN AT THE WORKING POINT OF
 THE PIER.

- ① PROPOSED MIN. VERTICAL CLEARANCE 19'-0"
 RAMP E7 STA. 165+23.16
 OFFSET 8'-6" RT.
 LOW CHORD ELEV. 663.89
- ② PIER NOS. 1 & 2 CONSTRUCTED AS PART
 OF CONTRACT JP33788(04).

BRIDGE "O" RAMP E7 OVER MOUSER CRK. & SKELLY DR.		TULSA COUNTY	DESIGN	DPE	7/20
GENERAL PLAN AND ELEVATION			DETAIL	SJL	7/20
(SHEET 1 OF 2)			CHECK	DPE	8/20
CONSTRUCT 97'-138'-146'-102' STEEL P.G. SPAN W/ 42' PARAPETS W/ 38'-0" CLEAR ROADWAY AT CENTERLINE STA. 164+20.50			GARVER		
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	JOB PIECE NO. 33788(09) SHEET NO. B001			

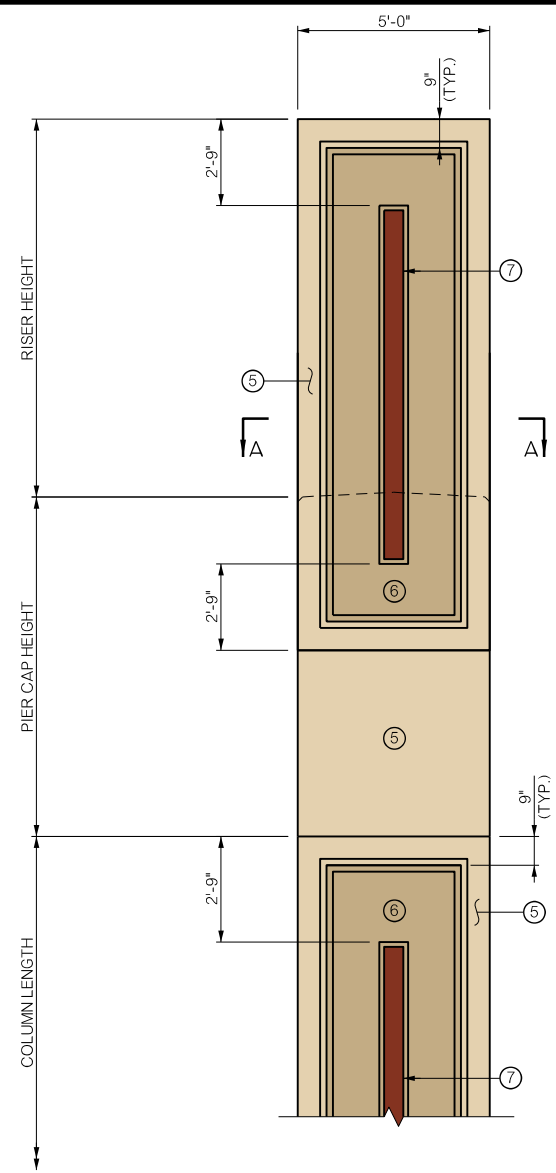
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① KEYED CONST. JOINT 3'-0" SQ. X 1 1/2" DEEP

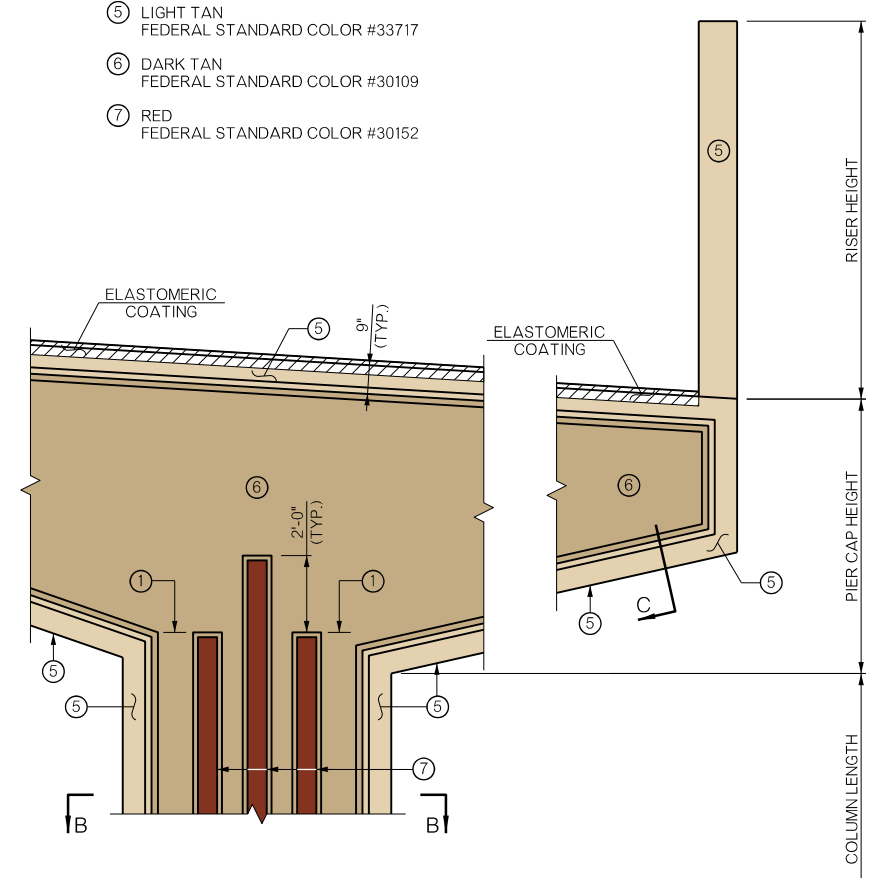
BRIDGE "O" RAMP E7 OVER MOOSER CRK. & SKELLY DR.		TULSA COUNTY	DESIGN	DPE	7/20
			DETAIL	SJL	8/20
			CHECK	DPE	8/20
PIER DETAILS			GARVER		
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION				
	JOB PIECE NO. 33788(09)		SHEET NO. B003		

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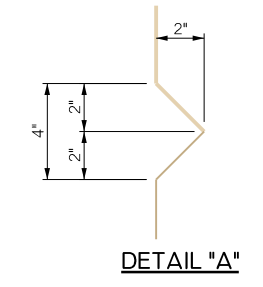
COLUMN ELEVATION

- ⑤ LIGHT TAN
FEDERAL STANDARD COLOR #33717
- ⑥ DARK TAN
FEDERAL STANDARD COLOR #30109
- ⑦ RED
FEDERAL STANDARD COLOR #30152

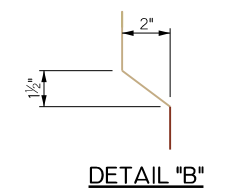


PIER CAP ELEVATION
(SHOWING TYPICAL COLUMN)

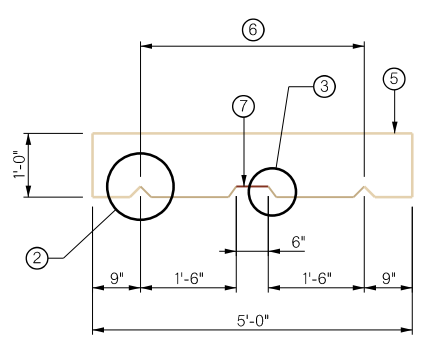
- ① TOP OF OUTSIDE VERTICAL BARS TO MATCH INSIDE CORNER OF V-NOTCH ON HIGH SIDE OF PIER CAP.
- ② 2" x 4" V-NOTCH. TYPICAL. SEE DETAIL "A".
- ③ 1½" x 2" CHAMFER TYPICAL AT ALL EDGES OF VERTICAL BARS ON RISER AND COLUMN. SEE DETAIL "B".
- ④ V-NOTCHES AND VERTICAL BARS ON COLUMNS SHALL EXTEND TO BOTTOM OF COLUMN.



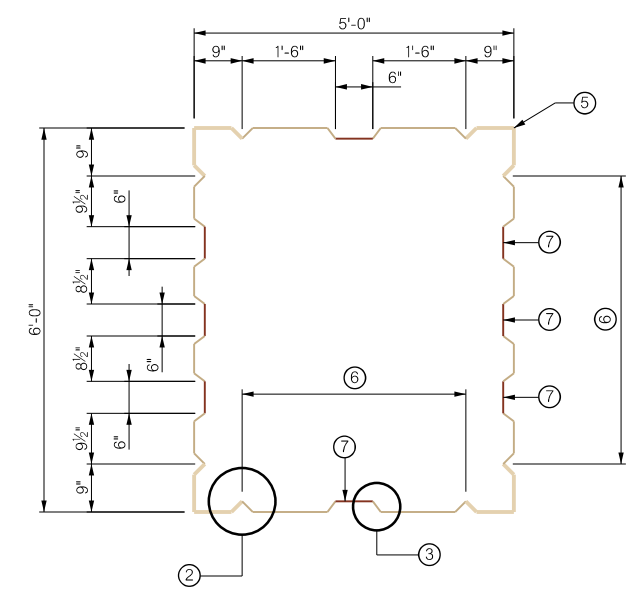
DETAIL "A"



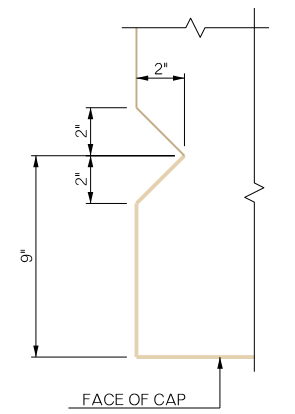
DETAIL "B"



SECTION A-A



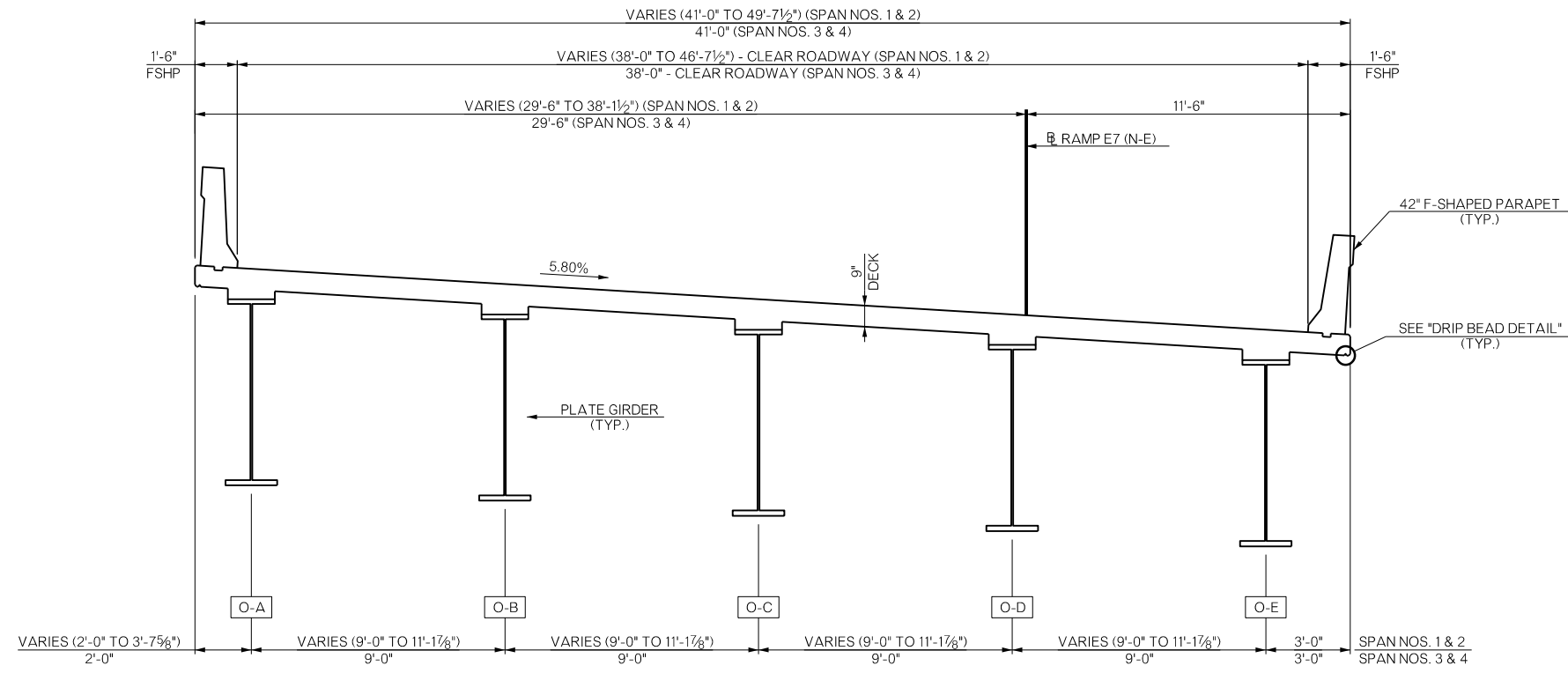
SECTION B-B



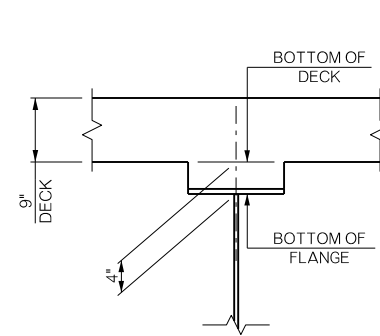
SECTION C-C

BRIDGE "O" RAMP E7 OVER MOOSER CRK. & SKELLY DR.		TULSA COUNTY	DESIGN	DPE	7/20	
PIER AESTHETICS DETAILS			DETAIL	S.J.L.	8/20	
			CHECK	DPE	8/20	
STATE OF OKLAHOMA			DEPARTMENT OF TRANSPORTATION			GARVER
						JOB PIECE NO. 33788(09)

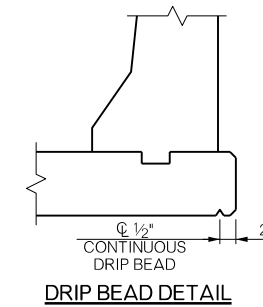
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TYPICAL SECTION
(LOOKING FORWARD STATION)



BEAM HAUNCH DETAIL



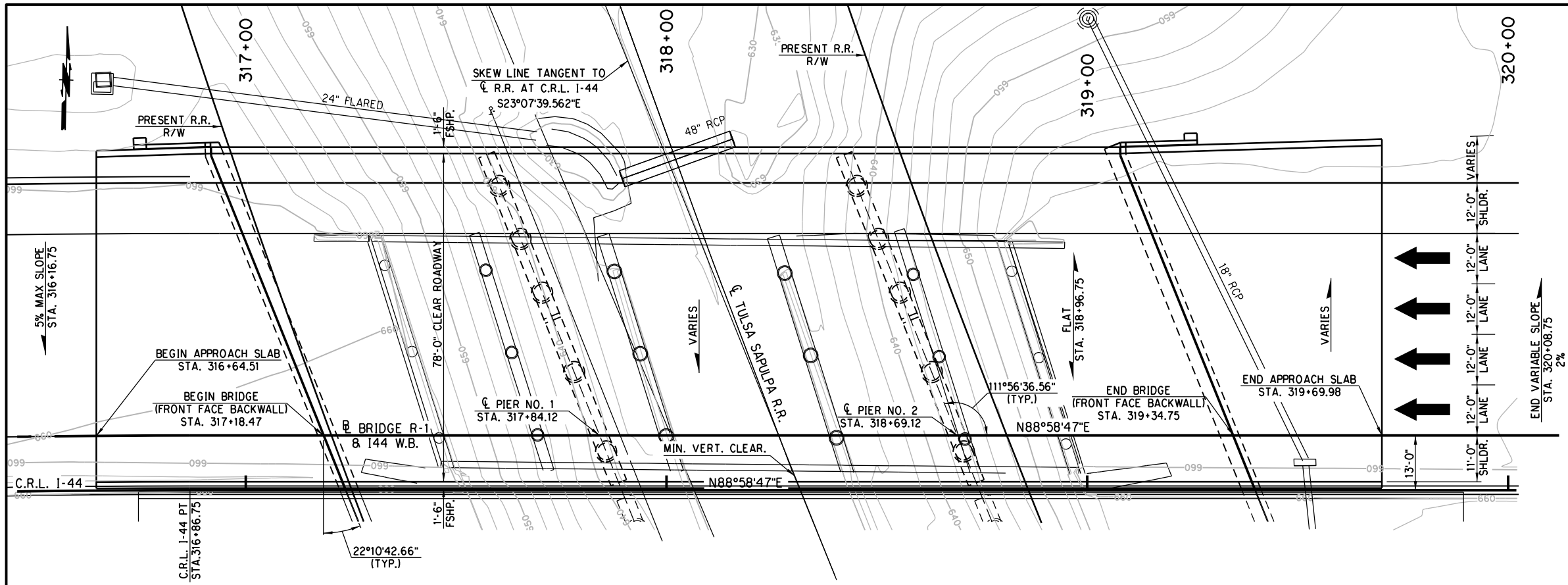
DRIP BEAD DETAIL

NOTE:
PLAN QUANTITIES FOR CLASS AA CONCRETE INCLUDE GIRDER HAUNCHES. THE HAUNCH HEIGHT SHOWN IS THE THEORETICAL HAUNCH HEIGHT AT THE CENTERLINE BEARING ONLY, MEASURED FROM BOTTOM OF DECK SLAB TO BOTTOM OF TOP FLANGE, AND VARIES ACROSS THE SPAN. DETERMINE THE ACTUAL HAUNCH HEIGHT (ACCOUNTING FOR BEAM CAMBER, DEAD LOAD DEFLECTION AND ROADWAY GRADE) AFTER ERECTION OF THE BEAMS AND SUBMIT TO THE ENGINEER FOR APPROVAL. THE ENGINEER WILL NOT MEASURE DIFFERENCES BETWEEN THE THEORETICAL AND THE ACTUAL HAUNCH HEIGHTS FOR PAYMENT.

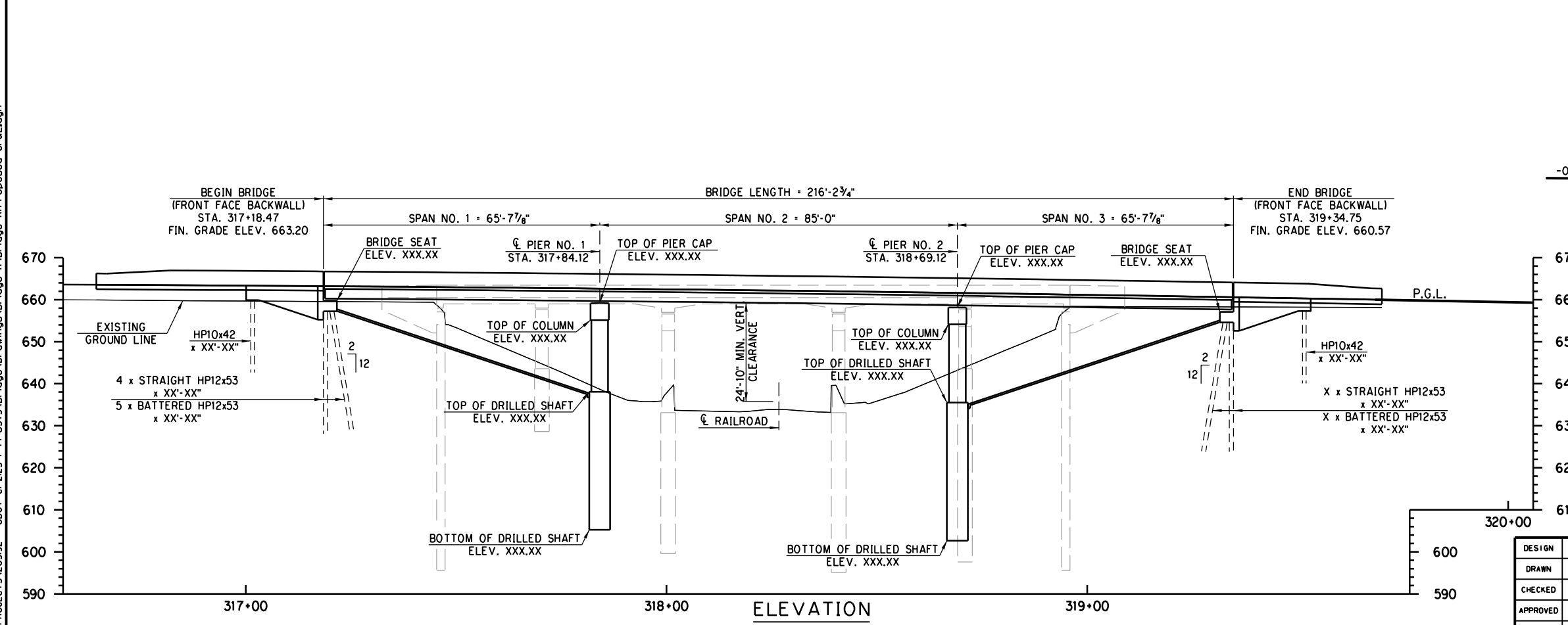
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BRIDGE "O" RAMP E7 OVER MOOSER CRK. & SKELLY DR.		TULSA COUNTY	DESIGN	DPE	7/20
			DETAIL	SJL	7/20
			CHECK	DPE	8/20
TYPICAL SECTION					
GARVER					
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION			
		JOB PIECE NO. 33788(09)		SHEET NO. _B005	

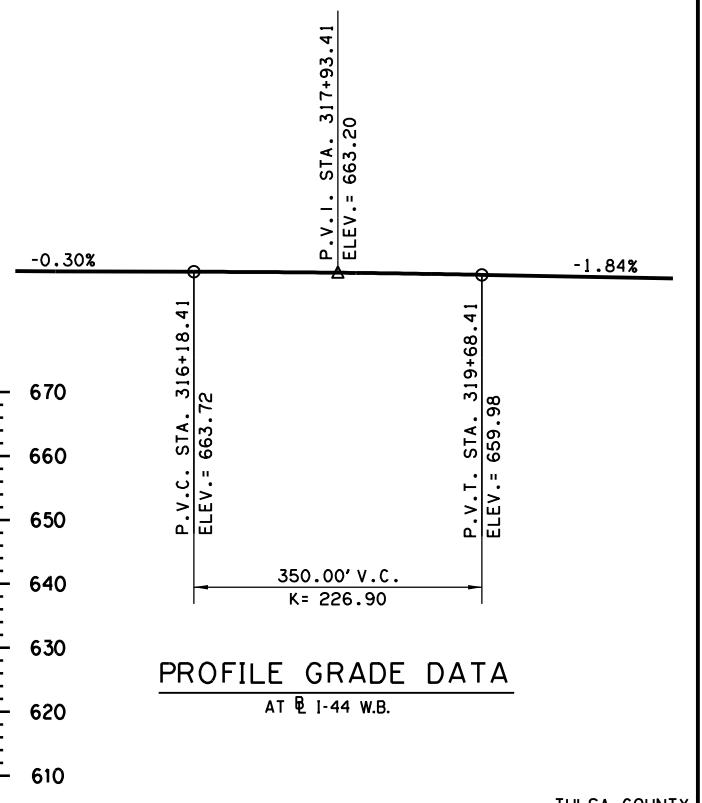
CURVE #1
PI STA = 313+44.95
X = 2560264.1908
Y = 402834.6142
Δ = 13°44'17.06" RT
D = 02°00'00.00"
T = 345.11'
L = 686.90'
R = 2864.79'
E = 20.71'
ed SUPER = 0.0601'
V = 25 mph



PLAN
SCALE: 1"=15'



ELEVATION
SCALE: 1"=15'



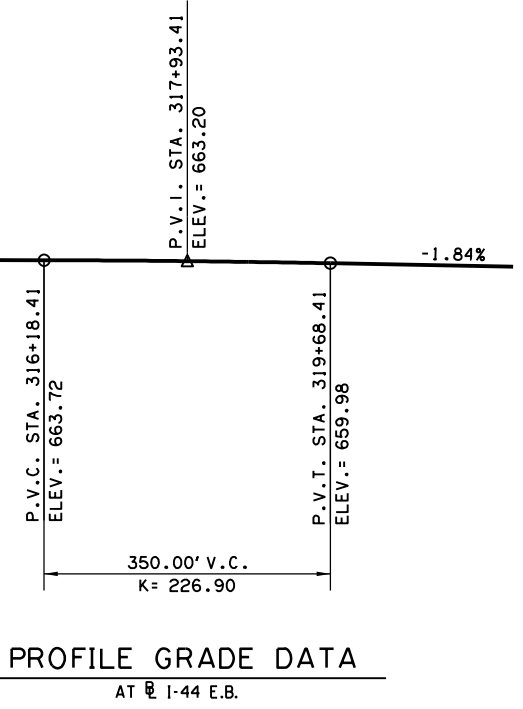
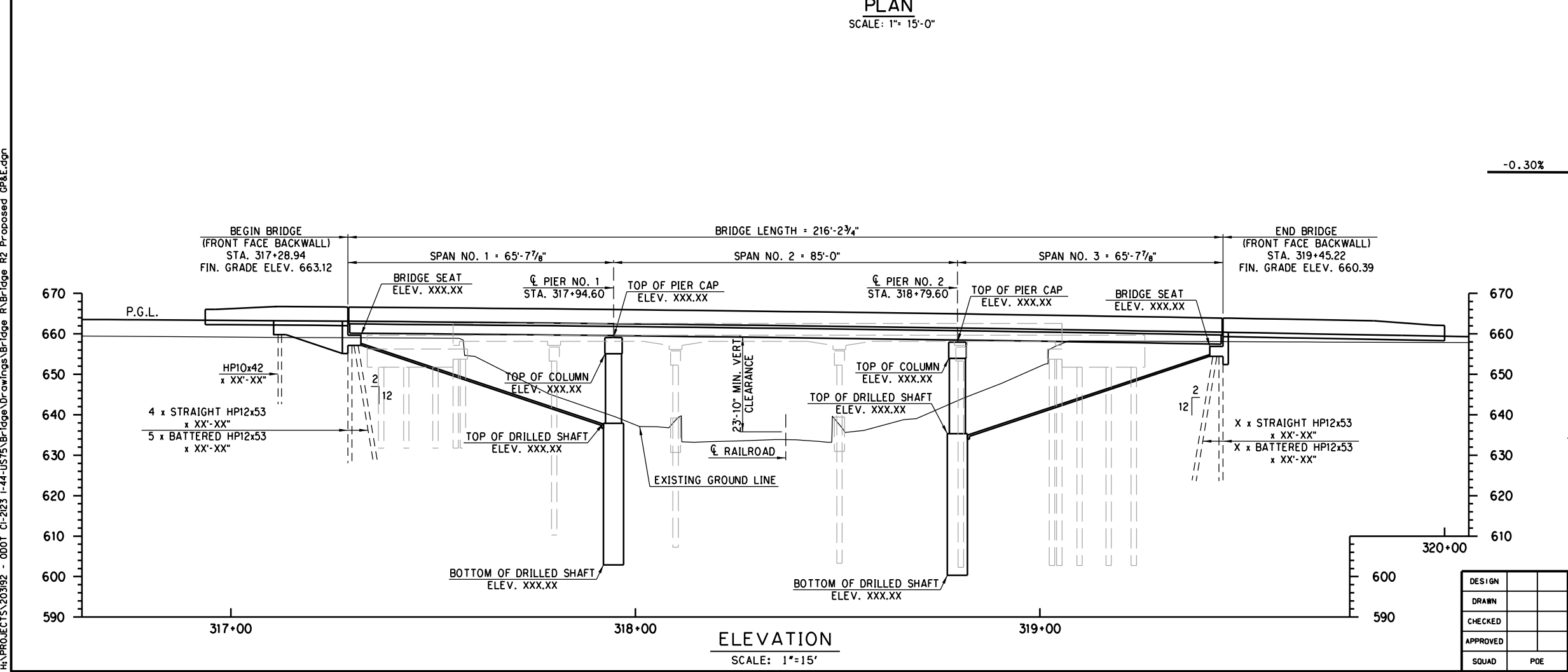
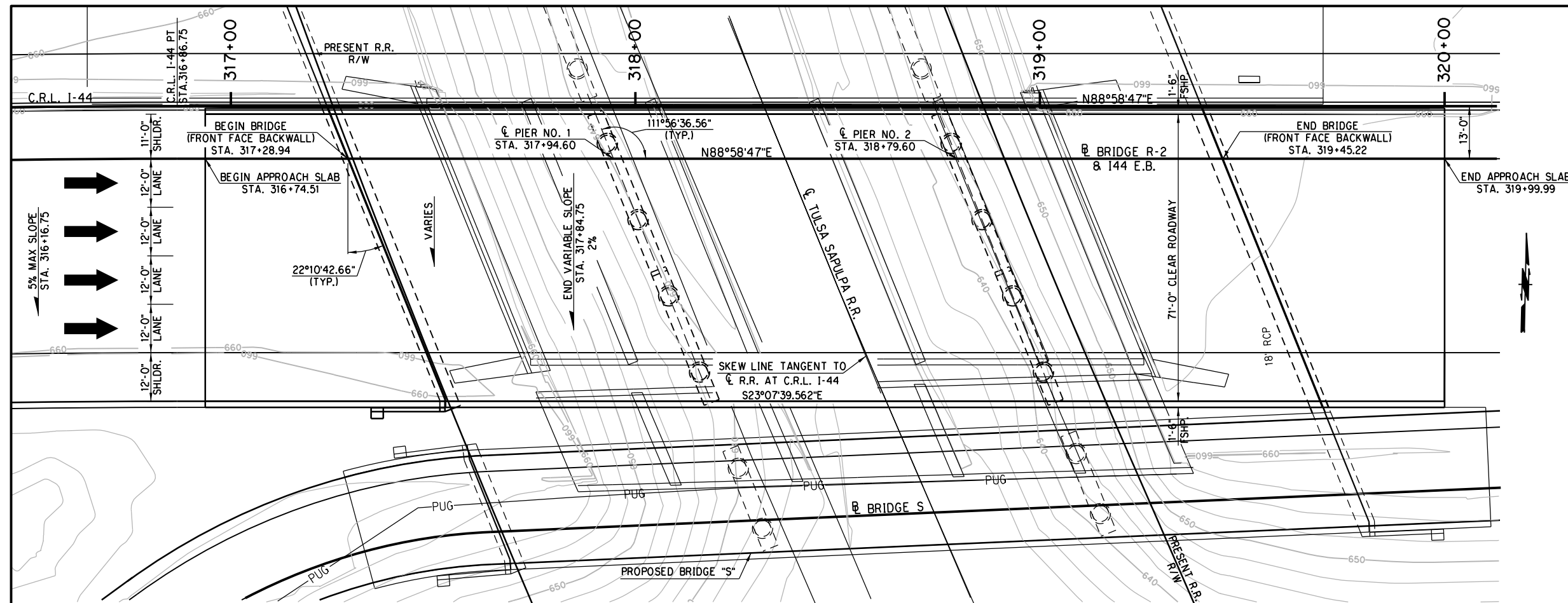
PROFILE GRADE DATA
AT I-44 W.B.

DESIGN		BRIDGE "R-1"	I-44 OVER TULSA-SAPULPA R.R.
DRAWN		GENERAL PLAN AND ELEVATION	
CHECKED		CONST. 65'-85'-65' CONTINUOUS R GIRDER	
APPROVED		SKEW 22.18° R.F. 71' CLR. RDWY. W/ 42" FSHP PARAPET	
SQUAD	POE	STATE JOB NO. JP33788(09)	SHEET NO. 006

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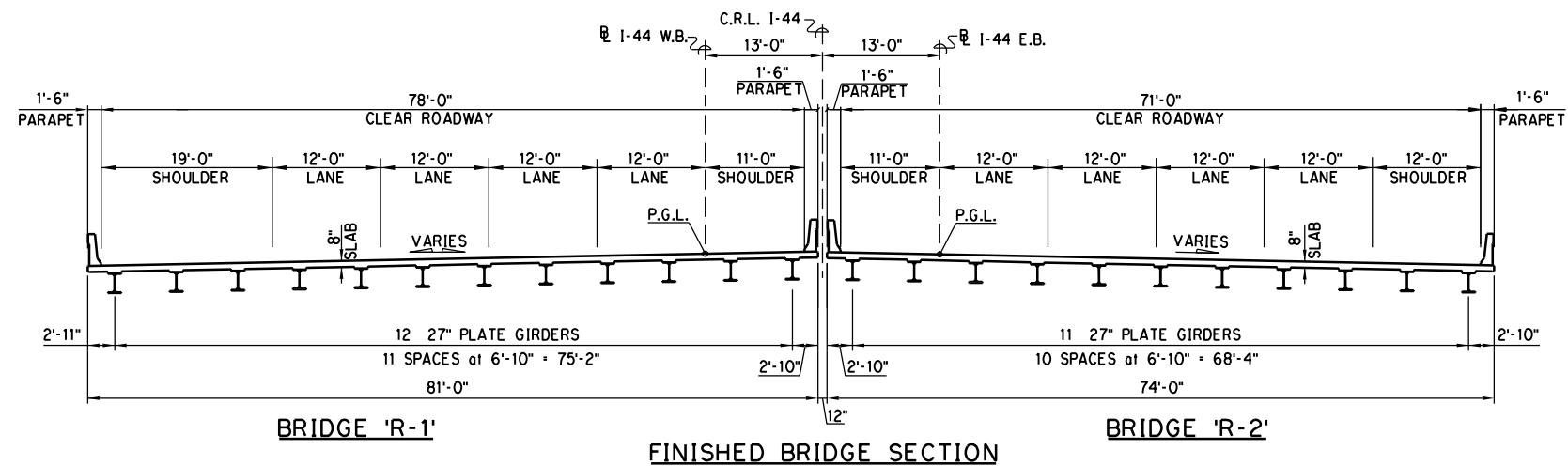
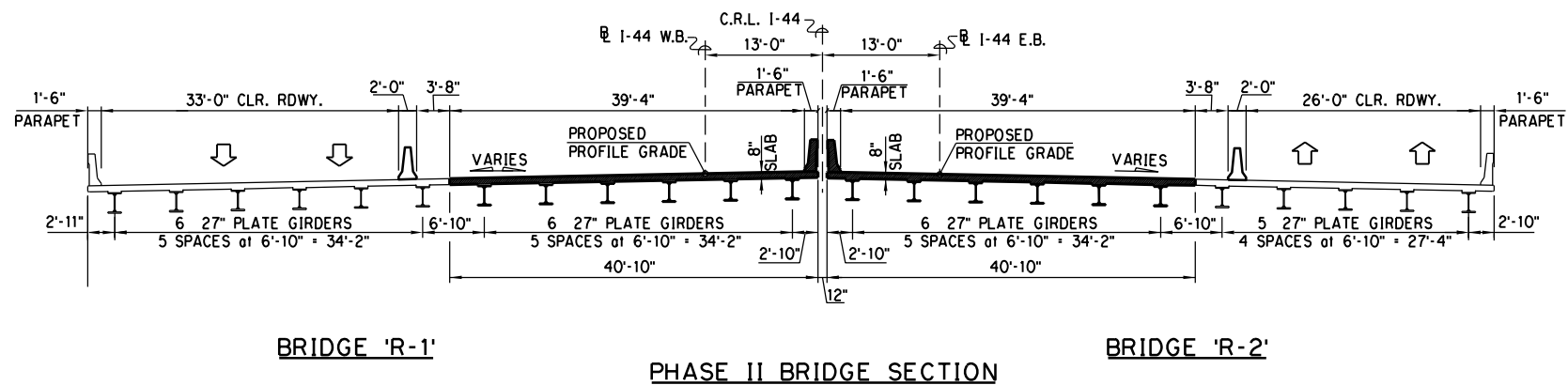
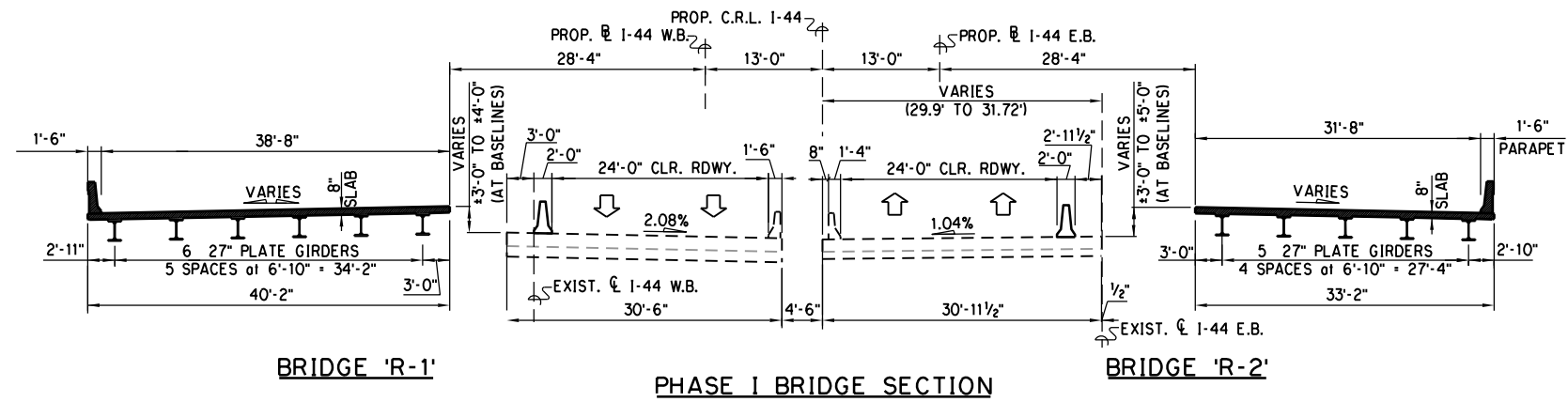
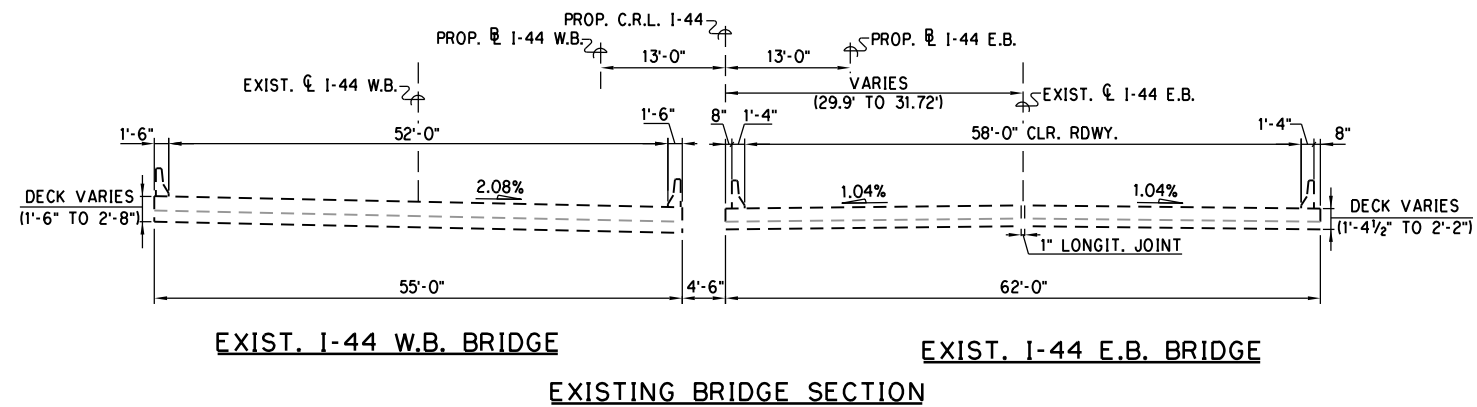
TULSA COUNTY

CURVE #1
 PI STA= 313+44.95
 X= 2560264.1908
 Y= 402834.6142
 Δ= 13°44'17.06" RT
 D= 02°00'00.00"
 T= 345.11'
 L= 686.90'
 R= 2864.79'
 E= 20.71'
 ed SUPER= 0.0600"
 V= 25 mph



DESIGN		BRIDGE "R-2"	I-44 OVER TULSA-SAPULPA R.R.
DRAWN		GENERAL PLAN AND ELEVATION	
CHECKED		I-44 E.B. CONST. 65'-85'-65' CONTINUOUS R GIRDER	
APPROVED		SKEW 22.18° R.F. 71' CLR. RDWY. W/ 42" FSHP PARAPET	
SQUAD	POE	STATE JOB NO. JP33788(09)	SHEET NO. 0007

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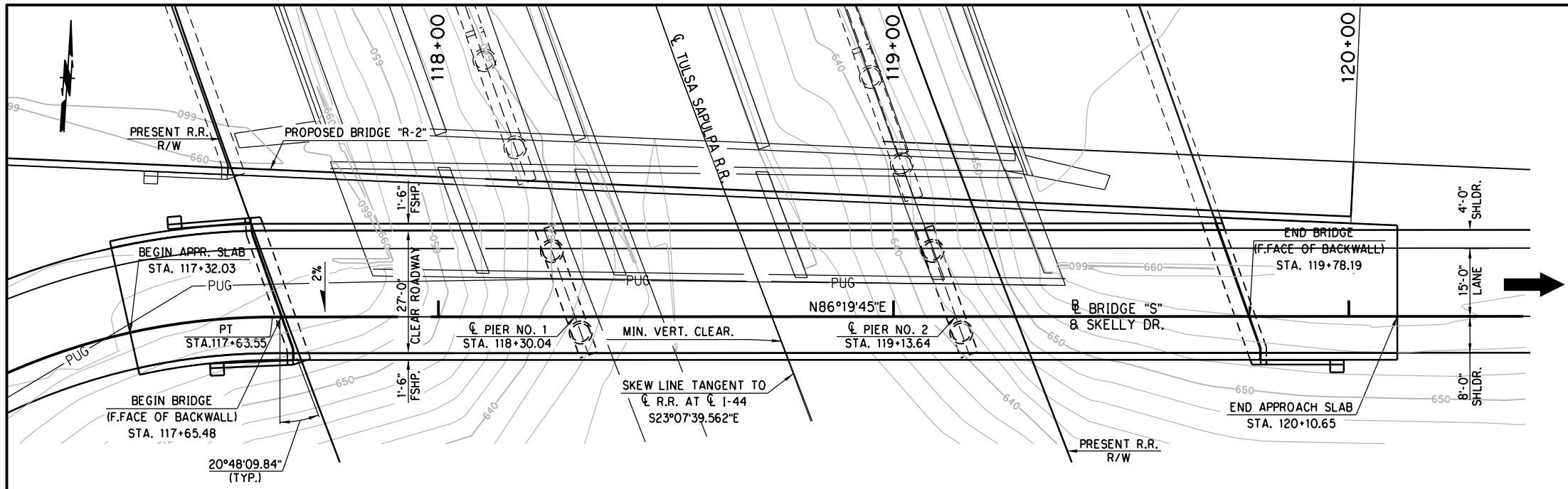


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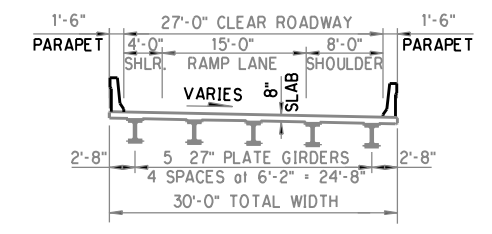
DESIGN		BRIDGE "R-1" & "R-2"	I-44 OVER TULSA SAPULPA R.R.
DRAWN		BRIDGE CONSTRUCTION SEQUENCE STATE JOB NO. <u>JP33788(09)</u> SHEET NO. <u>0008</u>	
CHECKED			
APPROVED			
SQUAD	POE		

TULSA COUNTY

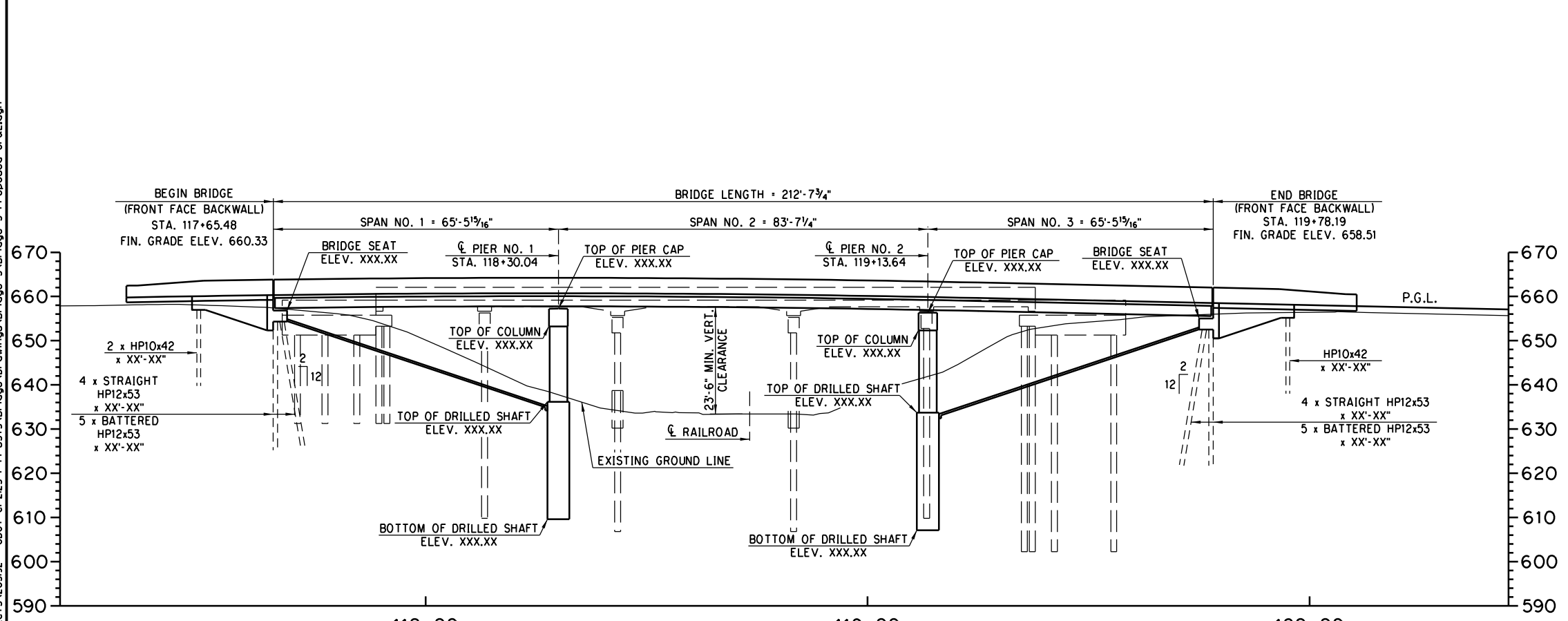
CURVE #1
 PI STA= 117+22.60
 X= 2560645.4769
 Y= 402735.2353
 Δ = 33°35'00.71" RT
 D= 39°47'19.45"
 T= 43.45'
 L= 84.40'
 R= 144.00'
 E= 6.41'
 ed SUPER= 0.0001'
 V= xx mph



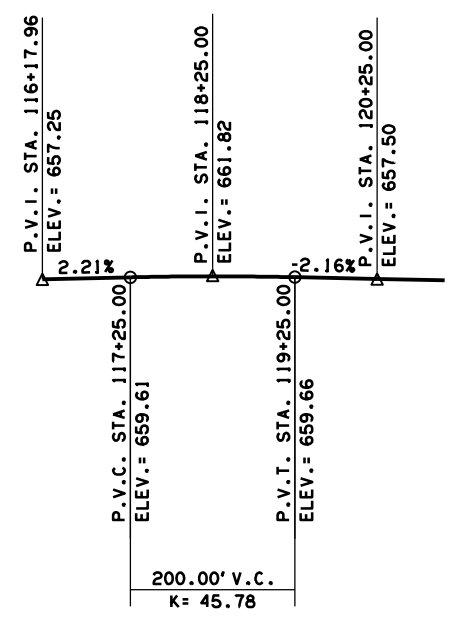
PLAN
SCALE: 1" = 15'-0"



TYPICAL SECTION



ELEVATION
SCALE: 1" = 15'



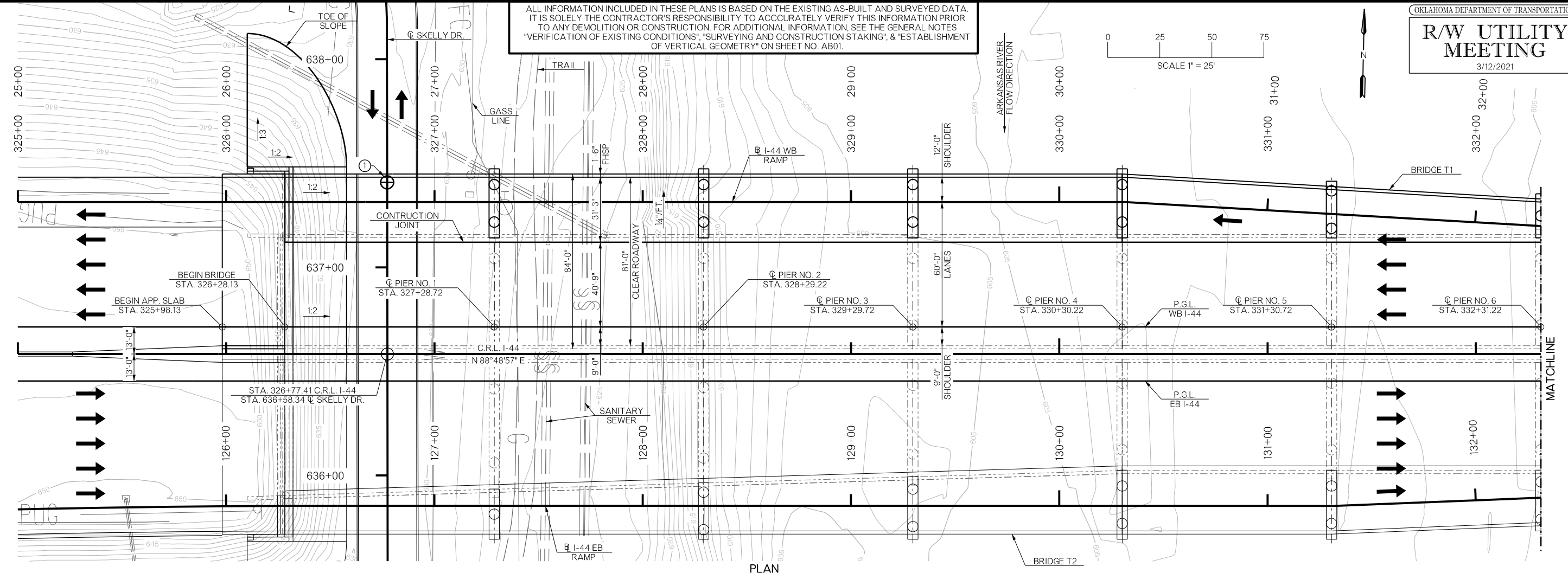
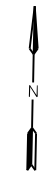
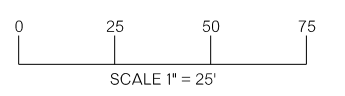
PROFILE GRADE DATA

DESIGN		BRIDGE "S" SKELLY DR. OVER TULSA-SAPULPA R.R.
DRAWN		GENERAL PLAN AND ELEVATION
CHECKED		CL BRIDGE STA. 118+71.87 @ SKELLY DR.
APPROVED		CONST. 65'-85'-65' CONTINUOUS PL GIRDERS
SQUAD	POE	SKEW 20.82° R.F. 27' CLR. RDWY. W/ 42" FSHP PARAPET
		STATE JOB NO. JP33788(09) SHEET NO. 009

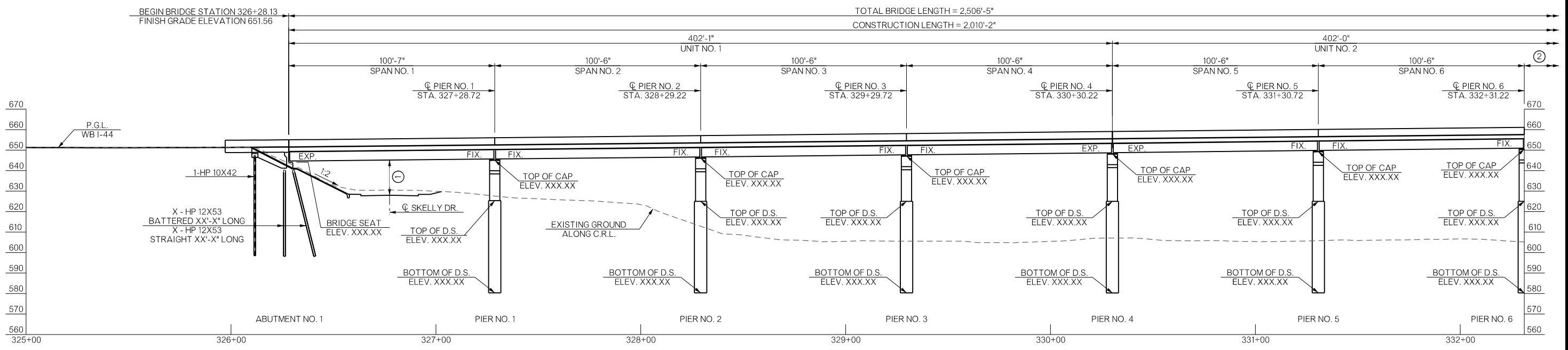
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TULSA COUNTY

ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.



PLAN



ELEVATION

- ① PROPOSED MIN. VERTICAL CLEARANCE 16'-11"
 C.R.L I-44 STA. 326+77.29
 OFFSET 69'-6" LT.
 LOW CHORD ELEV. 645.18
 NOTE: LOW CLEARANCE BASED OFF OF PROPOSED GEOMETRY.
- ② 100'-6" (SPAN NO. 7)

NOTE:
 FOR DESIGN DATA, VERTICAL PROFILE DATA, & FOUNDATION DATA SEE SHEET NO. B014.

BENCHMARK NO. 132 CUT X ON EAST END OF HEADWALL OFFSET 124.62' RT.	I-44 (A002) STA. 155+66.54 EL. 631.35
BENCHMARK NO. 133 CUT X ON NORTH EDGE OF LAMP POST OFFSET 171.91' RT.	I-44 (A002) STA. 163+22.25 EL. 628.16

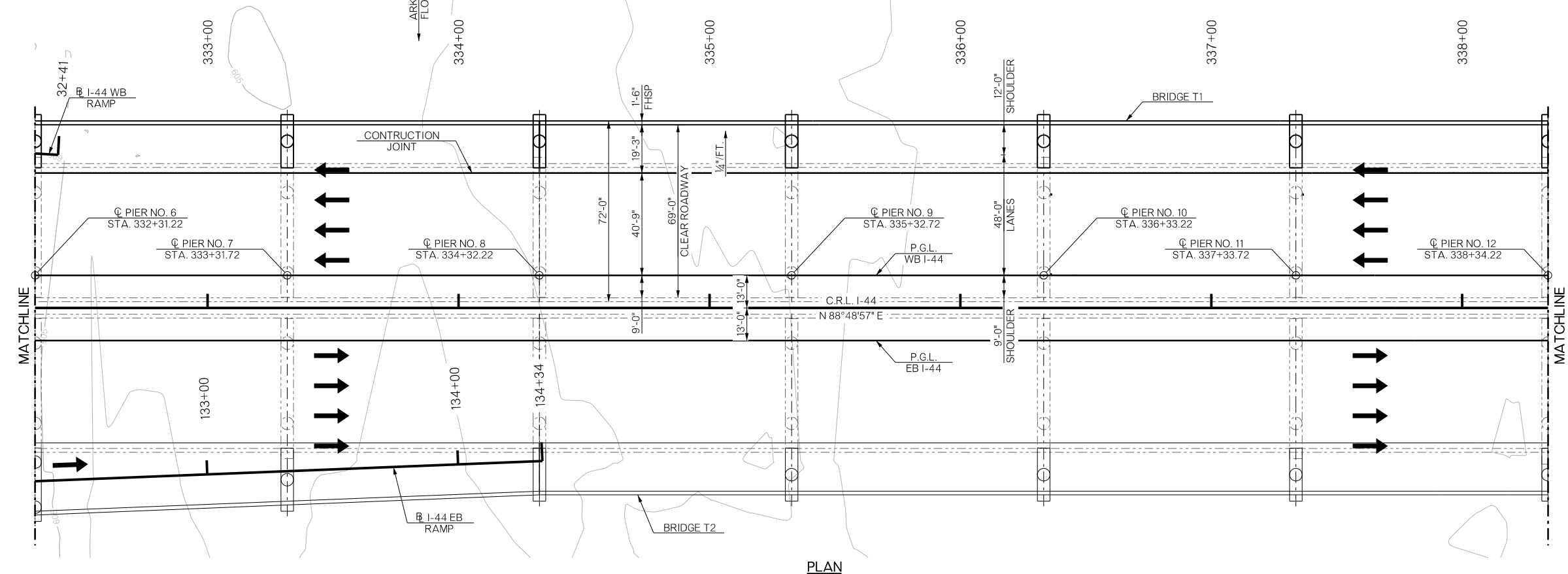
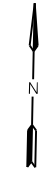
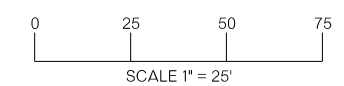
BRIDGE "T1" I-44 WB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
GENERAL PLAN AND ELEVATION			DETAIL	SJL	8/20
(SHEET 1 OF 5)			CHECK	DPE	8/20
WIDEN (20) 100'-6" TYPE J.P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+33.21			GARVER		
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	JOB PIECE NO. 33788(09) SHEET NO. B010			

3/12/2021 3:37:49 PM L:\2019\19103080 - ODOT Cl-2123 I-44 and US-75 Final WP_P0E Drawings\Bridges\I\3378809-T1-GENERAL PLAN AND ELEVATION I.dgn

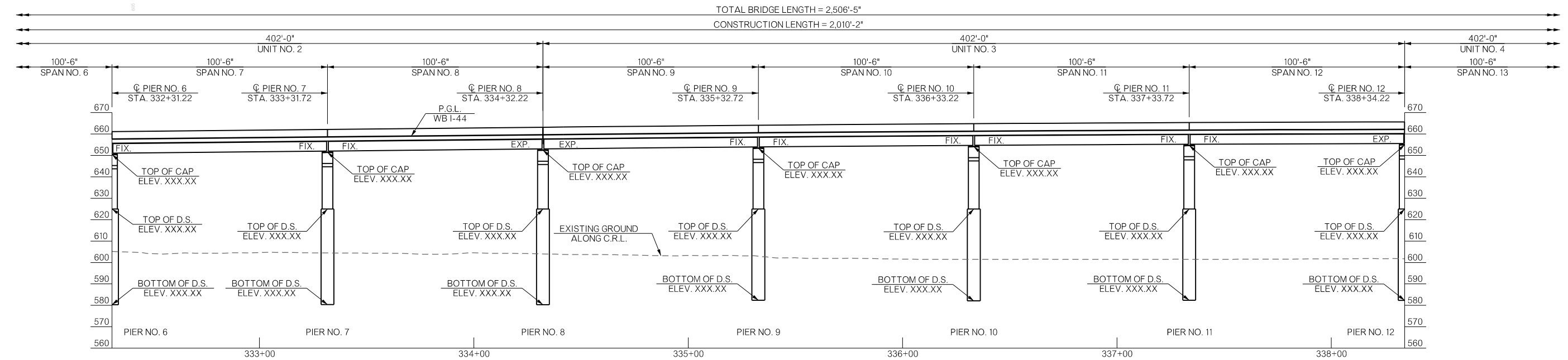
R/W UTILITY MEETING

3/12/2021

ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.



PLAN



ELEVATION

BENCHMARK NO. 132 I-44 (A002)
 CUT X ON EAST END OF HEADWALL
 OFFSET 124.62' RT. STA. 155+66.54 EL. 631.35

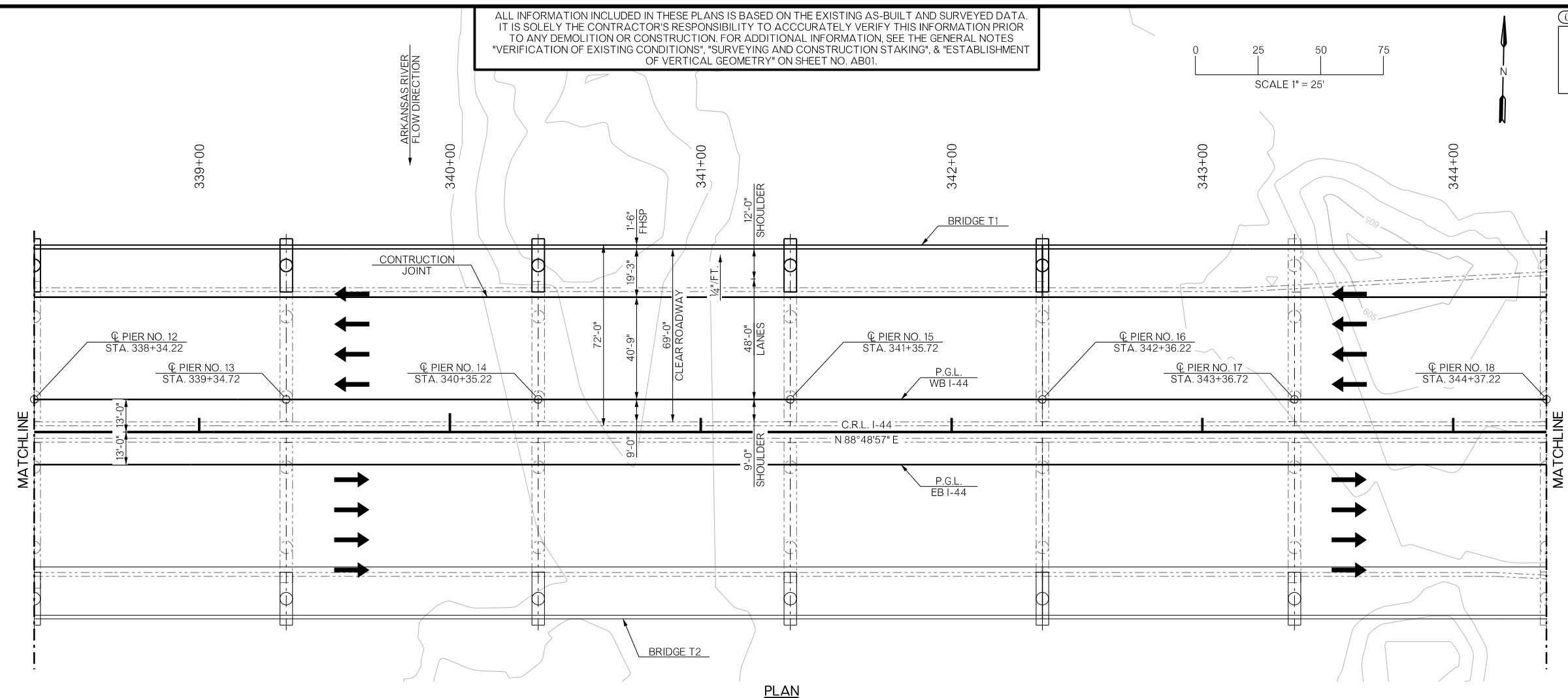
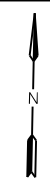
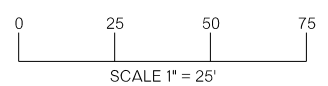
BENCHMARK NO. 133 I-44 (A002)
 CUT X ON NORTH EDGE OF LAMP POST
 OFFSET 171.91' RT. STA. 163+22.25 EL. 628.16

NOTE:
 FOR DESIGN DATA, VERTICAL PROFILE DATA, & FOUNDATION DATA SEE SHEET NO. B014.

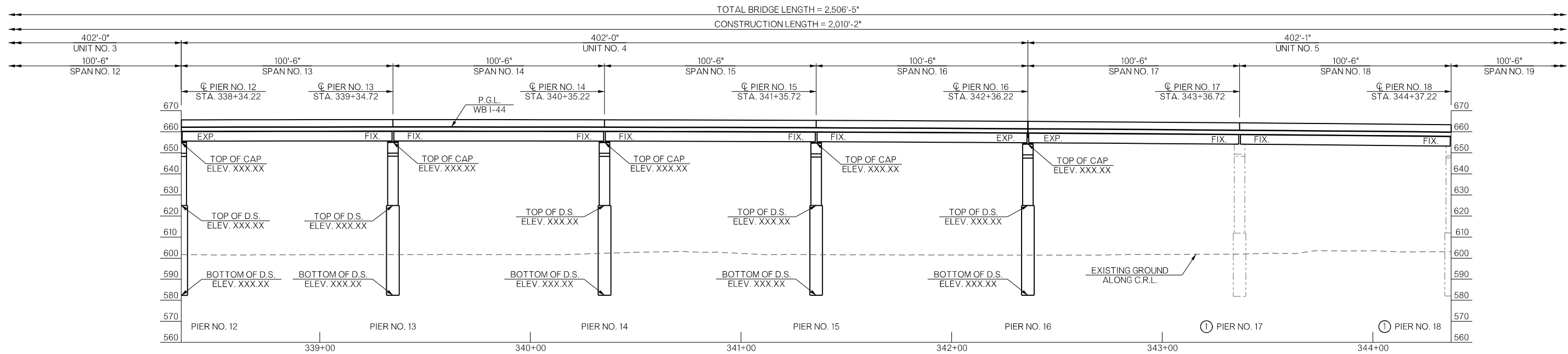
BRIDGE "T1" I-44 WB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
GENERAL PLAN AND ELEVATION			DETAIL	S.J.	8/20
(SHEET 2 OF 5)			CHECK	DPE	8/20
WIDEN (20) 100'-6" TYPE J.P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+33.21			GARVER		

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ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.



PLAN



ELEVATION

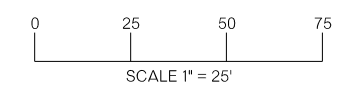
BENCHMARK NO. 132 CUT X ON EAST END OF HEADWALL OFFSET 124.62' RT. STA. 155+66.54	I-44 (A002) EL. 631.35
BENCHMARK NO. 133 CUT X ON NORTH EDGE OF LAMP POST OFFSET 171.91' RT. STA. 163+22.25	I-44 (A002) EL. 628.16

① EXISTING PIERS CONSTRUCTED AS PART OF CONTRACT JP06374(44).

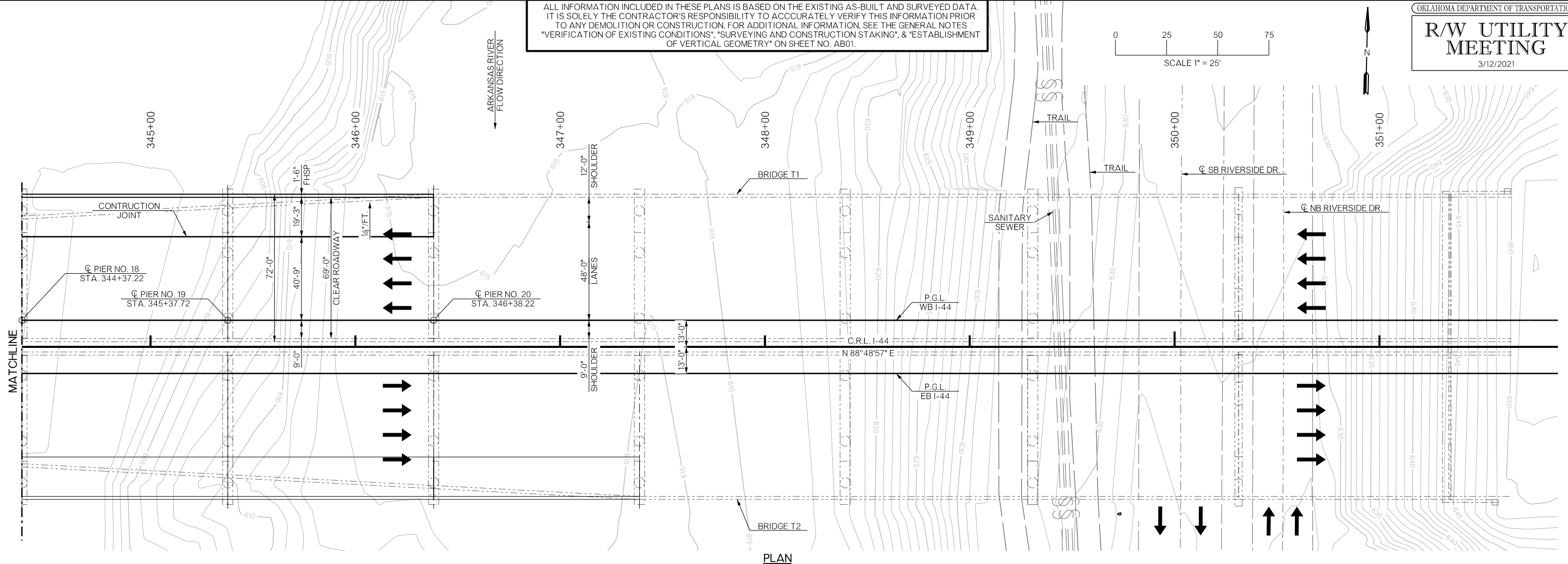
NOTE:
 FOR DESIGN DATA, VERTICAL PROFILE DATA, & FOUNDATION DATA SEE SHEET NO. B014.

BRIDGE "T1" I-44 WB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
GENERAL PLAN AND ELEVATION			DETAIL	S.J.	8/20
(SHEET 3 OF 5)			CHECK	DPE	8/20
WIDEN (20) 100'-6" TYPE J.P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+33.21		GARVER			
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	JOB PIECE NO. 33788(09)	SHEET NO. B012		

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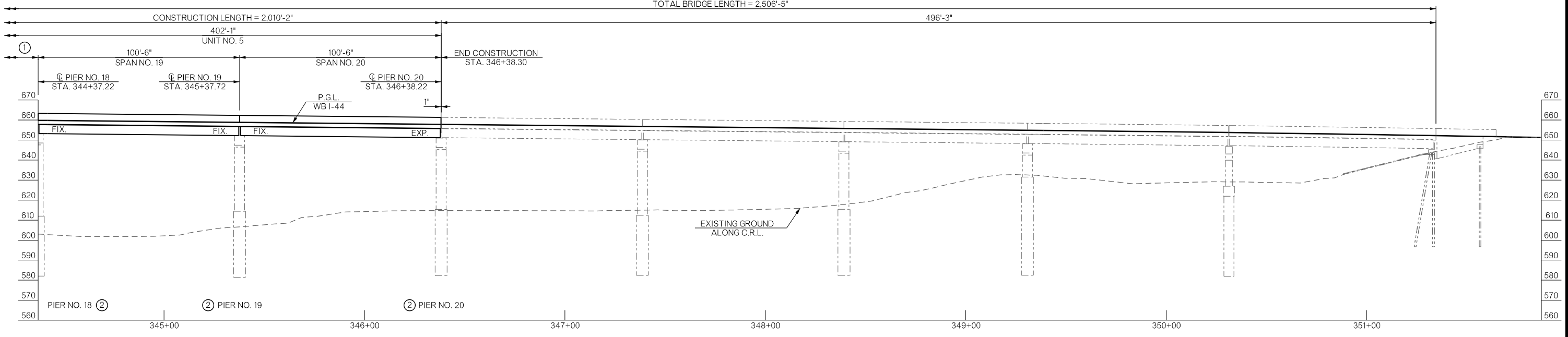


ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.



PLAN

TOTAL BRIDGE LENGTH = 2,506'-5"



ELEVATION

- ① 100'-6" (SPAN NO. 18)
- ② EXISTING PIERS CONSTRUCTED AS PART OF CONTRACT JP06374(44).

NOTE:
 FOR DESIGN DATA, VERTICAL PROFILE DATA, & FOUNDATION DATA SEE SHEET NO. B014.

BENCHMARK NO. 132 CUT X ON EAST END OF HEADWALL OFFSET 124.62' RT. STA. 155+66.54	I-44 (A002) EL. 631.35
BENCHMARK NO. 133 CUT X ON NORTH EDGE OF LAMP POST OFFSET 171.91' RT. STA. 163+22.25	I-44 (A002) EL. 628.16

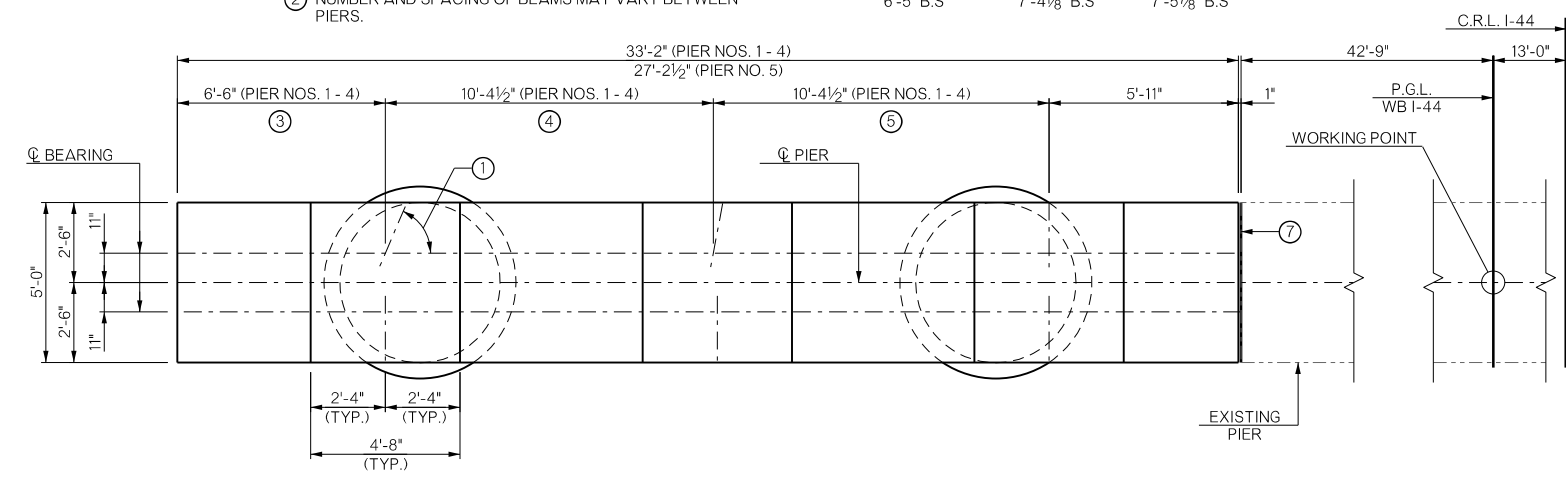
BRIDGE "T1" I-44 WB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
GENERAL PLAN AND ELEVATION			DETAIL	S.J.	8/20
(SHEET 4 OF 5)			CHECK	DPE	8/20
WIDEN (20) 100'-6" TYPE J.P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+33.21		GARVER			
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	JOB PIECE NO. 33788(09)	SHEET NO. B013		

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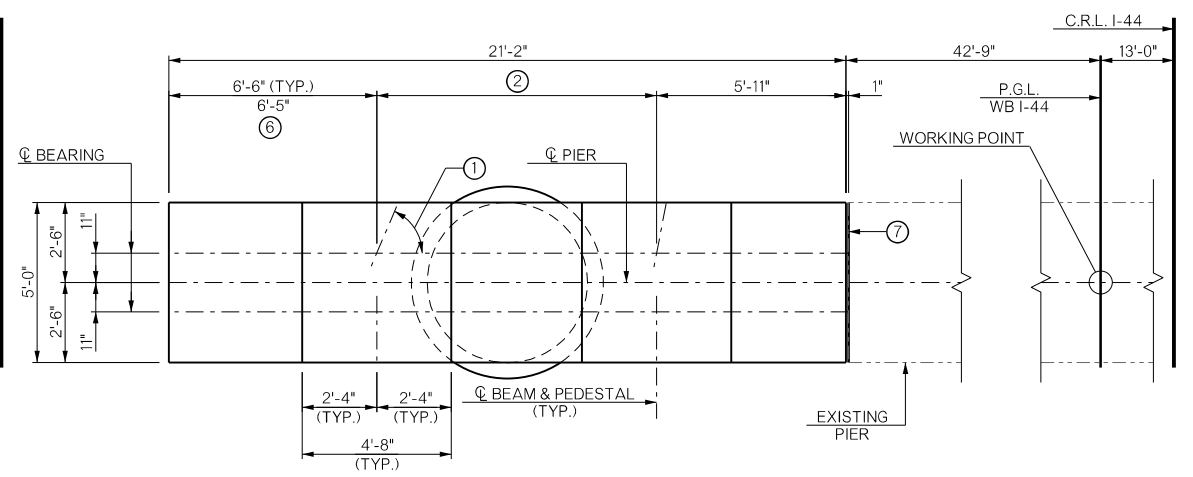
ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.

NOTE:
 ALL EDGES OF PIER CAP SHALL HAVE A 1/2" CHAMFER EXCEPT FOR PEDESTAL EDGES WHICH SHALL HAVE A 3/4" CHAMFER.

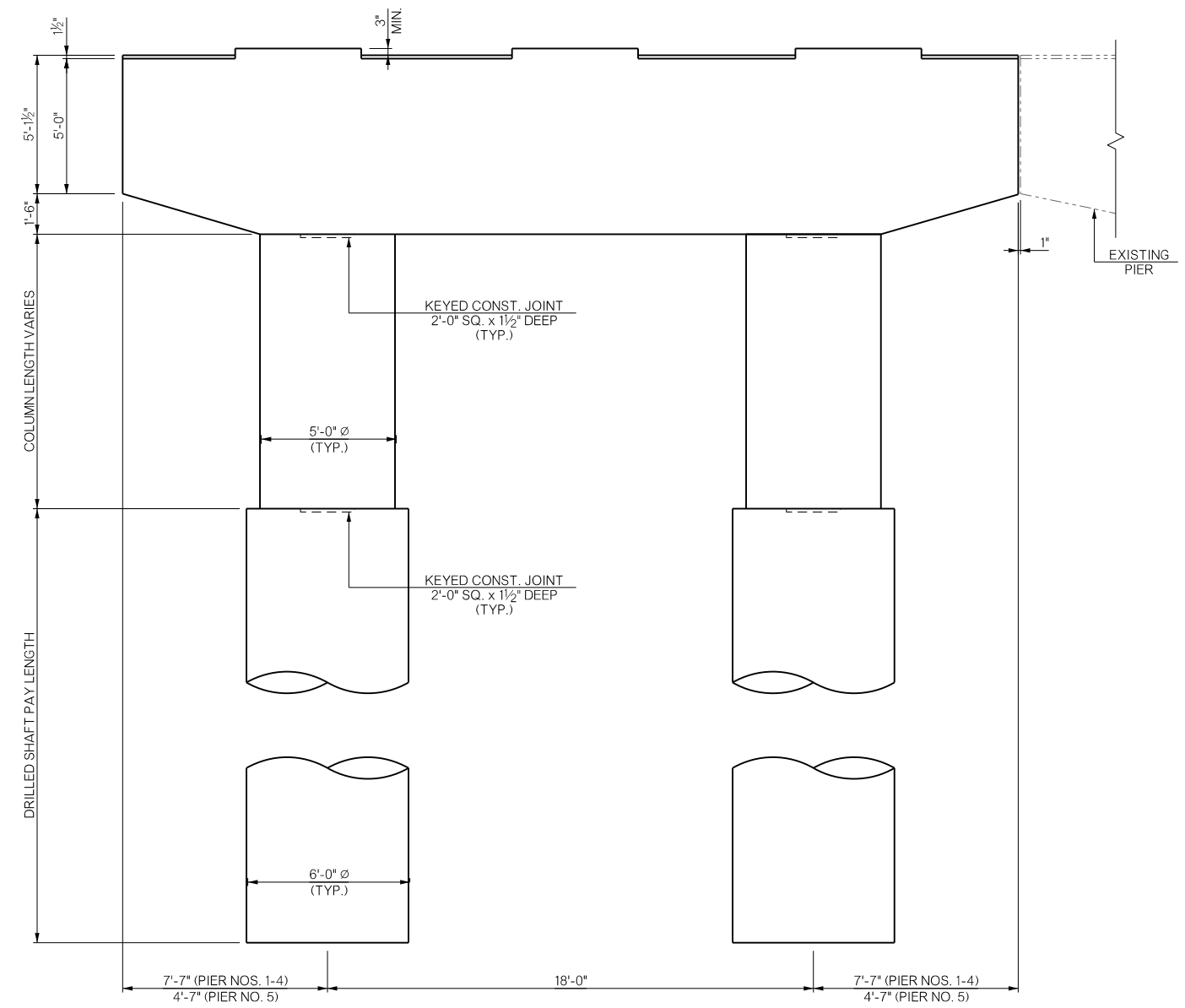
- ① ANGLE SHOWN EXAGGERATED FOR CLARITY, AND MAY VARY BETWEEN PIERS AND BEAMS.
- ② NUMBER AND SPACING OF BEAMS MAY VARY BETWEEN PIERS.
- ③ PIER NO.5: 6'-6 5/8" F.S. 6'-5" B.S.
- ④ PIER NO.5: 7'-3 3/8" F.S. 7'-4 1/8" B.S.
- ⑤ PIER NO.5: 7'-4 7/8" F.S. 7'-5 7/8" B.S.
- ⑥ AT PIER NO. 6 B.S. BEARING LINE.



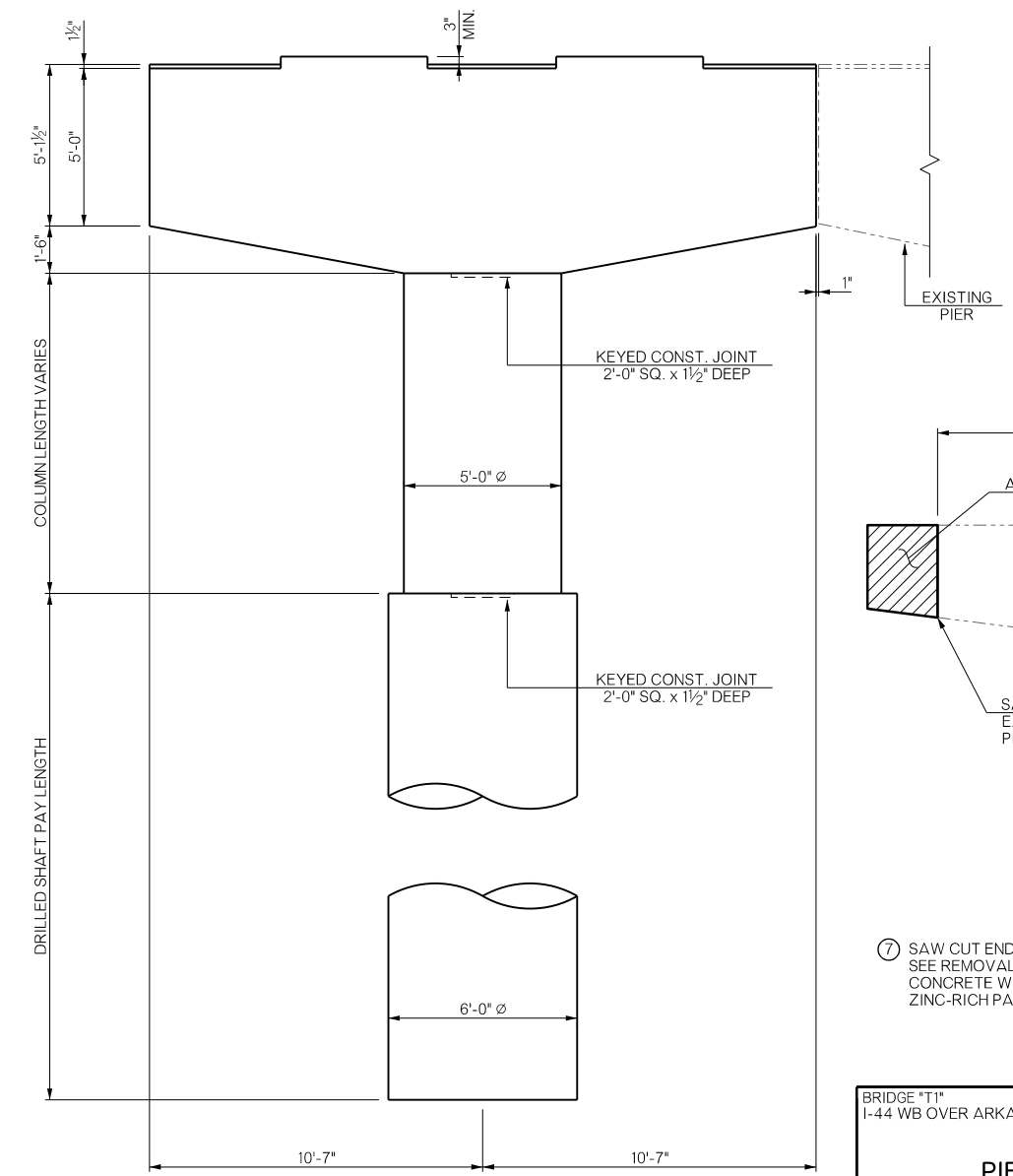
PLAN (PIER NOS. 1 - 5)



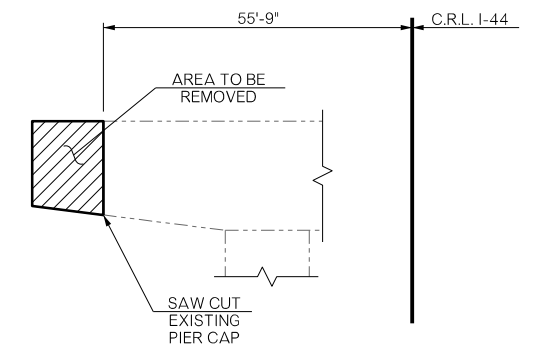
PLAN (PIER NOS. 6 - 16)



ELEVATION (PIER NOS. 1 - 5)
 (LOOKING FORWARD STATION)



ELEVATION (PIER NOS. 6 - 16)
 (LOOKING FORWARD STATION)



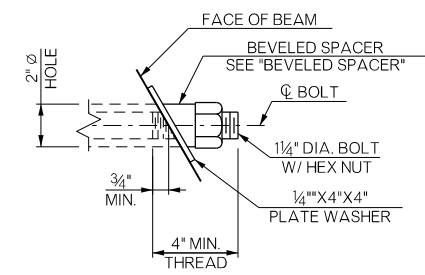
REMOVAL DETAIL

⑦ SAW CUT END OF CAP ON PIER NOS. 2, 4, 6, 8, 10, 12, 14, & 16. SEE REMOVAL DETAIL. PAINT CUT REBAR AND ADJOINING CONCRETE WITHIN 1" OF REBAR WITH TWO COATS OF ZINC-RICH PAINT (6 MIL. MINIMUM THICKNESS).

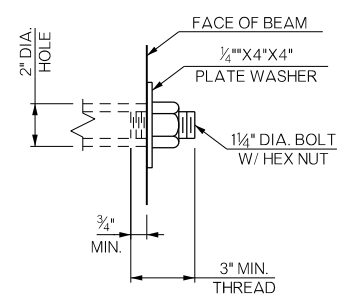
BRIDGE "T1" I-44 WB OVER ARKANSAS RIVER		TULSA COUNTY		DESIGN	DPE	8/20
PIER DETAILS		DETAIL	S.J.L.	8/20		
		CHECK	DPE	8/20		
		GARVER				
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION				
		JOB PIECE NO. 33788(09)		SHEET NO. _B015		

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ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.



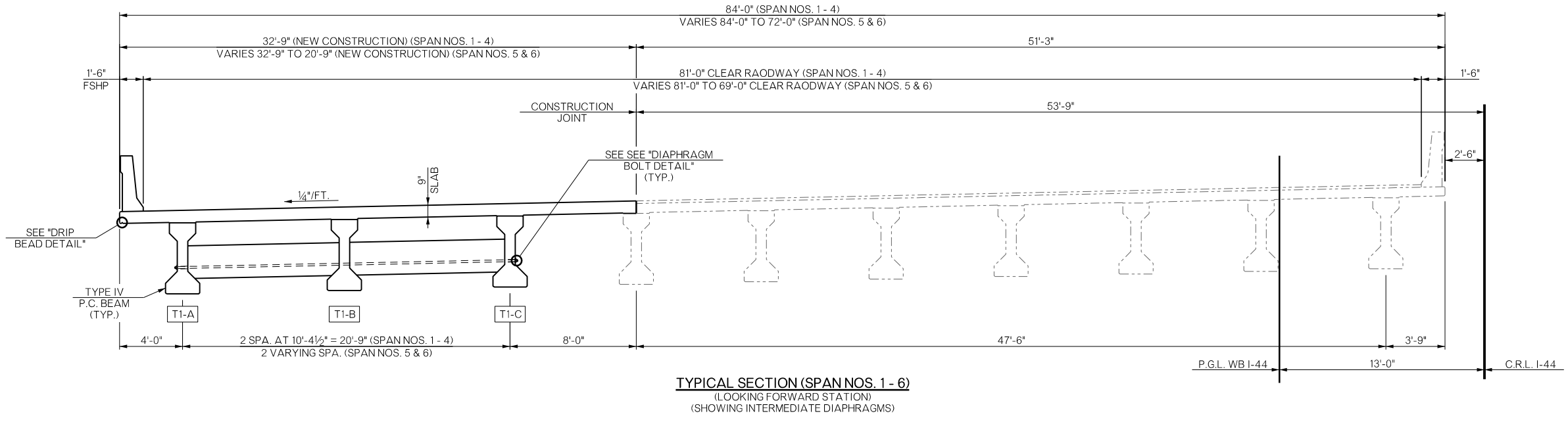
DIAPHRAGM BOLT ASSEMBLY DETAIL
 (SPAN NOS. 5 & 6 ONLY)



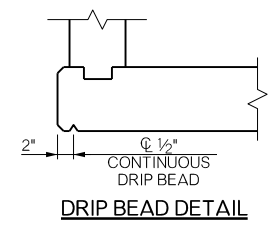
DIAPHRAGM BOLT ASSEMBLY DETAIL

DIAPHRAGM BOLT NOTES:
 PROVIDE STRUCTURAL STEEL FOR DIAPHRAGM BOLTS AND PLATE WASHERS IN ACCORDANCE WITH AASHTO M270 (ASTM A709), GRADE 50W (WEATHERING STEEL, CHARPY V-NOTCH TESTING NOT REQUIRED). THE CONTRACTOR MAY SUBSTITUTE A #10 REINFORCING BAR IN ACCORDANCE WITH AASHTO M31, GRADE 60, AND THREADED AT THE ENDS AS SHOWN FOR THE DIAPHRAGM BOLT AT NO ADDITIONAL COST TO THE DEPARTMENT. PROVIDE HEX NUTS IN ACCORDANCE WITH AASHTO M291 (ASTM A563).

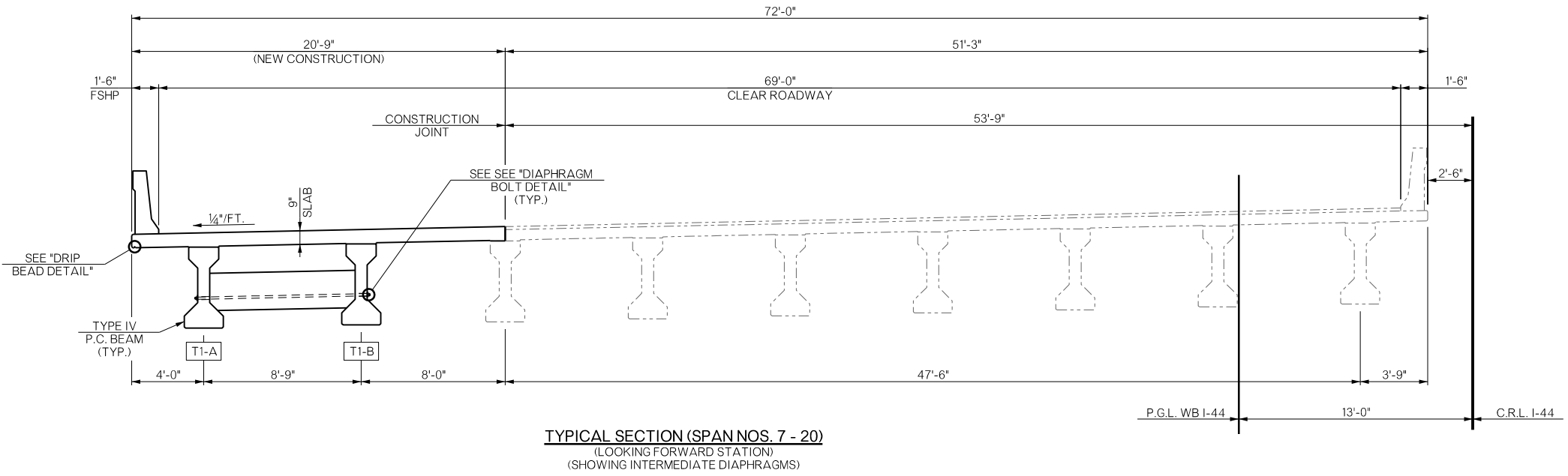
PAINT EXPOSED DIAPHRAGM BOLT, BEVELED SPACER, PLATE WASHER AND HEX NUT WITH TWO (2) COATS OF ZINC-RICH PAINT (6 MIL. MINIMUM THICKNESS) AFTER ASSEMBLY. INCLUDE ALL COST OF DIAPHRAGM BOLT, PLATE WASHER AND HEX NUT IN THE CONTRACT UNIT PRICE FOR "STRUCTURAL STEEL".



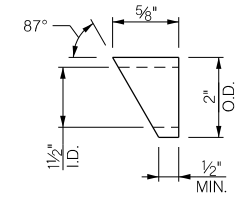
TYPICAL SECTION (SPAN NOS. 1 - 6)
 (LOOKING FORWARD STATION)
 (SHOWING INTERMEDIATE DIAPHRAGMS)



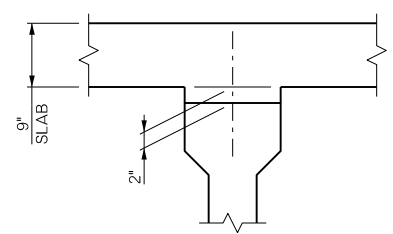
DRIP BEAD DETAIL



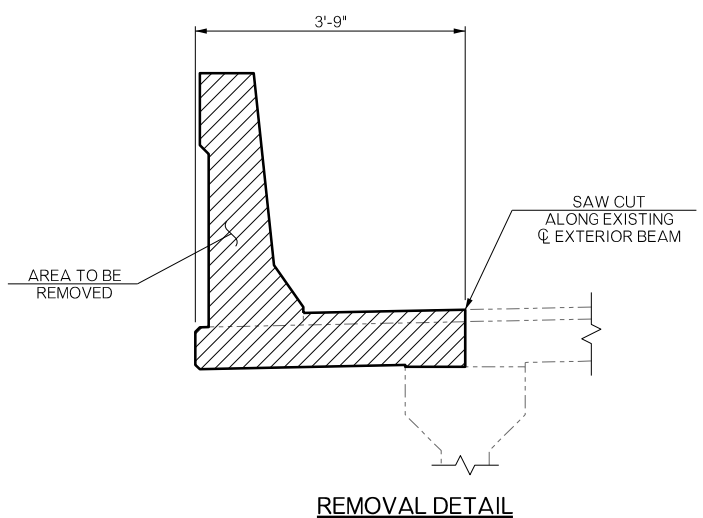
TYPICAL SECTION (SPAN NOS. 7 - 20)
 (LOOKING FORWARD STATION)
 (SHOWING INTERMEDIATE DIAPHRAGMS)



BEVELED SPACER
 (SPAN NOS. 5 & 6 ONLY)



BEAM HAUNCH DETAIL



REMOVAL DETAIL

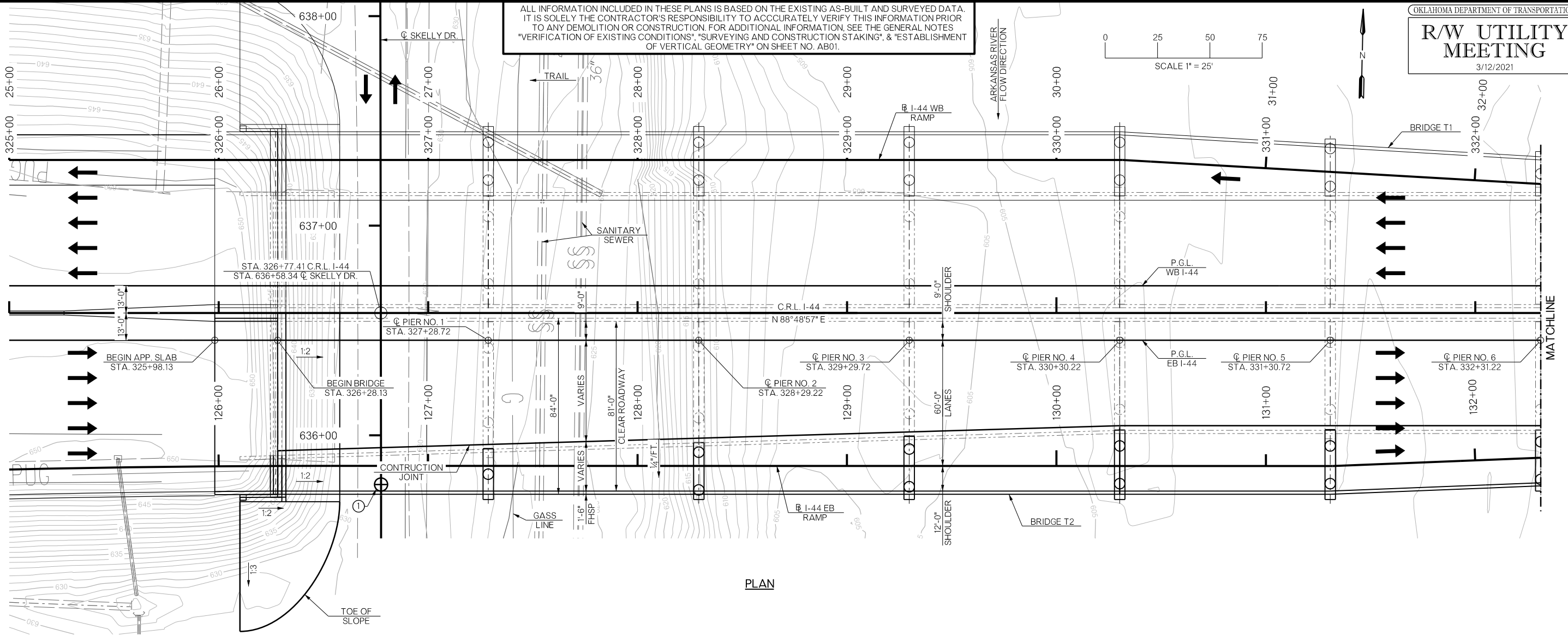
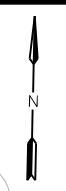
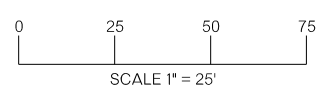
NOTE:
 EXTRA STRONG PIPE SLEEVE.

NOTE:
 PLAN QUANTITIES FOR CLASS AA CONCRETE INCLUDE BEAM HAUNCHES. THE HAUNCH HEIGHT SHOWN IS THE THEORETICAL HAUNCH HEIGHT AT THE CENTERLINE BEARING ONLY, MEASURED FROM BOTTOM OF DECK SLAB TO TOP OF BEAM, AND VARIES ACROSS THE SPAN. DETERMINE THE ACTUAL HAUNCH HEIGHT (ACCOUNTING FOR BEAM CAMBER, DEAD LOAD DEFLECTION AND ROADWAY GRADE) AFTER ERECTION OF THE BEAMS AND SUBMIT TO THE ENGINEER FOR APPROVAL. THE ENGINEER WILL NOT MEASURE DIFFERENCES BETWEEN THE THEORETICAL AND THE ACTUAL HAUNCH HEIGHTS FOR PAYMENT.

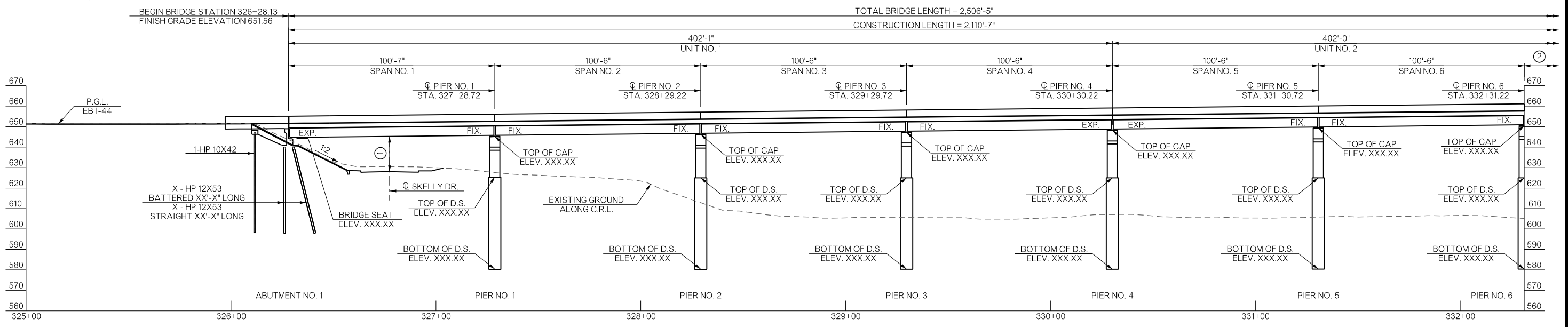
BRIDGE "T1" I-44 WB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
			DETAIL	SJL	8/20
			CHECK	DPE	8/20
TYPICAL SECTION					
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION			
		JOB PIECE NO. 33788(09)		SHEET NO. _B016	

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ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.



PLAN



ELEVATION

BENCHMARK NO. 132 CUT X ON EAST END OF HEADWALL OFFSET 124.62' RT. STA. 155+66.54	I-44 (A002) EL. 631.35
BENCHMARK NO. 133 CUT X ON NORTH EDGE OF LAMP POST OFFSET 171.91' RT. STA. 163+22.25	I-44 (A002) EL. 628.16

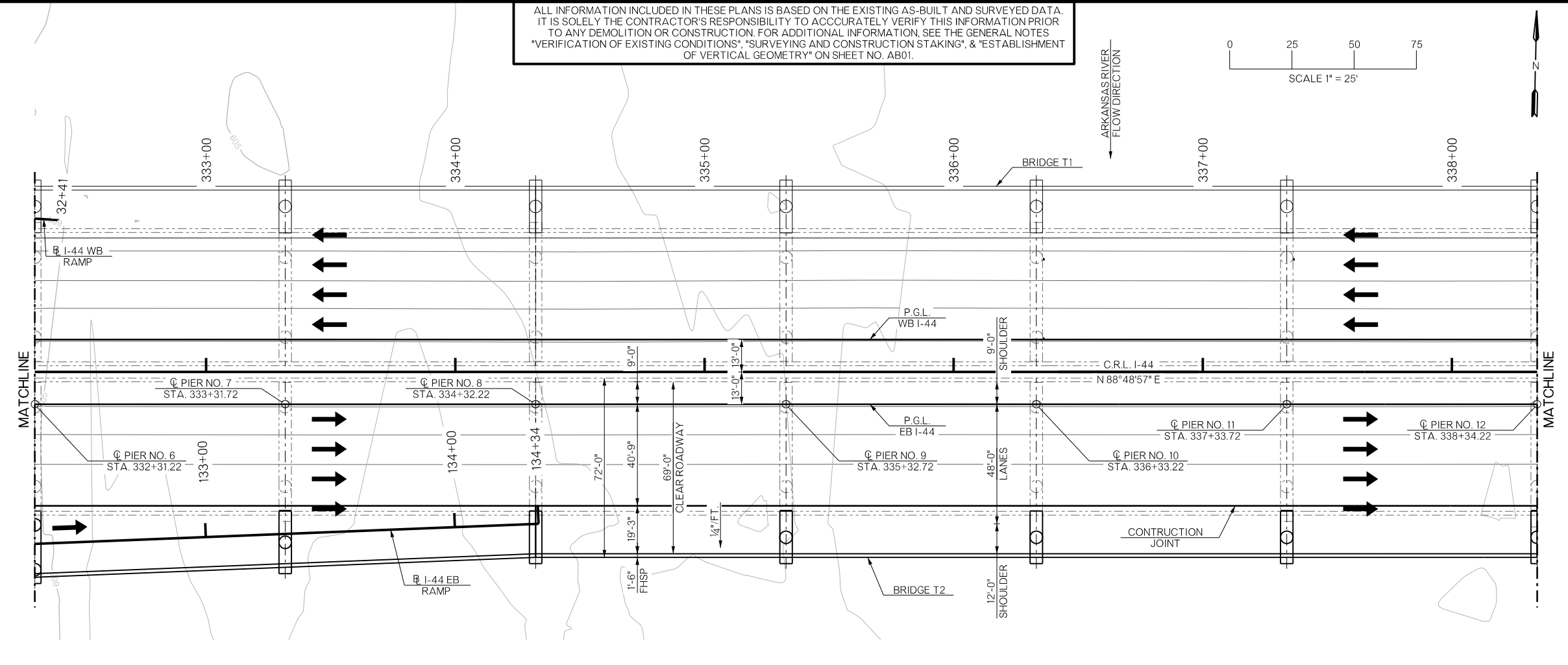
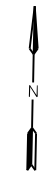
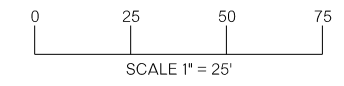
- ① PROPOSED MIN. VERTICAL CLEARANCE 17'-5"
C.R.L I-44 STA. 326+77.52
OFFSET 69'-6" RT.
LOW CHORD ELEV. 645.22
NOTE: LOW CLEARANCE BASED OFF OF PROPOSED GEOMETRY.
- ② 100'-6" (SPAN NO. 7)

NOTE:
FOR DESIGN DATA, VERTICAL PROFILE DATA, & FOUNDATION
DATA SEE SHEET NO. B021.

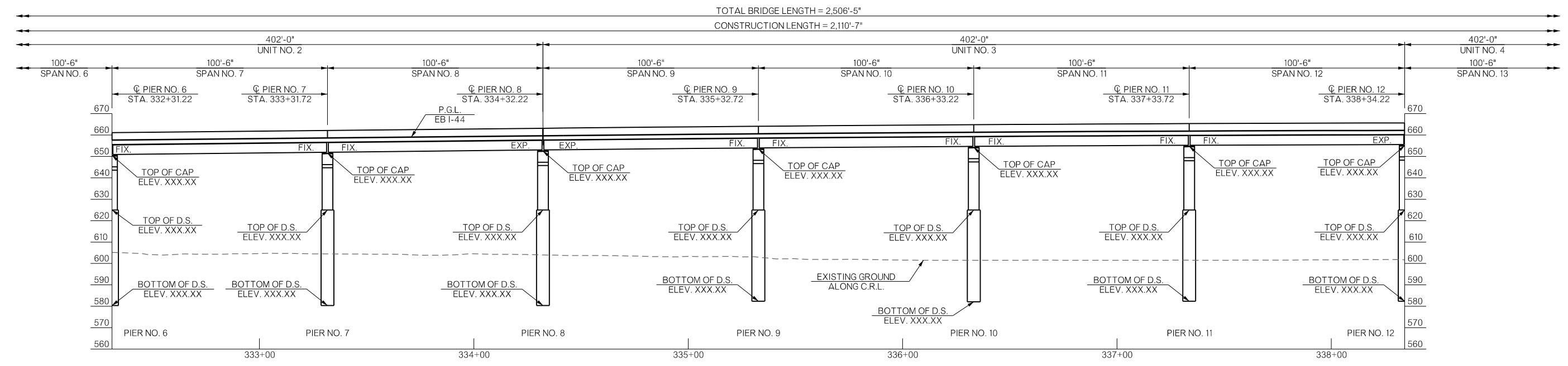
BRIDGE "T2" I-44 EB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
GENERAL PLAN AND ELEVATION			DETAIL	SJL	8/20
(SHEET 1 OF 5)			CHECK	DPE	8/20
WIDEN (21) 100'-6" TYPE J.P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+83.43			GARVER		
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	JOB PIECE NO. 33788(09)	SHEET NO. B017		

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ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.



PLAN



ELEVATION

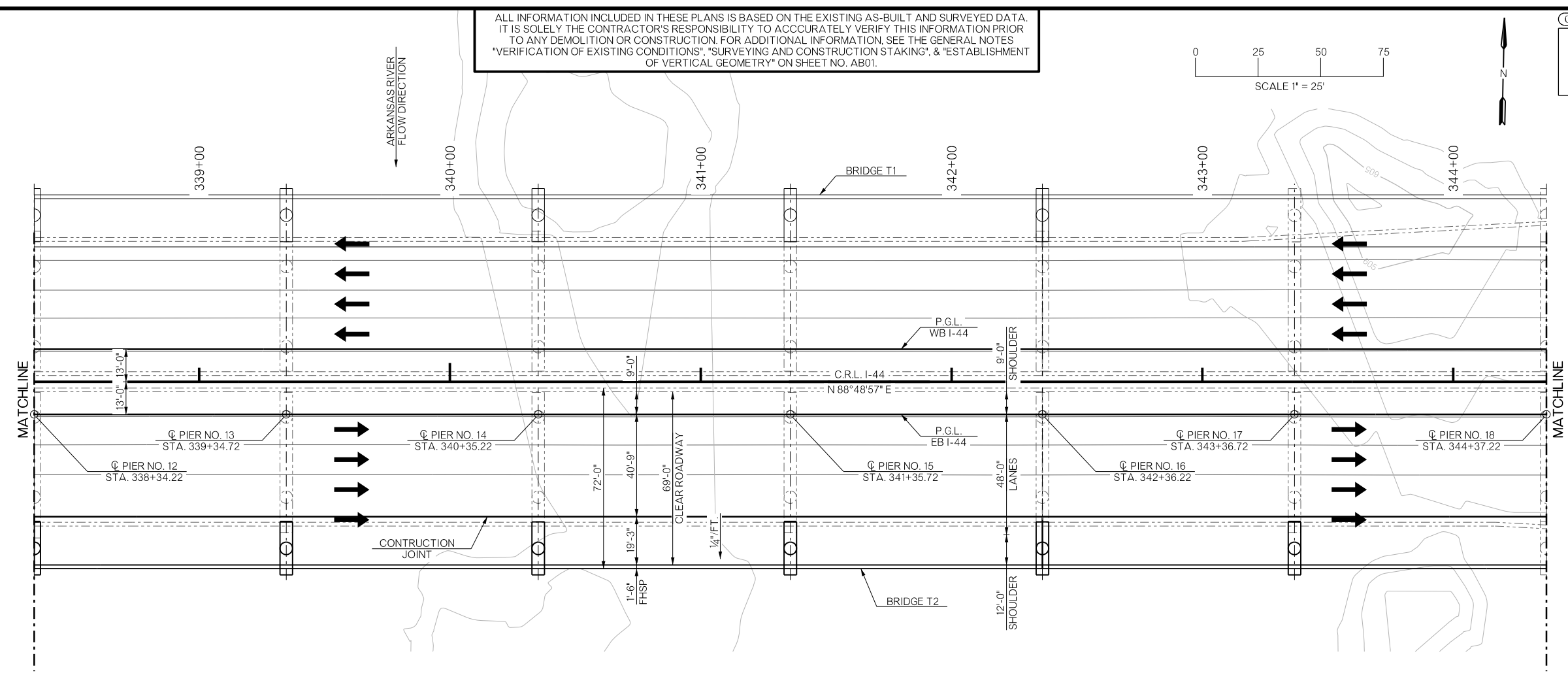
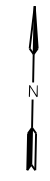
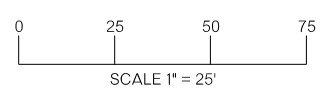
BENCHMARK NO. 132 CUT X ON EAST END OF HEADWALL OFFSET 124.62' RT. STA. 155+66.54	I-44 (A002) EL. 631.35
BENCHMARK NO. 133 CUT X ON NORTH EDGE OF LAMP POST OFFSET 171.91' RT. STA. 163+22.25	I-44 (A002) EL. 628.16

NOTE:
 FOR DESIGN DATA, VERTICAL PROFILE DATA, & FOUNDATION DATA SEE SHEET NO. B021.

BRIDGE "T2" I-44 EB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
GENERAL PLAN AND ELEVATION			DETAIL	SJL	8/20
(SHEET 2 OF 5)			CHECK	DPE	8/20
WIDEN (2) 100'-6" TYPE J.P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+83.43		GARVER			
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	JOB PIECE NO. 33788(09)	SHEET NO. _B018		

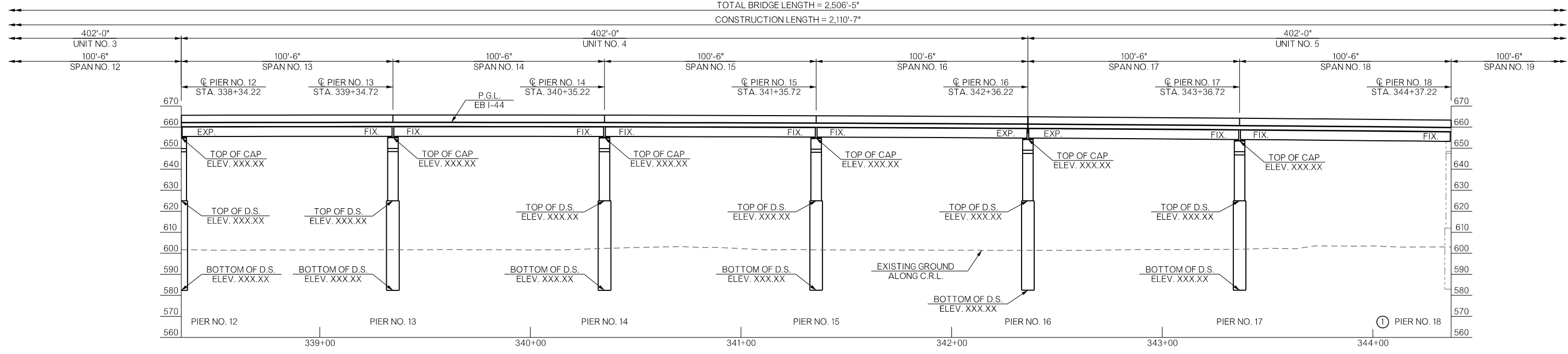
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PLAN

TOTAL BRIDGE LENGTH = 2,506'-5"
CONSTRUCTION LENGTH = 2,110'-7"



ELEVATION

BENCHMARK NO. 132 CUT X ON EAST END OF HEADWALL OFFSET 124.62' RT. STA. 155+66.54	I-44 (A002) EL. 631.35
BENCHMARK NO. 133 CUT X ON NORTH EDGE OF LAMP POST OFFSET 171.91' RT. STA. 163+22.25	I-44 (A002) EL. 628.16

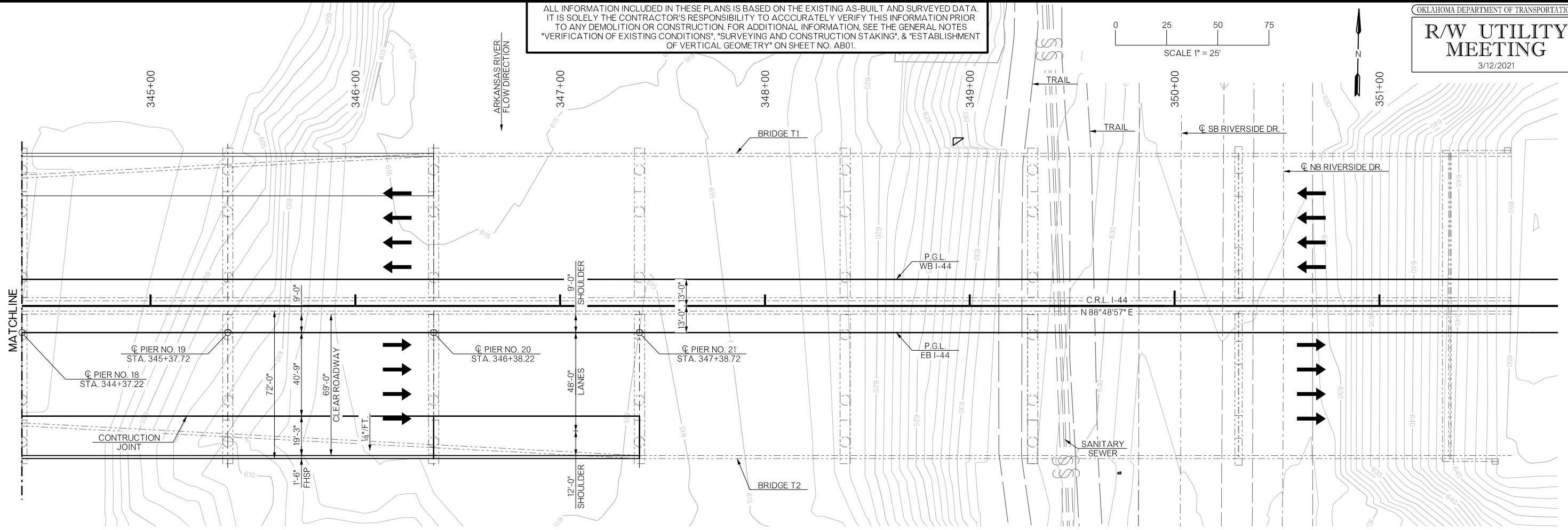
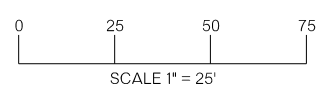
Ⓢ EXISTING PIERS CONSTRUCTED AS PART OF CONTRACT JP06374(44).

NOTE:
FOR DESIGN DATA, VERTICAL PROFILE DATA, & FOUNDATION DATA SEE SHEET NO. B021.

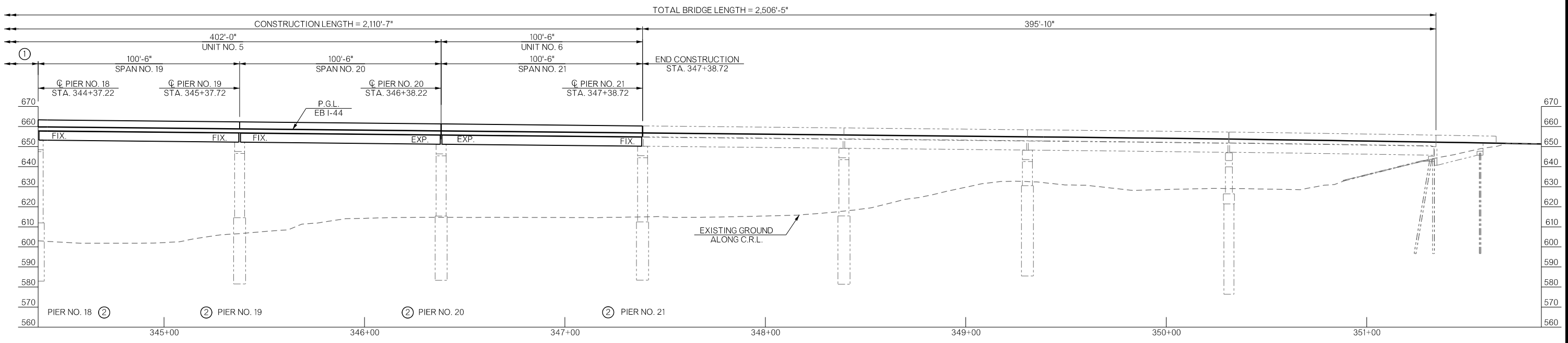
BRIDGE "T2" I-44 EB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
GENERAL PLAN AND ELEVATION			DETAIL	SJL	8/20
(SHEET 3 OF 5)			CHECK	DPE	8/20
WIDEN (21) 100'-6" TYPE J P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+83.43		GARVER			
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	JOB PIECE NO. 33788(09)	SHEET NO. B019		

3/12/2021 3:37:58 PM L:\2019\9103080 - 000T Cl-2123 I-44 and US-75 Final\WP_P0E\Drawings\Bridges T2\3378809-T2-GENERAL PLAN AND ELEVATION 3.dgn

ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.



PLAN



ELEVATION

- ① 100'-6" (SPAN NO. 18)
- ② EXISTING PIERS CONSTRUCTED AS PART OF CONTRACT JP06374(44).

NOTE:
 FOR DESIGN DATA, VERTICAL PROFILE DATA, & FOUNDATION DATA SEE SHEET NO. B021.

BENCHMARK NO. 132 CUT X ON EAST END OF HEADWALL OFFSET 124.62' RT. STA. 155+66.54	I-44 (A002) EL. 631.35
BENCHMARK NO. 133 CUT X ON NORTH EDGE OF LAMP POST OFFSET 171.91' RT. STA. 163+22.25	I-44 (A002) EL. 628.16

BRIDGE "T2" I-44 EB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
GENERAL PLAN AND ELEVATION			DETAIL	SJL	8/20
(SHEET 4 OF 5)			CHECK	DPE	8/20
WIDEN (21) 100'-6" TYPE J P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+83.43		GARVER			
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION	JOB PIECE NO. 33788(09)	SHEET NO. B020		

3/12/2021 3:37:58 PM L:\2019\9103080 - 000T Cl-2123 I-44 and US-75 Final\WP_P0E Drawings\Bridges T2\3378809-T2-GENERAL PLAN AND ELEVATION 4.dgn

DESIGN DATA (LOAD AND RESISTANCE FACTOR DESIGN)

CLASS AA CONCRETE F'C = 4,000 P.S.I.
CLASS A CONCRETE F'C = 3,000 P.S.I.
REINFORCING STEEL (GRADE 60) FY = 60,000 P.S.I.
STRUCTURAL STEEL (M270, GR. 50W) FY = 50,000 P.S.I.
STRUCTURAL STEEL (PILING) (M270, GR. 50) FY = 50,000 P.S.I.
STAINLESS STEEL A240 (TYPE 316) FY = 30,000 P.S.I.

LOADING: HL93 OR OKLAHOMA OVERLOAD TRUCK, 20 P.S.F. FUTURE WEARING SURFACE, AND 5 P.S.F. STAY-IN-PLACE FORMS.

DESIGN: AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, 8TH EDITION WITH CURRENT INTERIMS.

ANSI/AASHTO/AWS: D1.5 BRIDGE WELDING CODE
ANSI/AASHTO/AWS: D1.6 STRUCTURAL WELDING CODE - STAINLESS STEEL

LRFD OPERATING RATING: XX.X (HL-93)

ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.

STANDARDS

FSHP-42-2-00E
EJ-SQ-04E
EJ-DTL-02E
HP1-2-01E

FOUNDATION DATA ABUTMENT CAPS (HP 12X53 PILING) 1

FACTORED PILE REACTION (TONS/PILE) = XX.X
PILE LENGTH (FEET) = XX.X

1 ALL ABUTMENT PILING SHALL BE DRIVEN THROUGH COMPACTED FILL. PILING SHALL BE DRIVEN TO POINT BEARING ON SOLID FOUNDATION MATERIAL AT THE APPROXIMATE ELEVATION SHOWN ON THE PLANS. IF THE AXIAL LOAD RESISTANCE IS NOT OBTAINED AT THIS ELEVATION, DRIVING SHALL CONTINUE UNTIL THE AXIAL LOAD RESISTANCE IS OBTAINED. THE LENGTH OF THE STEEL PILING SHOWN ON THE PLANS IS FOR ESTIMATING PURPOSES ONLY.

FOUNDATION DATA PIERS (72" DIAMETER DRILLED SHAFTS)

Table with 17 columns (PIER NO. 1 to 17) and 10 rows of resistance and reaction data. Values are represented as XX.XX or XXXX.

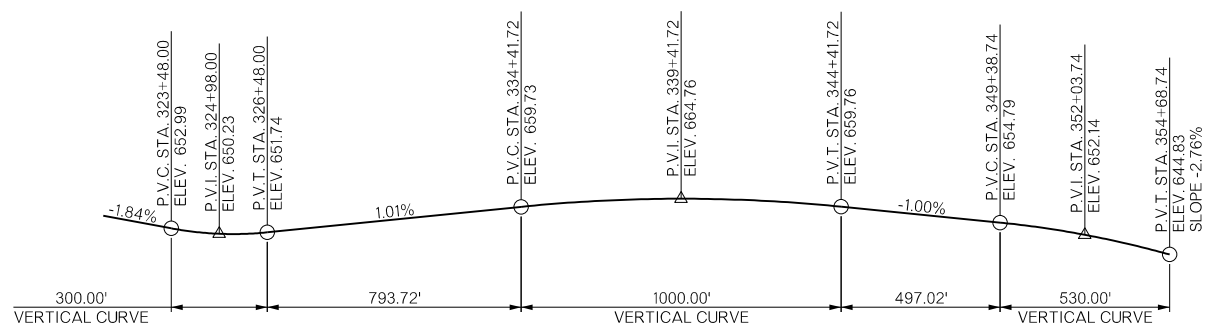
NOTE: THE CONTRACTOR IS RESPONSIBLE FOR BECOMING FAMILIAR WITH THE GEOTECHNICAL REPORTS AND SITE SUBSURFACE CONDITIONS, AND THEIR IMPLICATIONS ON THE DIFFICULTY OF DRILLING PROCESSES, PRIOR TO BEGINNING WORK.

INDEX OF SHEETS

Table with 2 columns: SHEET NO. and TITLE. Lists sheets B017 through B024 and their corresponding titles.

SUMMARY OF QUANTITIES

Large empty table with 7 columns: ITEM, UNIT, ABUTMENTS, PIERS, SUPER-STRUCTURE, APPROACH SLABS, TOTAL.



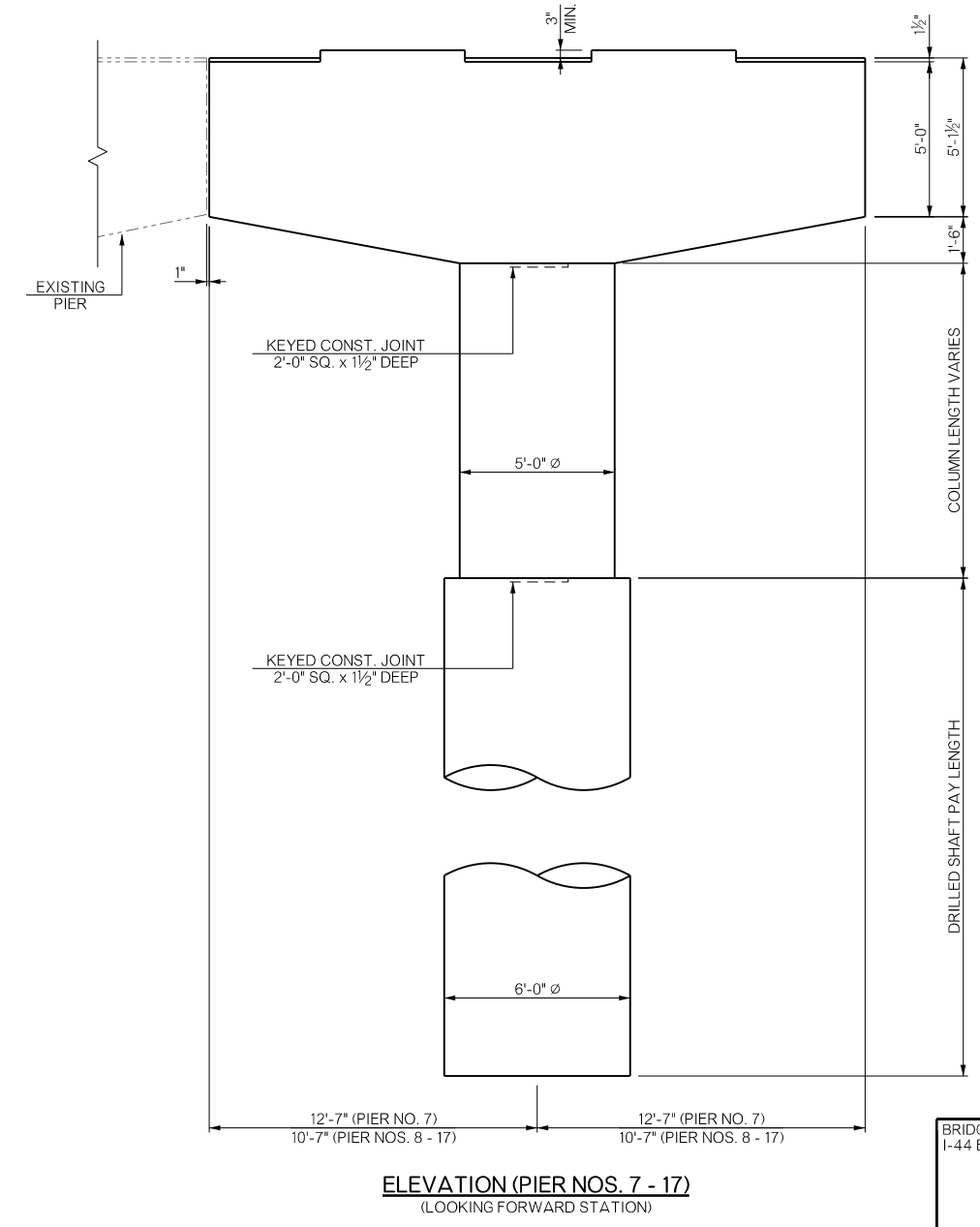
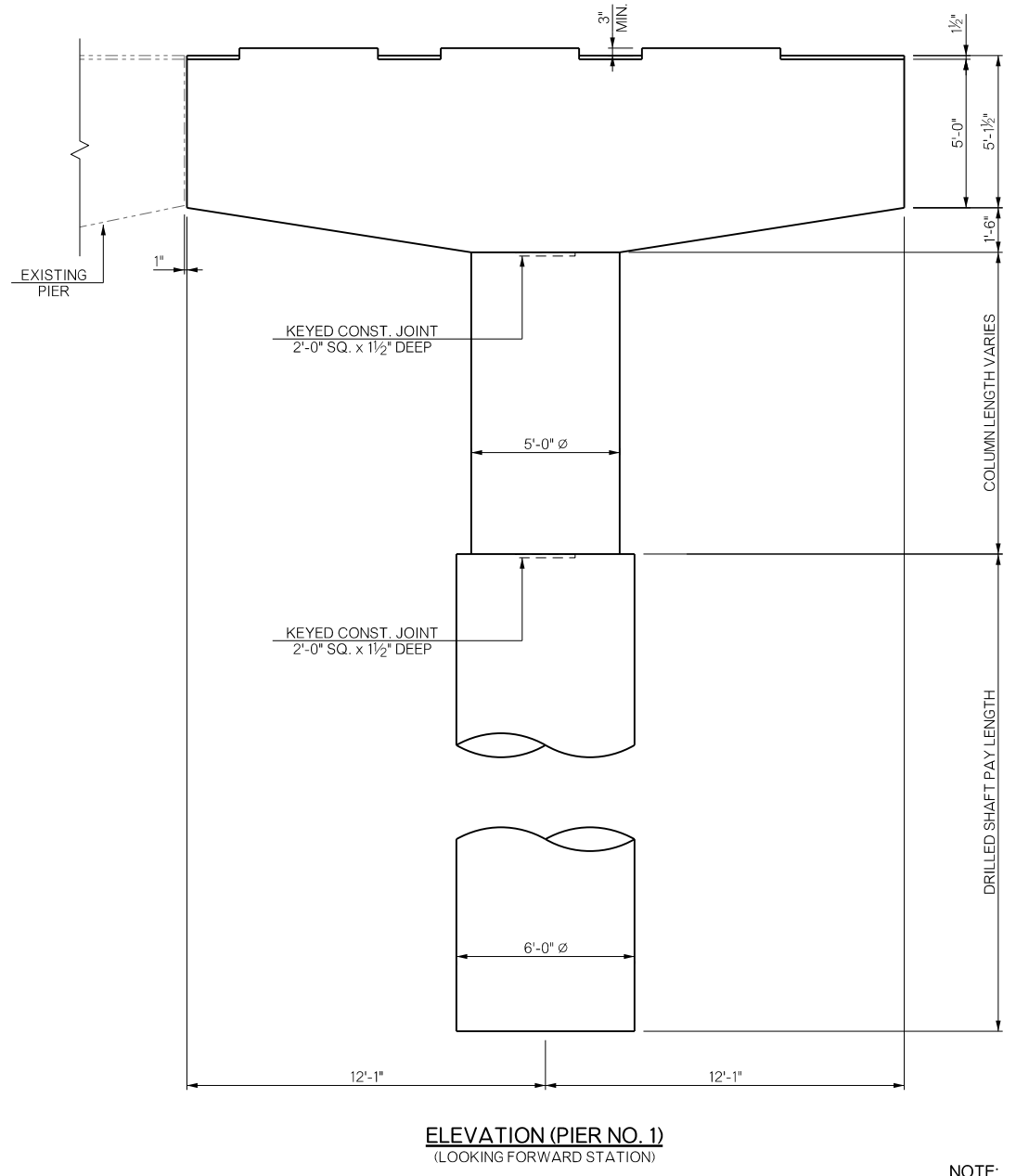
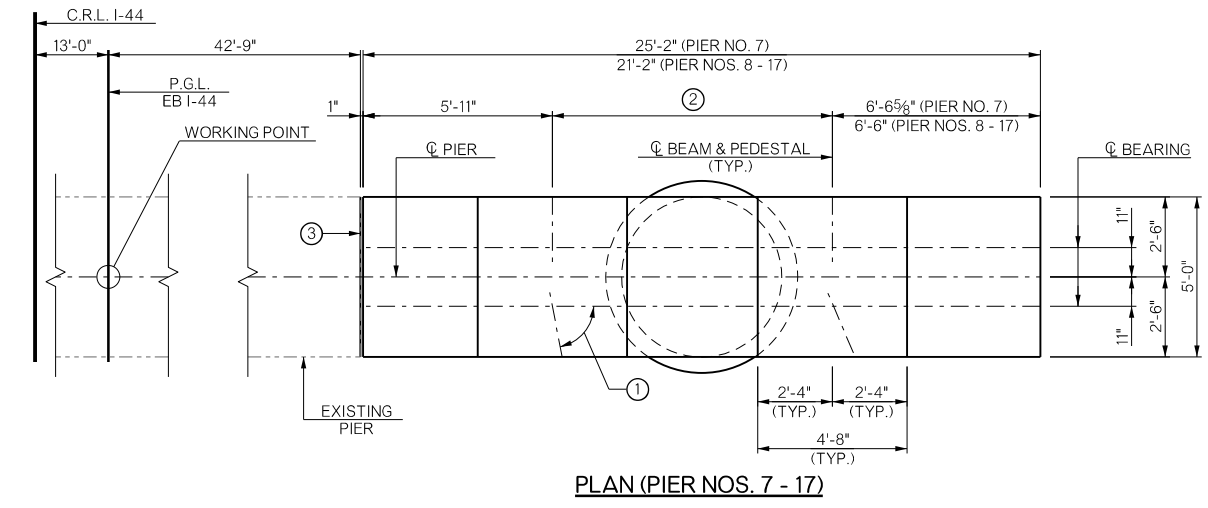
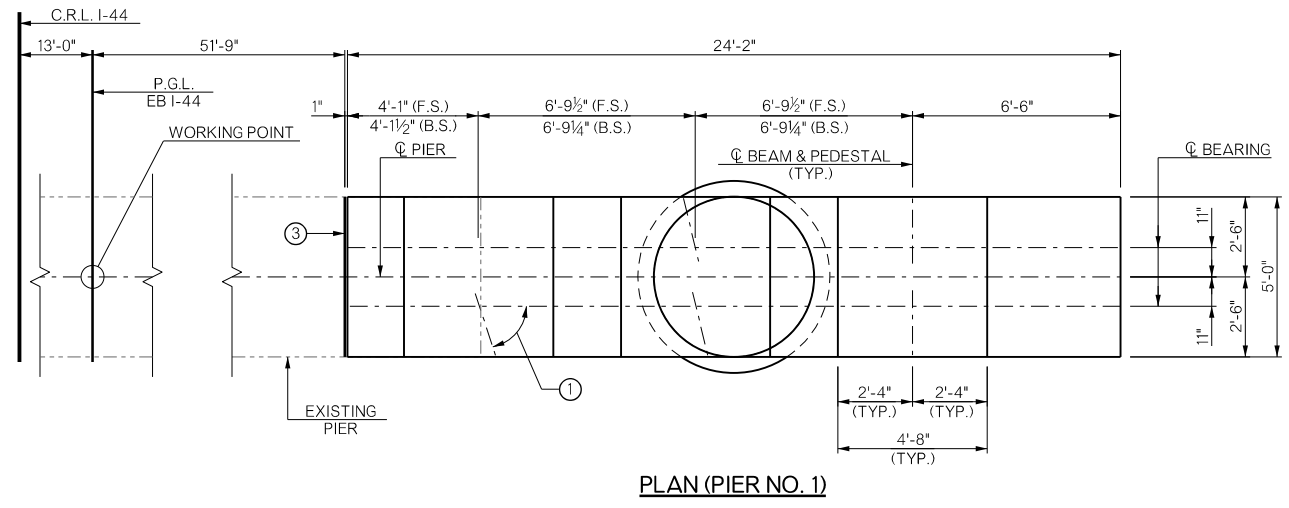
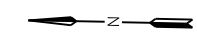
VERTICAL PROFILE DATA

Project information block including: BRIDGE "T2" I-44 EB OVER ARKANSAS RIVER, TULSA COUNTY, GENERAL PLAN AND ELEVATION (SHEET 5 OF 5), WIDEN (21) 100'-6" TYPE J.P.C.B. SPANS W/ 42" PARAPETS W/ VARYING CLEAR ROADWAY AT CENTERLINE STA. 336+83.43, DESIGNER: GARVER, DATE: 8/20.

- ① ANGLE SHOWN EXAGGERATED FOR CLARITY, AND MAY VARY BETWEEN PIERS AND BEAMS.
- ② NUMBER AND SPACING OF BEAMS MAY VARY BETWEEN PIERS.

ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.

- ③ SAW CUT END OF CAP ON PIER NOS. 1, 7, 9, 11, 12, 13, 15, & 17. SEE REMOVAL DETAIL ON SHEET NUMBER B023. PAINT CUT REBAR AND ADJOINING CONCRETE WITHIN 1" OF REBAR WITH TWO COATS OF ZINC-RICH PAINT (6 MIL. MINIMUM THICKNESS).

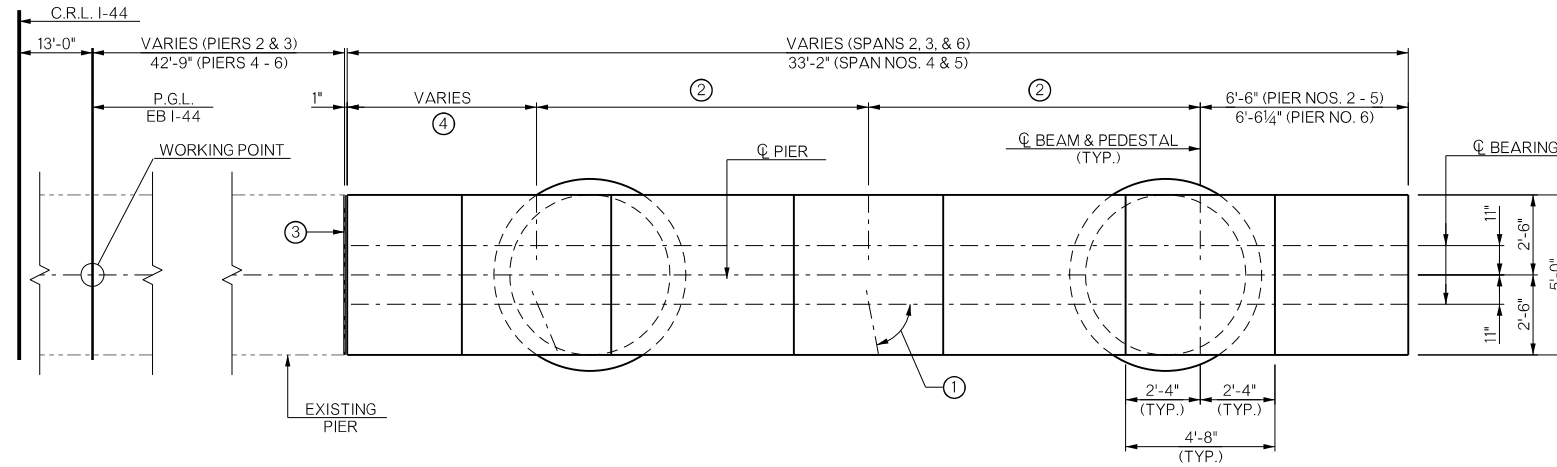
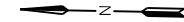


NOTE:
ALL EDGES OF PIER CAP SHALL HAVE A 1/2" CHAMFER EXCEPT FOR PEDESTAL EDGES WHICH SHALL HAVE A 3/4" CHAMFER.

3/12/2021 3:38:00 PM L:\2019\19103080 - 000T Cl-2123 I-44 and US-75 Final\WP_P0E\Drawings\Bridges T2\3378809-T2-PIER DETAILS I.dgn

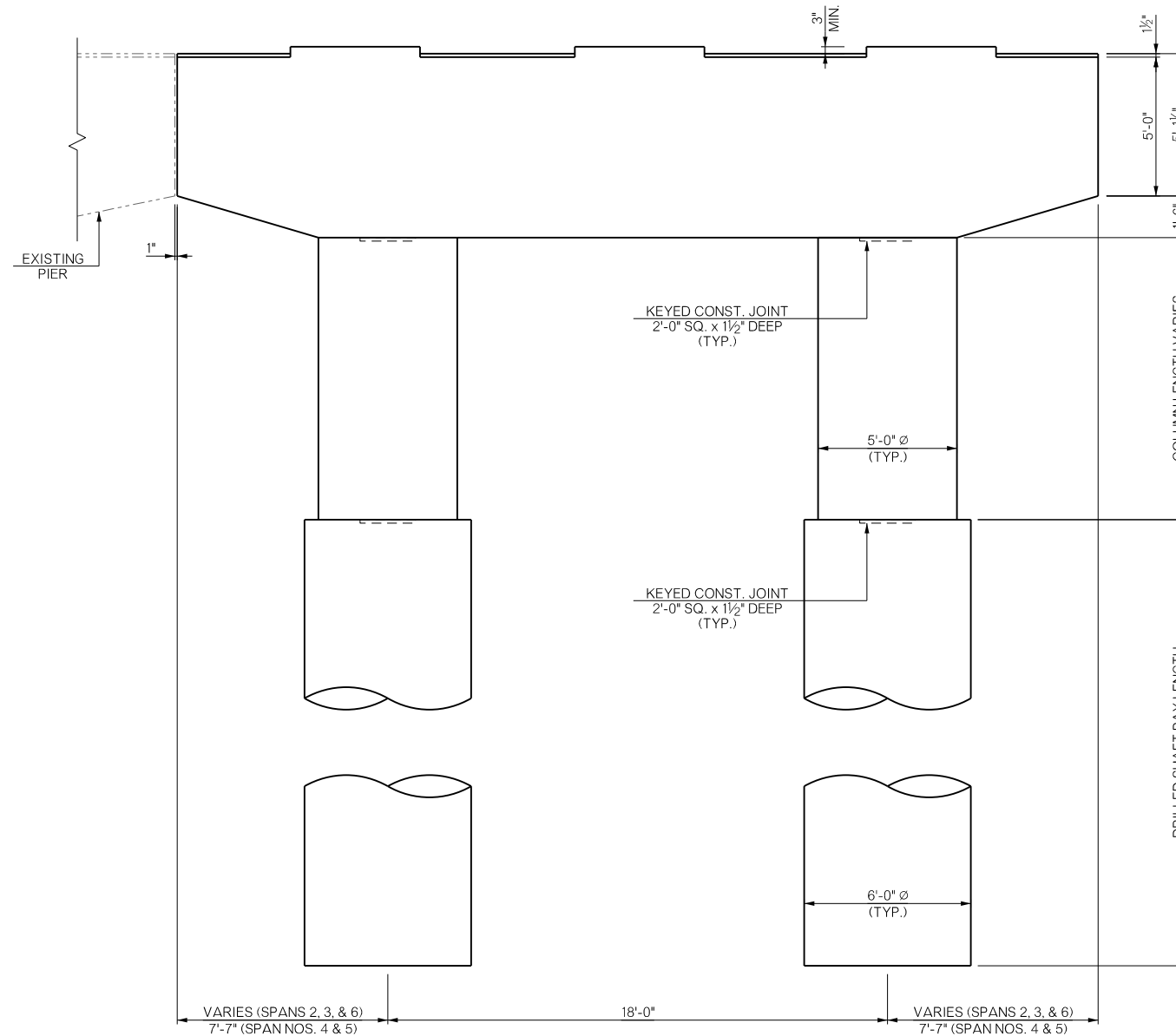
BRIDGE "T2" I-44 EB OVER ARKANSAS RIVER		TULSA COUNTY	DESIGN	DPE	8/20
			DETAIL	SJL	8/20
			CHECK	DPE	8/20
PIER DETAILS (SHEET 1 OF 2)		GARVER			
STATE OF OKLAHOMA	DEPARTMENT OF TRANSPORTATION		SHEET NO. B022		
JOB PIECE NO. 33788(09)					

ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.

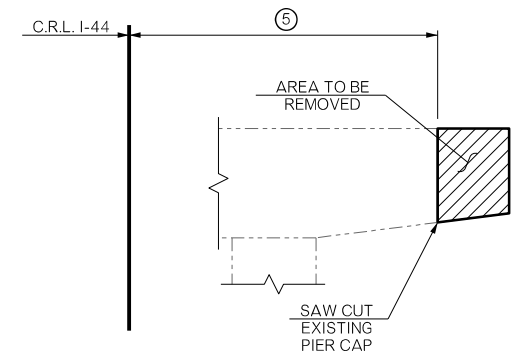


PLAN (PIER NOS. 2 - 6)

- ① ANGLE SHOWN EXAGGERATED FOR CLARITY, AND MAY VARY BETWEEN PIERS AND BEAMS.
- ② VARIES AT ALL LOCATIONS EXCEPT: 10'-4 1/2" (PIER NO. 4 F.S. & PIER NO. 5 B.S. BEARING LINES)
- ③ SAW CUT END OF CAP ON PIER NOS. 3 & 5. SEE REMOVAL DETAIL. PAINT CUT REBAR AND ADJOINING CONCRETE WITHIN 1" OF REBAR WITH TWO COATS OF ZINC-RICH PAINT (6 MIL. MINIMUM THICKNESS).
- ④ 5'-11" (PIER NO 4 F.S. BEARING LINE, & PIER NOS. 5 & 6) 5'-11 3/8" (PIER NO 4 B.S. BEARING LINE)
- ⑤ 64'-9" (PIER NO. 2) 58'-9" (PIER NO. 2) 55'-9" (TYP.)



ELEVATION (PIER NOS. 2 - 6)
(LOOKING FORWARD STATION)



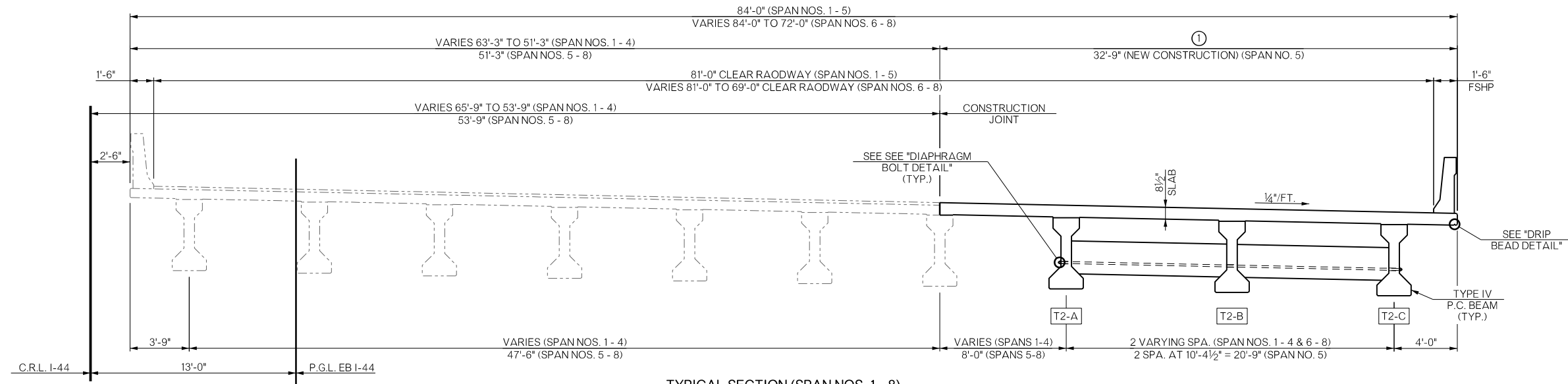
REMOVAL DETAIL

NOTE:
ALL EDGES OF PIER CAP SHALL HAVE A 1/2" CHAMFER EXCEPT FOR PEDESTAL EDGES WHICH SHALL HAVE A 3/4" CHAMFER.

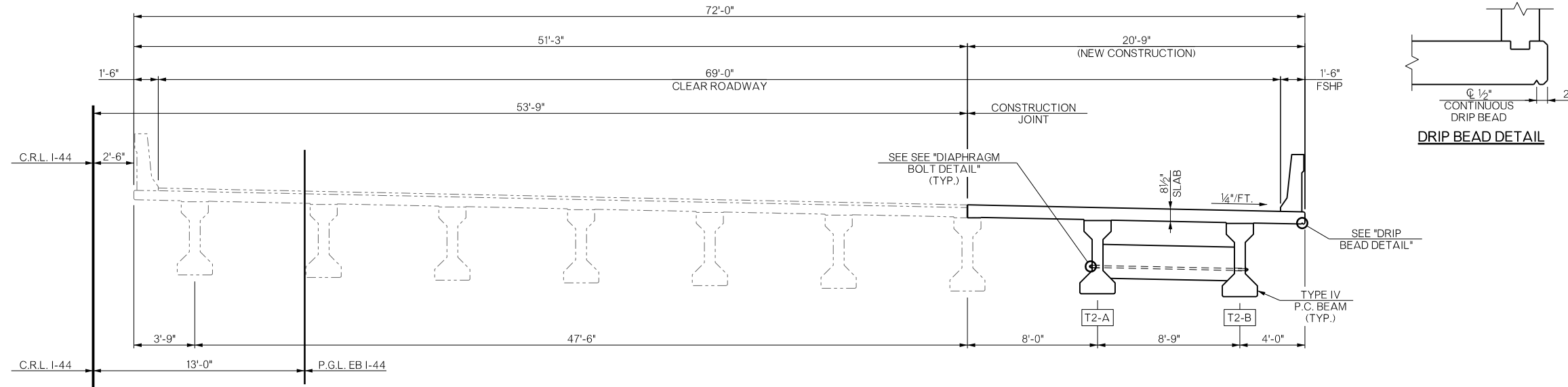
BRIDGE "T2" I-44 EB OVER ARKANSAS RIVER		TULSA COUNTY		DESIGN	DPE	8/20
PIER DETAILS (SHEET 2 OF 2)		DETAIL	S.J.L.	8/20		
		CHECK	DPE	8/20		
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION		GARVER		
		JOB PIECE NO. 33788(09)				

ALL INFORMATION INCLUDED IN THESE PLANS IS BASED ON THE EXISTING AS-BUILT AND SURVEYED DATA. IT IS SOLELY THE CONTRACTOR'S RESPONSIBILITY TO ACCURATELY VERIFY THIS INFORMATION PRIOR TO ANY DEMOLITION OR CONSTRUCTION. FOR ADDITIONAL INFORMATION, SEE THE GENERAL NOTES "VERIFICATION OF EXISTING CONDITIONS", "SURVEYING AND CONSTRUCTION STAKING", & "ESTABLISHMENT OF VERTICAL GEOMETRY" ON SHEET NO. AB01.

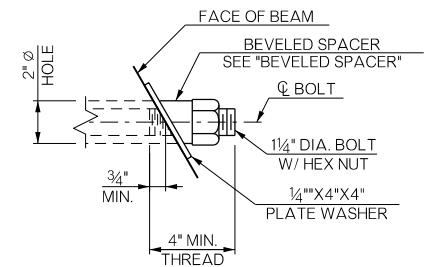
① 20'-9" TO 32'-9" (NEW CONSTRUCTION) (SPAN NOS. 1 - 4)
32'-9" TO 20'-9" (NEW CONSTRUCTION) (SPAN NOS. 6 - 8)



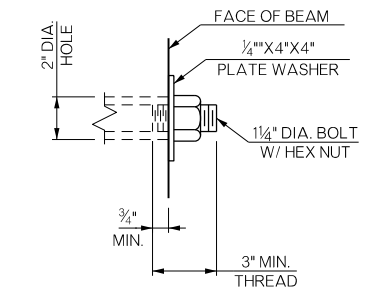
TYPICAL SECTION (SPAN NOS. 1 - 8)
(LOOKING FORWARD STATION)
(SHOWING INTERMEDIATE DIAPHRAGMS)



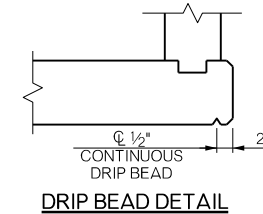
TYPICAL SECTION (SPAN NOS. 9 - 21)
(LOOKING FORWARD STATION)
(SHOWING INTERMEDIATE DIAPHRAGMS)



DIAPHRAGM BOLT ASSEMBLY DETAIL
(SPAN NOS. 1 - 4 & 6 - 8 ONLY)



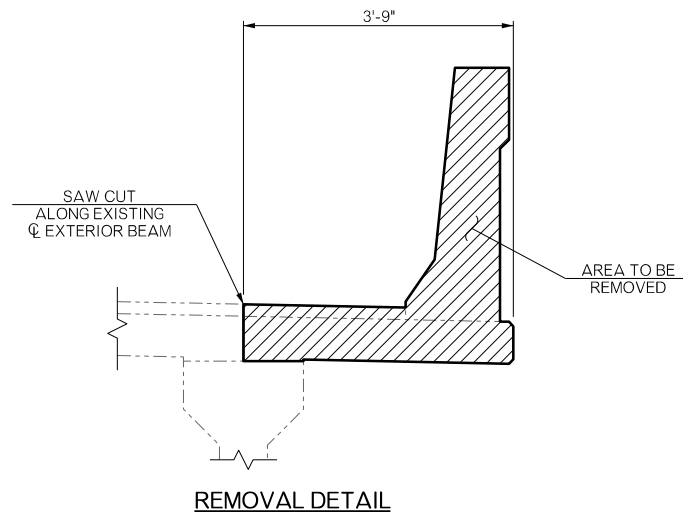
DIAPHRAGM BOLT ASSEMBLY DETAIL



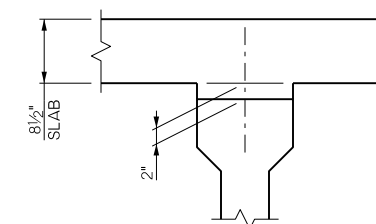
DRIP BEAD DETAIL

DIAPHRAGM BOLT NOTES:
PROVIDE STRUCTURAL STEEL FOR DIAPHRAGM BOLTS AND PLATE WASHERS IN ACCORDANCE WITH AASHTO M270 (ASTM A709), GRADE 50W (WEATHERING STEEL, CHARPY V-NOTCH TESTING NOT REQUIRED). THE CONTRACTOR MAY SUBSTITUTE A #10 REINFORCING BAR IN ACCORDANCE WITH AASHTO M31, GRADE 60, AND THREADED AT THE ENDS AS SHOWN FOR THE DIAPHRAGM BOLT AT NO ADDITIONAL COST TO THE DEPARTMENT. PROVIDE HEX NUTS IN ACCORDANCE WITH AASHTO M291 (ASTM A563).

PAINT EXPOSED DIAPHRAGM BOLT, BEVELED SPACER, PLATE WASHER AND HEX NUT WITH TWO (2) COATS OF ZINC-RICH PAINT (6 MIL. MINIMUM THICKNESS) AFTER ASSEMBLY. INCLUDE ALL COST OF DIAPHRAGM BOLT, PLATE WASHER AND HEX NUT IN THE CONTRACT UNIT PRICE FOR "STRUCTURAL STEEL".

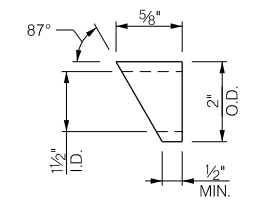


REMOVAL DETAIL



BEAM HAUNCH DETAIL

NOTE:
PLAN QUANTITIES FOR CLASS AA CONCRETE INCLUDE BEAM HAUNCHES. THE HAUNCH HEIGHT SHOWN IS THE THEORETICAL HAUNCH HEIGHT AT THE CENTERLINE BEARING ONLY, MEASURED FROM BOTTOM OF DECK SLAB TO TOP OF BEAM, AND VARIES ACROSS THE SPAN. DETERMINE THE ACTUAL HAUNCH HEIGHT (ACCOUNTING FOR BEAM CAMBER, DEAD LOAD DEFLECTION AND ROADWAY GRADE) AFTER ERECTION OF THE BEAMS AND SUBMIT TO THE ENGINEER FOR APPROVAL. THE ENGINEER WILL NOT MEASURE DIFFERENCES BETWEEN THE THEORETICAL AND THE ACTUAL HAUNCH HEIGHTS FOR PAYMENT.



BEVELED SPACER
(SPAN NOS. 1 - 4 & 6 - 8 ONLY)

NOTE:
EXTRA STRONG PIPE SLEEVE.

BRIDGE "T2" I-44 EB OVER ARKANSAS RIVER		TULSA COUNTY		DESIGN	DPE	8/20
				DETAIL	S.J.L.	8/20
				CHECK	DPE	8/20
TYPICAL SECTION				GARVER		
STATE OF OKLAHOMA		DEPARTMENT OF TRANSPORTATION				
		JOB PIECE NO. 33788(09)		SHEET NO. B024		

DESIGN DATA

DESIGNED IN ACCORDANCE WITH 2007 AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS AND INTERIM SPECIFICATIONS FROM 2008.

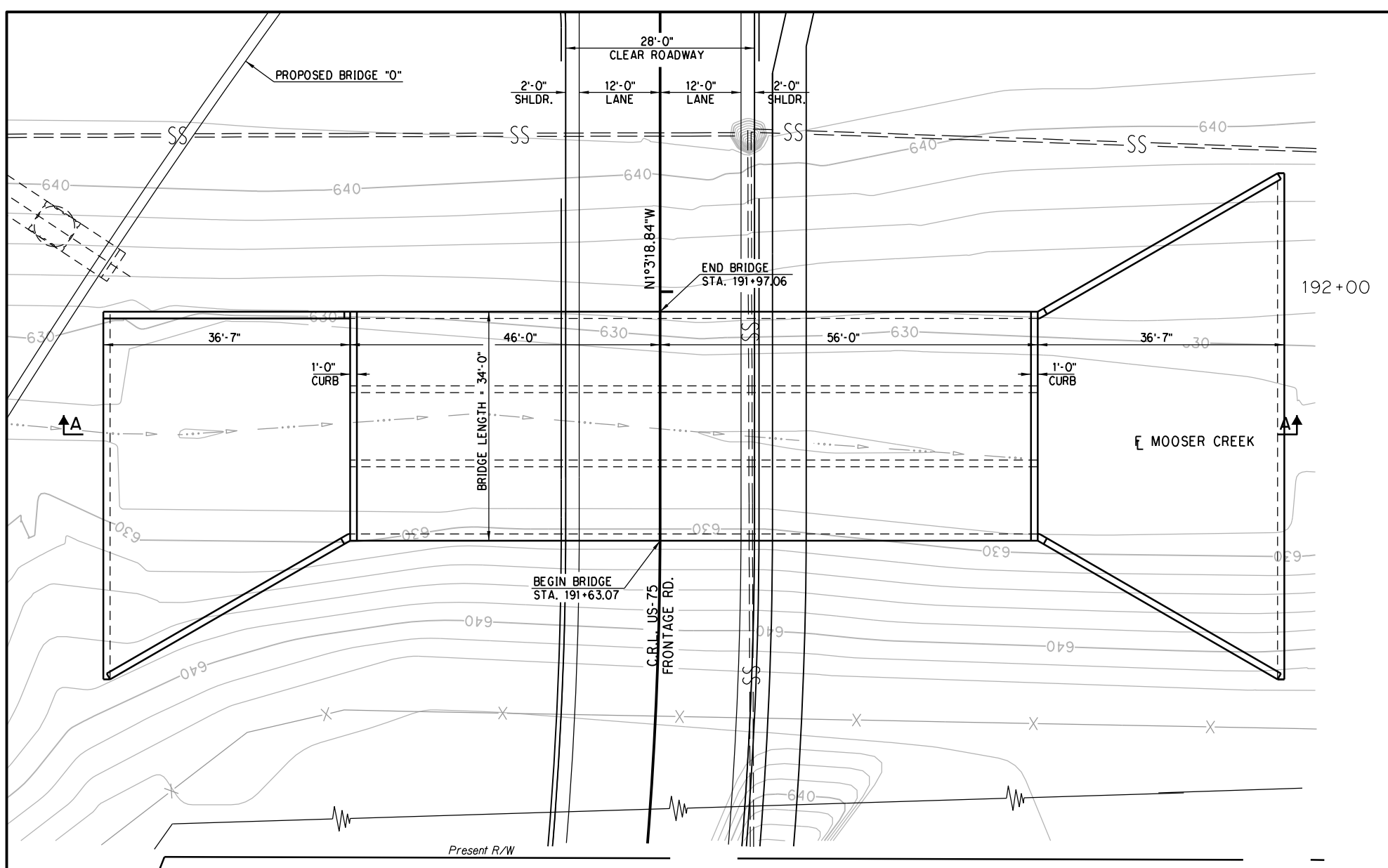
DESIGNED FOR HL-93 AND ODOT OVERLOAD TRUCK

MATERIALS:
CONCRETE (CLASS AA): f'c = 4 KSI
REINFORCING STEEL: fy = 60 KSI

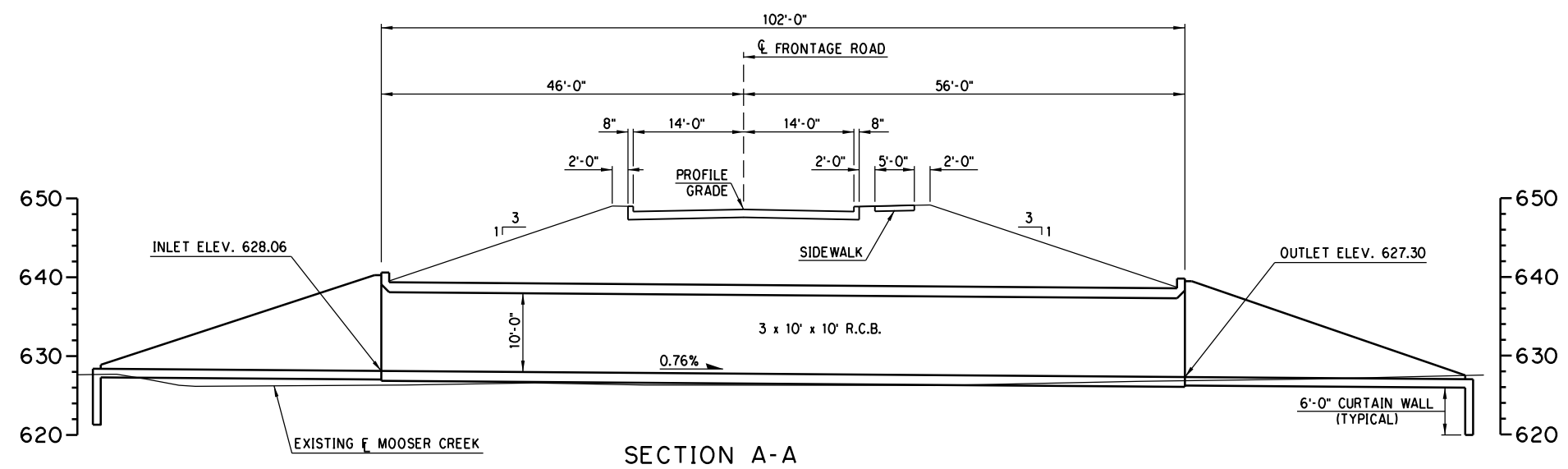
ODOT STANDARDS

RCB-C3-10(2-12)
RCB-E3-H10-0-1
RCB-E3-H10-0-2

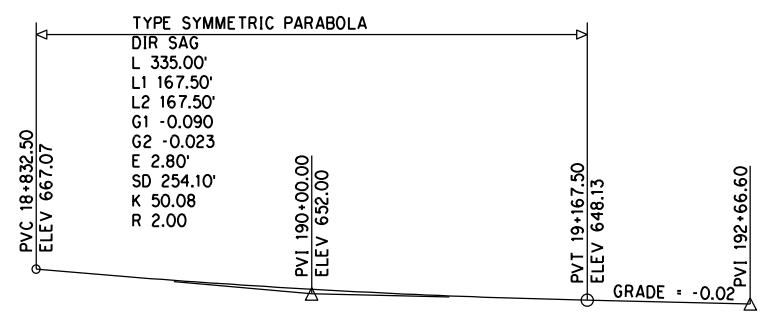
HORIZONTAL CURVE DATA
PI STA. = 189+66.54
X = 2557831.2707
Y = 401523.8914
R = 600.00'
L = 411.54'
T = 214.24'
E = 37.10'
Delta = 39°28'23"
Dc = 9°32'57.47"
V = 35 M.P.H.
E% = NC



PLAN
SCALE: 1"=10'



SECTION A-A



PROFILE GRADE DATA

DESIGN		BRIDGE "U"	U.S. 75 FRONTAGE ROAD OVER MOOSER CREEK
DRAWN		GENERAL PLAN AND ELEVATION BRIDGE STA. 191+80.06 C.R.L. US-75 FRNTG. RD. CONST. TRIPLE 10' x 10' SPAN R.C.B. SKEW 0° STATE JOB NO. JP33788(09) SHEET NO. B025	
CHECKED			
APPROVED			
SQUAD	POE		

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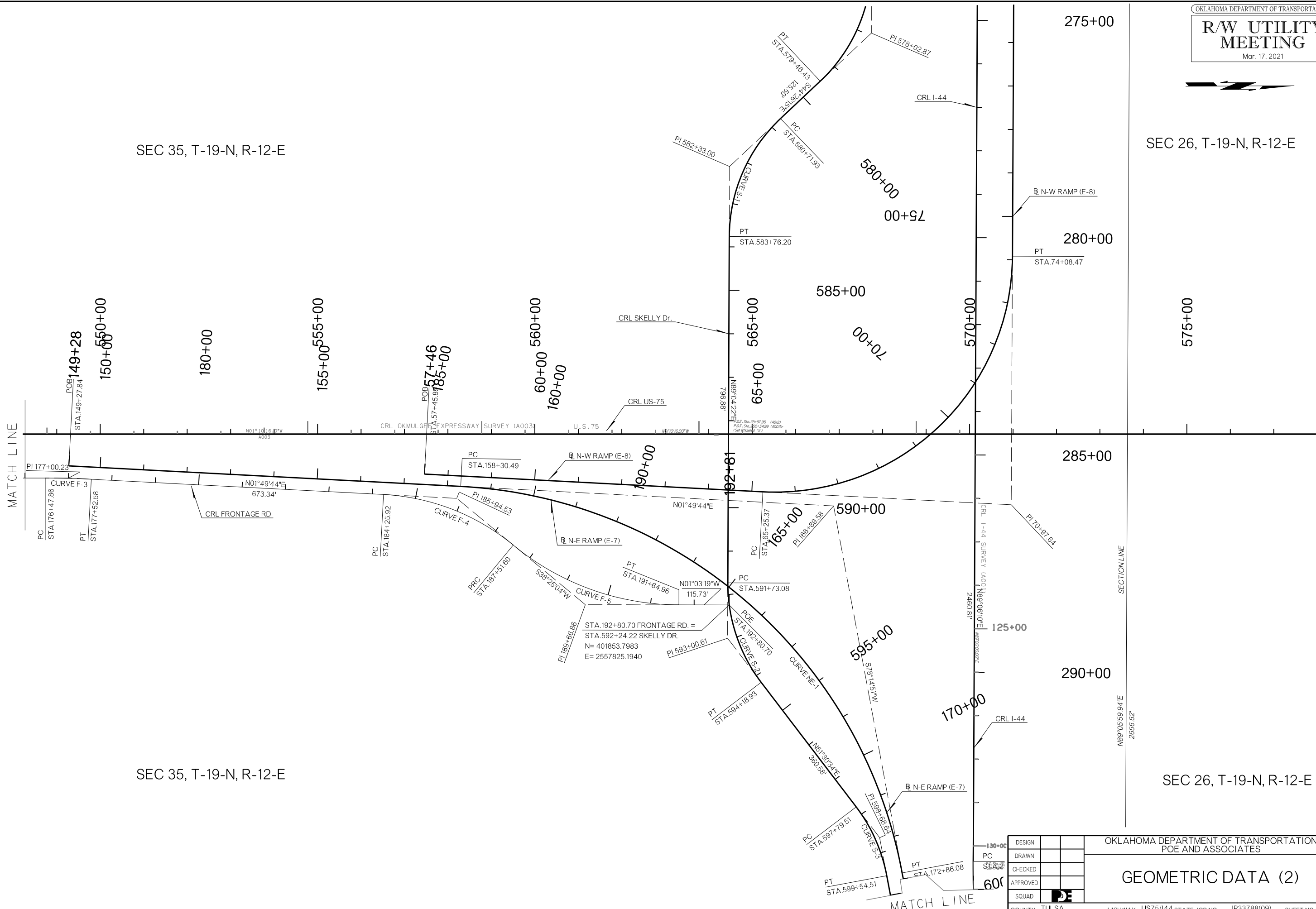


SEC 35, T-19-N, R-12-E

SEC 26, T-19-N, R-12-E

SEC 35, T-19-N, R-12-E

SEC 26, T-19-N, R-12-E



3/18/2021 H:\203192 US75-144\Plan Sheets\33788(09)-Geo 2.dgn

DESIGN	
DRAWN	
CHECKED	
APPROVED	
SQUAD	

OKLAHOMA DEPARTMENT OF TRANSPORTATION
POE AND ASSOCIATES

GEOMETRIC DATA (2)

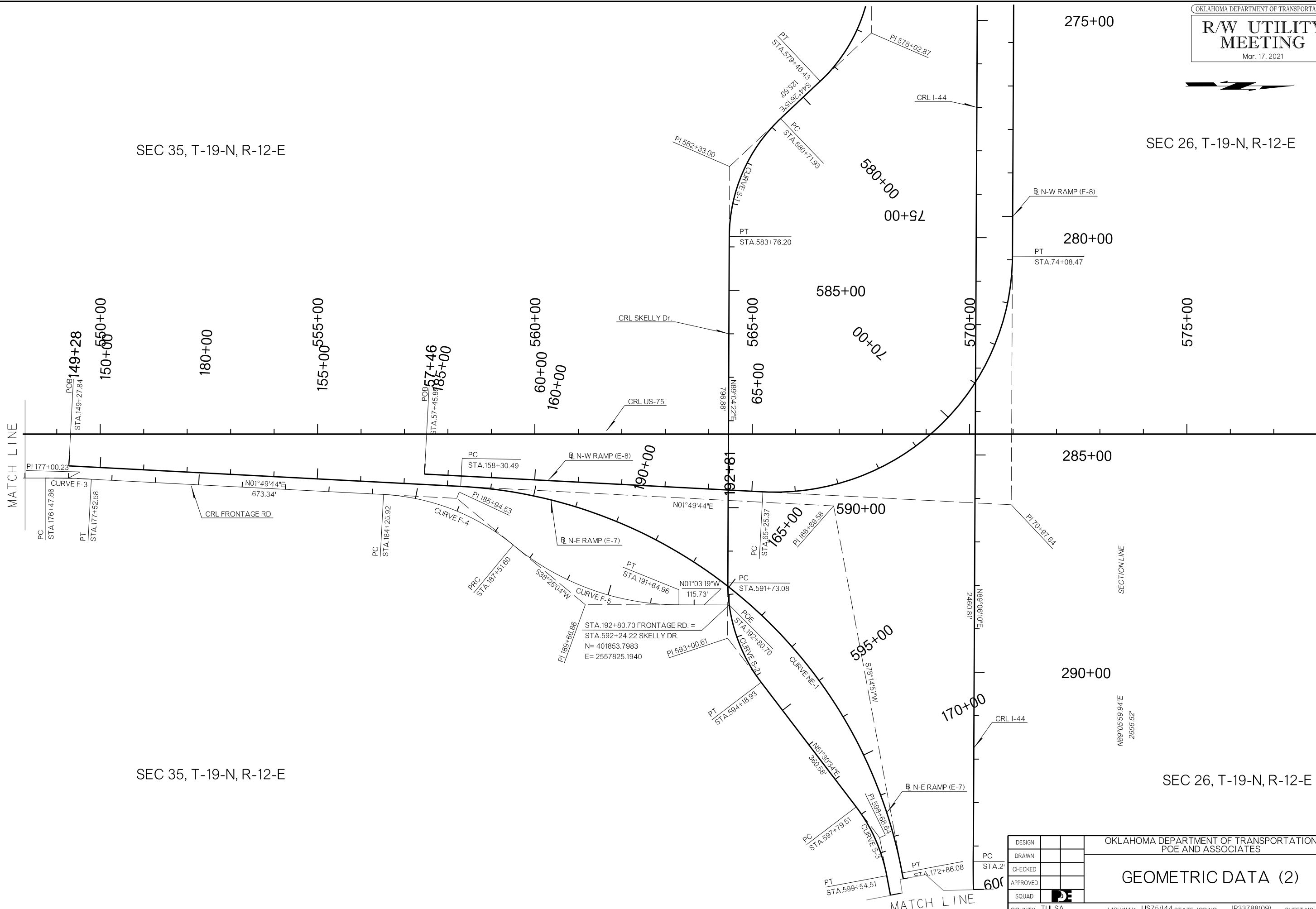


SEC 35, T-19-N, R-12-E

SEC 26, T-19-N, R-12-E

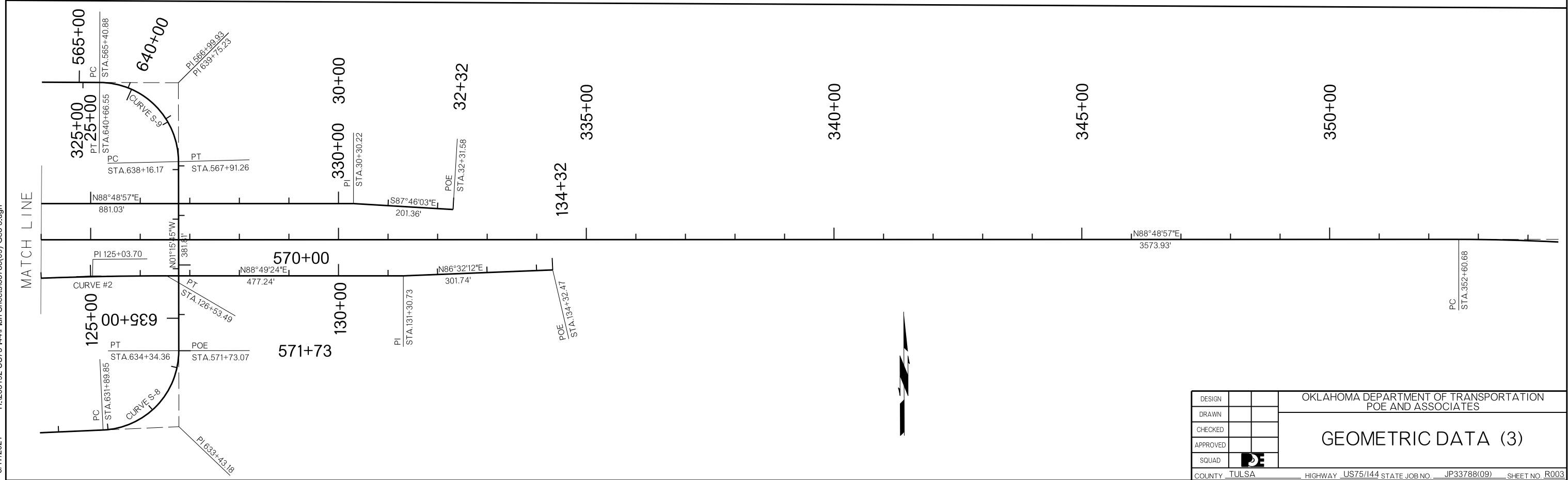
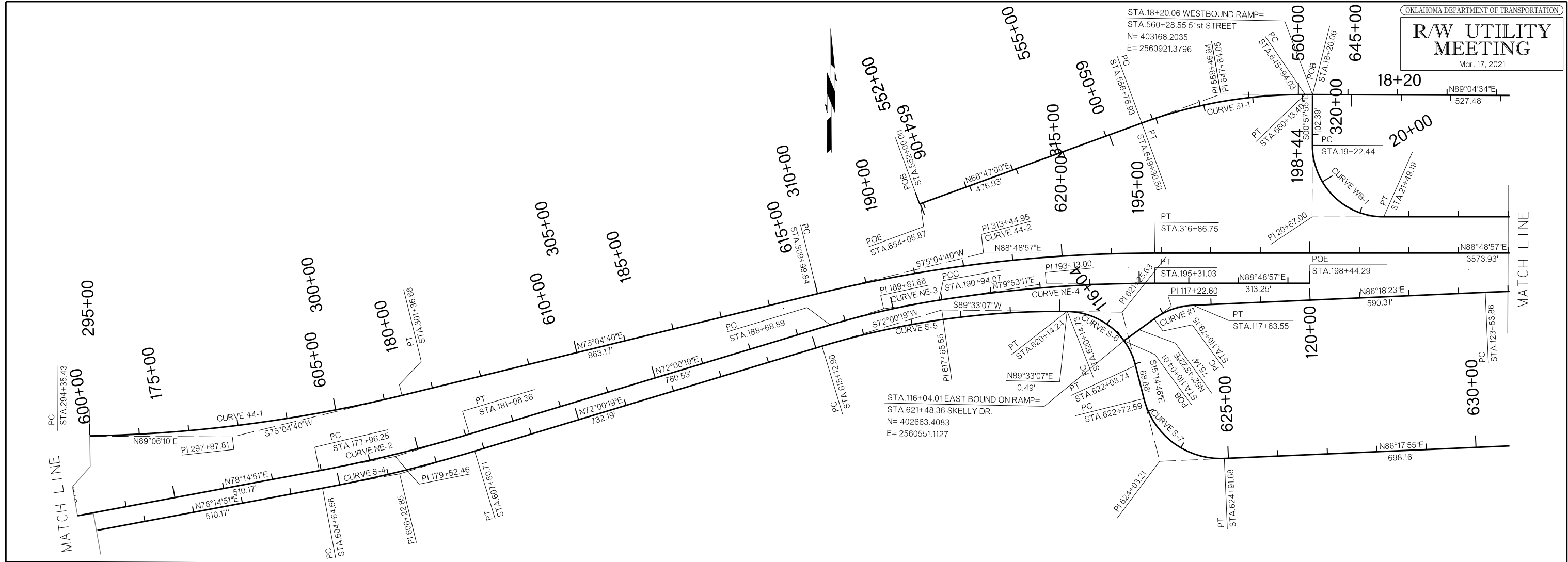
SEC 35, T-19-N, R-12-E

SEC 26, T-19-N, R-12-E



DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION
DRAWN		POE AND ASSOCIATES
CHECKED		<h2>GEOMETRIC DATA (2)</h2>
APPROVED		
SQUAD		
COUNTY - TULSA	HIGHWAY - US75/144 STATE JOB NO. - JP33788(09) SHEET NO. R002	

3/17/2021 H:\203192 US75-144\Plan Sheets\33788(09)-Geo 2.dgn



DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN		GEOMETRIC DATA (3) COUNTY - TULSA HIGHWAY - US75/144 STATE JOB NO. - JP33788(09) SHEET NO. R003
CHECKED		
APPROVED		
SQUAD		

3/17/2021 H:\203192 US75-144\Plan Sheets\33788(09)-Geo 3.dgn

R/W UTILITY MEETING

Mar. 17, 2021

51st St		CARDINAL POINTS			CURVE DATA					SUPERELEVATION		
CURVE NO	STATION	X(EASTING) (FT)	Y(NORTHING) (FT)	DELTA	DEGREE	RADIUS (FT)	TANGENT (FT)	ARC LENGTH (FT)	EXTERNAL (FT)	DESIGN SPEED (MPH)	e _{max} (%)	S (%)
	POB	552+00.00	2560133.1560	402931.0929								
	PC	556+76.93	2560577.7604	403103.6913								
51-1	PI	558+46.94	2560736.2506	403165.2182	20°17'33.73"	RT	6°01'52.08"	950.00	170.01	336.47	15.09	35 6.00 5.00
	CC	2560921.5594	402218.0829									
	PT	560+13.40	2560906.2423	403167.9594								

I44 CRL		CARDINAL POINTS			CURVE DATA					SUPERELEVATION		
CURVE NO	STATION	X(EASTING) (FT)	Y(NORTHING) (FT)	DELTA	DEGREE	RADIUS (FT)	TANGENT (FT)	ARC LENGTH (FT)	EXTERNAL (FT)	DESIGN SPEED (MPH)	e _{max} (%)	S (%)
	POB	200+00.00	2548969.6976	402271.5493								
	PI	216+81.66	2550651.1460	402298.2230								
	PI	242+00.00	2553169.2236	402334.5786								
	PI	269+74.61	2555943.3078	402388.6800								
	PC	294+35.43	2558403.8198	402427.2141								
44-1	PI	297+87.81	2558756.1629	402432.7322	14°01'30.00"	LT	2°00'00.00"	2864.79	352.39	701.25	21.59	65 6.00 5.00
	CC	2558358.9597	405291.6519									
	PT	301+36.68	2559096.6654	402523.4744								
	PC	309+99.84	2559930.7222	402745.7465								
44-2	PI	313+44.95	2560264.1908	402834.6142	13°44'17.06"	RT	2°00'00.00"	2864.79	345.11	686.90	20.71	65 6.00 5.00
	CC	2560668.4281	399977.5681									
	PT	316+86.75	2560609.2240	402841.7462								

US 75 CRL		CARDINAL POINTS			CURVE DATA					SUPERELEVATION		
CURVE NO	STATION	X(EASTING) (FT)	Y(NORTHING) (FT)	DELTA	DEGREE	RADIUS (FT)	TANGENT (FT)	ARC LENGTH (FT)	EXTERNAL (FT)	DESIGN SPEED (MPH)	e _{max} (%)	S (%)
	POB	500+00.00	2557563.9336	395400.0832								
	PC	595+11.78	2557369.5306	404909.8769								
75-1	PI	605+02.51	2557349.2820	405900.3990	19°37'14.04"	LT	1°00'00.00"	5729.58	990.73	1962.06	85.02	65 6.00 NC
	CC	2551641.1495	404792.7751									
	PT	614+73.84	2556997.6019	406826.6091								
	POE	640+72.49	2556075.1539	409256.0350								

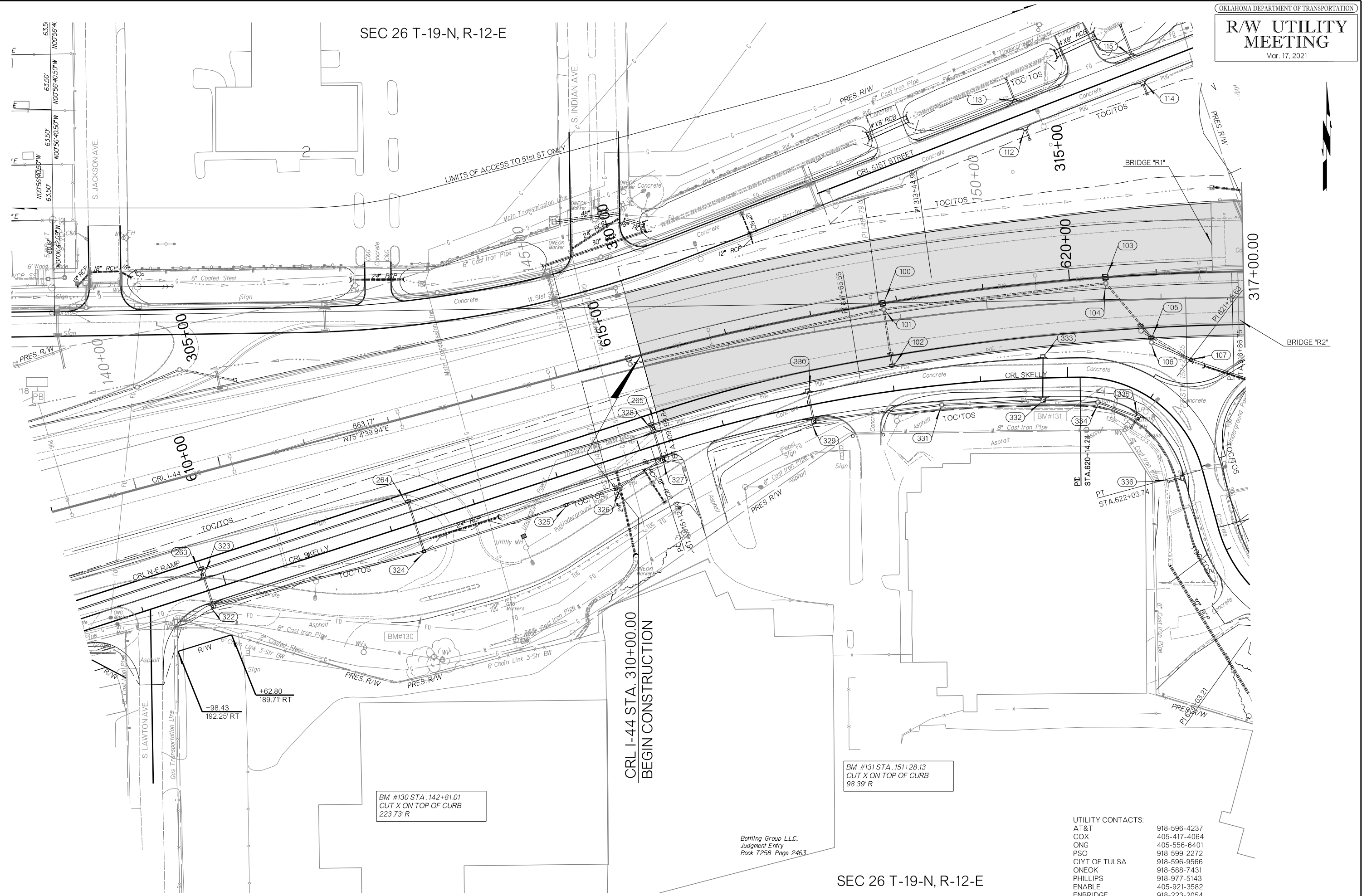
East Frontage Road		CARDINAL POINTS			CURVE DATA					SUPERELEVATION		
CURVE NO	STATION	X(EASTING) (FT)	Y(NORTHING) (FT)	DELTA	DEGREE	RADIUS (FT)	TANGENT (FT)	ARC LENGTH (FT)	EXTERNAL (FT)	DESIGN SPEED (MPH)	e _{max} (%)	S (%)
	POB	150+00.00	2557794.7904	397660.1448								
	PC	157+50.71	2557779.4473	398410.7000								
F-1	PI	158+48.75	2557779.4473	398508.7373	21°45'44.85"	LT	11°14'04.08"	510.00	98.04	193.71	9.34	35 6.00 NC
	CC	2557269.4473	398410.7000									
	PT	159+44.42	2557743.0990	398599.7873								
	PC	162+59.87	2557626.1457	398892.7481								
F-2	PI	163+52.51	2557591.7974	398978.7883	20°35'28.89"	RT	11°14'04.08"	510.00	92.64	183.29	8.35	35 6.00 NC
	CC	2558099.7974	399081.8354									
	PT	164+43.15	2557539.9040	399071.4120								
	PC	176+47.86	2557565.2821	400275.8642								
F-3	PI	177+00.23	2557564.2117	400328.2251	3°00'00.00"	RT	2°51'53.24"	2000.00	52.37	104.72	0.69	35 6.00 NC
	CC	2559564.8643	400316.7405									
	PT	177+52.58	2557565.8831	400380.5703								
	PC	184+25.92	2557537.3727	401053.5671								
F-4	PI	185+94.53	2557592.7539	401222.0927	36°35'19.97"	RT	11°14'04.08"	510.00	168.61	325.68	27.15	35 6.00 NC
	CC	2558097.1129	401037.2905									
	PT	187+51.60	2557697.5276	401354.1999								
F-5	PI	189+66.86	2557831.2897	401522.8582	39°28'22.85"	LT	9°32'57.47"	800.00	215.26	413.36	37.45	35 6.00 NC
	CC	2557227.4272	401727.0345									
	PT	191+64.96	2557827.3254	401738.0842								
	POE	192+80.70	2557825.1940	401853.7983								

I44 WB OFF RAMP		CARDINAL POINTS			CURVE DATA					SUPERELEVATION		
CURVE NO	STATION	X(EASTING) (FT)	Y(NORTHING) (FT)	DELTA	DEGREE	RADIUS (FT)	TANGENT (FT)	ARC LENGTH (FT)	EXTERNAL (FT)	DESIGN SPEED (MPH)	e _{max} (%)	S (%)
	POB	18+20.06	2560921.3796	403168.2035								
	PC	19+22.44	2560923.1043	403065.8312								
WB-1	PI	20+67.00	2560925.5392	402921.3002	90°13'08.38"	LT	39°47'19.45"	144.00	144.55	226.75	60.04	20 6.00 6.00
	PT	21+49.19	2561070.0598	402924.2875								
	PI	30+30.22	2561950.9007	402942.4950								
	POE	32+31.58	2562152.1058	402934.6515								

I44 EB ON RAMP		CARDINAL POINTS			CURVE DATA					SUPERELEVATION		
CURVE NO	STATION	X(EASTING) (FT)	Y(NORTHING) (FT)	DELTA	DEGREE	RADIUS (FT)	TANGENT (FT)	ARC LENGTH (FT)	EXTERNAL (FT)	DESIGN SPEED (MPH)	e _{max} (%)	S (%)
	POB	116+04.01	2560551.1127	402663.4083								
EB-1	PI	117+22.60	2560645.4769	402735.2353	33°35'00.71"	RT	39°47'19.45"	144.00	43.45	84.40	6.41	6.00
	CC	2560698.1171	402594.3339									
	PT	117+63.55	2560688.8402	402738.0348								
	PC	123+53.86	2561277.9270	402776.0647								
EB-2	PI	125+03.70	2561427.4503	402785.7175	2°31'01.77"	RT	0°50'24.41"	6820.00		149.83	299.62	6.00
	CC	2561717.2937	395970.2321									
	PT	126+53.49	2561577.2533	402788.7942								
	PI	131+30.73	2562054.3967	402798.5938								
	POE	134+32.47	2562355.5843	402816.8219								

Skelly Dr		CARDINAL POINTS			CURVE DATA					SUPERELEVATION		
CURVE NO	STATION	X(EASTING) (FT)	Y(NORTHING) (FT)	DELTA	DEGREE	RADIUS (FT)	TANGENT (FT)	ARC LENGTH (FT)	EXTERNAL (FT)	DESIGN SPEED (MPH)	e _{max} (%)	S (%)
	PC	580+71.93	2556703.6749	401948.9991								
S-1	PI	582+33.00	2556816.4467	401833.9913	46°29'22.43"	LT	15°16'43.95"	375.00	161.07	304.27	33.13	35 6.00 -3.80
	CC	2556971.4299	402211.5486									
	PT	583+76.20	2556977.4979	401836.5977								
	PC	591+73.08	2557774.2762	401849.4922								
S-2	PI	593+00.61	2557901.7863	401851.5558	37°33'48.22"	LT	15°16'43.95"	375.00	127.53	245.85	21.09	35 6.00 -3.80
	CC	2557768.2083	402224.4431									
	PT	594+18.93	2558001.6028	401930.9266								
	PC	597+79.51	2558233.8315	402155.3454								
S-3	PI	598+68.64	2558353.5893	402210.8145	26°44'16.91"	RT	15°16'43.95"	375.00	89.12	175.00	10.45	35 6.00 3.80
	CC	2558517.2261	401861.8289									
	PT	599+54.51	2558440.8443	402228.9676								
	PC	604+64.68	2558940.3157	402332.8807								
S-4	PI	606+22.85	2559095.1732	402365.0982	6°14'32.04"	LT	1°58'30.64"	2900.79	158.17	316.03	4.31	35 6.00 NC
	CC	2558349.4690	405172.8590									
	PT	607+80.71	2559245.6095	402413.9627								
	PC	615+12.90	2559941.9839	402640.1580								
S-5	PI	617+65.55	2560182.2724	402718.2081	17°32'48.59"	RT	3°30'00.00"	1637.02	252.65	501.34	19.38	35 6.00 NC
	CC	2560447.7091	401083.2109									
	PT	620+14.24	2560434.9114	402720.1832								
	PC	620+14.73	2560435.4041	402720.1870								
S-6	PI	621+25.63	2560546.2994	402721.0540	75°12'06.73"	RT	39°47'19.45"	144.00	110.90	189.00	37.75	20 6.00 NC
	CC	2560436.5299	402576.1914									
	PT	622+03.74	2560575.4618	402614.0584								
	PC	622+72.59	2560593.5689	402547.6243								
S-7	PI	624+03.21	2560627.9178	402421.5998	78°27'19.43"	LT	35°48'35.50"	160.00				

SEC 26 T-19-N, R-12-E



CRL I-44 STA. 310+00.00
 BEGIN CONSTRUCTION

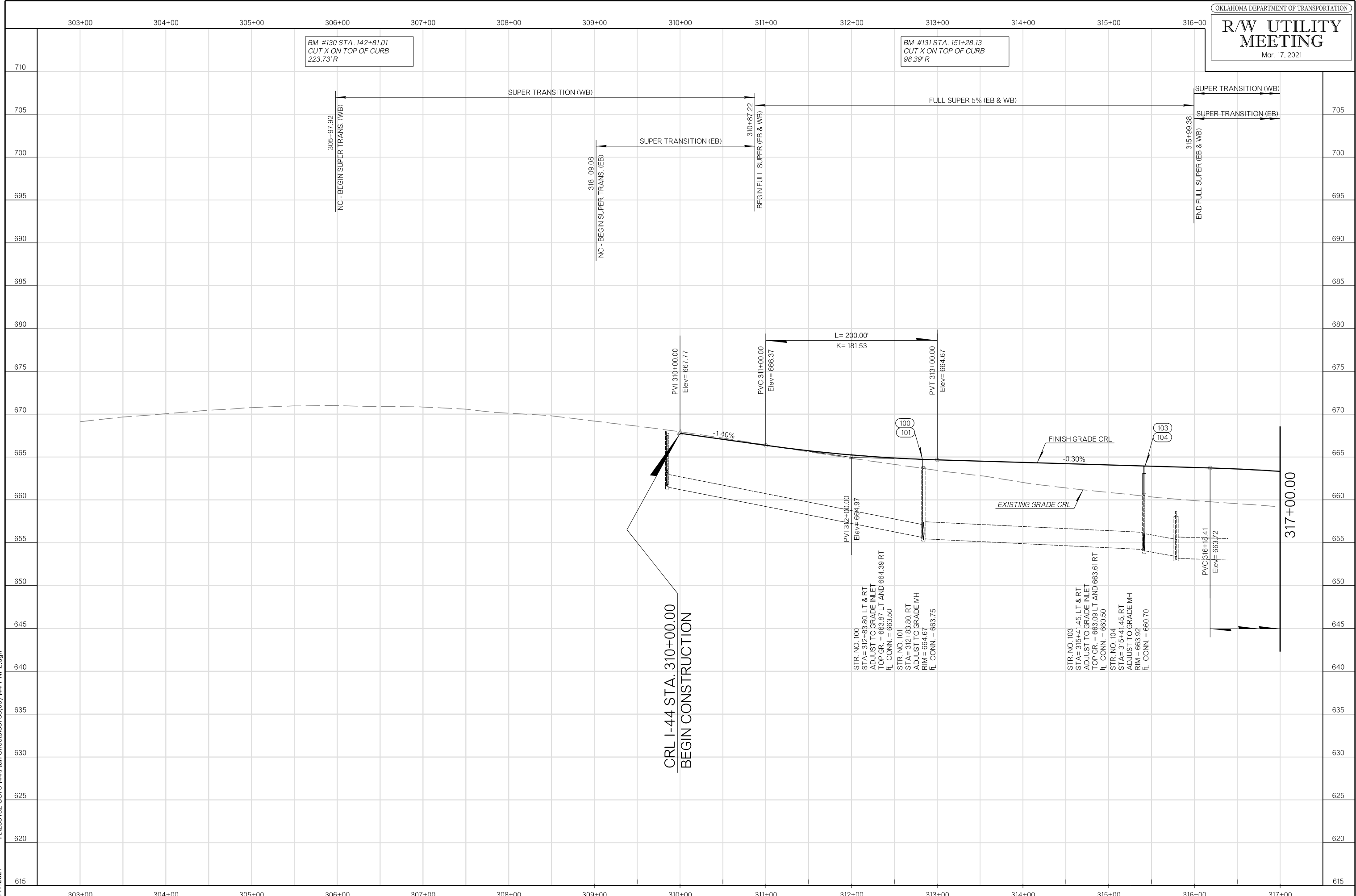
BM #130 STA. 142+81.01
 CUT X ON TOP OF CURB
 223.73' R

BM #131 STA. 151+28.13
 CUT X ON TOP OF CURB
 98.39' R

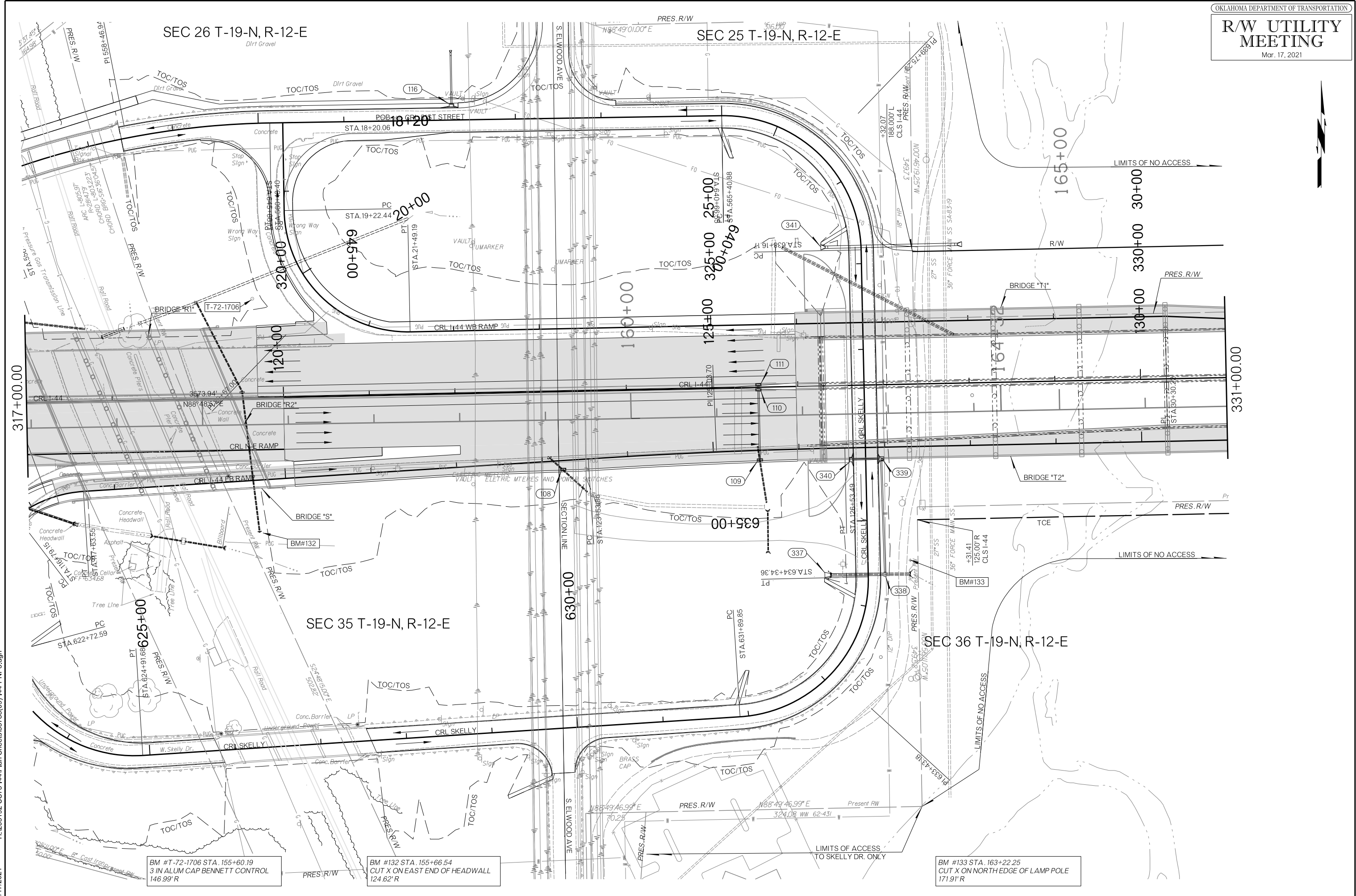
Bottling Group LLC.
 Judgment Entry
 Book 7258 Page 2463

- UTILITY CONTACTS:
- AT&T 918-596-4237
 - COX 405-417-4064
 - ONG 405-556-6401
 - PSO 918-599-2272
 - CIYT OF TULSA 918-596-9566
 - ONEOK 918-588-7431
 - PHILLIPS 918-977-5143
 - ENABLE 405-921-3582
 - ENBRIDGE 918-223-2054
 - LEVEL 3 918-519-9019

SEC 26 T-19-N, R-12-E



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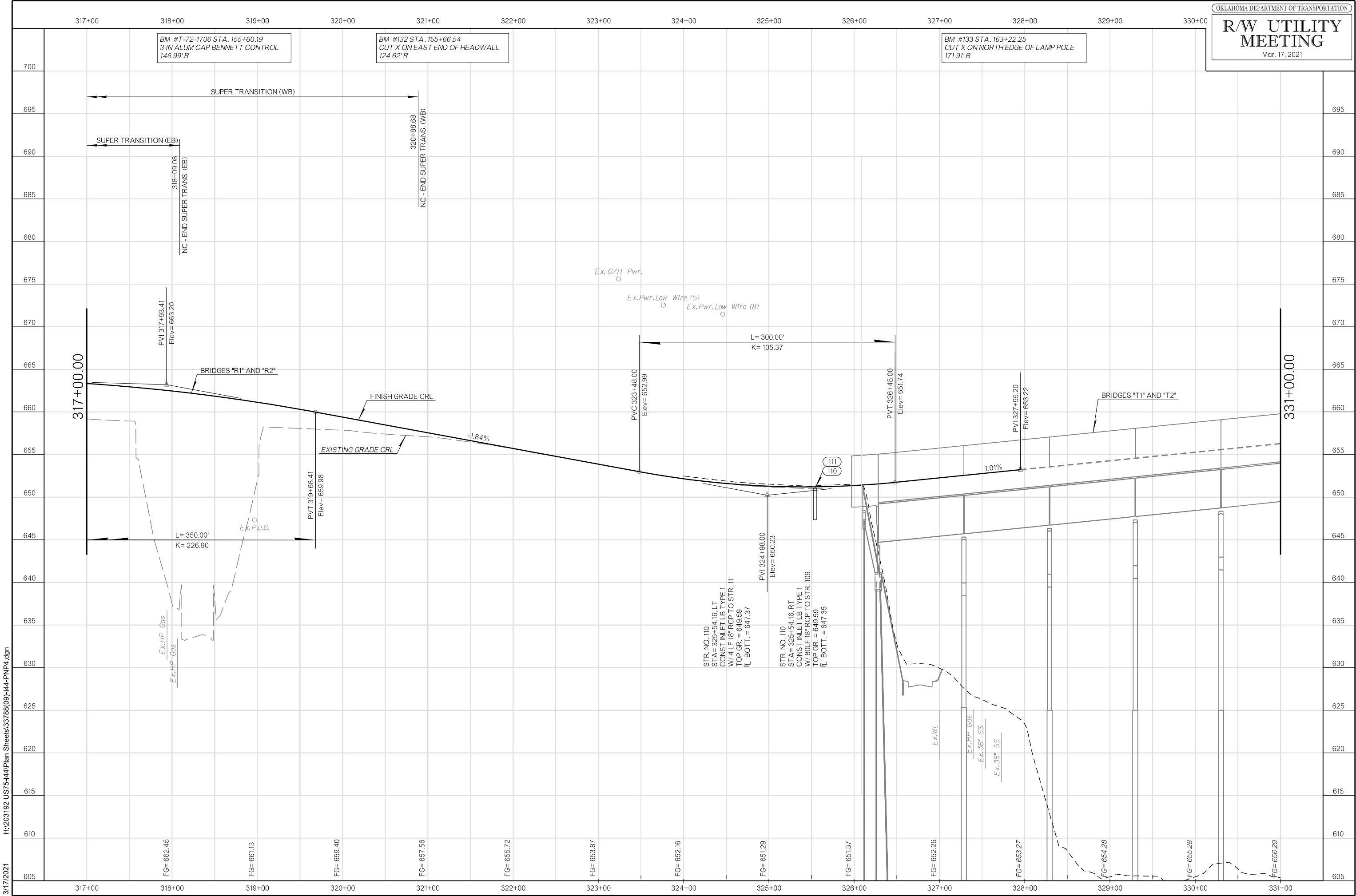


BM #72-1706 STA. 155+60.19
 3 IN ALUM CAP BENNETT CONTROL
 146.99'R

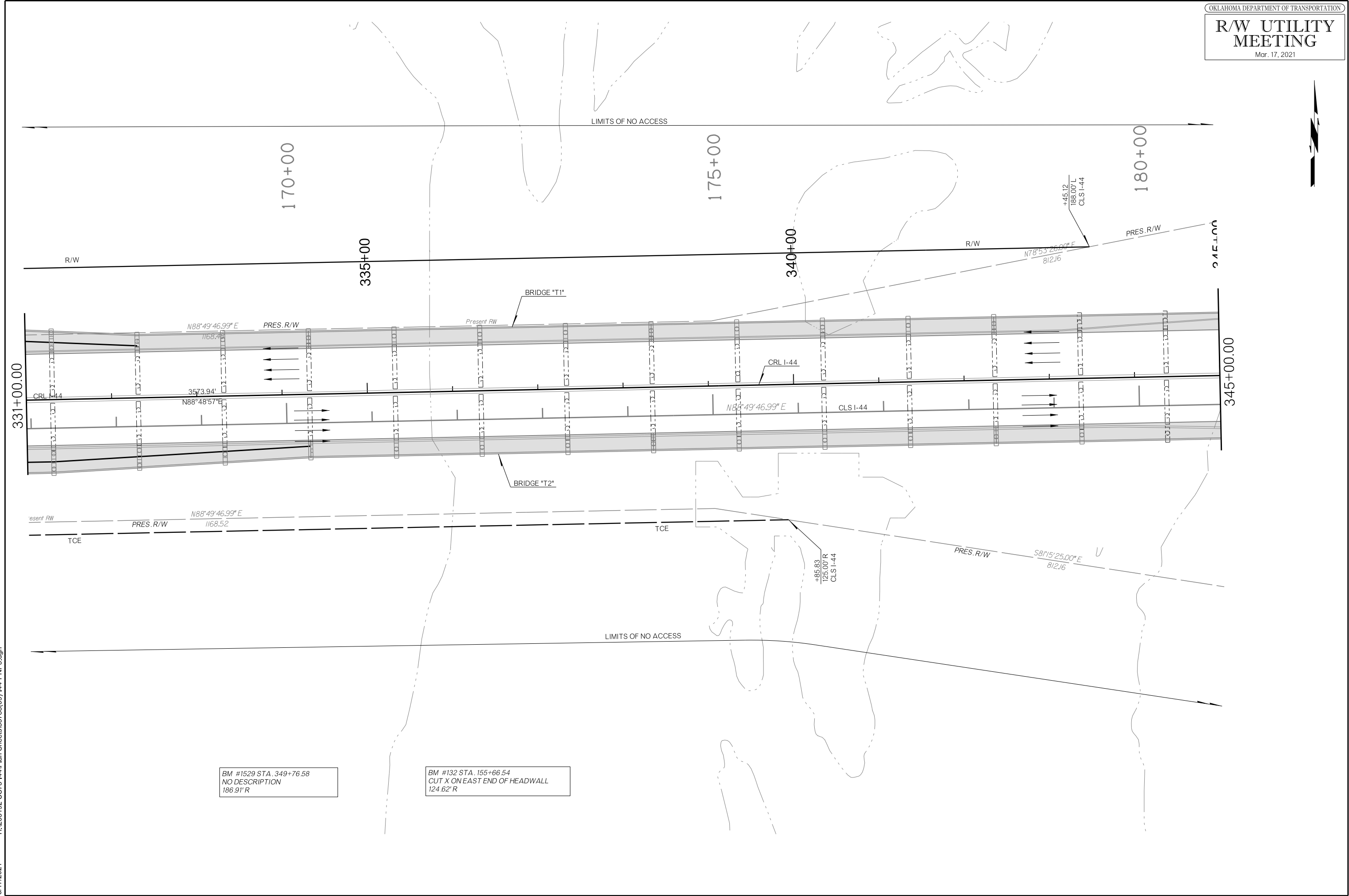
BM #132 STA. 155+66.54
 CUT X ON EAST END OF HEADWALL
 124.62'R

BM #133 STA. 163+22.25
 CUT X ON NORTH EDGE OF LAMP POLE
 171.91'R

H:\203192 US75-144 Plan Sheets\33788(09)144-PNP3.dgn
 3/17/2021



3/17/2021 H:\203192 US75-I44 Plan Sheets\3788(09)-I44-PNP4.dgn



BM #1529 STA. 349+76.58
 NO DESCRIPTION
 186.91' R

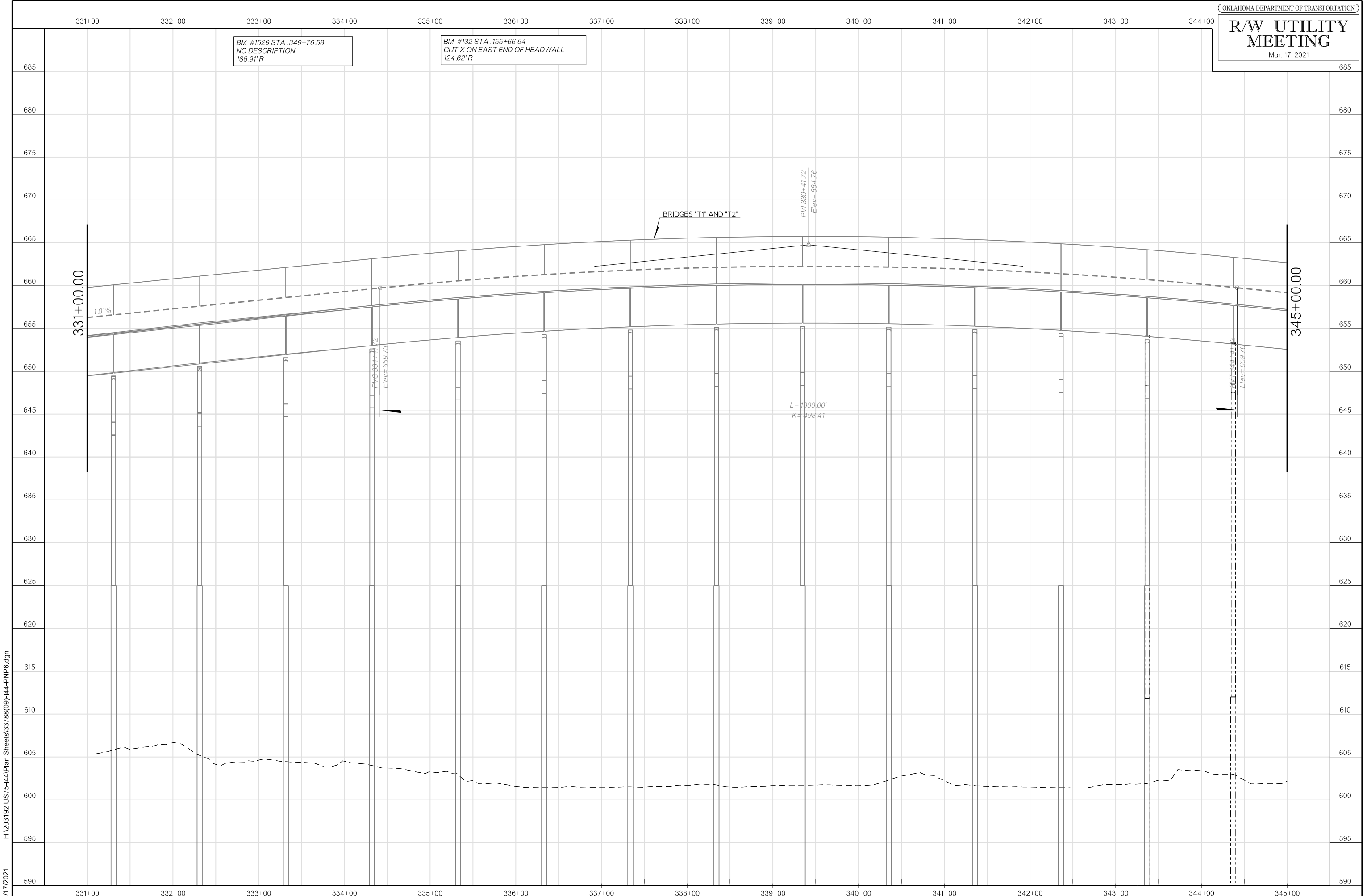
BM #132 STA. 155+66.54
 CUT X ON EAST END OF HEADWALL
 124.62' R

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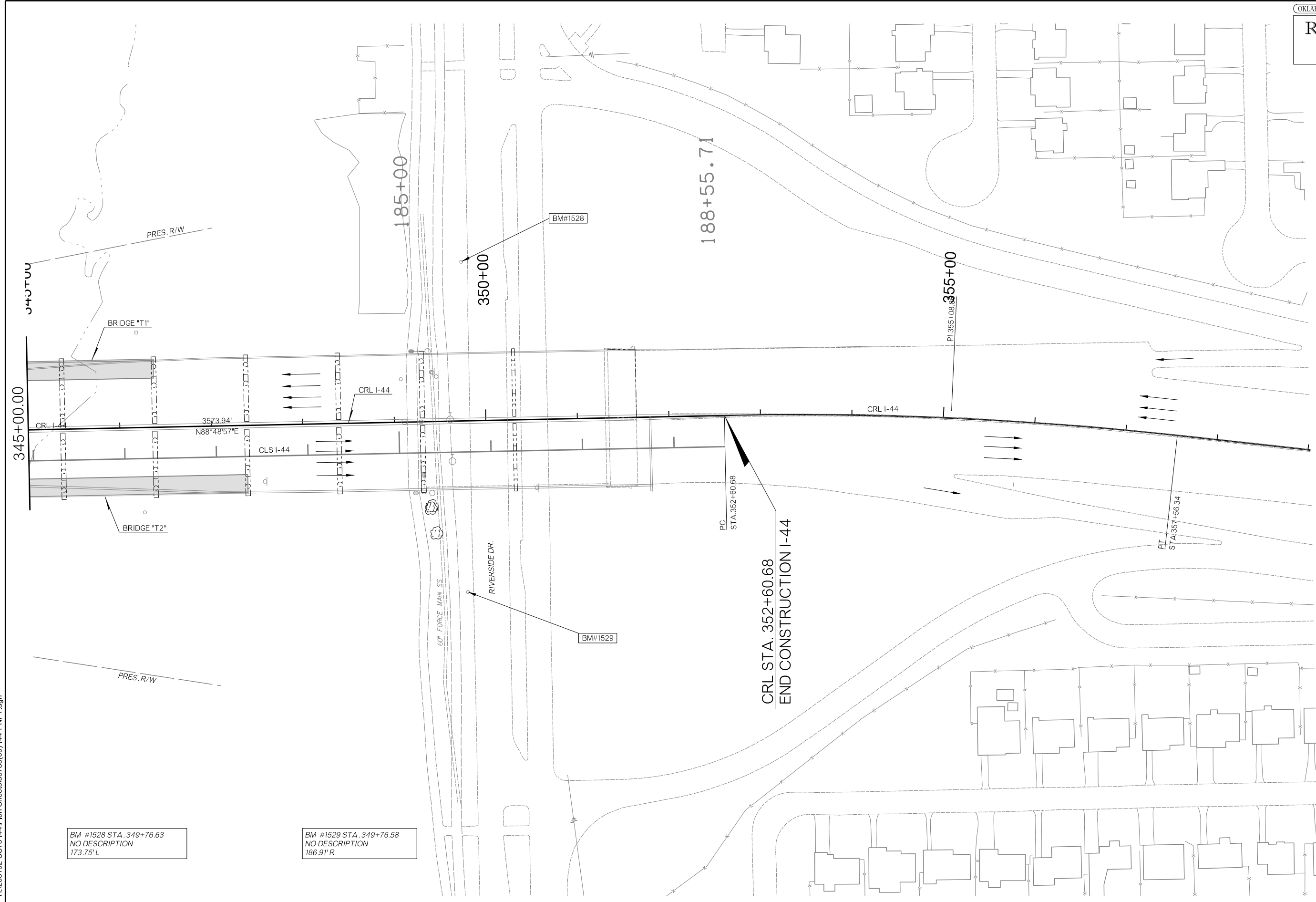
331+00 332+00 333+00 334+00 335+00 336+00 337+00 338+00 339+00 340+00 341+00 342+00 343+00 344+00

BM #1529 STA. 349+76.58
NO DESCRIPTION
186.91' R

BM #132 STA. 155+66.54
CUT X ON EAST END OF HEADWALL
124.62' R



3/17/2021 H:\203192 US75-I44 Plan Sheets\3788(09)-I44-PNP6.dgn



**CRL STA. 352+60.68
 END CONSTRUCTION I-44**

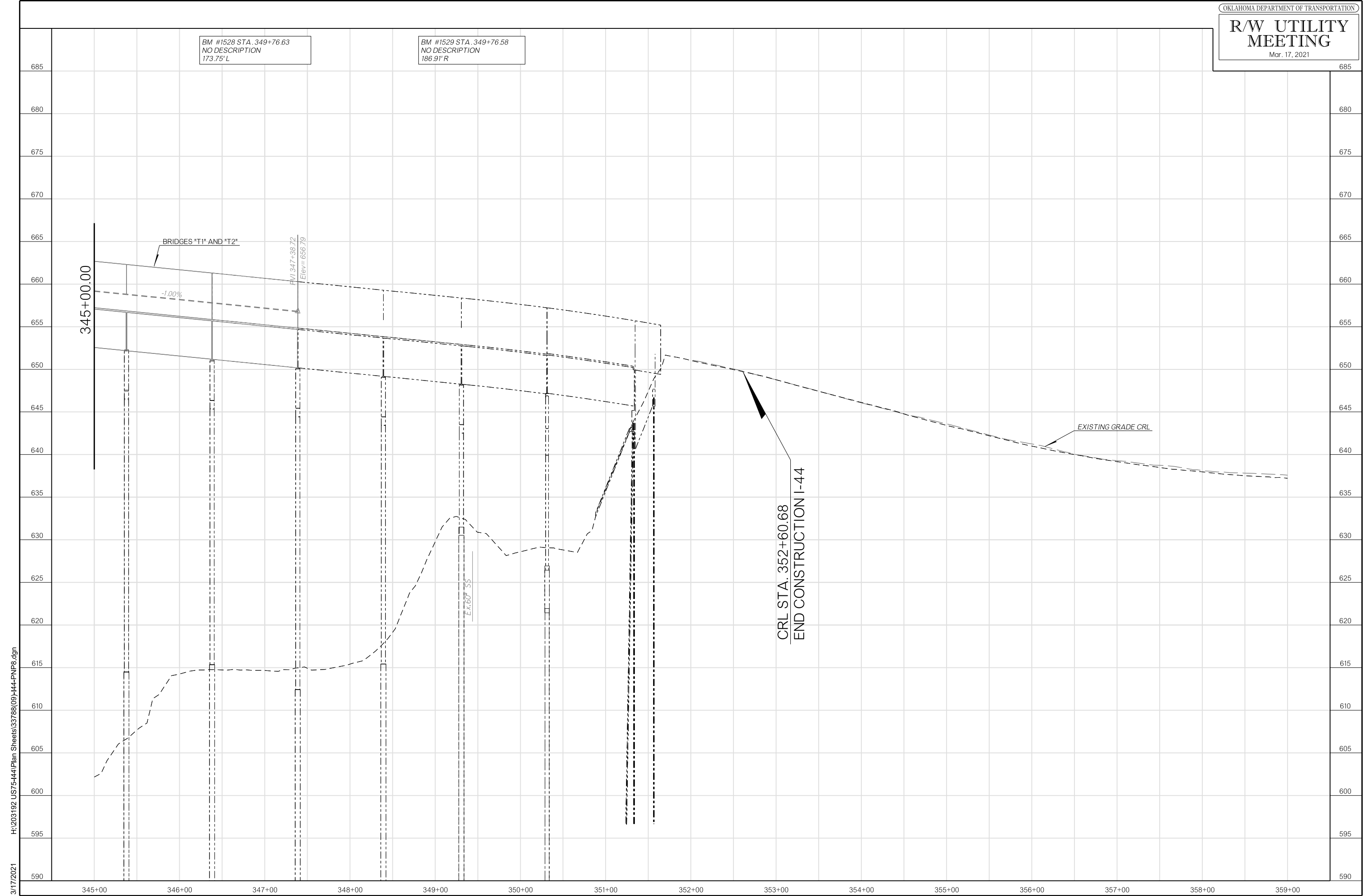
BM #1528 STA. 349+76.63
 NO DESCRIPTION
 173.75' L

BM #1529 STA. 349+76.58
 NO DESCRIPTION
 186.91' R

3/17/2021 H:\203192 US75-144\Plan Sheets\33788(09)-144-PNP7.dgn

BM #1528 STA. 349+76.63
 NO DESCRIPTION
 173.75' L

BM #1529 STA. 349+76.58
 NO DESCRIPTION
 186.91' R



3/17/2021 H:\203192 US75-I44\Plan Sheets\3788(09)-I44-PNP8.dgn

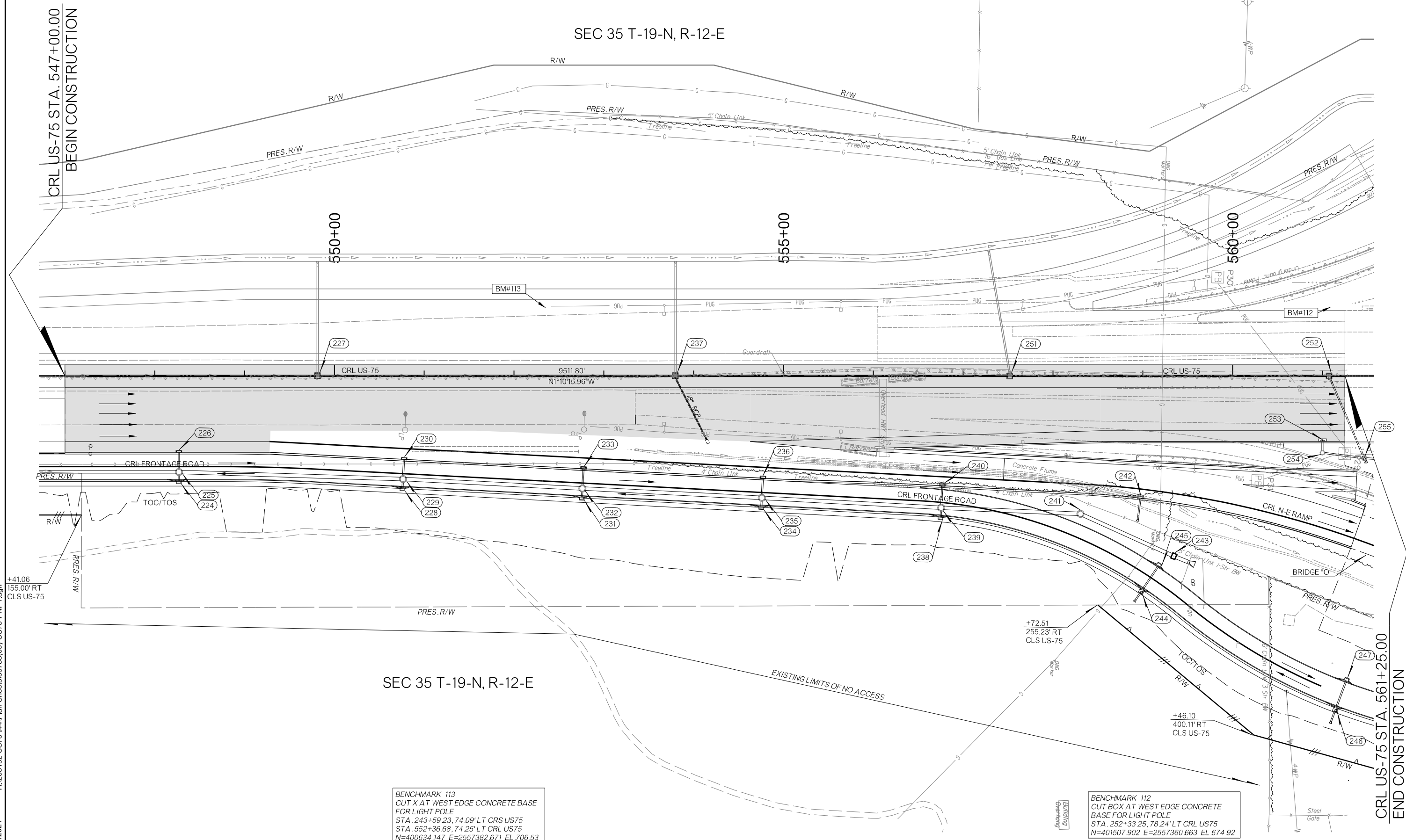


SEC 35 T-19-N, R-12-E

CRL US-75 STA. 547+00.00
 BEGIN CONSTRUCTION

CRL US-75 STA. 561+25.00
 END CONSTRUCTION

SEC 35 T-19-N, R-12-E



BENCHMARK 113
 CUT X AT WEST EDGE CONCRETE BASE
 FOR LIGHT POLE
 STA. 243+59.23, 74.09' LT CRS US75
 STA. 552+36.68, 74.25' LT CRL US75
 N=400634.147 E=2557382.671 EL.706.53

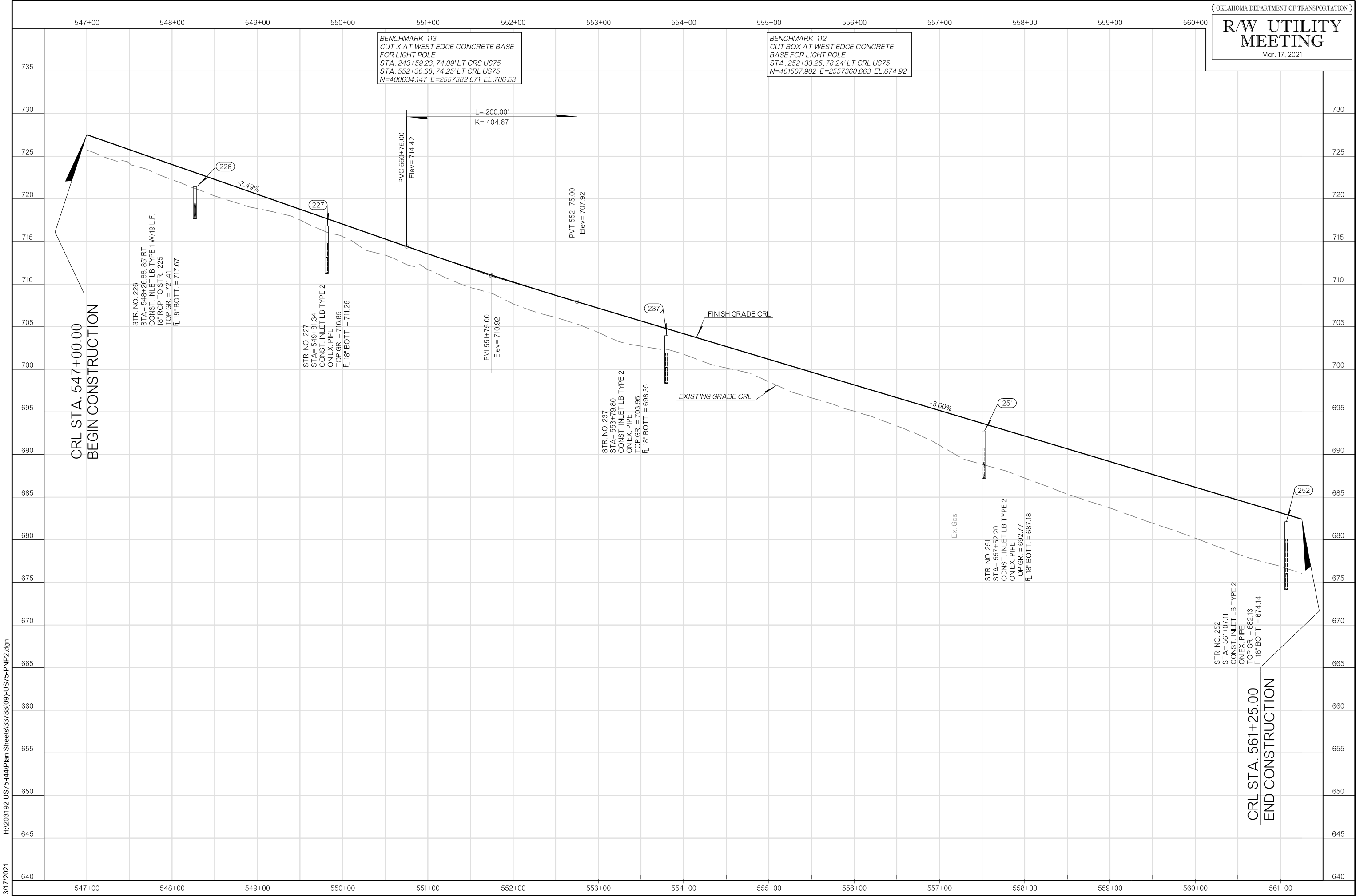
BENCHMARK 112
 CUT BOX AT WEST EDGE CONCRETE
 BASE FOR LIGHT POLE
 STA. 252+33.25, 78.24' LT CRL US75
 N=401507.902 E=2557360.663 EL.674.92

+41.06
 155.00' RT
 CLS US-75

+72.51
 255.23' RT
 CLS US-75

+46.10
 400.11' RT
 CLS US-75

3/17/2021 H:\203192 US75-I44 Plan Sheets\3788(09)-US75-PNP1.dgn



H:\203192 US75-I44 Plan Sheets\33788(09)-US75-PNP2.dgn

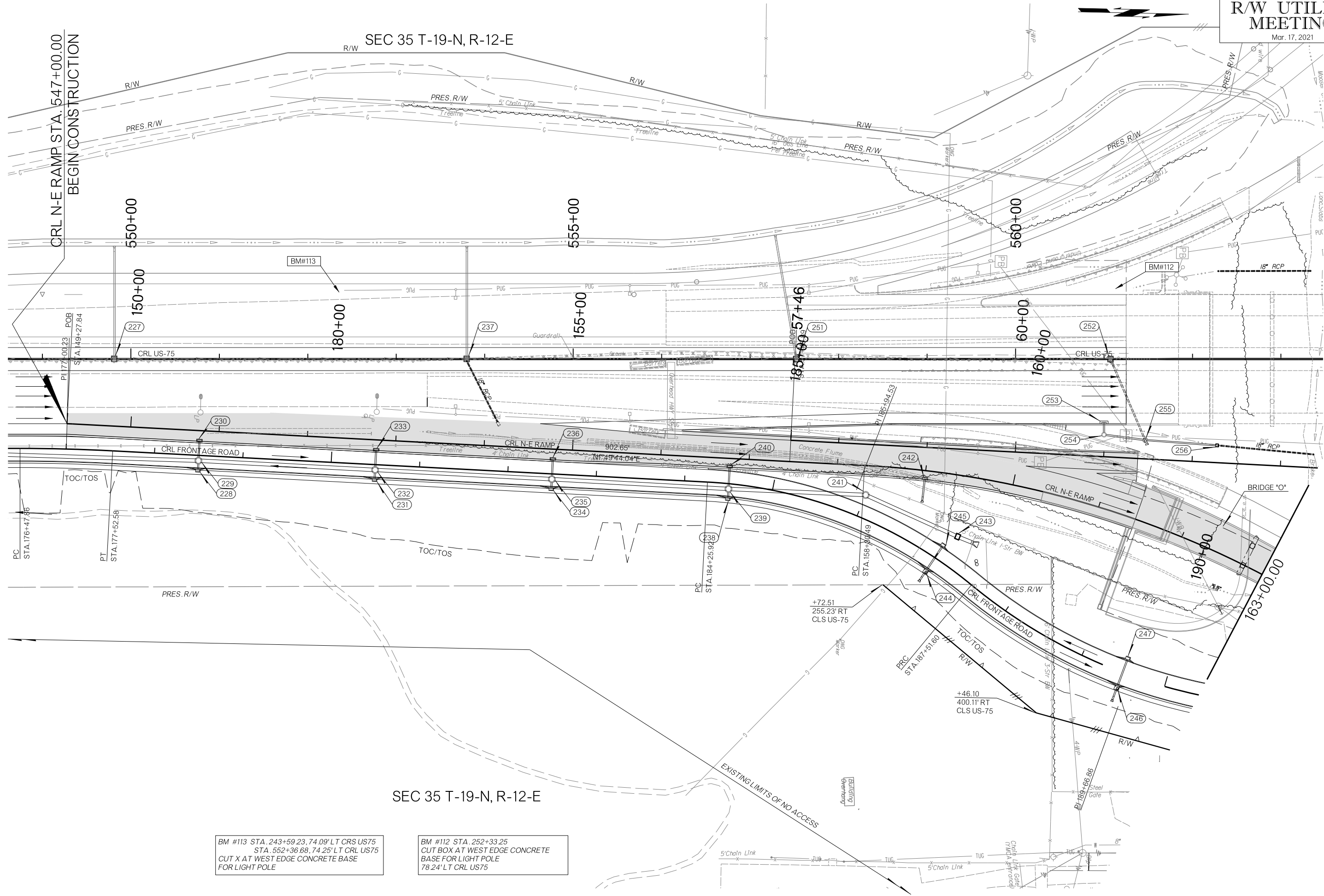
3/17/2021



SEC 35 T-19-N, R-12-E

SEC 35 T-19-N, R-12-E

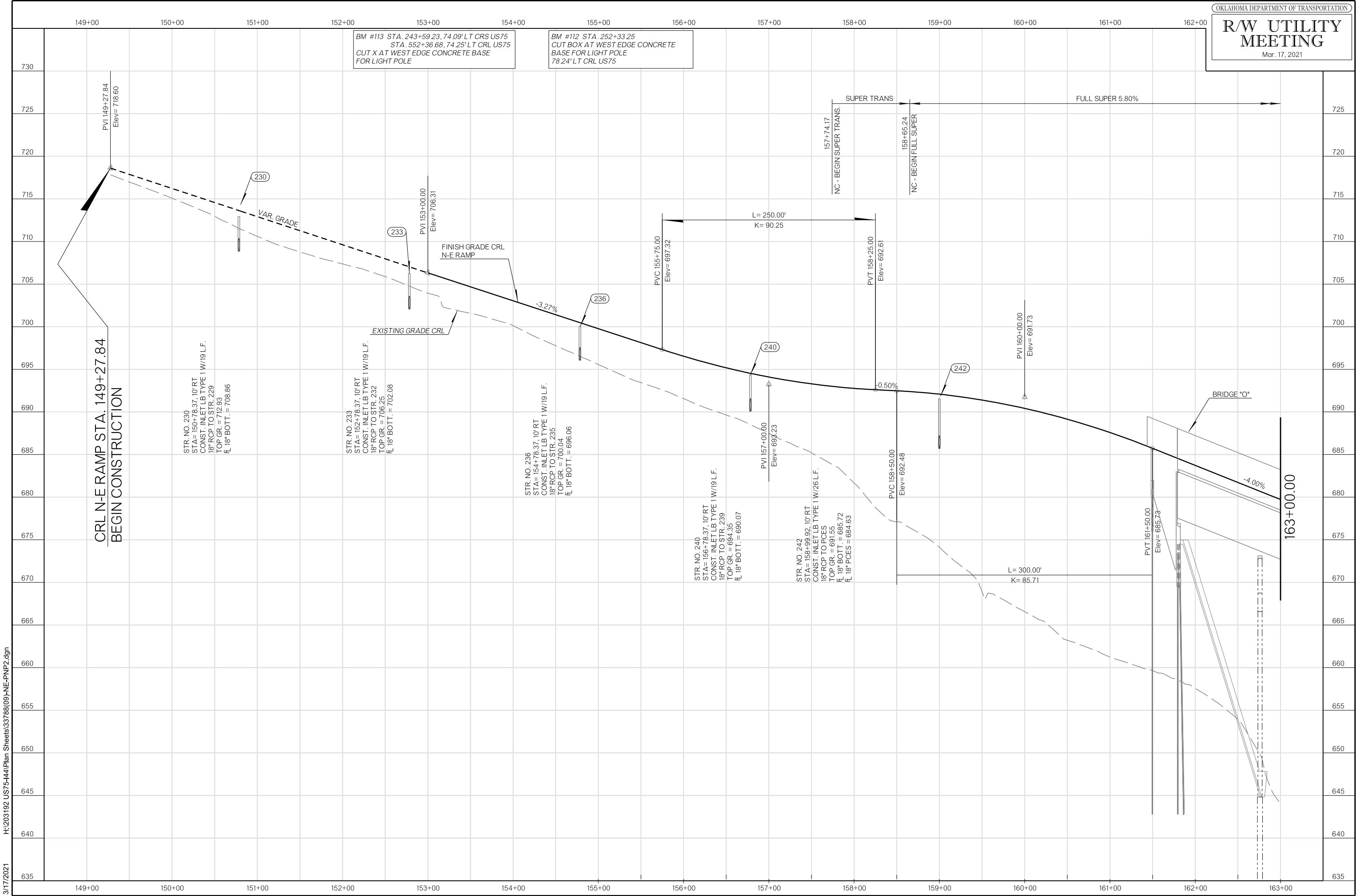
CRL N-E RAMP STA. 547+00.00
BEGIN CONSTRUCTION



BM #113 STA. 243+59.23, 74.09' LT CRS US75
STA. 552+36.68, 74.25' LT CRL US75
CUT X AT WEST EDGE CONCRETE BASE
FOR LIGHT POLE

BM #112 STA. 252+33.25
CUT BOX AT WEST EDGE CONCRETE
BASE FOR LIGHT POLE
78.24' LT CRL US75

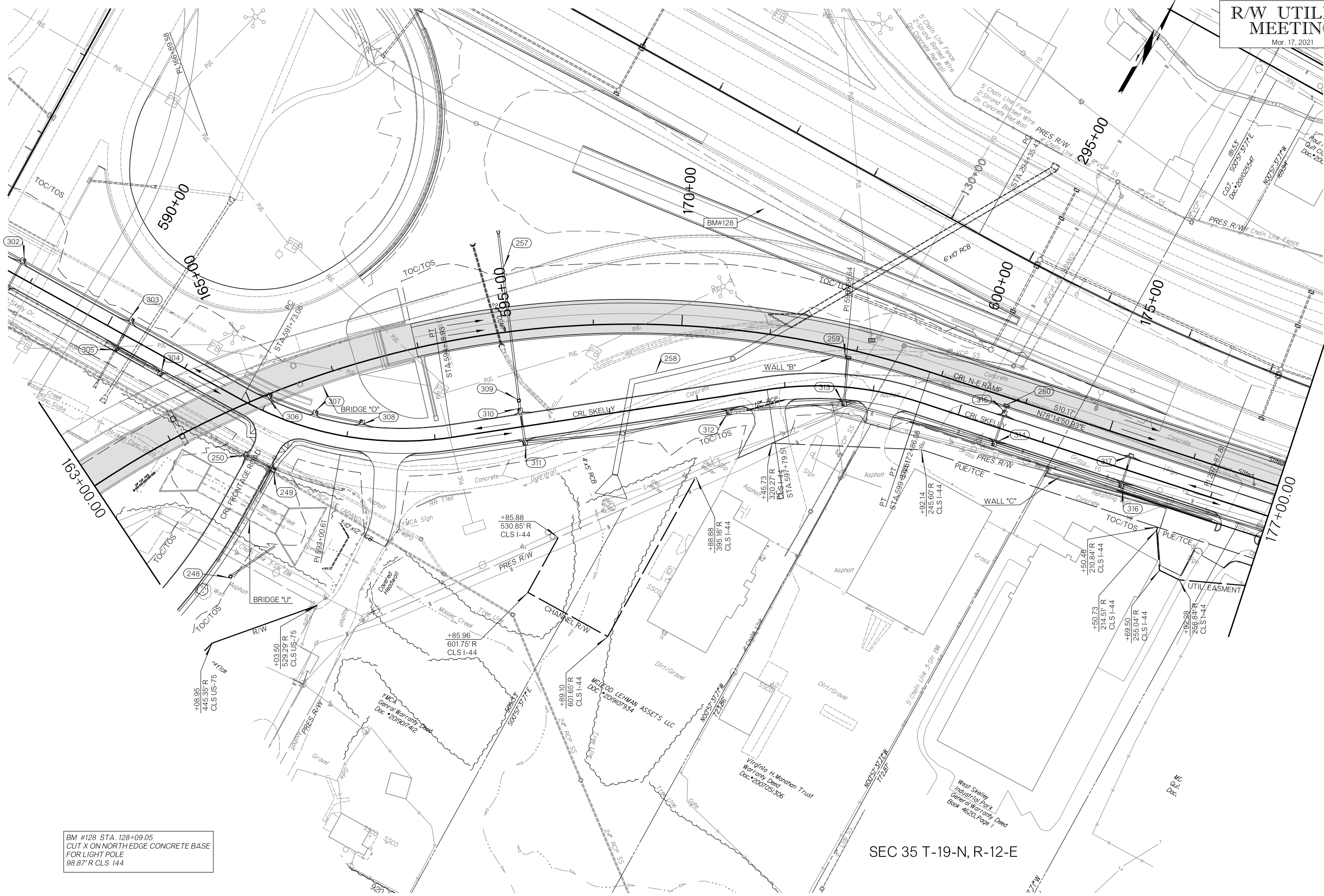
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R/W UTILITY MEETING

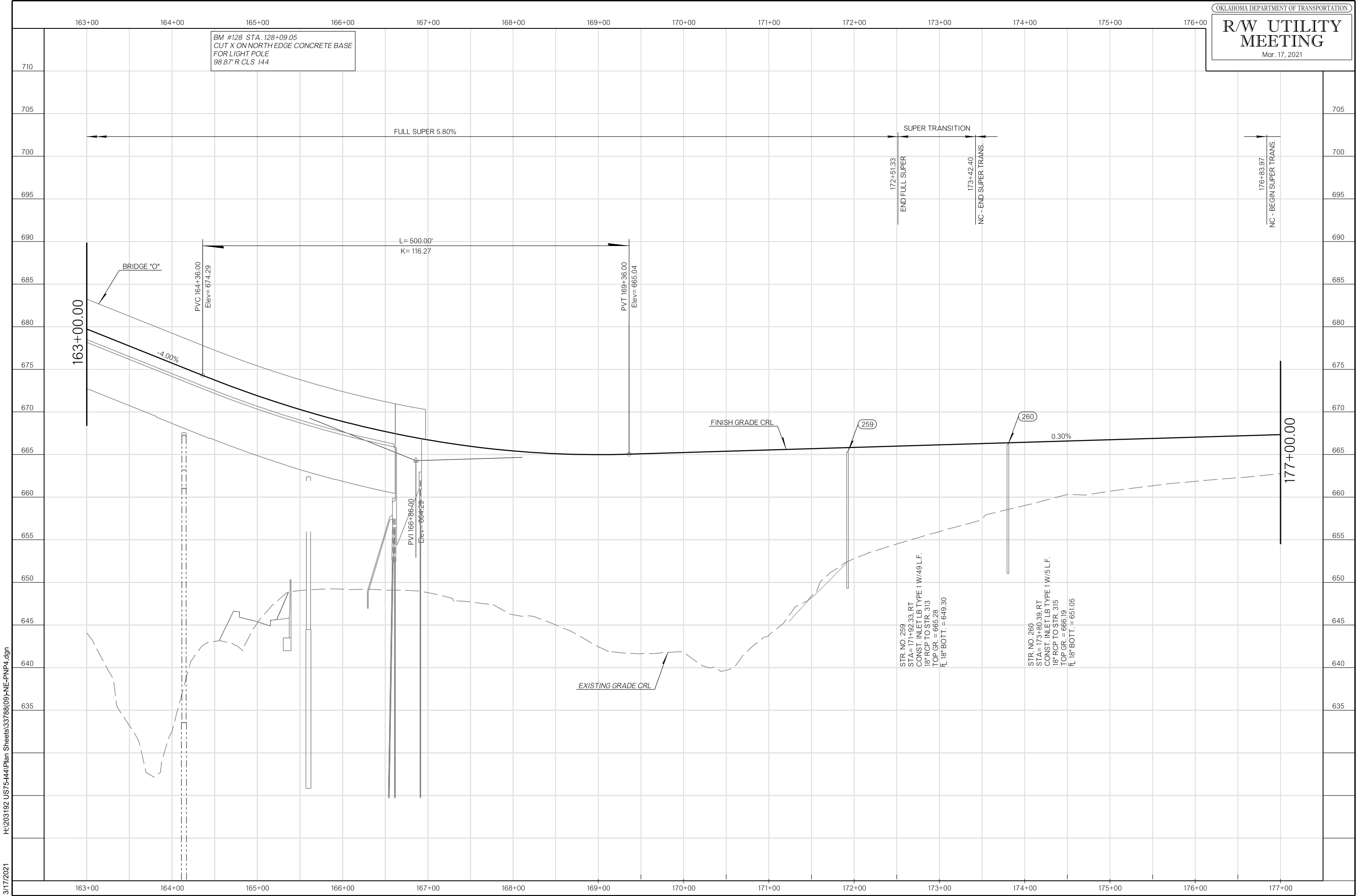
Mar. 17, 2021



BM #128 STA. 128+09.05
 CUT X ON NORTH EDGE CONCRETE BASE
 FOR LIGHT POLE
 98.87' R CLS 144

SEC 35 T-19-N, R-12-E

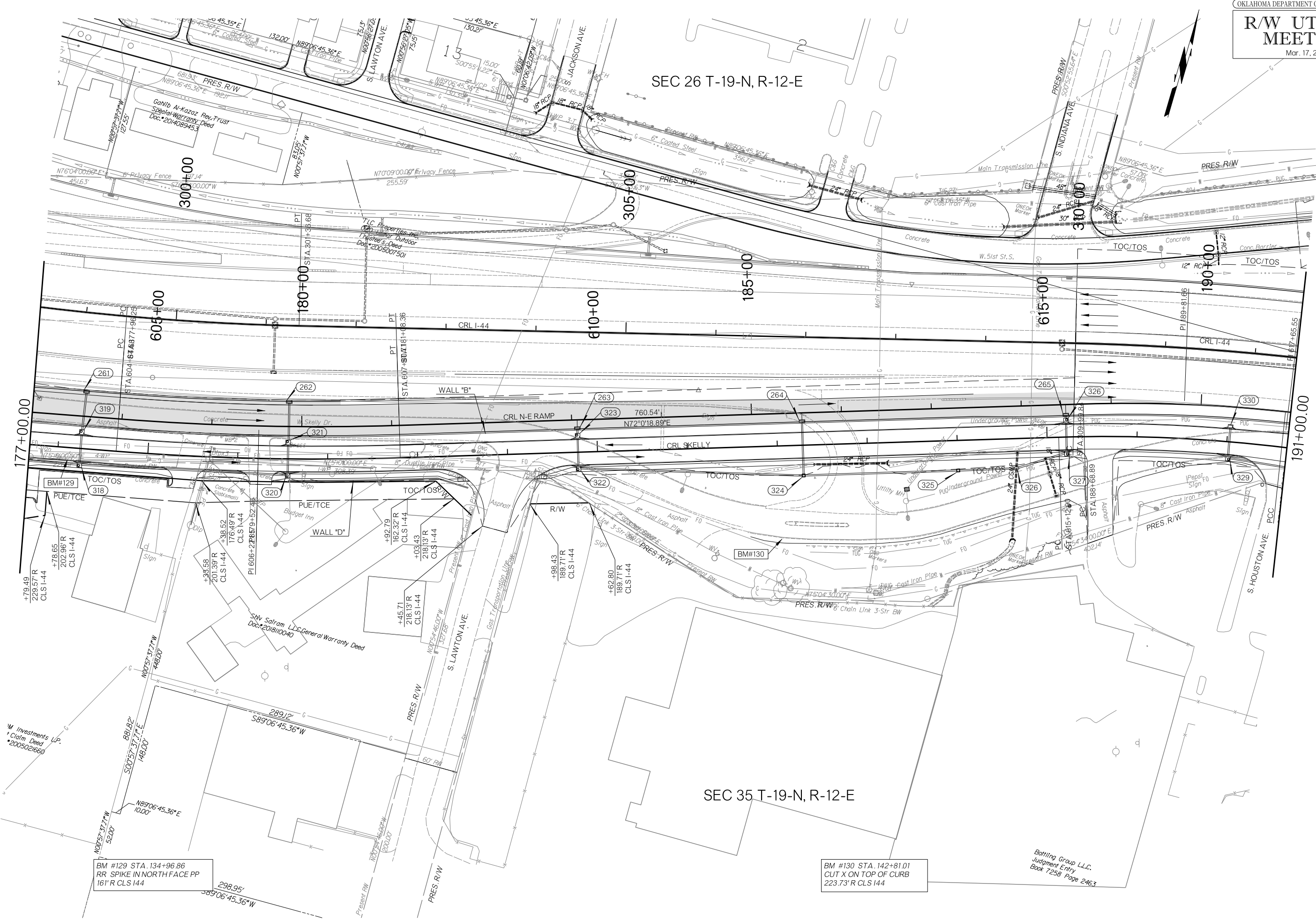
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3/17/2021 H:\203192 US75-144\Plan Sheets\33788(09)-NE-PNP4.dgn

SEC 26 T-19-N, R-12-E

SEC 35 T-19-N, R-12-E

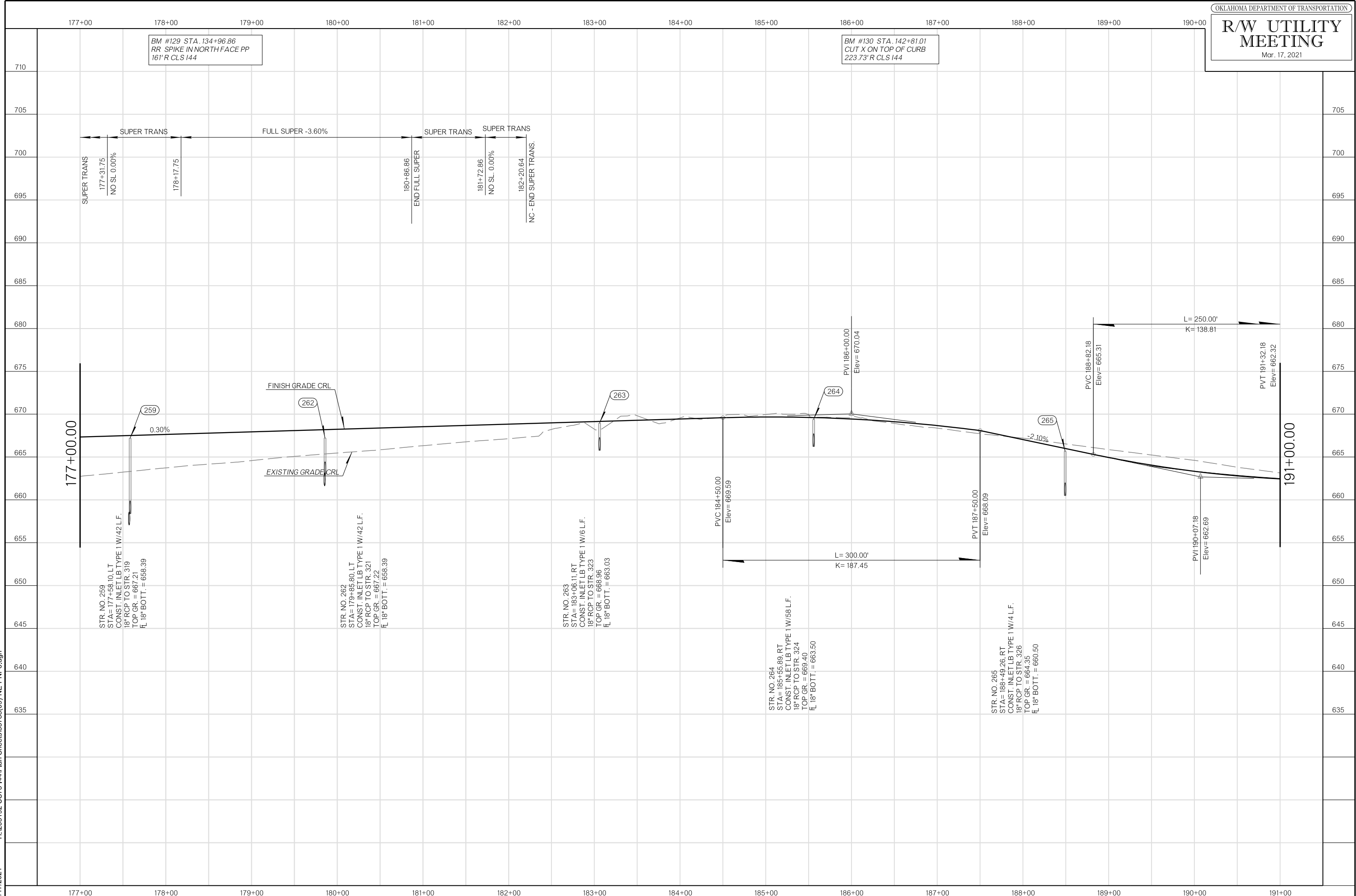


BM #129 STA. 134+96.86
 RR SPIKE IN NORTH FACE PP
 161' R CLS I44

BM #130 STA. 142+81.01
 CUT X ON TOP OF CURB
 223.73' R CLS I44

Boiling Group LLC.
 Judgment Entry
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BM #129 STA. 134+96.86
 RR SPIKE IN NORTH FACE PP
 161' R CLS 144

BM #130 STA. 142+81.01
 CUT X ON TOP OF CURB
 223.73' R CLS 144

SUPER TRANS 177+31.75 NO SL 0.00%
 FULL SUPER -3.60%
 SUPER TRANS 180+86.86 END FULL SUPER
 SUPER TRANS 181+72.86 NO SL 0.00%
 SUPER TRANS 182+20.64 NC - END SUPER TRANS.

STR. NO. 259
 STA=177+58.10, LT
 CONST. INLET LB TYPE 1 W/42 L.F.
 18" RCP TO STR. 319
 TOP GR. = 667.21
 18" BOT. = 658.39

STR. NO. 262
 STA=179+85.80, LT
 CONST. INLET LB TYPE 1 W/42 L.F.
 18" RCP TO STR. 321
 TOP GR. = 667.22
 18" BOT. = 658.39

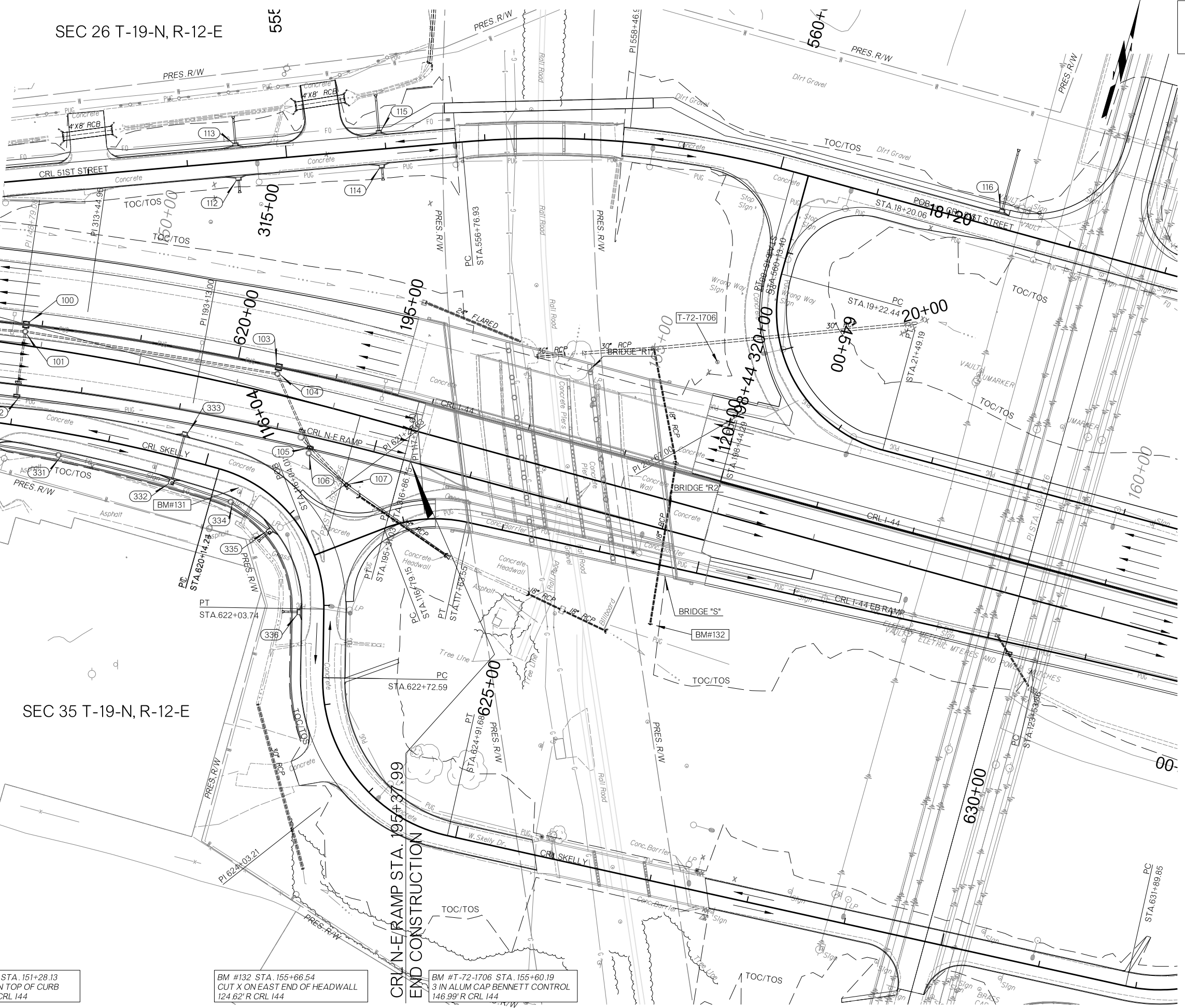
STR. NO. 263
 STA=183+06.11, RT
 CONST. INLET LB TYPE 1 W/6 L.F.
 18" RCP TO STR. 323
 TOP GR. = 668.96
 18" BOT. = 663.03

STR. NO. 264
 STA=185+55.89, RT
 CONST. INLET LB TYPE 1 W/58 L.F.
 18" RCP TO STR. 324
 TOP GR. = 669.40
 18" BOT. = 663.50

STR. NO. 265
 STA=188+49.26, RT
 CONST. INLET LB TYPE 1 W/4 L.F.
 18" RCP TO STR. 326
 TOP GR. = 664.35
 18" BOT. = 660.50

3/17/2021 H:\203192 US75-144\Plan Sheets\3788(09)-NE-PNP6.dgn

SEC 26 T-19-N, R-12-E 555



SEC 35 T-19-N, R-12-E

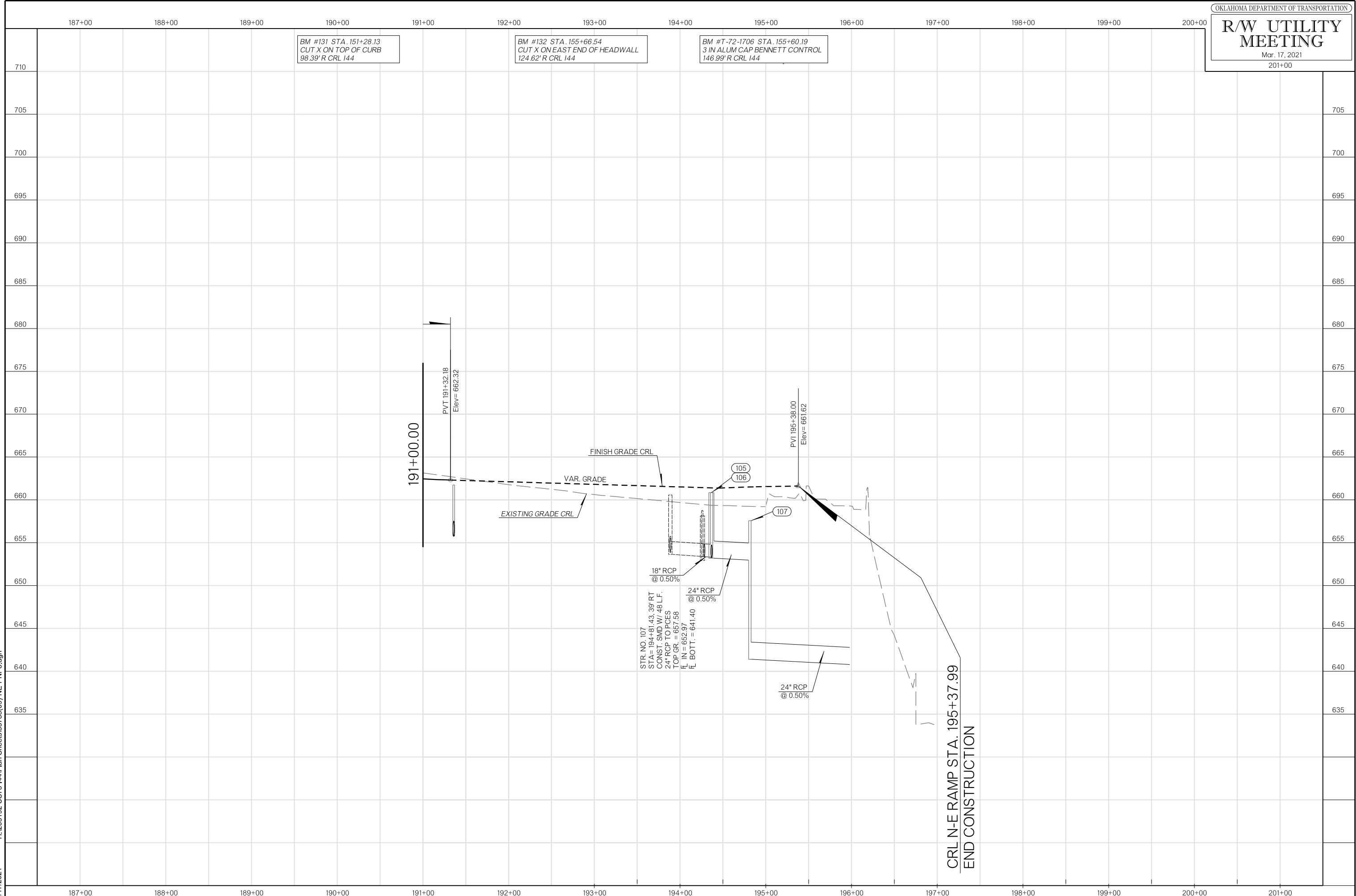
BM #131 STA. 151+28.13
 CUT X ON TOP OF CURB
 98.39' R CRL 144

BM #132 STA. 155+66.54
 CUT X ON EAST END OF HEADWALL
 124.62' R CRL 144

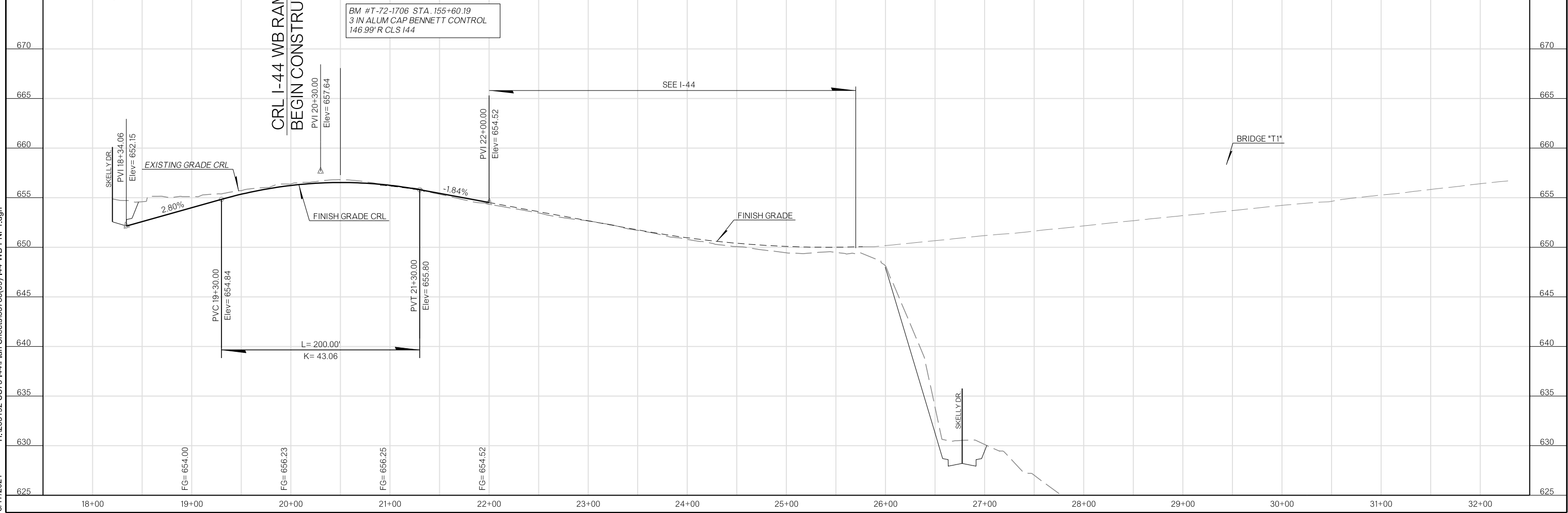
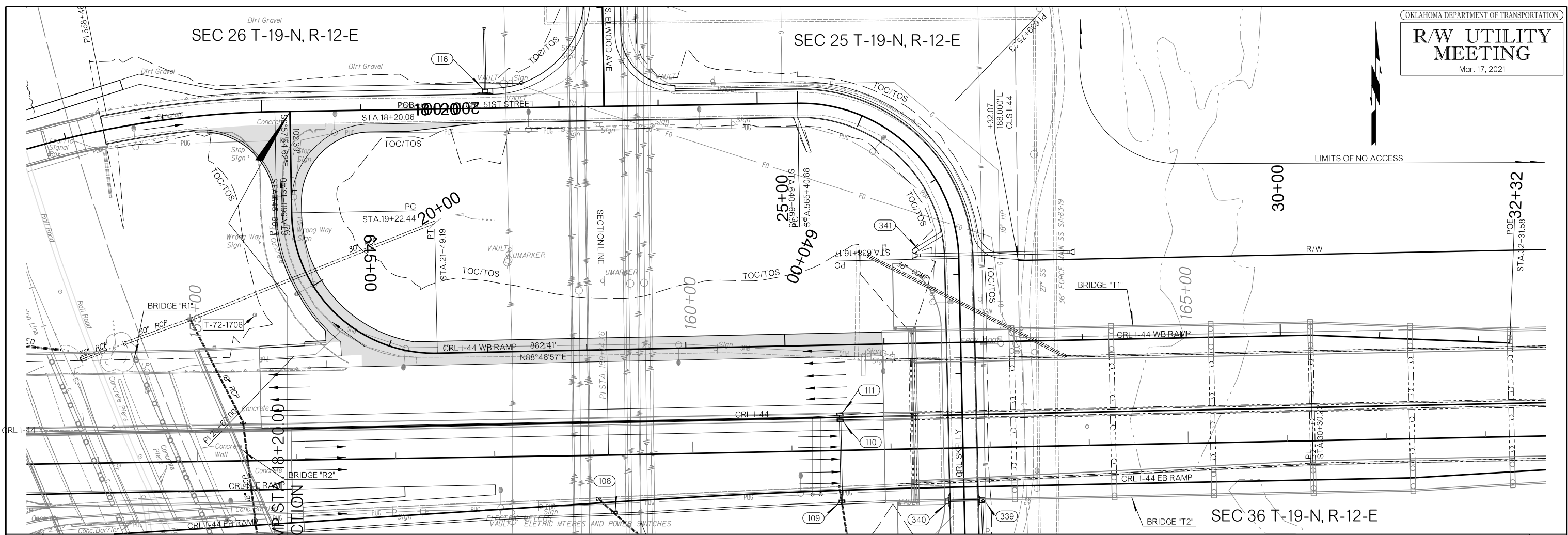
BM #T-72-1706 STA. 155+60.19
 3 IN ALUM CAP BENNETT CONTROL
 146.99' R CRL 144

**CRL N-E RAMP STA. 195+37.99
 END CONSTRUCTION**

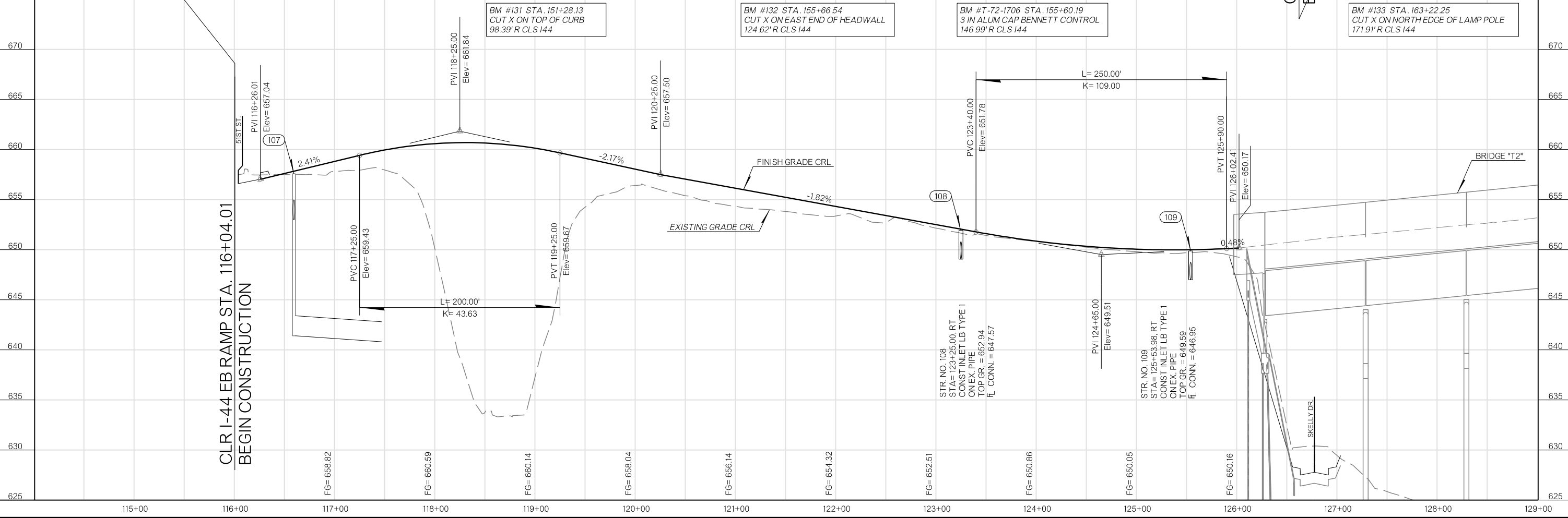
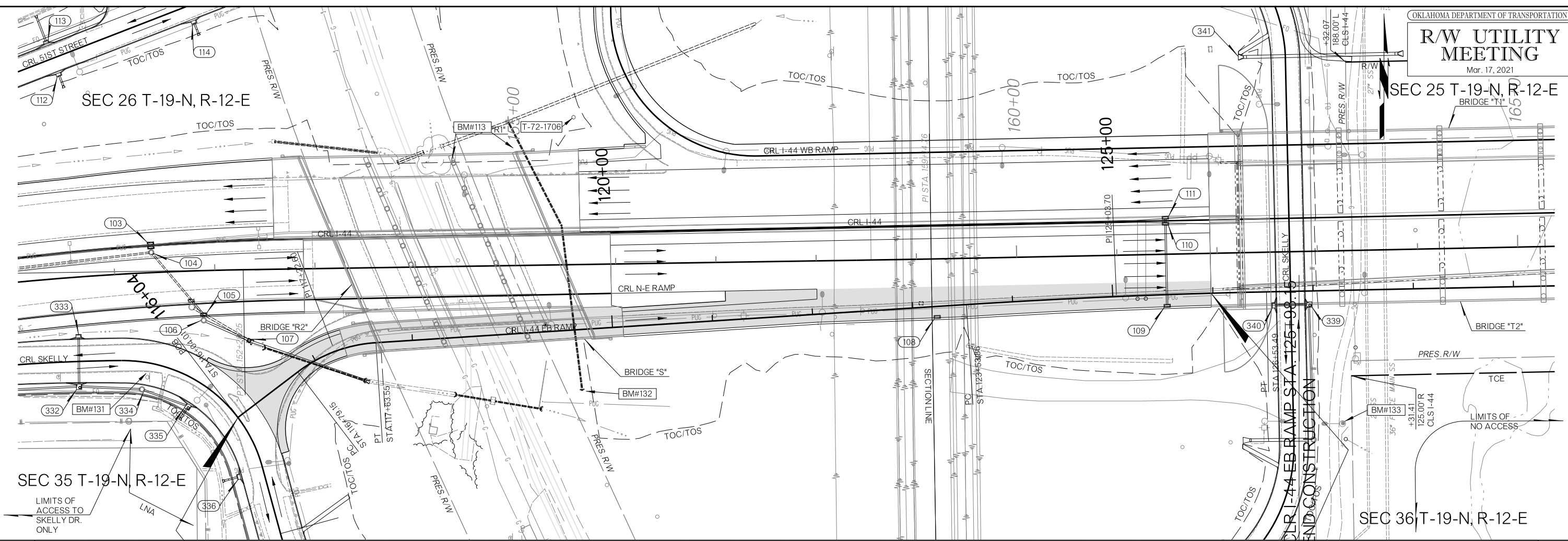
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3/17/2021 H:\203192 US75-I44\Plan Sheets\3788(09)-NE-PNP8.dgn



3/17/2021 H:\203192 US75-144 Plan Sheets\33788(09)144-WB-PNP1.dgn



CLR I-44 EB RAMP STA. 116+04.01
BEGIN CONSTRUCTION

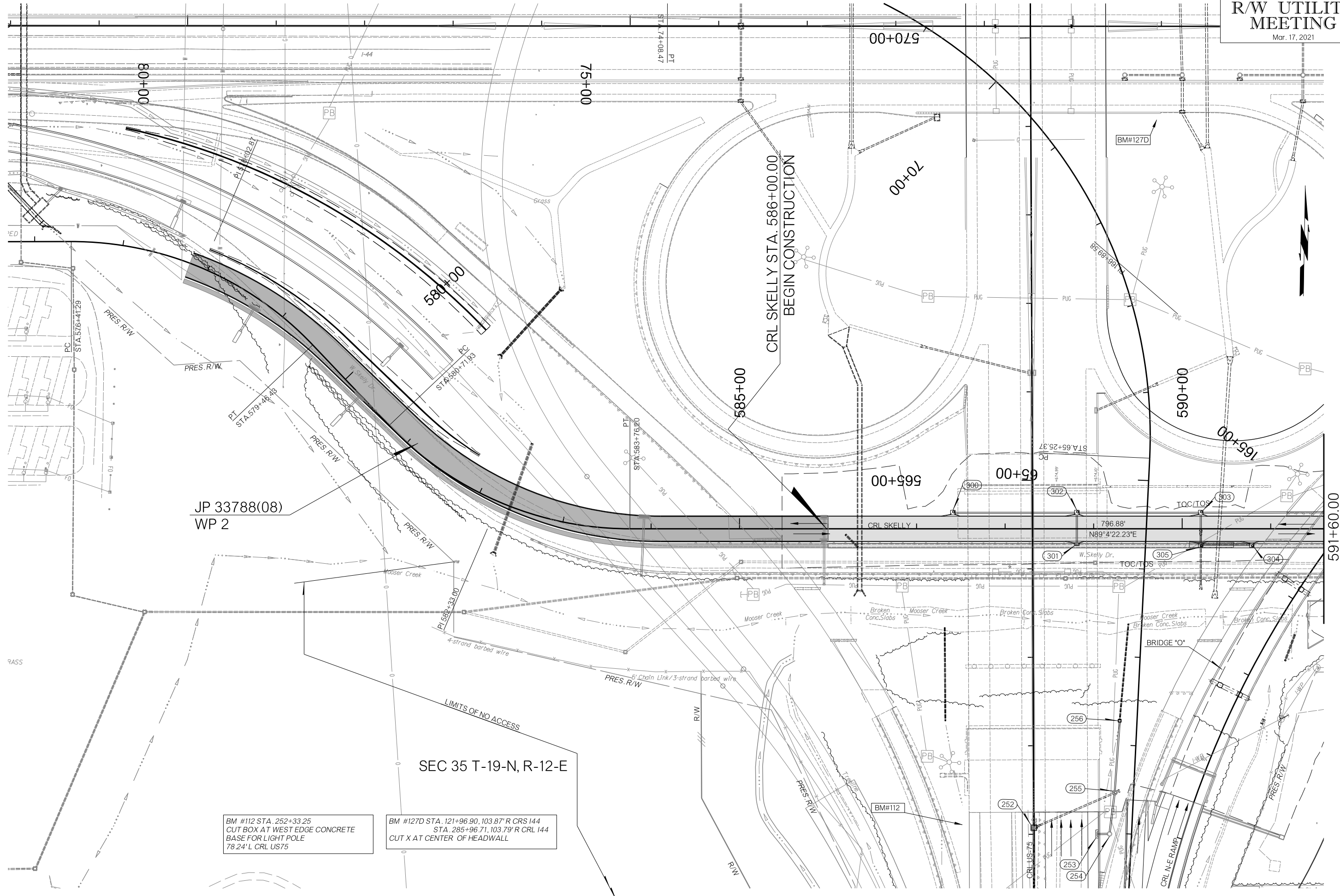
CLR I-44 EB RAMP STA. 125+00.50
END CONSTRUCTION

BM #131 STA. 151+28.13
CUT X ON TOP OF CURB
98.39' R CLS 144

BM #132 STA. 155+66.54
CUT X ON EAST END OF HEADWALL
124.62' R CLS 144

BM #T-72-1706 STA. 155+60.19
3 IN ALUM CAP BENNETT CONTROL
146.99' R CLS 144

BM #133 STA. 163+22.25
CUT X ON NORTH EDGE OF LAMP POLE
171.91' R CLS 144



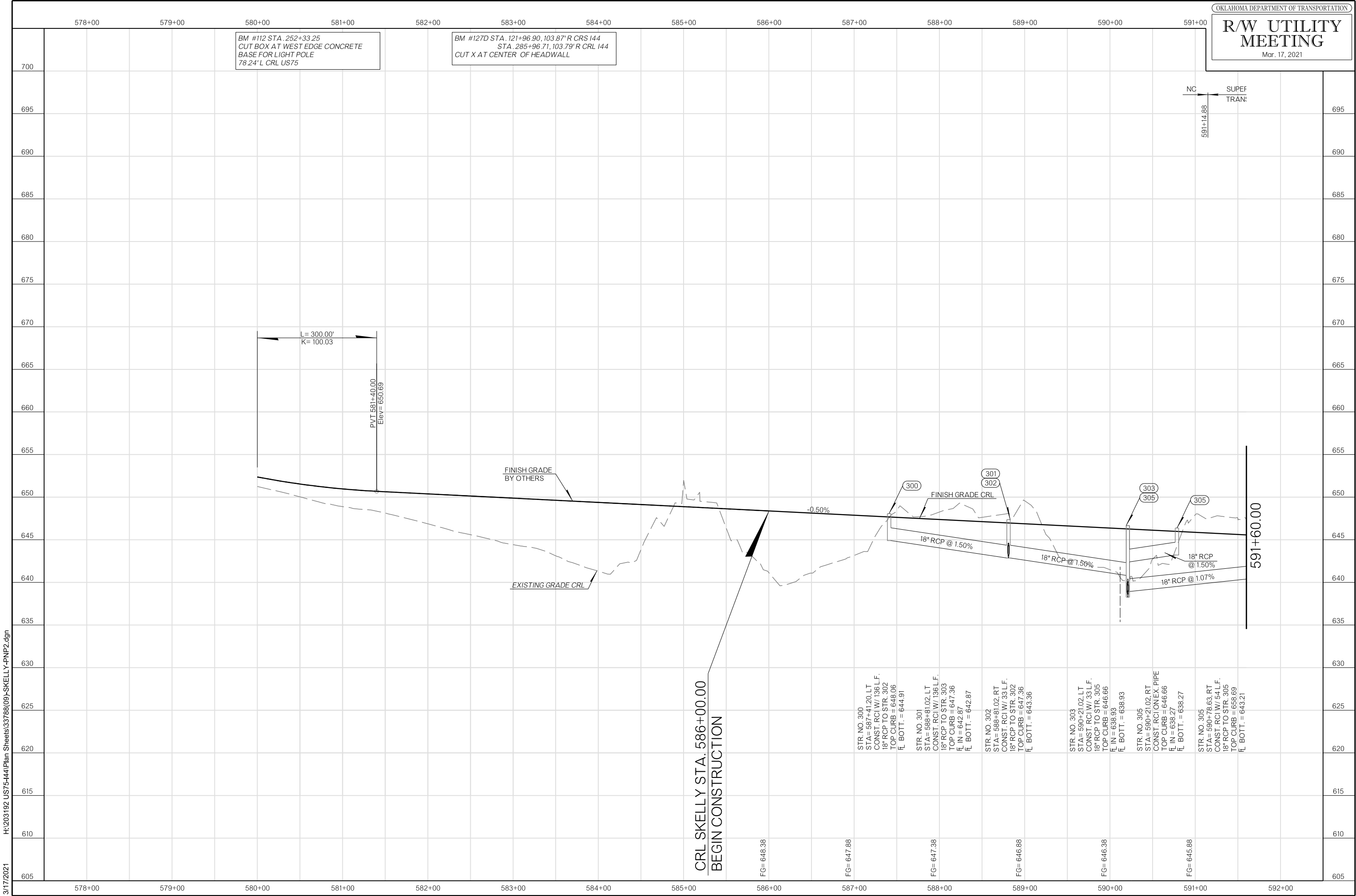
JP 33788(08)
 WP 2

SEC 35 T-19-N, R-12-E

BM #112 STA. 252+33.25
 CUT BOX AT WEST EDGE CONCRETE
 BASE FOR LIGHT POLE
 78.24' L CRL US75

BM #127D STA. 121+96.90, 103.87' R CRS I44
 STA. 285+96.71, 103.79' R CRL I44
 CUT X AT CENTER OF HEADWALL

3/17/2021 H:\203192 US75-I44 Plan Sheets\33788(09)-SKELLY-PNP1.dgn



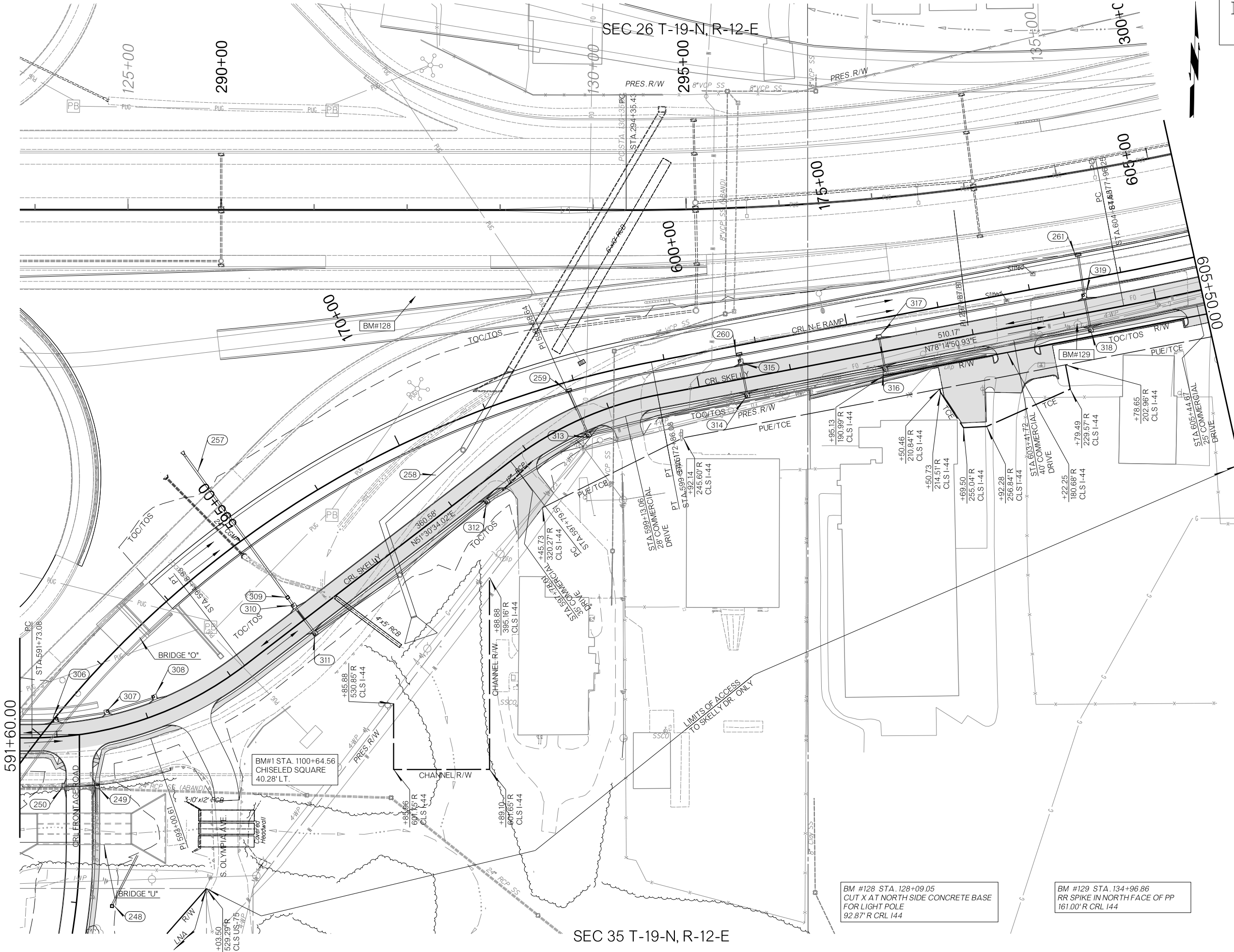
BM #112 STA. 252+33.25
 CUT BOX AT WEST EDGE CONCRETE
 BASE FOR LIGHT POLE
 78.24' L CRL US75

BM #127D STA. 121+96.90, 103.87' R CRS 144
 STA. 285+96.71, 103.79' R CRL 144
 CUT X AT CENTER OF HEADWALL

3/17/2021 H:\203192 US75-144\Plan Sheets\33788(09)-SKELLY-PNP2.dgn

SEC 26 T-19-N, R-12-E

SEC 35 T-19-N, R-12-E



591+60.00

125+00
290+00

130+00
175+00
295+00
300+C

600+00

605+00

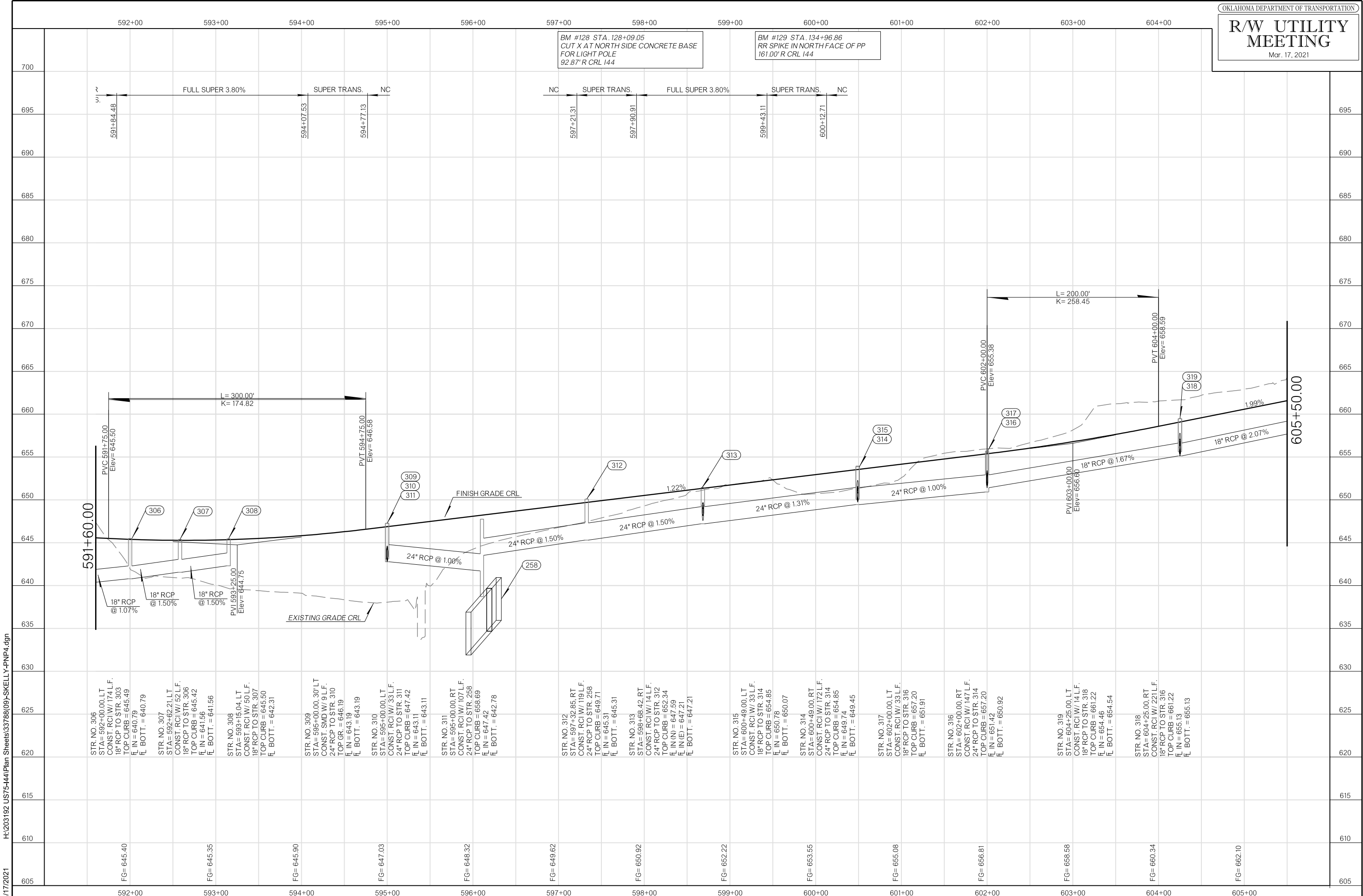
605+50.00

BM#1 STA. 1100+64.56
CHISELED SQUARE
40.28' LT.

BM #128 STA. 128+09.05
CUT X AT NORTH SIDE CONCRETE BASE
FOR LIGHT POLE
92.87' R CRL I44

BM #129 STA. 134+96.86
RR SPIKE IN NORTH FACE OF PP
161.00' R CRL I44

3/17/2021 H:\203192 US75-I44 Plan Sheets\33788(09)-SKELLY-PNP3.dgn



BM #128 STA. 128+09.05
 CUT X AT NORTH SIDE CONCRETE BASE
 FOR LIGHT POLE
 92.87' R CRL 144

BM #129 STA. 134+96.86
 RR SPIKE IN NORTH FACE OF PP
 161.00' R CRL 144

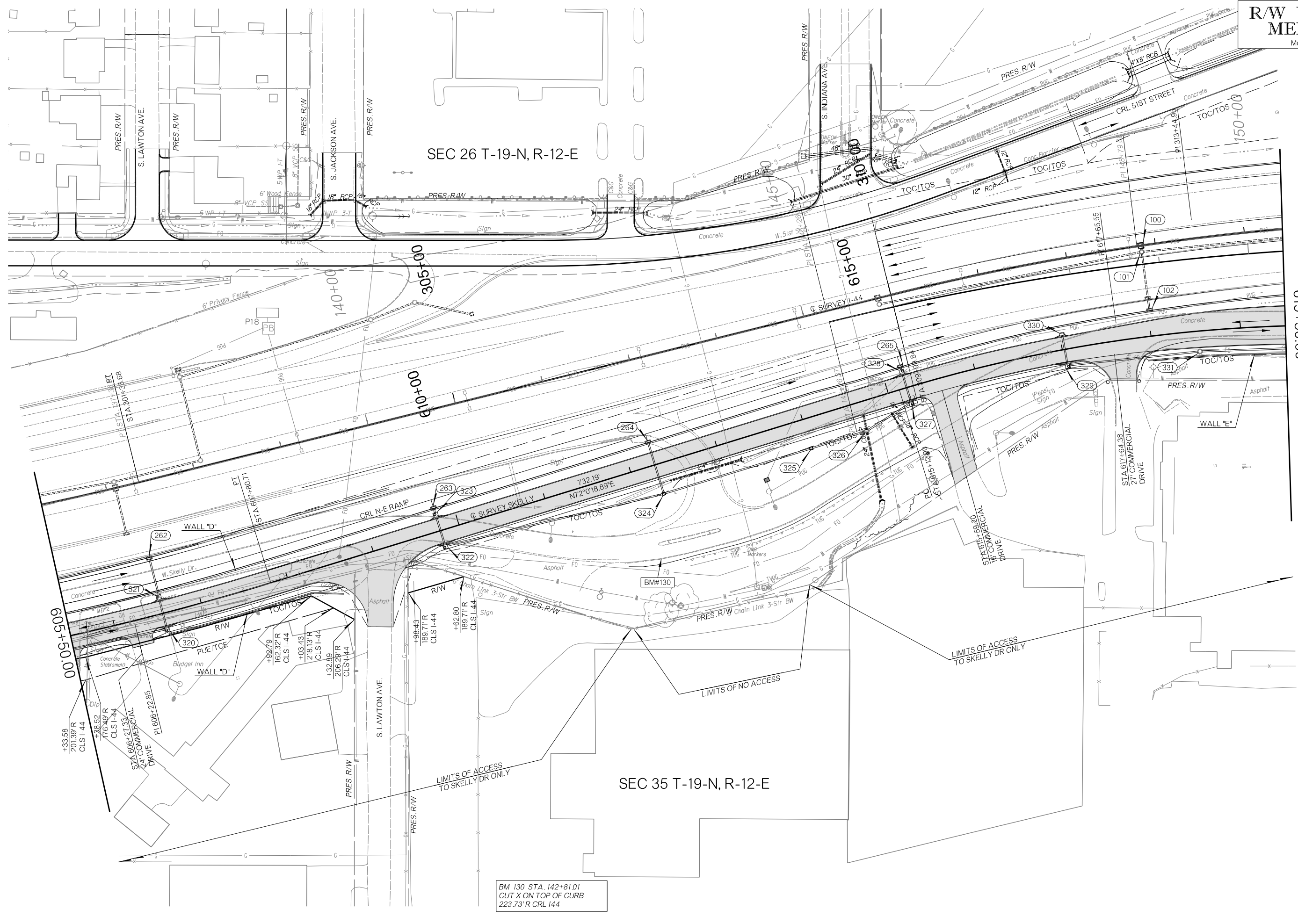
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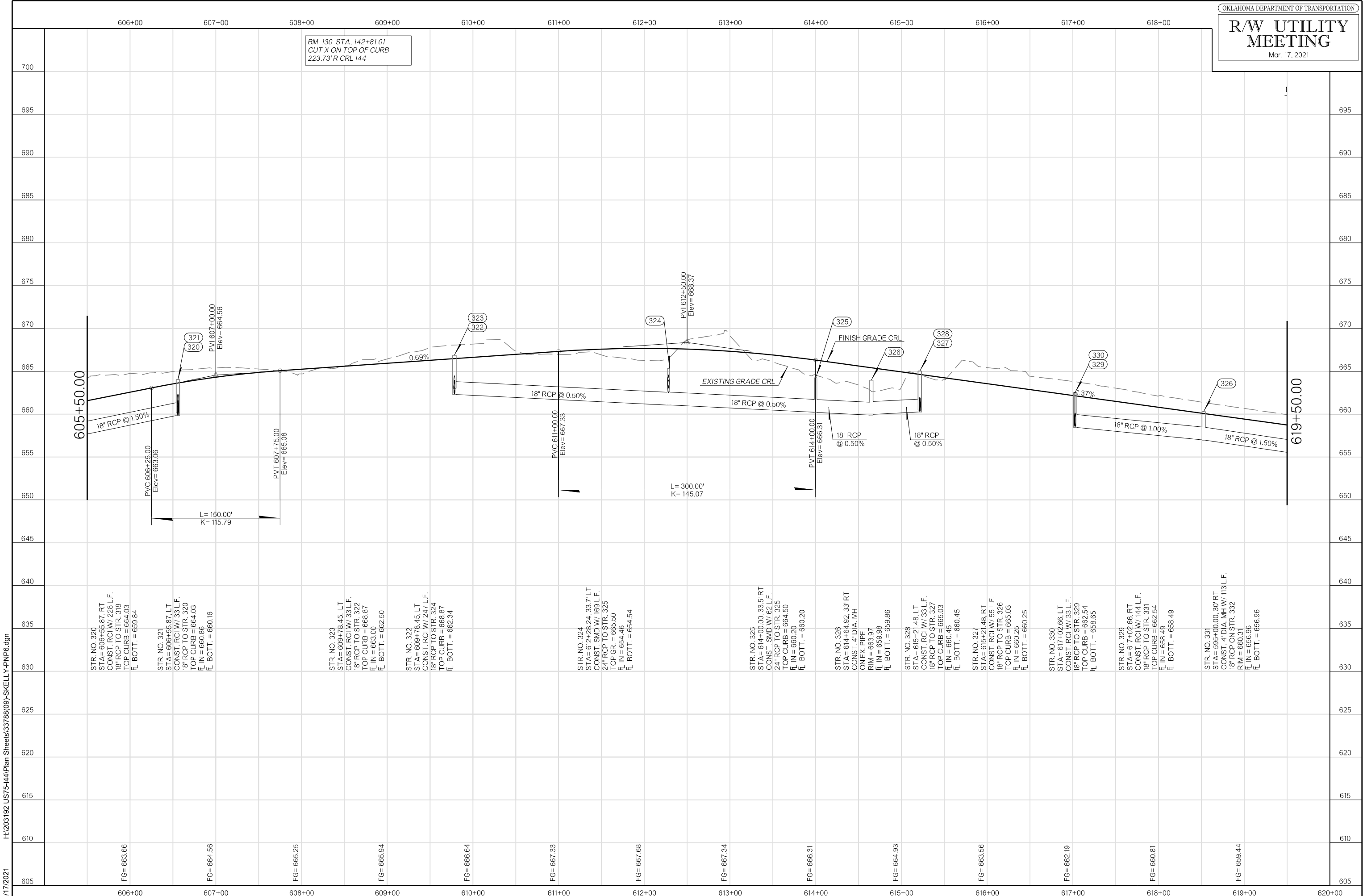
SEC 26 T-19-N, R-12-E

SEC 35 T-19-N, R-12-E

BM 130 STA. 142+81.01
CUT X ON TOP OF CURB
223.73'R CRL I44

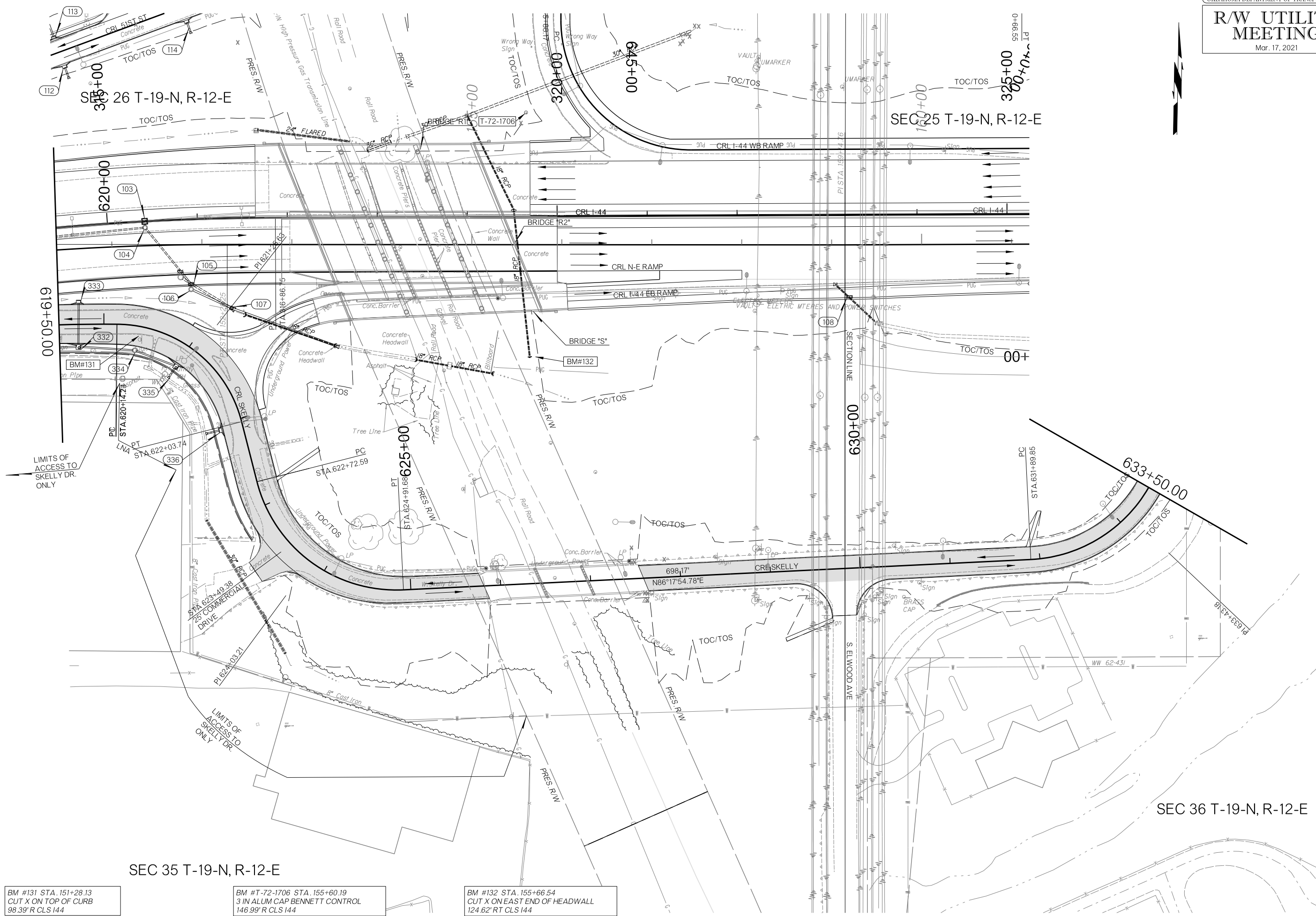
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3/17/2021 H:\203192 US75-I44 Plan Sheets\3788(09)-SKELLY-PNP6.dgn

635	STR. NO. 320 STA=606+55.87, RT CONST. RCIW/ 228 L.F. 18" RCP TO STR. 318 TOP CURB = 664.03 FL BOTTL. = 659.84
635	STR. NO. 321 STA=606+55.87, LT CONST. RCIW/ 33 L.F. 18" RCP TO STR. 320 TOP CURB = 664.03 FL IN = 660.86 FL BOTTL. = 660.16
635	STR. NO. 323 STA=609+78.45, LT CONST. RCIW/ 33 L.F. 18" RCP TO STR. 322 TOP CURB = 668.87 FL IN = 663.00 FL BOTTL. = 662.50
635	STR. NO. 322 STA=609+78.45, LT CONST. RCIW/ 247 L.F. 18" RCP TO STR. 324 TOP CURB = 668.87 FL BOTTL. = 662.34
635	STR. NO. 324 STA=612+28.24, 33.7' LT CONST. SMD W/ 169 L.F. 24" RCP TO STR. 325 TOP GR. = 665.50 FL IN = 654.46 FL BOTTL. = 654.54
635	STR. NO. 325 STA=614+00.00, 33.5' RT CONST. SMD W/ 62 L.F. 24" RCP TO STR. 325 TOP CURB = 664.50 FL IN = 660.20 FL BOTTL. = 660.20
635	STR. NO. 326 STA=614+64.92, 33' RT CONST. 4" DIA. MH ON EX. PIPE RIM = 663.97 FL IN = 659.98 FL BOTTL. = 659.86
635	STR. NO. 328 STA=615+21.48, LT CONST. RCIW/ 33 L.F. 18" RCP TO STR. 327 TOP CURB = 665.03 FL IN = 660.45 FL BOTTL. = 660.45
635	STR. NO. 327 STA=615+21.48, RT CONST. RCIW/ 55 L.F. 18" RCP TO STR. 326 TOP CURB = 665.03 FL IN = 660.25 FL BOTTL. = 660.25
635	STR. NO. 330 STA=617+02.66, LT CONST. RCIW/ 33 L.F. 18" RCP TO STR. 329 TOP CURB = 662.34 FL BOTTL. = 663.65
635	STR. NO. 329 STA=617+02.66, RT CONST. RCIW/ 144 L.F. 18" RCP TO STR. 331 TOP CURB = 662.54 FL IN = 658.49 FL BOTTL. = 658.49
635	STR. NO. 331 STA=595+00.00, 30' RT CONST. 4" DIA. MH W/ 113 L.F. 18" RCP ON STR. 332 RIM = 660.31 FL IN = 656.96 FL BOTTL. = 656.96

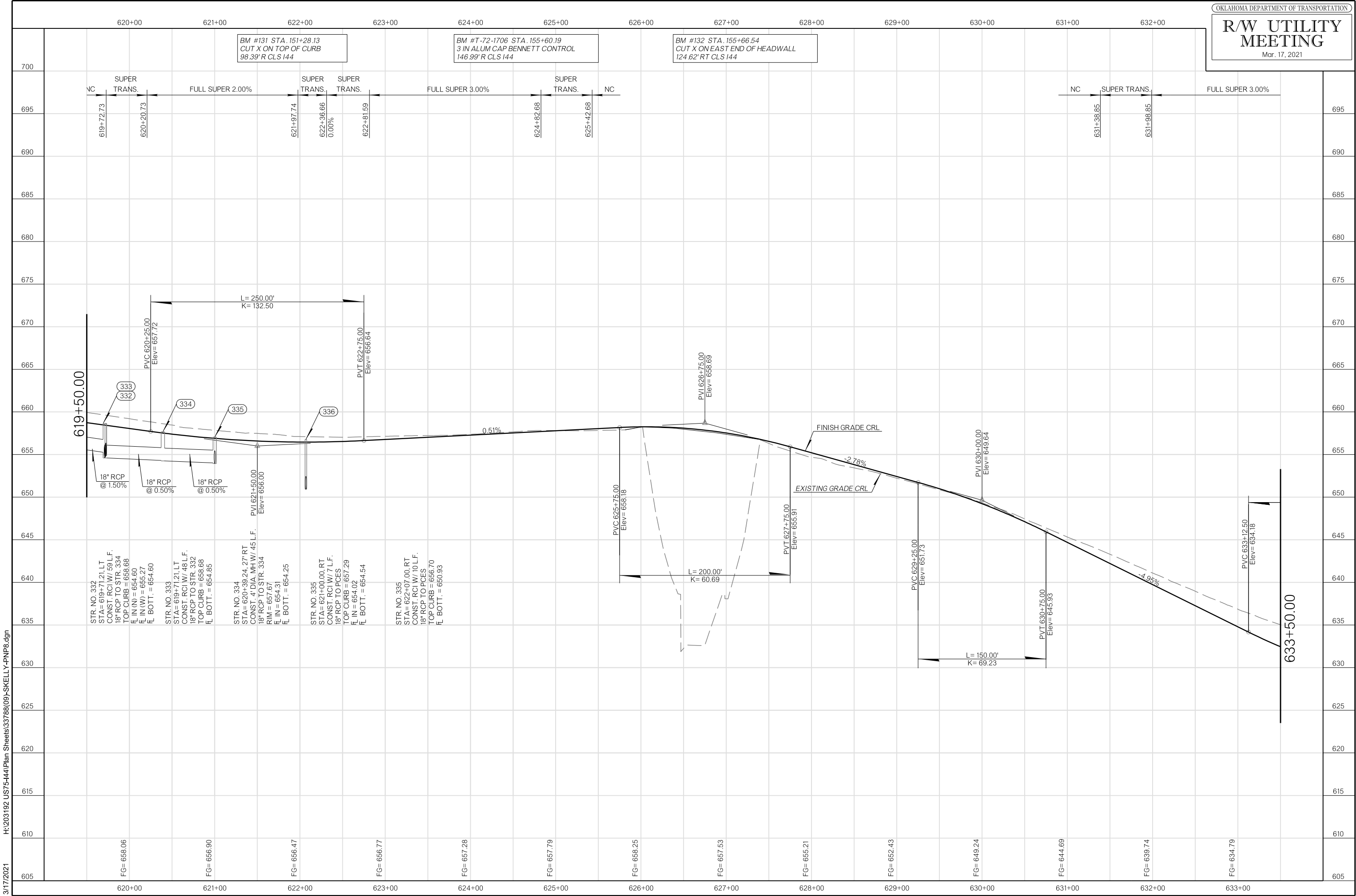


BM #131 STA. 151+28.13
 CUT X ON TOP OF CURB
 98.39' R CLS 144

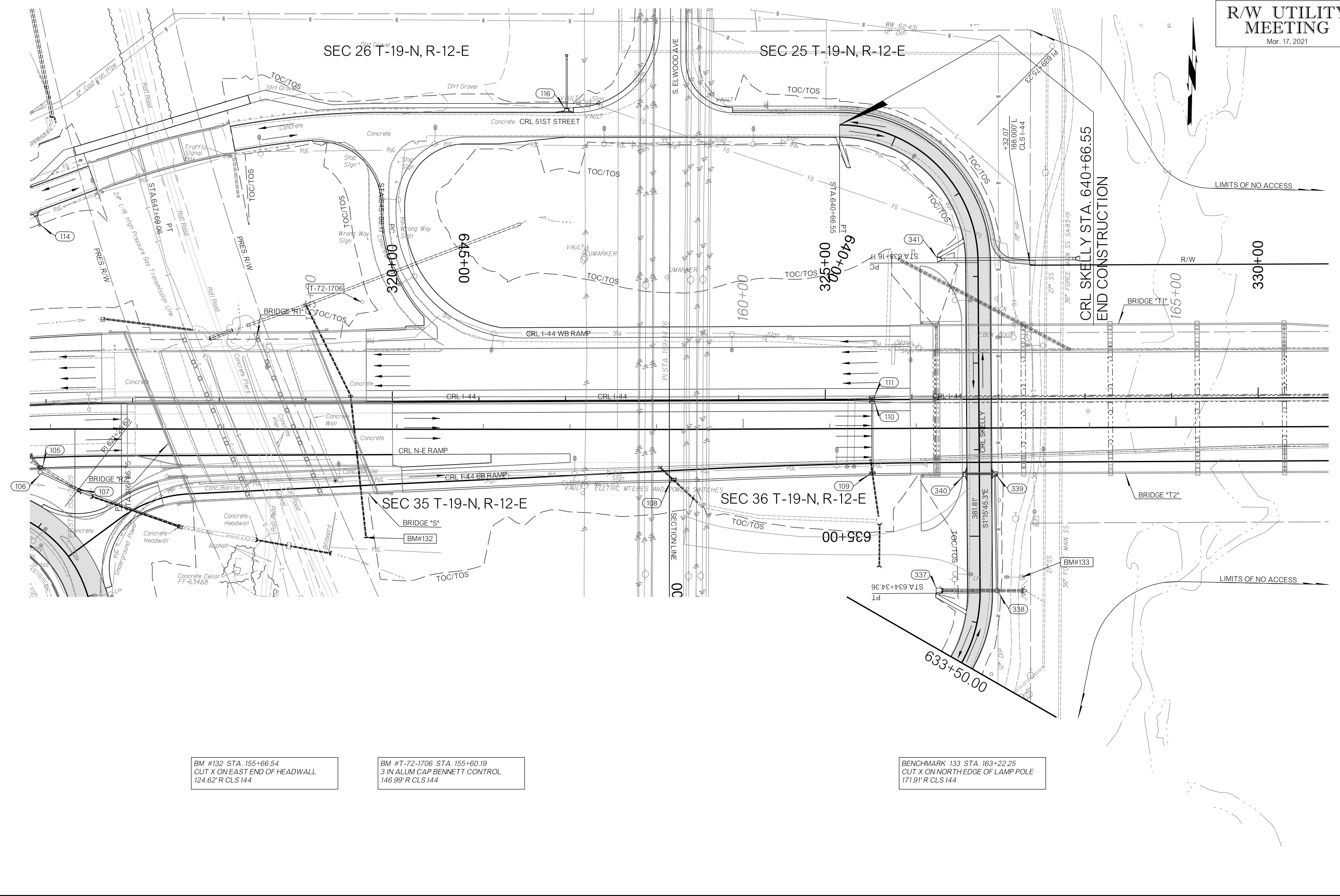
BM #T-72-1706 STA. 155+60.19
 3 IN ALUM CAP BENNETT CONTROL
 146.99' R CLS 144

BM #132 STA. 155+66.54
 CUT X ON EAST END OF HEADWALL
 124.62' RT CLS 144

3/17/2021 H:\203192 US75-144\Plan Sheets\33788(09)-SKELLY-PNP7.dgn



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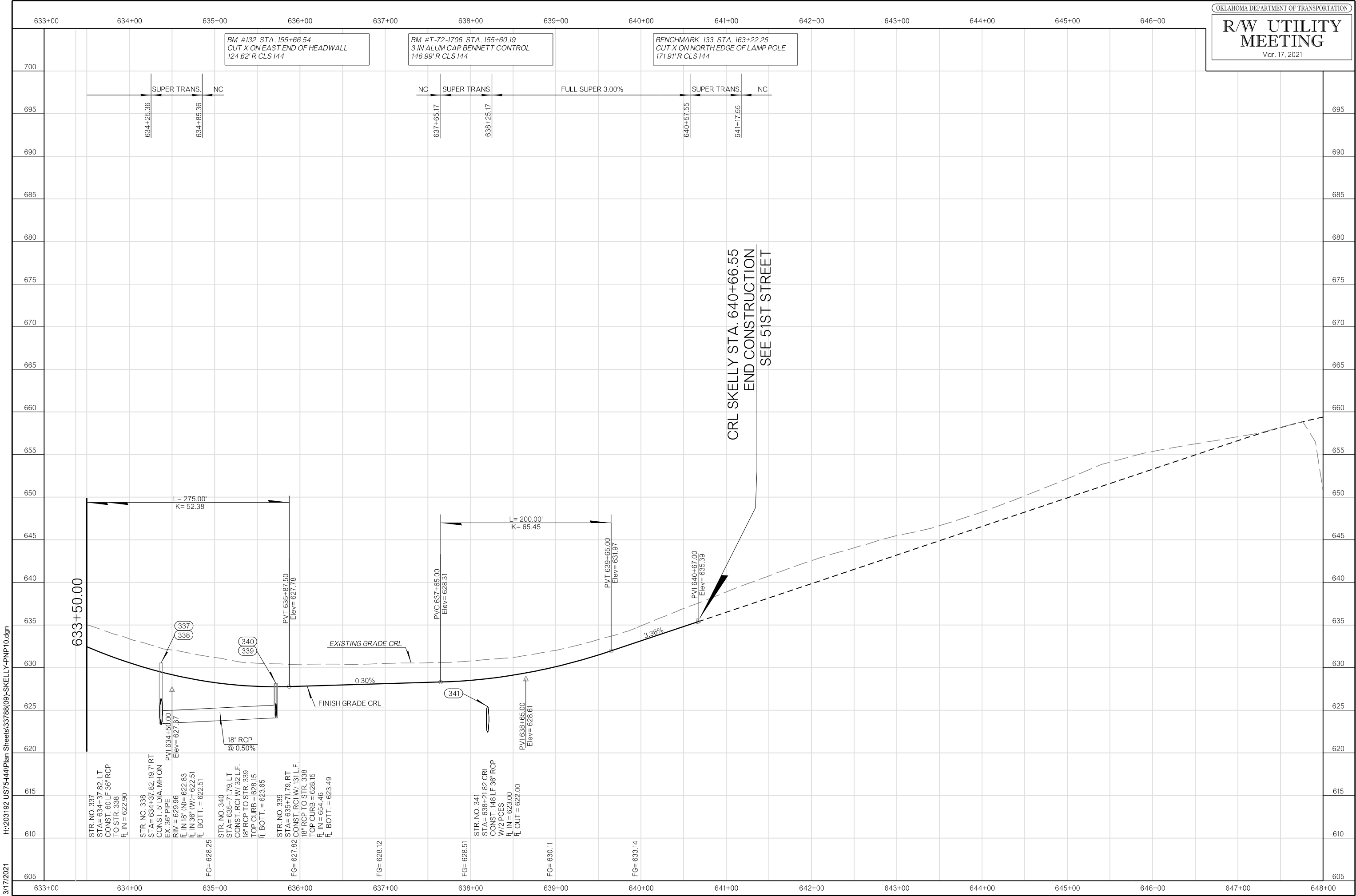


BM #132 STA. 155+66.54
 CUT X ON EAST END OF HEADWALL
 124.62' R CLS I44

BM #T-72-1706 STA. 155+60.19
 3 IN ALUM CAP BENNETT CONTROL
 146.99' R CLS I44

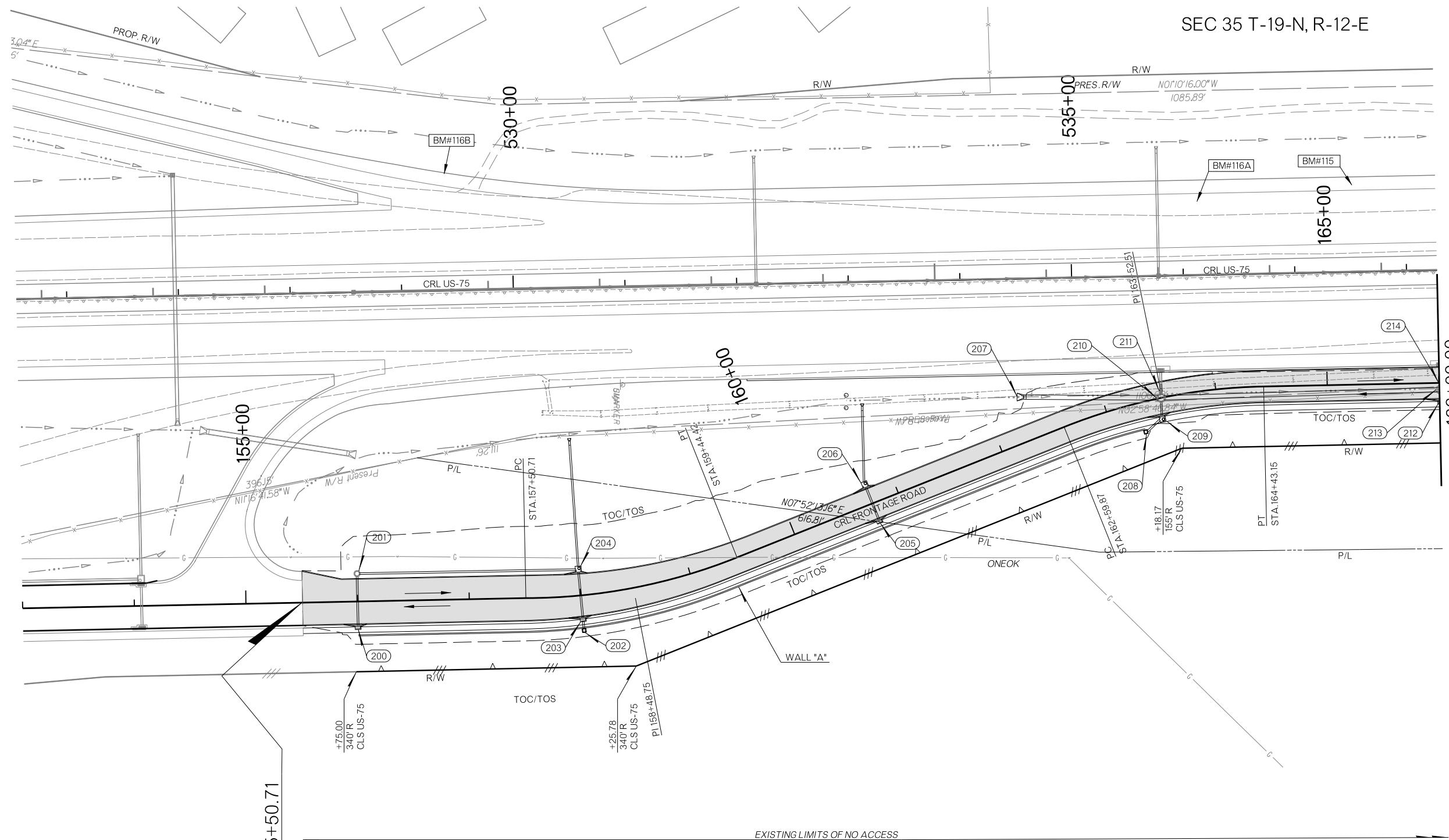
BENCHMARK 133 STA. 163+22.25
 CUT X ON NORTH EDGE OF LAMP POLE
 171.91' R CLS I44

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SEC 35 T-19-N, R-12-E



CRL FRONTAGE ROAD STA. 155+50.71
BEGIN CONSTRUCTION

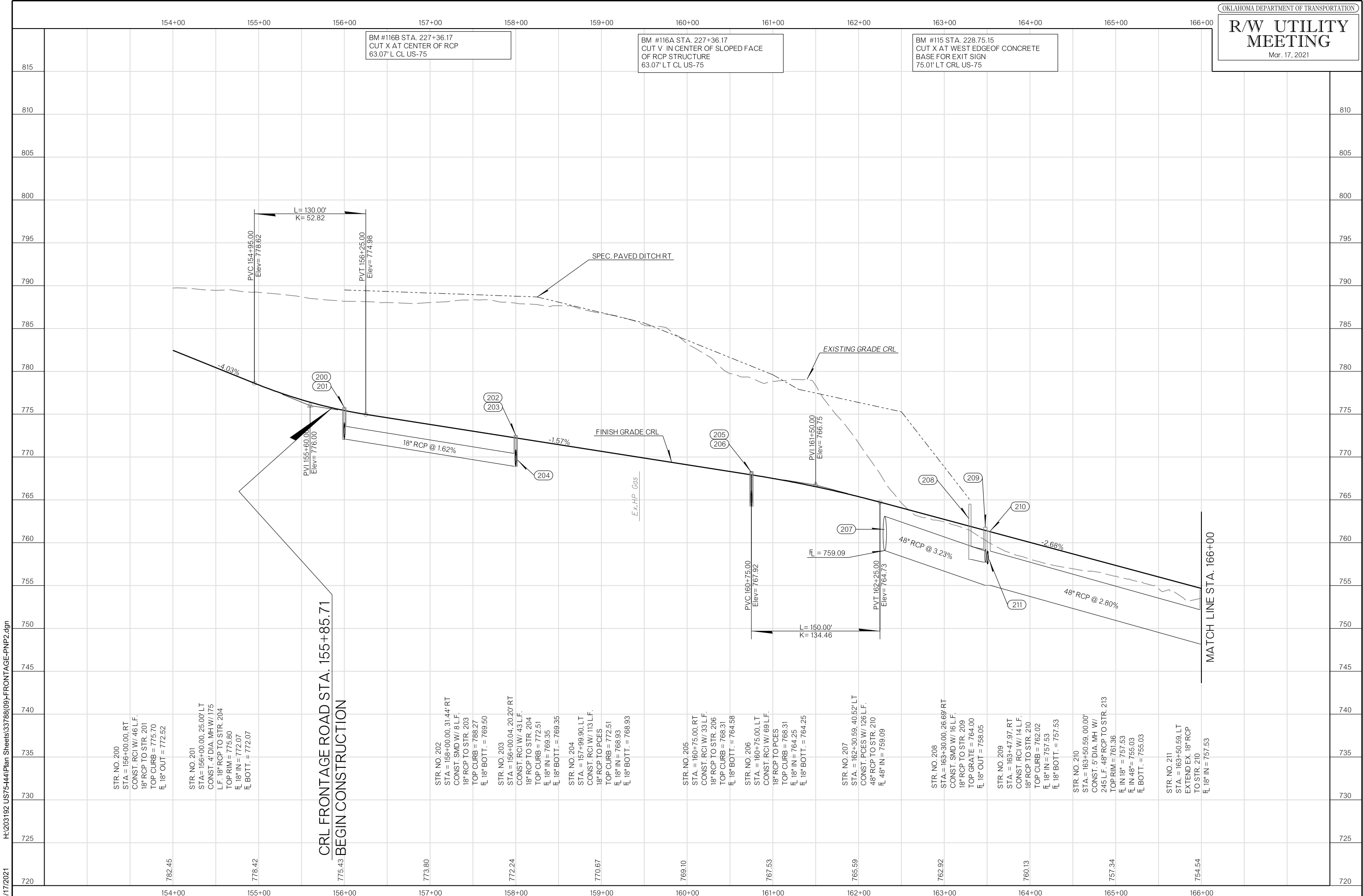
SEC 35 T-19-N, R-12-E

BENCHMARK 116B
 CUT X AT CENTER OF RCP
 STA. 227+36.17, 63.07' LT CL US-75
 N=398335.748 E=2557401.431 EL. 771.43

BENCHMARK 116A
 CUT V IN CENTER OF SLOPED FACE
 OF RCP STRUCTURE
 STA. 227+36.17, 63.07' LT CL US-75
 N=399011.652 E=2557426.859 EL. 760.24

BENCHMARK 115
 CUT X AT WEST EDGE OF CONCRETE
 BASE FOR EXIT SIGN
 STA. 228.75.15, 75.01' LT CL US-75
 N=399150.361 E=2557412.085 EL. 755.55

PLACED BY ONE
DOC # 20160020



BM #116B STA. 227+36.17
 CUT X AT CENTER OF RCP
 63.07' LT CL US-75

BM #116A STA. 227+36.17
 CUT V IN CENTER OF SLOPED FACE
 OF RCP STRUCTURE
 63.07' LT CL US-75

BM #115 STA. 228.75.15
 CUT X AT WEST EDGE OF CONCRETE
 BASE FOR EXIT SIGN
 75.01' LT CRL US-75

STR. NO. 200
 STA = 156+00.00, RT
 CONST. RCI W/ 46 L.F.
 18" RCP TO STR. 201
 TOP CURB = 775.70
 FL 18" OUT = 772.52

STR. NO. 201
 STA = 156+00.00, 25.00' LT
 CONST. 4" DIA. MH W/ 175
 L.F. 18" RCP TO STR. 204
 TOP RIM = 775.80
 FL 18" IN = 772.07
 FL 18" BOT. = 772.07

**CRL FRONTAGE ROAD STA. 155+85.71
 BEGIN CONSTRUCTION**

STR. NO. 202
 STA = 158+00.00, 31.44' RT
 CONST. SMD W/ 8 L.F.
 18" RCP TO STR. 203
 TOP CURB = 788.27
 FL 18" BOT. = 769.50

STR. NO. 203
 STA = 156+00.04, 20.20' RT
 CONST. RCI W/ 43 L.F.
 18" RCP TO STR. 204
 TOP CURB = 772.51
 FL 18" IN = 769.35
 FL 18" BOT. = 769.35

STR. NO. 204
 STA = 157+99.90, LT
 CONST. RCI W/ 113 L.F.
 18" RCP TO PCEs
 TOP CURB = 772.51
 FL 18" IN = 768.93
 FL 18" BOT. = 768.93

STR. NO. 205
 STA = 160+75.00, RT
 CONST. RCI W/ 33 L.F.
 18" RCP TO STR. 206
 TOP CURB = 768.31
 FL 18" BOT. = 764.58

STR. NO. 206
 STA = 160+75.00, LT
 CONST. RCI W/ 69 L.F.
 18" RCP TO PCEs
 TOP CURB = 768.31
 FL 18" IN = 764.25
 FL 18" BOT. = 764.25

STR. NO. 207
 STA = 162+30.59, 40.52' LT
 CONST. PCEs W/ 126 L.F.
 48" RCP TO STR. 210
 FL 48" IN = 759.09

STR. NO. 208
 STA = 163+30.00, 26.68' RT
 CONST. SMD W/ 16 L.F.
 18" RCP TO STR. 209
 TOP GRATE = 764.00
 FL 18" OUT = 758.05

STR. NO. 209
 STA = 168+47.97, RT
 CONST. RCI W/ 14 L.F.
 18" RCP TO STR. 210
 TOP CURB = 762.02
 FL 18" IN = 757.53
 FL 18" BOT. = 757.53

STR. NO. 210
 STA = 163+50.59, 00.00'
 CONST. 5" DIA. MH W/
 245 L.F. 48" RCP TO STR. 213
 TOP RIM = 761.36
 FL 18" IN = 757.53
 FL 18" IN 48" = 755.03
 FL 18" BOT. = 755.03

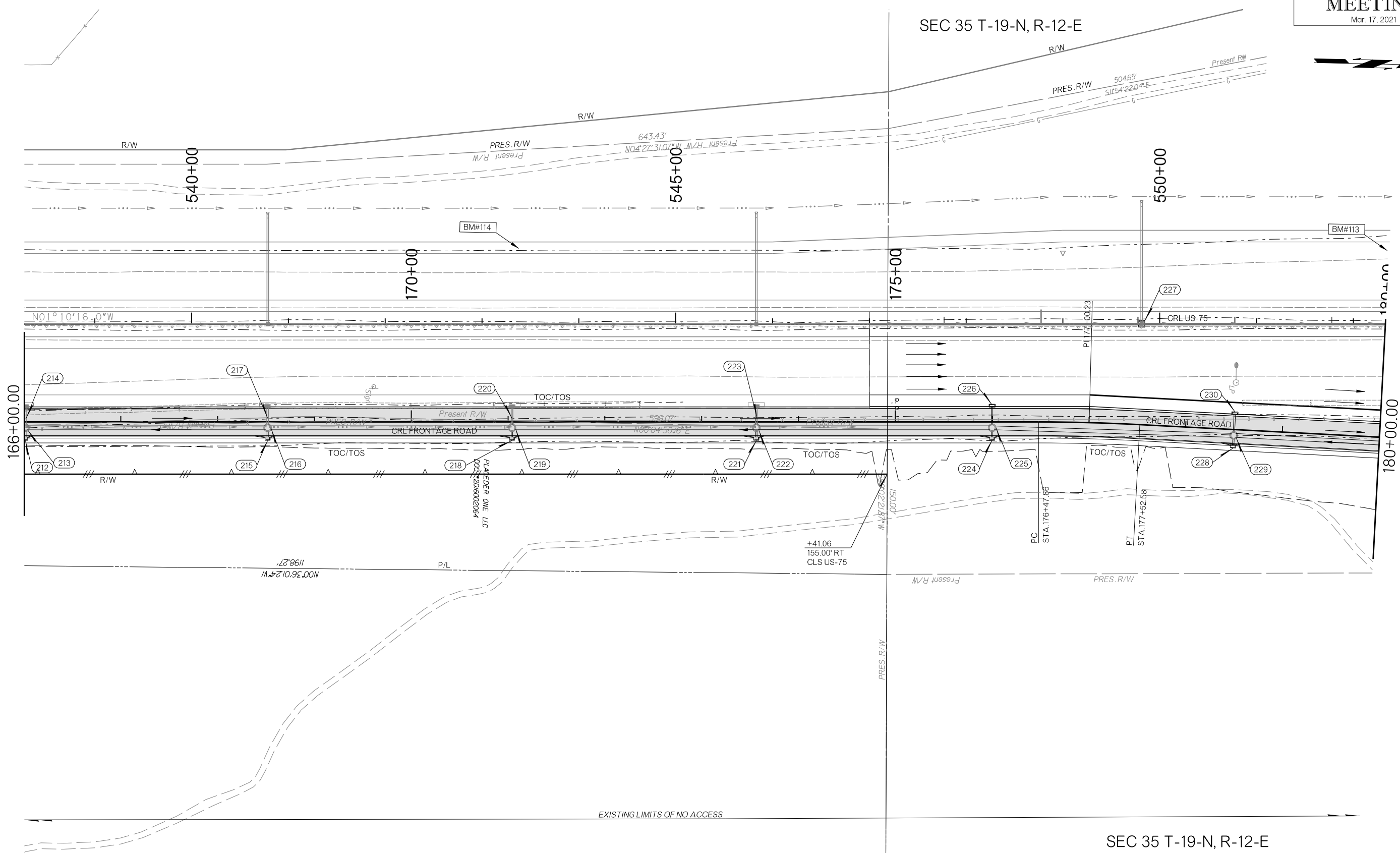
STR. NO. 211
 STA = 163+50.59, LT
 EXTEND EX 18" RCP
 TO STR. 210
 FL 18" IN = 757.53

MATCH LINE STA. 166+00

3/17/2021 H:\203192 US75-I44\Plan Sheets\33788(09)-FRONTAGE-PNP2.dgn



SEC 35 T-19-N, R-12-E



EXISTING LIMITS OF NO ACCESS

SEC 35 T-19-N, R-12-E

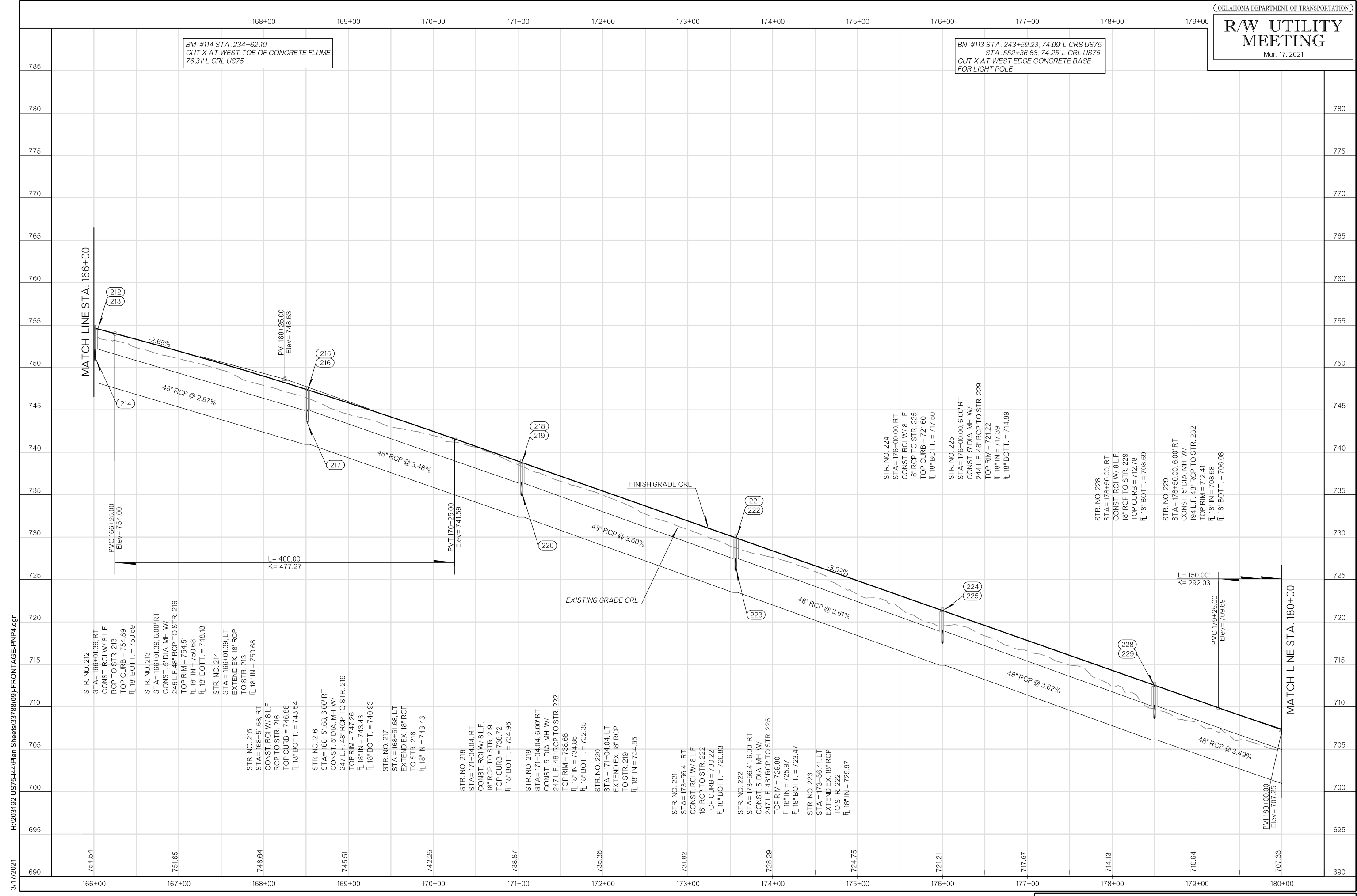
BM #114 STA. 234+62.10
 CUT X AT WEST TOE OF CONCRETE FLUME
 76.31' L CRL US75

BM #113 STA. 243+59.23, 74.09' L CRS US75
 STA. 552+36.68, 74.25' L CRL US75
 CUT X AT WEST EDGE CONCRETE BASE
 FOR LIGHT POLE

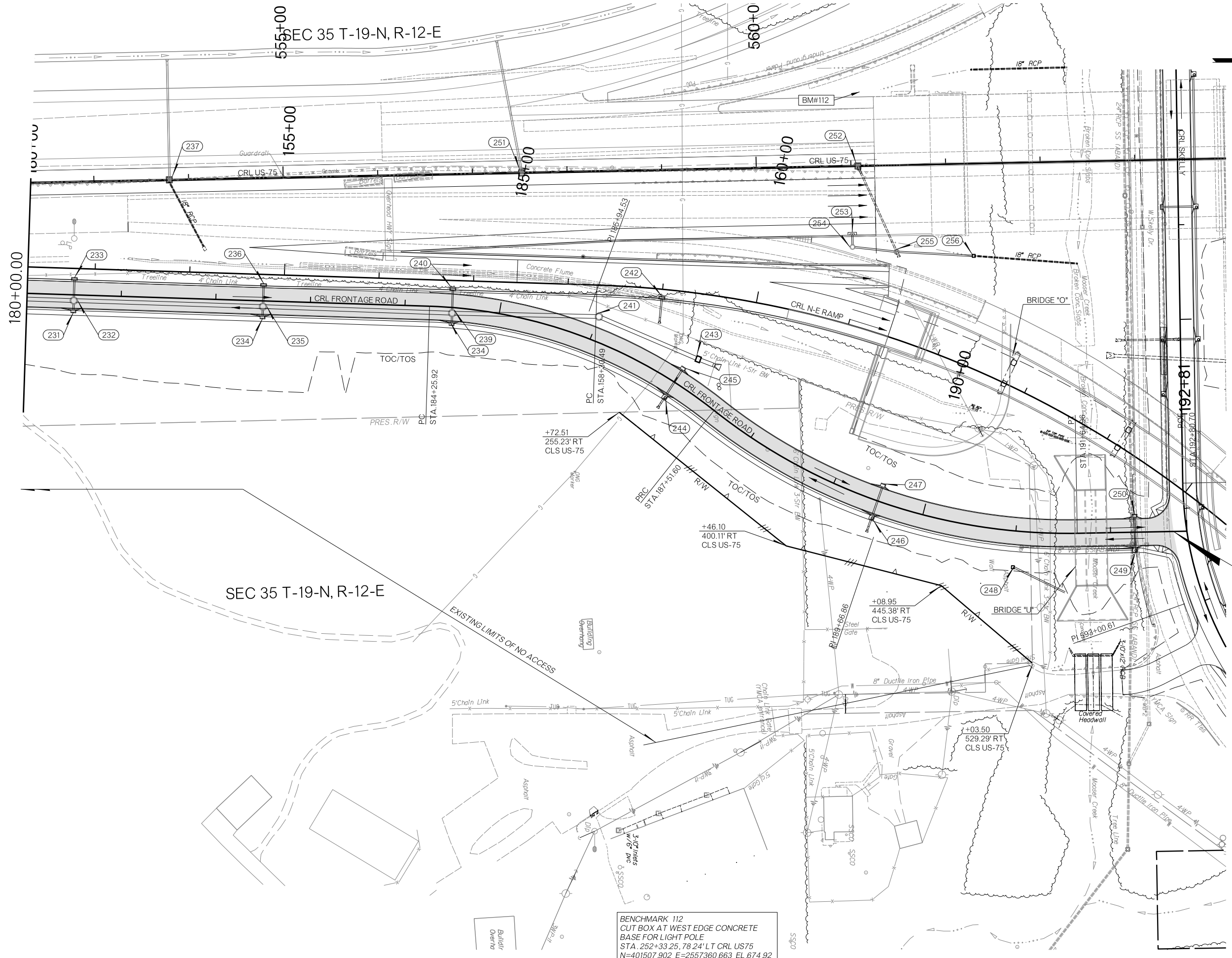
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 3/17/2021

BM #114 STA. 234+62.10
 CUT X AT WEST TOE OF CONCRETE FLUME
 76.31'L CRL US75

BN #113 STA. 243+59.23, 74.09'L CRS US75
 STA. 552+36.68, 74.25'L CRL US75
 CUT X AT WEST EDGE CONCRETE BASE
 FOR LIGHT POLE



3/17/2021 H:\203192 US75-I44 Plan Sheets\33788(09)-FRONTAGE-PNP4.dgn



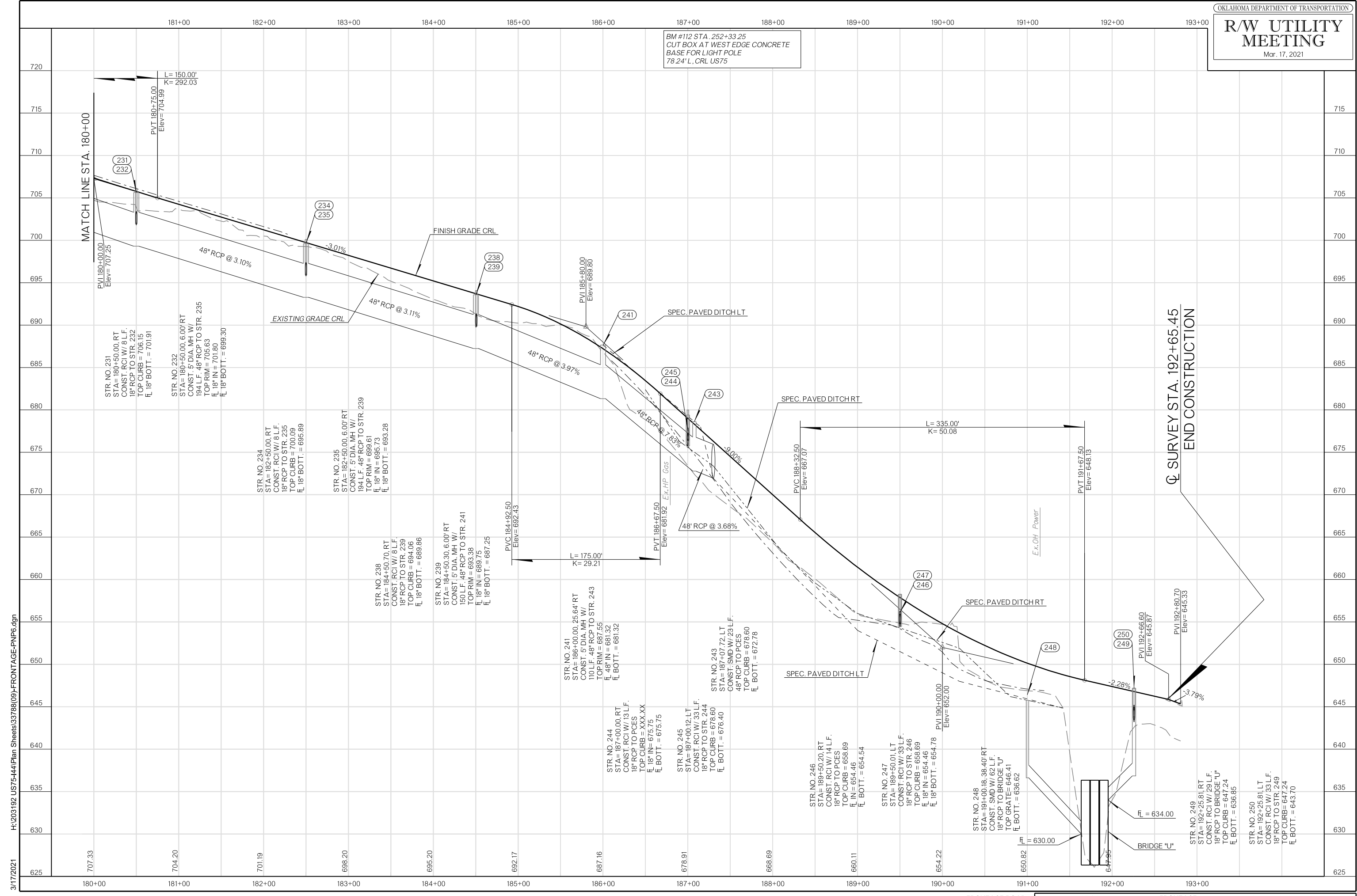
SEC 35 T-19-N, R-12-E

EXISTING LIMITS OF NO ACCESS

CRL FRONTAGE ROAD STA. 192+65.45
 END CONSTRUCTION

BENCHMARK 112
 CUT BOX AT WEST EDGE CONCRETE
 BASE FOR LIGHT POLE
 STA. 252+33.25, 78.24' LT CRL US75
 N=401507.902 E=2557360.663 EL.674.92

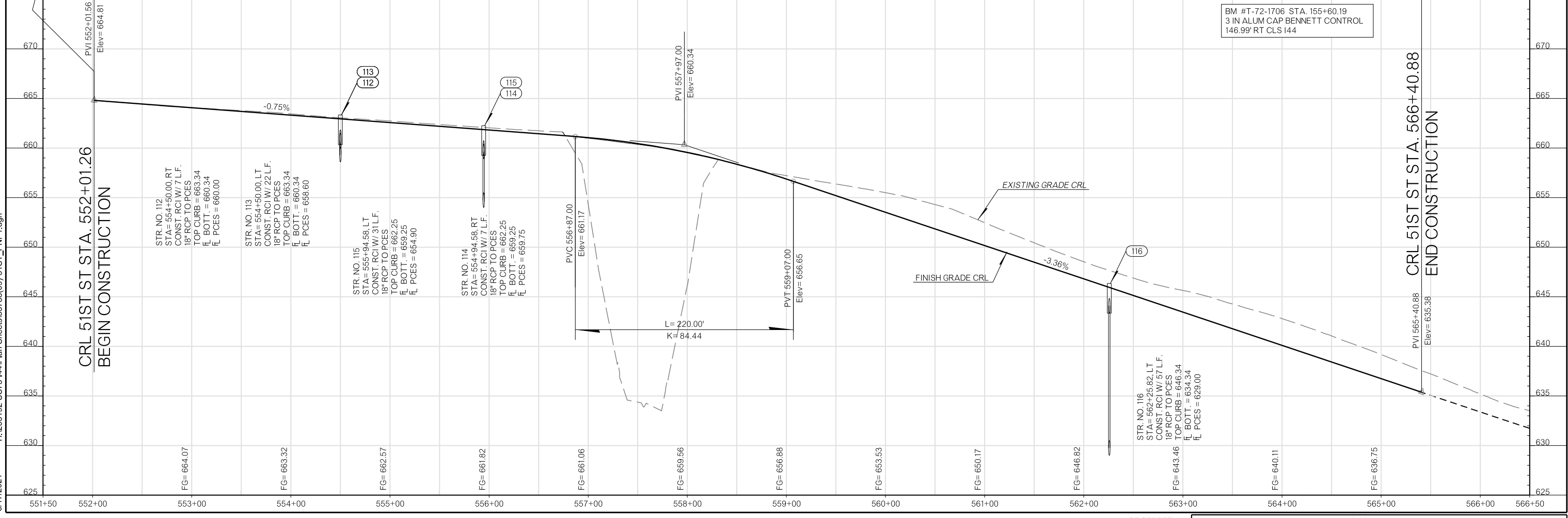
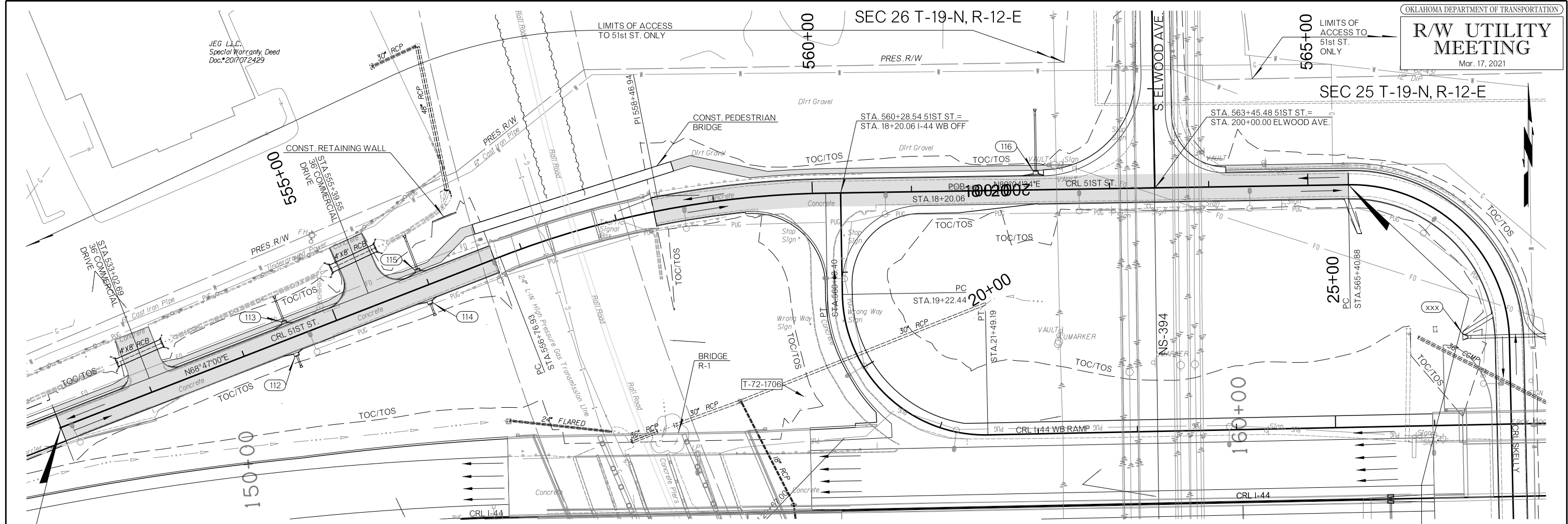
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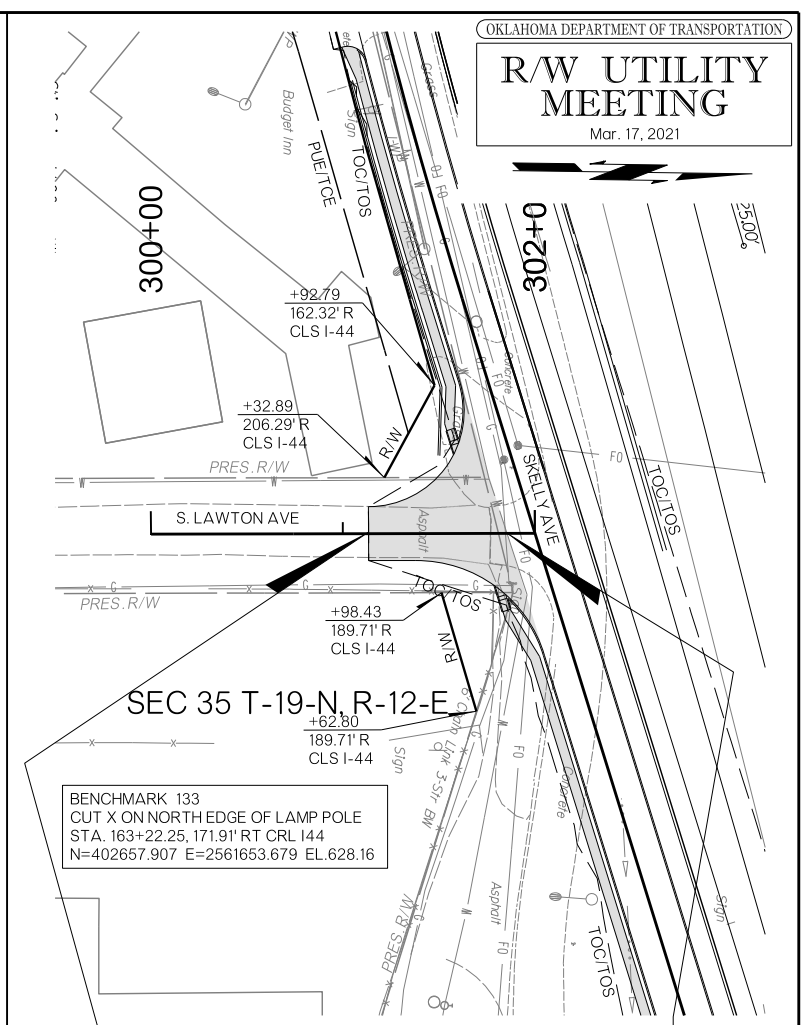
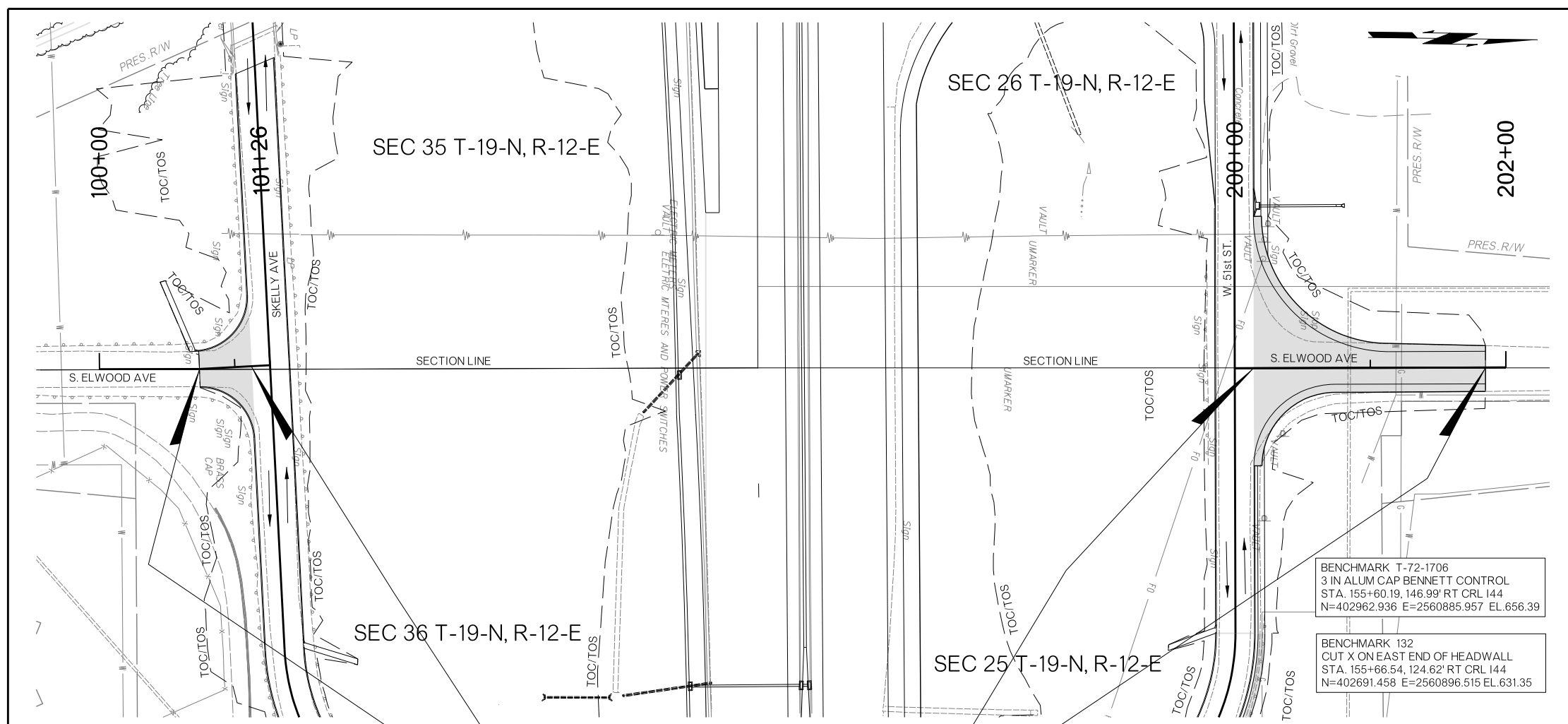
BM #112 STA. 252+33.25
 CUT BOX AT WEST EDGE CONCRETE
 BASE FOR LIGHT POLE
 78.24'L, CRL US75

Q SURVEY STA. 192+65.45
 END CONSTRUCTION

3/17/2021 H:\203192 US75-I44 Plan Sheets\33788(09)-FRONTAGE-PNP6.dgn



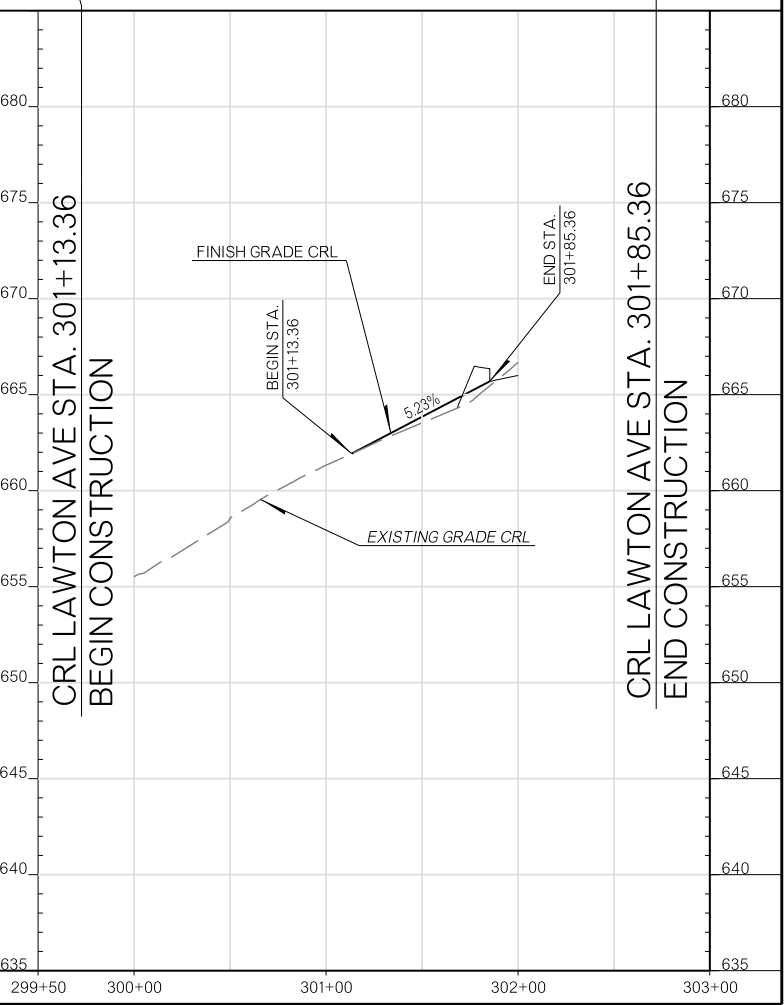
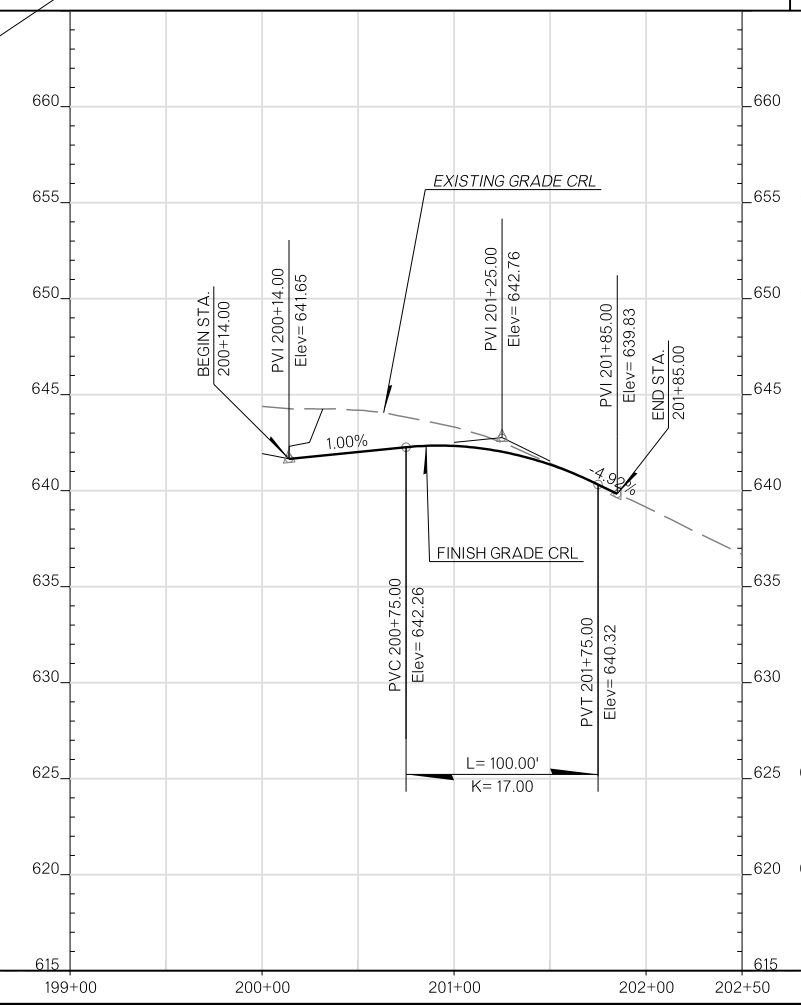
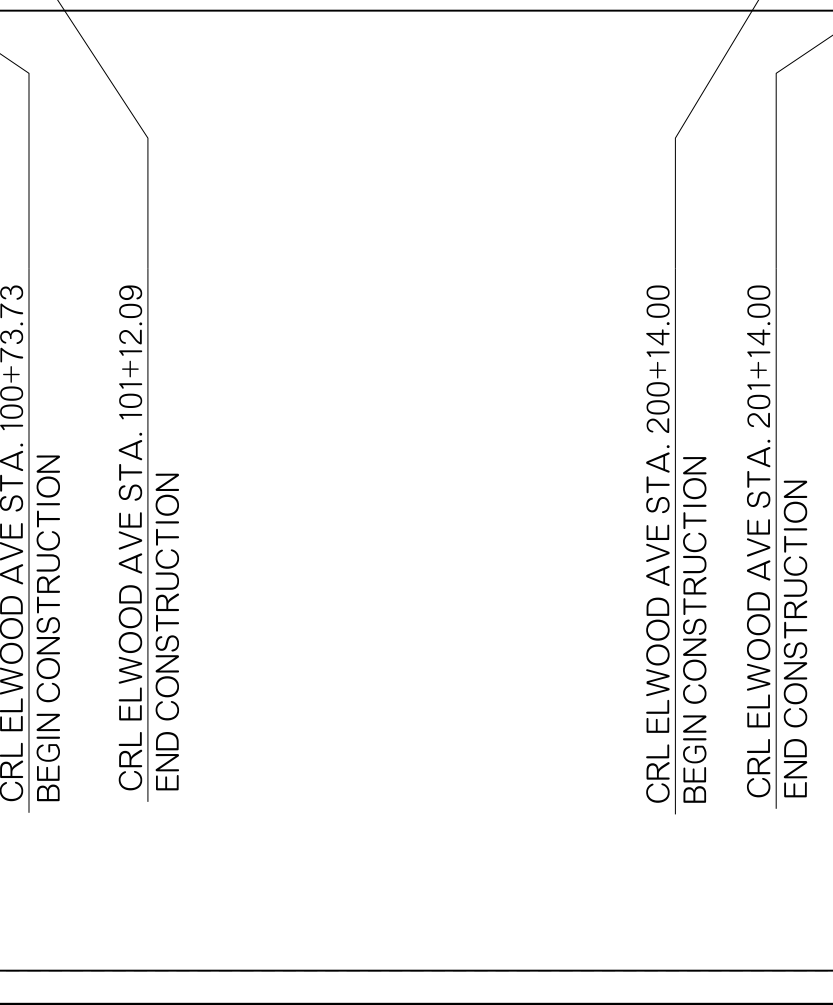
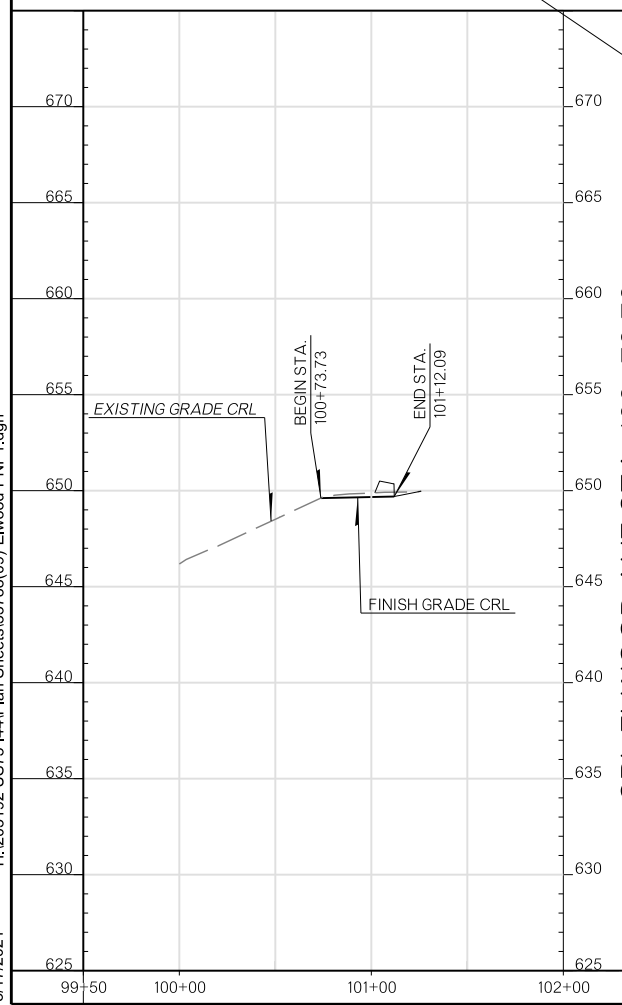
BM #T-72-1706 STA. 155+60.19
 3 IN ALUM CAP BENNETT CONTROL
 146.99' RT CLS I44



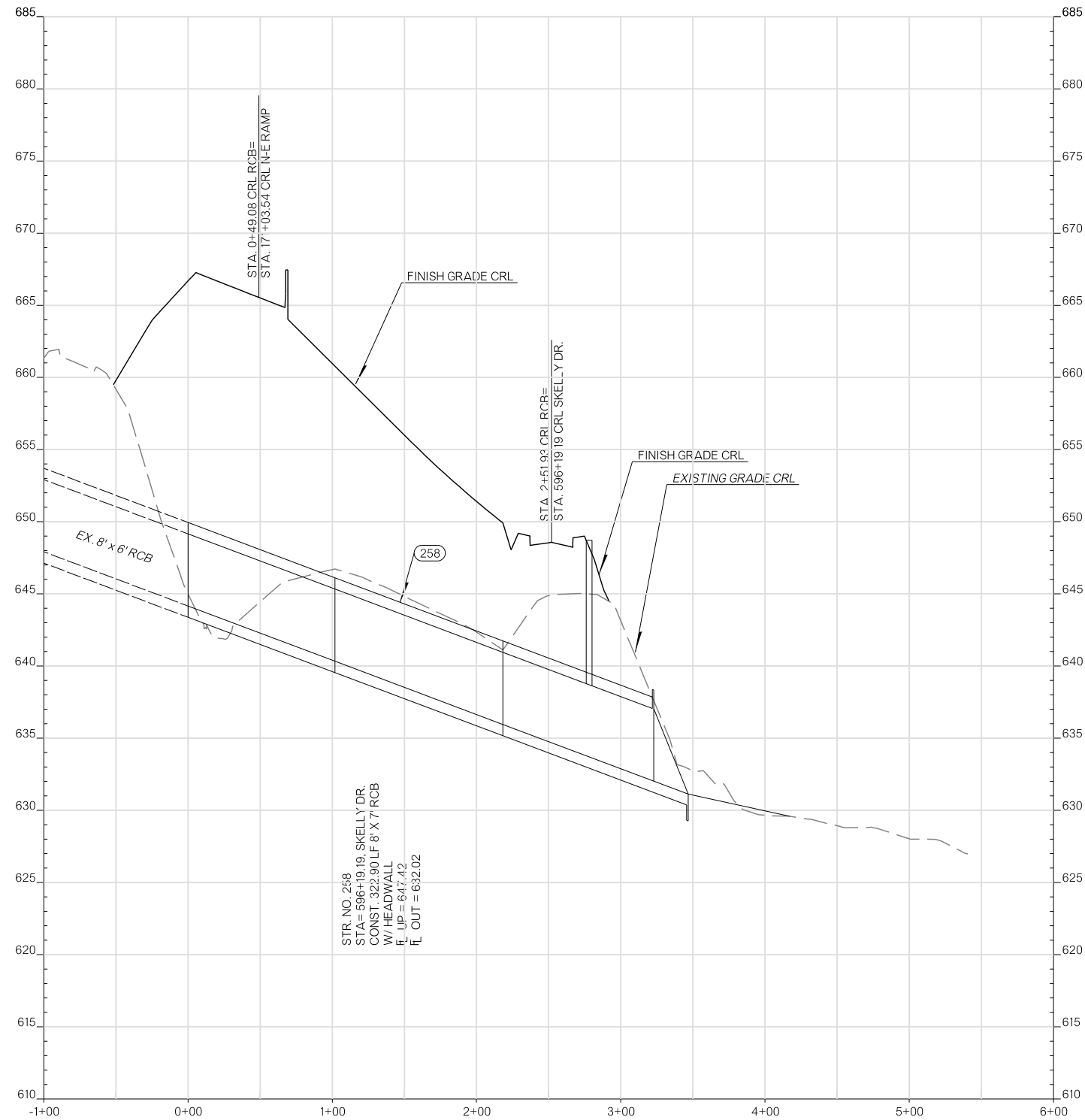
BENCHMARK T-72-1706
 3 IN ALUM CAP BENNETT CONTROL
 STA. 155+60.19, 146.99' RT CRL I44
 N=402962.936 E=2560885.957 EL.656.39

BENCHMARK 132
 CUT X ON EAST END OF HEADWALL
 STA. 155+66.54, 124.62' RT CRL I44
 N=402691.458 E=2560896.515 EL.631.35

BENCHMARK 133
 CUT X ON NORTH EDGE OF LAMP POLE
 STA. 163+22.25, 171.91' RT CRL I44
 N=402657.907 E=2561653.679 EL.628.16



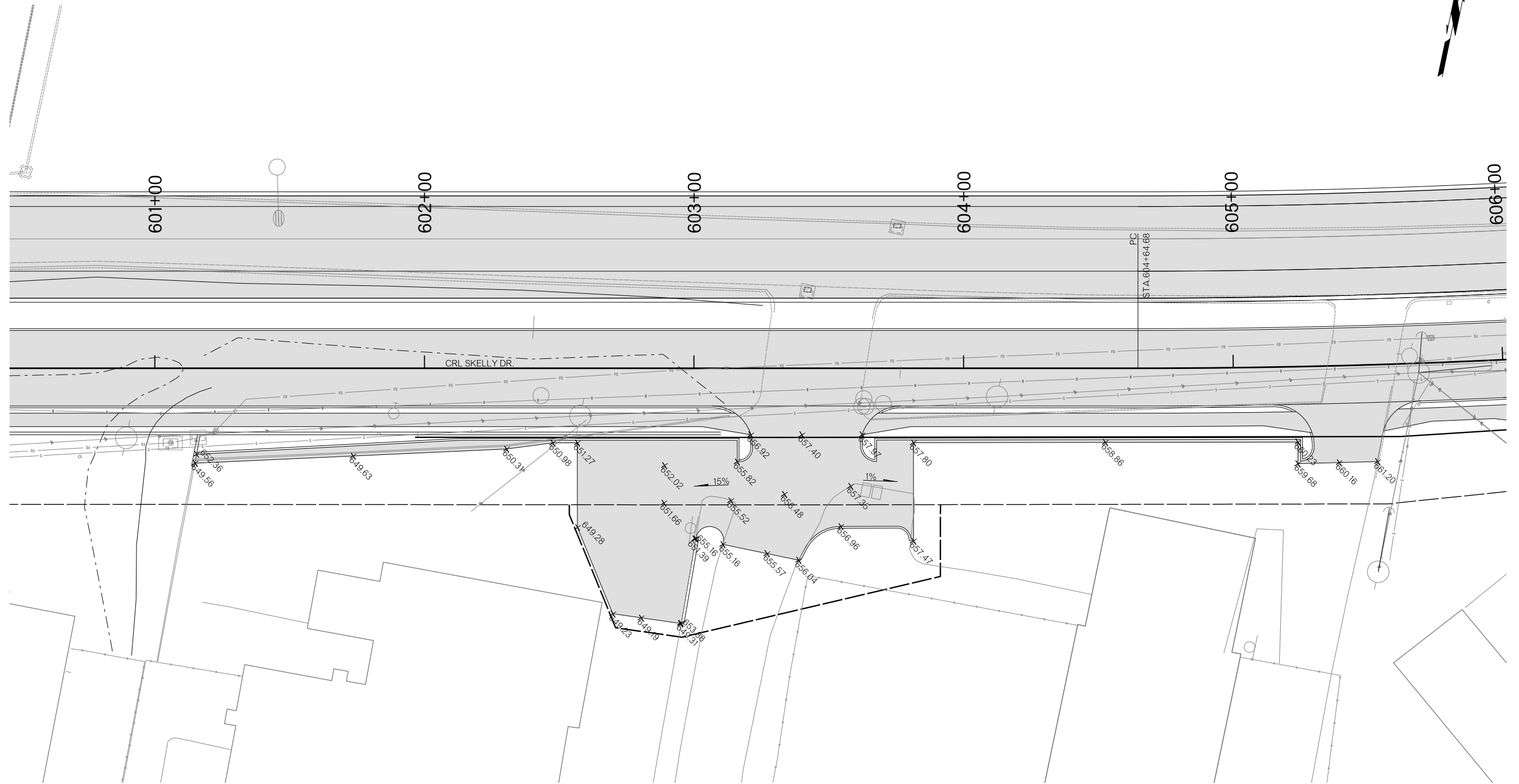
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3/17/2021

DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES STORM SEWER PROFILE
DRAWN		
CHECKED		
APPROVED		
SQUAD		
COUNTY - TULSA		HIGHWAY - US75/144 STATE JOB NO. - JP33788(09) SHEET NO. R043



H:\203192 US75-144\Plan Sheets\33788(09)-Driveway.dgn

3/17/2021

DESIGN		OKLAHOMA DEPARTMENT OF TRANSPORTATION POE AND ASSOCIATES
DRAWN		
CHECKED		
APPROVED		
SQUAD		
COUNTY - TULSA		HIGHWAY - US75/144 STATE JOB NO. - JP33788(09) SHEET NO. R044

DRIVEWAY DETAIL

SURVEY CONTROL DATA

1. POSITIONAL CONTROL:

A. POSITIONAL CONTROL FOR THIS SURVEY IS THE NGS OKLAHOMA STATE PLANE COORDINATE SYSTEM, NAD83 (2011), LAMBERT PROJECTION (NORTH ZONE 3501).

2. BEARINGS:

THE BEARINGS SHOWN HEREIN OR HEREON ARE GRID BEARINGS DERIVED FROM THE NGS OKLAHOMA STATE PLANE COORDINATE SYSTEM AND ARE NOT ASTRONOMICAL.

3. VERTICAL CONTROLS:

A. LEVEL DATUM IS NAVD 88 FROM SWO 5117(2) SURVEY CONTROL.

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION

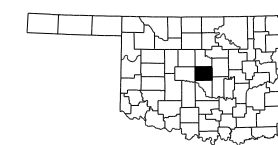
SURVEY OF
STATE HIGHWAY
SWO 5443(2)
J/P NO. 33788(09)
TULSA COUNTY

I-44-US 75 Interchange, Work Package #3.

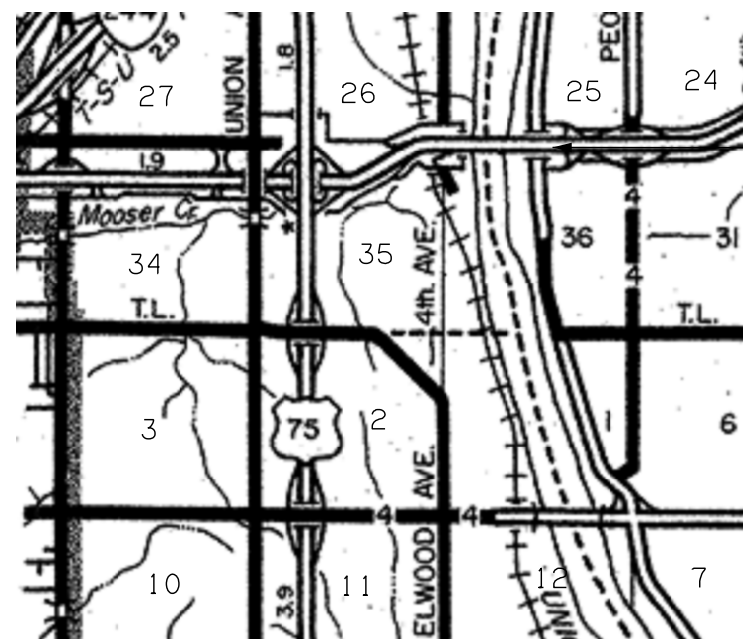
R-12-E

SHEET INDEX

NO.	DESCRIPTION
SD01 OF SD17	TITLE SHEET
SD02-SD03 OF SD17	HISTORICAL LETTER
SD04-SD06 OF SD17	BM & CHECKEVELS
SD07-SD09 OF SD17	COGO DATA
SD10-SD11 OF SD17	ALIGNMENT DATA
SD12-SD17 OF SD17	SURVEY DATA SHEETS



LOCATION MAP



PROJECT EXTENTS

SWO 5443(2) Job/Piece 33788(09) CI No. 2123B

LAND SURVEYOR'S CERTIFICATION

I hereby certify that all land and property sub-division distances, angles, corners, and monumentation made or used in conjunction with this survey and depicted or recorded herein or hereon were recovered, established or re-established in substantial conformity with:

- Applicable instructions contained in the U.S. Government Bureau of Land Management publication "Manual of Survey Instruction".
- Its supplement, "Restoration of Lost or Obliterated Corners and Sub-division of Sections".
- "Oklahoma Minimum Standards for the Practice of Land Surveying" as adopted by the State Board of Licensure for Professional Engineers and Land Surveyors: and
- Sound land surveying practices:

including a thorough search, study, analysis and consideration of all existing records and field evidence.

I further certify that all survey monuments depicted exist and that all land survey work was done by me or under my direct supervision.

DATED THIS 31st DAY OF August, 2020

POE & ASSOCIATES INC.
Oklahoma City, Oklahoma

SURVEYOR'S NAME - SIGNATURE

SURVEYOR'S NAME - JUSTIN LEE TALCOTT

OKLAHOMA REGISTERED LAND SURVEYOR NO. 1646

CERTIFICATE OF AUTHORIZATION NO. 541

STATE OF OKLAHOMA
DEPARTMENT OF TRANSPORTATION



UTILITIES

UTILITY	PHONE NUMBER
PSO	918-250-6211
COX	405-417-4064
AT&T	918-596-4237
ONG	405-556-6401
CITY OF TULSA	918-596-2595
ONEOK Field services	918-588-7431
Phillips 66	918-977-5143
Enable Midstream	405-921-3582
Enbridge Energy	918-223-2054

CALL BEFORE YOU DIG
THE NEW NATIONAL LOCATE NUMBER

••811••

US-75 PROJECT LENGTH 9,136.0 Ft. 1.73 MI.

BEGINNING SATTION : STA. 170+00.00
ENDING STATION : STA. 261+35.82

I-44 PROJECT LENGTH 4,580.0 Ft. 0.87 MI.

BEGINNING SATTION : STA. 120+52.12
ENDING STATION : STA. 166+32.00

Electronic File Transfer Disclaimer:

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THIS SURVEY MEETS THE OKLAHOMA MINIMUM STANDARDS FOR THE PRACTICE OF LAND SURVEYING AS ADOPTED BY THE OKLAHOMA STATE BOARD OF REGISTRATION FOR PROFESSIONAL ENGINEERS AND LAND SURVEYORS, MAY 17, 2010.

SPECIFICATIONS FOR SURVEYS FOR PRIMARY AND SECONDARY HIGHWAYS DATED MAY 1, 1999 GOVERN.

SDS 01 of 18



PLS		POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA SURVEY DIVISION	
DRAWN		SURVEY DATA SHEET	
CHECKED			
APPROVED			
CREW	POE		
SWO <u>5443(2)</u>		PROJECT NO. <u>33788(09)</u> SHEET NO. <u>S001</u>	

Oklahoma Department of Transportation

Survey Division (405)521-2621 Fax (405)522-0364

Date: August 31, 2020

SWO 5443(2), I-44 Tulsa County
 Historical Letter & Written Report
 Page 2 of 3

To: Mr. Kyle King, Chief of Surveys
 From: Justin Lee Talcott, Professional Land Surveyor
 Subject: SWO 5443(2), I/P 33788(09) – I-44 - Tulsa County
 I-44/U.S. 75 Interchange, Work Package 3

Historical Letter & Written Report

1. GENERAL:

Survey Began: November 1, 2019
 Survey Completed: August 31, 2020

Personnel on this Survey:

Justin Lee Talcott Professional Land Surveyor, OK #1646
 Mike Watson Professional Land Surveyor, OK #1516
 Jeremy Brownrigg Assistant Surveyor
 Tremetria Williams CAD Technician

2. PURPOSE:

The purpose of this survey was to obtain adequate information to aid in the design of Work Package #3 of the I-44/U.S. 75 Interchange.

3. SURVEY LIMITS:

U.S. 75: This survey limits began at West 61st Street and extended North to an area just North of W. Skelly Drive and includes the width being just West of the U.S. 75 centerline to a line approximately 775.00 feet East of the US 75 centerline (approx. 1.73 miles).

I-44: This survey limits began approximately at the existing East bound exit ramp loop and extended East to the East end of the Arkansas River bridge and includes an area South of I-44 varying in width from approximately 550.00 feet to 800.00 South of the I-44 centerline, and also including an area North of I-44 between South Indian Ave and the West edge of the Arkansas River (approx. 0.87 miles).

4. ALIGNMENT:

U.S. 75/I-44: The centerline of survey for this project is along and identical to the existing centerline and was provided to us by Isaacs Surveying, a sub-consultant working for Benham.

1. STATIONING:

U.S. 75: Centerline stationing was provided to us by Isaacs Surveying, a sub-consultant working for Benham and begins at Station 170+00.00 and extends North to Station 261+35.82 at the U.S. 75 & I-44 intersection for our project.

I-44: Centerline stationing was provided to us by Isaacs Surveying, a sub-consultant working for Benham and begins at Station 120+52.12 being the intersection of U.S. 75 & I-44 and extends East to Station 166+32.00 being a point on the existing bridge just West of the West bank of the Arkansas River.

2. HORIZONTAL CONTROL:

Horizontal control for this survey is NGS Oklahoma State Plane Coordinate System, NAD83(HPGN), Lambert Projection, North Zone. We used the existing control points 72-01601, 72-01602, 72-01604, 72-01605 and 72-01607 provided to us by Joe Farmer PLS, #1799.

3. VERTICAL CONTROL:

Vertical Control for this survey is NAVD88, established by differential leveling by Isaacs Surveying, a sub-consultant working for Benham.

4. MEASUREMENT UNITS:

The distances, coordinates, and elevations shown on this survey are in US Survey Feet. All angles and bearings shown are in degrees, minutes, and seconds.

5. TOPOGRAPHY/DIGITAL TERRAIN MODEL:

LiDAR data was collected by Frontier Land Surveying and we used traditional survey methods of RTK and total stations to update any surface features and to collect all topography.

6. LAND TIES:

All land ties required for this survey was provided to us by Isaacs Surveying, a sub-consultant working for Benham.

7. RIGHT OF WAY

Right of way within the survey was taken from SWO5443(3) plans provided by Isaacs Surveying and checked and referenced to deeds and plats acquired from the Tulsa County courthouse at the county clerk and county appraisers offices.

8. ENVIRONMENTAL CONCERNS:

There were no hazardous waste sites or environmental concerns encountered during this survey.

PLS			POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA	
DRAWN			SURVEY DIVISION	
CHECKED			SURVEY DATA SHEET	
APPROVED				
CREW	POE	SWO 5443(2)	PROJECT NO. 33788(09)	SHEET NO. S002

SWO 5443(2), I-44 Tulsa County
 Historical Letter & Written Report
 Page 3 of 3

1. UTILITIES:

All utility companies servicing the project extent were contacted, after first contacting OKIE during the course of this survey. Survey crew personnel met with representatives when available to discuss locations.

2. DRAINAGE:

Topographic data was collected for the existing drainage structures as well as cross and side drains where needed throughout the project survey limits.

3. EQUIPMENT USED:

Topcon GR5 GPS receivers, Sokkia SRX3 robotic total station.

4. DATA SUBMITTED:

1. ODOT form SD-1, Transmittal Letter
2. ODOT form SD-7, Public and Privately Owned Utilities List
3. ODOT form SD-11 for 2 monuments
4. ODOT form SD-41, Surveyor's Certification
5. Coordinate List
6. Historical Letter
7. BM's and Check Levels (PDF)
8. Alignments (PDF)
9. SWO4995_1_V1.dgn
10. SWO4995_1_V1_TOPO.dgn
11. SWO4995_1_V1_TRI.dgn
12. SWO4995_1_V1_SFF.dgn
13. SWO4995_1_V1_DRA.dgn
14. SWO4995_1_V1.alg
15. SWO4995_1_V1_EXISTING.dtm

Justin Lee Talcott
 Professional Land Surveyor

PLS			POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA	
DRAWN			SURVEY DIVISION	
CHECKED			SURVEY DATA SHEET	
APPROVED				
CREW	POE	SWO 5443(2)	PROJECT NO. 33788(09)	SHEET NO. S003

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SW05443_BM1.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
T-72-1601				706.04		706.04	N. 403766.132 E. 2557392.970 2IN ALUMINUM CAP IN CONCRETE POST STA 274+90.35 0.22' RT CL US-75 (A003)	
to TP 6	11.800	11.800	11.800	694.240			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to T-72-1602	12.510	12.510	12.510	681.730		681.73	N. 402864.778 E. 2557317.738 2IN ALUMINUM CAP IN CONCRETE POST STA 265+90.72 93.42' LT CL US-75 (A003)	
to TP 7	10.110	10.111	10.111	671.620			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 110	-2.321	-2.320	-2.320	673.940			N. 402773.164 E. 2556827.839 RR SPIKE IN SE FACE OF PP STA 114+65.27 370.54' LT CL I-44 (A001)	
to TP 8	11.500	11.505	11.503	662.438			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 111	9.857	9.857	9.857	652.581			N. 402270.704 E. 2556706.127 CUT BOX AT EAST SIDE OF CONCRETE BASE FOR LIGHT POLE STA 113+35.68 129.94' RT CL I-44 (A001)	

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SW05443_BM2.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
to BM 114	-5.749	-5.744	-5.747	734.472			N. 399737.162 E. 2557398.790 CUT X AT WEST TOE OF CONCRETE FLUME STA 234+62.10 76.31' LT CL US-75 (A003)	
to TP 8	-12.093	-12.089	-12.091	746.563			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 115	-8.987	-8.991	-8.989	755.552			N. 399150.361 E. 2557412.085 CUT X AT WEST EDGE OF WEST CONCRETE BASE FOR EXIT SIGN STA 228+75.15 75.01' LT CL US-75 (A003)	
to BM 116A	-4.683	-4.683	-4.683	760.235			N. 399011.652 E. 2557426.859 CUT V IN CENTER OF SLOPED FACE OF RCP STRUCTURE STA 227+36.17 63.07' LT CL US-75 (A003)	
to TP 9	-9.482	-9.478	-9.480	769.715			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 116B	-1.717	-1.719	-1.718	771.433			N. 398335.748 E. 2557401.431 CUT X AT CENTER OF RCB STA 220+60.92 102.31' LT CL US-75 (A003)	
to TP 10	-9.317	-9.317	-9.317	780.750			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 11	-8.831	-8.829	-8.830	789.580			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 117	-0.416	-0.416	-0.416	789.996			N. 397547.565 E. 2557203.225 RR SPIKE IN SE FACE OF POWER POLE STA 212+76.96 316.58' LT CL US-75 (A003)	
to TP 12	-1.439	-1.439	-1.439	791.435			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SW05443_BM2.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
BM 111				652.581		652.581	N. 402270.704 E. 2556706.127 CUT BOX AT EAST SIDE OF CONCRETE BASE FOR LIGHT POLE STA 113+35.68 129.94' RT CL I-44 (A001)	
to TP 1	-5.388	-5.392	-5.390	657.971			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 2	-11.540	-11.553	-11.547	669.518			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 112	-5.404	-5.399	-5.402	674.919			N. 401507.902 E. 2557360.663 CUT BOX AT WEST EDGE OF CONCRETE BASE FOR LIGHT POLE STA 252+33.25 78.24' LT CL US-75 (A003)	
to TP 3	-11.104	-11.105	-11.105	686.024			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 4	-9.399	-9.395	-9.397	695.421			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 5	-10.079	-10.079	-10.079	705.500			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 113	-1.028	-1.027	-1.028	706.527			N. 400634.147 E. 2557382.671 CUT X AT WEST EDGE OF CONCRETE BASE FOR LIGHT POLE STA 243+59.23 74.09' LT CL US-75 (A003)	
to TP 6	-11.086	-11.091	-11.089	717.616			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 7	-11.108	-11.111	-11.110	728.725			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SW05443_BM2.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
to BM 118	-5.345	-5.344	-5.345	796.779			N. 397008.522 E. 2557338.546 #5 REBAR WITH 3IN. ALUMINUM CAP STA 207+35.26 192.31' LT CL US-75 (A003)	
to TP 13	11.039	11.040	11.040	785.740			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 119	8.900	8.894	8.897	776.843			N. 396526.601 E. 2557430.688 CUT X AT CENTER OF HEADWALL STA 202+51.56 110.04' LT CL US-75 (A003)	
to TP 14	-8.327	-8.324	-8.326	785.168			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 120	-1.701	-1.697	-1.699	786.867			N. 395797.510 E. 2557495.601 PK NAIL SET 1FT FROM WEST EDGE OF ASPHALT IN LINE WITH EXIT SIGN STA 195+21.29 60.04' LT CL US-75 (A003)	
to TP 15	-2.163	-2.165	-2.164	789.031			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 121	-0.766	-0.766	-0.766	789.797			N. 395017.227 E. 2557478.868 CUT X ON WEST EDGE OF CONCRETE BASE FOR SIGN STA 187+41.51 92.71' LT CL US-75 (A003)	
to BM 122	6.286	6.289	6.287	783.510			N. 394804.718 E. 2557449.939 CUT X TOP OF 18IN RCP PIPE STA 185+29.64 125.98' LT CL US-75 (A003)	
to TP 16	-10.533	-10.534	-10.534	794.043			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 123	3.797	3.795	3.796	790.247			N. 394082.103 E. 2557393.180 #5 REBAR WITH 2IN. ALUMINUM CAP STA 178+08.34 197.5' LT CL US-75 (A003)	

PLS			POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA SURVEY DIVISION SURVEY DATA SHEET PROJECT NO. <u>33788(09)</u> SHEET NO. <u>S004</u>
DRAWN			
CHECKED			
APPROVED			
CREW	POE	SWO 5443(2)	

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SWO5443_BM3.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
BM 111				652.581		652.581	N. 402270.704 E. 2556706.127 CUT BOX AT EAST SIDE OF CONCRETE BASE FOR LIGHT POLE STA 113+35.68 129.94' RT CL I-44 (A001)	
to TP 17	-9.859	-9.858	-9.858	662.440			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 110	-11.505	-11.502	-11.504	673.943			N. 402773.164 E. 2556827.839 RR SPIKE IN SE FACE OF PP STA 114+65.27 370.54' LT CL I-44 (A001)	
to TP 18	2.343	2.341	2.342	671.601			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 19	-10.783	-10.784	-10.784	682.385			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 109	-11.035	-11.032	-11.034	693.418			N. 403320.018 E. 2557343.580 CUT X IN CENTER OF CONC ISLAND STA 270+45.33 58.28' LT CL US-75 (A003)	
to TP 20	-4.746	-4.747	-4.746	698.165			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 108	-8.833	-8.835	-8.834	706.999			N. 403974.229 E. 2557325.324 #3 REBAR W/ CA 7767 PLASTIC CAP STA 276+99.78 63.16' LT CL US-75 (A003)	
to TP 21	-2.169	-2.168	-2.168	709.167			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 107	7.689	7.686	7.687	701.480			N. 404476.975 E. 2557239.457 #5 REBAR WITH 2IN. ALUMINUM CAP STA 282+04.18 138.73' LT CL US-75 (A003)	

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SWO5443_BM3.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
to BM 106	0.220	0.213	0.217	701.263			N. 404555.567 E. 2557267.573 CUT X AT CENTER OF HEADWALL STA 282+82.18 109.02' LT CL US-75 (A003)	
to TP 22	-6.921	-6.918	-6.920	708.183			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 105	1.667	1.668	1.668	706.515			N. 405267.832 E. 2557261.244 CUT X AT CENTER OF HEADWALL STA 290+00.39 89.22' LT CL US-75 (A003)	
to TP 23	-4.473	-4.467	-4.470	710.985			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 104	10.017	10.020	10.019	700.967			N. 405877.591 E. 2557190.540 CUT X AT NW CORNER OF HEADWALL STA 296+23.28 74.83' LT CL US-75 (A003)	
to BM 103	9.225	9.229	9.227	691.740			N. 406141.877 E. 2557003.281 #5 REBAR WITH 2IN. ALUMINUM CAP STA 299+29.43 199.97' LT CL US-75 (A003)	
to TP 24	4.411	4.404	4.407	687.332			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 25	7.984	7.977	7.981	679.352			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 102	9.421	9.418	9.419	669.932			N. 406818.819 E. 2556925.263 CUT X AT WEST EDGE OF CONCRETE BASE FOR LIGHT POLE STA 306+14.69 69.92' LT CL US-75 (A003)	
to TP 26	10.262	10.263	10.263	659.670			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SWO5443_BM3.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
to TP 27	6.558	6.562	6.560	653.110			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 28	9.097	9.099	9.098	644.012			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 101	12.467	12.461	12.464	631.548			N. 407498.372 E. 2556571.824 CUT X AT NE CORNER OF HEADWALL STA 313+75.46 158.98' LT CL US-75 (A003)	
to TP 29	-9.683	-9.691	-9.687	641.235			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to TP 30	4.607	4.609	4.608	636.627			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
to BM 100	1.714	1.709	1.712	634.915			N. 408100.004 E. 2556351.136 CUT X ON WEST SIDE OF HEADWALL STA 320+16.25 151.61' LT CL US-75 (A003)	
to SCP 1A	-2.904	-2.907	-2.906	637.821			TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	

PLS			POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA	
DRAWN			SURVEY DIVISION	
CHECKED			SURVEY DATA SHEET	
APPROVED				
CREW	POE	SWO	5443/21	PROJECT NO. 33788(09) SHEET NO. S005

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SW05443_BM4.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
BM 111				652.581		652.581	N. 402270.704 E. 2556706.127 CUT BOX AT EAST SIDE OF CONCRETE BASE FOR LIGHT POLE STA 113+35.68 129.94' RT CL I-44 (A001)	
to	4.811	4.809	4.807				N. 402277.282 E. 2557167.476 CUT X ON HEADWALL STA 117+97.08 130.61' RT CL I-44 (A001)	
BM 127A				647.774				
to	1.929	1.929	1.929				N. 402288.258 E. 2557285.446 CUT X ON WEST END OF HEADWALL STA 119+15.21 121.49' RT CL I-44 (A001)	
BM 127B				645.845				
to	0.387	0.386	0.386				N. 402254.291 E. 2557296.140 CUT X ON CL HEADWALL STA 119+25.36 155.62' RT CL I-44 (A001)	
BM 127C				645.459				
to	-9.414	-9.414	-9.414				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 4				654.873				
to	5.304	5.304	5.304				N. 402310.301 E. 2557566.829 CUT X AT CENTER OF HEADWALL STA 121+96.83 103.87' RT CL I-44 (A001)	
BM 127D				649.569				
to	-5.918	-5.917	-5.918				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 5				655.486				
to	-2.418	-2.419	-2.418				N. 402330.919 E. 2558178.734 CUT X ON NORTH SIDE OF CONCRETE BASE FOR LIGHT POLE STA 128+09.05 92.87' RT CL I-44 (A001)	
BM 128				657.905				
to	-3.060	-3.064	-3.062				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 6				660.967				
to	-0.960	-0.961	-0.960				N. 402313.101 E. 2558890.728 RR SPIKE IN NORTH FACE OF PP STA 134+96.86 161' RT CL I-44 (A001)	
BM 129				661.927				
to	-6.547	-6.546	-6.546				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 7				668.474				

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SW05443_BM4.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
to	-0.809	-0.811	-0.810				N. 402447.592 E. 2559680.063 CUT X ON TOP OF CURB STA 142+81.01 223.73' RT CL I-44 (A001)	
BM 130				669.284				
to	5.054	5.057	5.055				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 8				664.228				
to	5.324	5.326	5.325				N. 402709.264 E. 2560461.239 CUT X ON TOP OF CURB STA 151+28.13 98.39' RT CL I-44 (A002)	
BM 131				658.903				
to	11.111	11.142	11.127				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 9				647.777				
to	9.755	9.754	9.754				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 10				638.022				
to	6.673	6.673	6.673				N. 402691.458 E. 2560896.515 CUT X ON EAST END OF HEADWALL STA 155+66.54 124.62' RT CL I-44 (A002)	
BM 132				631.349				
to	5.002	4.992	4.997				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 11				626.352				
to	-1.812	-1.811	-1.812				N. 402657.907 E. 2561653.679 CUT X ON NORTH EDGE OF LAMP POLE STA 165+22.25 171.91' RT CL I-44 (A002)	
BM 133				628.164				
to	-8.620	-8.620	-8.620				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 12				636.784				
to	-11.470	-11.470	-11.470				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 13				648.204				
to	-8.190	-8.190	-8.190				N. 402962.936 E. 2560885.957 BIN ALLUM CAP BENNETT CONTROL STA 155+60.19 146.99' LT CL I-44 (A002)	
T-72-1706				656.394		656.42		

CHECK LEVELS THROUGH PROJECT CONTROL							BENCH MARK LIST	
SW05443_BM5.xlsx							NAVD 88 DATUM	
BM No.	Run 1	Run 2	Mean Diff.	Unadj. Elev.	Adj. Elev.	Published Elev.	BM Description	
BM 111				652.581		652.581	N. 402270.704 E. 2556706.127 CUT BOX AT EAST SIDE OF CONCRETE BASE FOR LIGHT POLE STA 113+35.68 129.94' RT CL I-44 (A001)	
to	-5.830	-5.827	-5.829				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 1				658.410				
to	-11.141	-11.143	-11.142				N. 402253.939 E. 2556122.416 BIN ALLUM CAP BENNETT CONTROL STA 107+51.84 137.52' RT CL I-44 (A001)	
T-72-1704				669.552		669.59		
to	-0.397	-0.401	-0.399				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 2				669.951				
to	4.178	4.179	4.177				N. 402121.990 E. 2555421.017 CUT X ON TOP OF CURB STA 100+47.34 256.28' RT CL I-44 (A001)	
BM 125				665.773				
to	-1.935	-1.935	-1.935				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
SCP 11				667.708				
to	-1.065	-1.059	-1.062				N. 402187.090 E. 2554725.367 CUT X ON TOP OF CURB STA 93+53.11 177.41' RT CL I-44 (A001)	
BM 124				668.770				
to	-2.319	-2.319	-2.319				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 3				671.089				
to	-3.756	-3.756	-3.756				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 4				674.845				
to	-0.435	-0.436	-0.435				TEMPORARY TURN POINT - LOCATION NOT DESCRIBED	
TP 5				675.781				
to	5.587	5.586	5.587				N. 402224.468 E. 2553026.547 BIN ALLUM CAP BENNETT CONTROL Offsite	
T-72-1604				669.694		669.70		

PLS			POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA	
DRAWN			SURVEY DIVISION	
CHECKED			SURVEY DATA SHEET	
APPROVED				
CREW	POE	SWO	5443/21	PROJECT NO. 33788(09) SHEET NO. S006

Table with columns: POINT NAME, NORTHING, EASTING, ELEVATION, DESCRIPTION. Contains survey data for points 1-1112.

Table with columns: POINT NAME, NORTHING, EASTING, ELEVATION, DESCRIPTION. Contains survey data for points 1005-1112.

Table with columns: POINT NAME, NORTHING, EASTING, ELEVATION, DESCRIPTION. Contains survey data for points 1113-1222.

Table with columns: POINT NAME, NORTHING, EASTING, ELEVATION, DESCRIPTION. Contains survey data for points 1223-1330.

Table with columns: POINT NAME, NORTHING, EASTING, ELEVATION, DESCRIPTION. Contains survey data for points 1331-1430.

POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA SURVEY DIVISION SURVEY DATA SHEET. Includes fields for PLS, DRAWN, CHECKED, APPROVED, CREW, POE, SWO, PROJECT NO., SHEET NO., and SDO7.

POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION	POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION	POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION	POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION	POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION
1431	398815.417	2557594.989	761.404	EC	1539	402627.789	2561575.965	629.213	NG	1650	402912.978	2561551.539	644.969	LINE	1758	402852.945	2561749.417	646.987	LINE	1866	403027.121	2563832.891	632.392	RIP
1432	398863.231	2557592.104	760.393	EC	1540	402632.135	2561581.339	630.868	NG	1651	402913.184	2561551.530	645.108	LINE	1759	402852.821	2561749.415	648.094	LINE	1867	402991.373	2563832.175	632.647	TM
1433	398864.070	2557594.281	759.648	EL	1541	402637.598	2561574.757	629.903	LINE	1652	402916.539	2561551.464	645.147	LINE	1760	402849.730	2561749.535	648.134	LINE	1868	402971.068	2563834.672	632.530	EC1
1434	398863.950	2557596.048	759.745	EL	1542	402652.641	2561574.699	629.895	LINE RC	1653	402916.994	2561551.483	644.971	LINE	1761	402849.483	2561749.588	647.043	LINE	1869	402974.828	2563828.628	632.128	GB
1435	398863.998	2557597.913	760.350	EC1	1543	402635.096	2561586.719	631.916	NG	1654	402918.404	2561551.467	644.961	LINE	1762	402845.306	2561749.507	647.026	LINE	1870	402976.968	2563846.223	632.565	EC
1436	398911.190	2557595.259	759.187	EC1	1544	402651.637	2561584.187	631.537	NG	1655	402794.392	2561553.667	645.382	LINE ST	1763	402845.236	2561749.601	647.953	LINE	1871	402927.923	2563846.833	632.642	EC
1437	398911.106	2557593.444	758.470	EL	1545	402665.211	2561576.995	630.435	NG	1656	402797.136	2561607.265	646.138	LINE	1764	402842.071	2561749.664	647.971	LINE	1872	402927.669	2563835.614	632.523	EC1
1438	398910.847	2557591.379	758.532	EL	1546	402673.275	2561556.199	629.229	NG	1657	402799.238	2561650.798	646.229	LINE	1765	402841.950	2561749.719	647.075	LINE	1873	402927.570	2563828.531	632.546	GB
1439	398910.676	2557589.651	759.272	EC	1547	402718.130	2561483.323	626.345	FLUME ST	1658	402914.200	2561551.145	645.408	LINE ST	1766	402837.722	2561749.649	647.050	LINE	1874	402908.741	2563833.032	632.473	LP
1440	398963.472	2557592.679	758.132	EC1	1548	402719.294	2561483.644	625.735	FLUME ST	1659	402914.788	2561598.378	646.042	LINE	1767	402837.664	2561749.744	647.788	LINE	1875	402880.158	2563828.934	632.550	GB
1441	398963.588	2557590.705	757.461	EL	1549	402720.650	2561483.573	625.676	FLUME	1660	402916.220	2561647.590	646.374	LINE	1768	402834.415	2561749.744	647.799	LINE	1876	402881.250	2563836.508	632.727	EC1
1442	398963.350	2557590.404	757.501	EL	1550	402722.543	2561483.627	626.284	FLUME	1661	402906.496	2561649.642	635.259	EL PIER	1769	402834.341	2561749.868	647.066	LINE	1877	402880.824	2563847.802	632.895	EC
1443	398963.647	2557588.582	757.422	EL	1551	402733.903	2561277.547	640.843	FLUME ST	1662	402874.689	2561650.492	635.265	EL PIER	1770	402830.092	2561749.765	647.058	LINE	1878	402835.058	2563848.555	632.999	EC
1444	398963.298	2557586.948	758.184	EC	1552	402734.973	2561277.776	640.297	FLUME	1663	402844.167	2561650.827	635.267	EL PIER	1771	402829.934	2561749.817	647.628	LINE	1879	402836.772	2563837.481	632.891	EC1
1445	399026.421	2557583.490	756.982	EL	1553	402737.371	2561279.538	640.162	FLUME	1664	402808.507	2561651.645	635.269	EL PIER	1772	402826.775	2561749.865	647.621	LINE	1880	402838.457	2563828.827	632.664	TM
1446	399027.238	2557585.556	756.219	EL	1554	402737.786	2561279.791	640.642	FLUME	1665	402909.540	2561749.128	635.261	EL PIER	1773	402826.632	2561749.894	647.041	LINE	1881	402820.937	2563835.799	632.364	GB
1447	399027.042	2557587.450	756.265	EL	1555	402735.366	2561285.875	639.310	FLUME ST	1666	402877.129	2561749.544	635.254	EL PIER	1774	402822.477	2561749.902	647.115	LINE	1882	402777.756	2563837.271	632.149	GB
1448	399027.286	2557589.076	756.925	EC1	1556	402734.298	2561285.905	639.763	FLUME	1667	402847.827	2561750.042	635.254	EL PIER	1775	402822.312	2561749.967	647.488	LINE	1883	402778.714	2563846.298	632.279	EC1
1449	399098.453	2557585.665	755.095	EC1	1557	402732.238	2561285.278	639.787	FLUME	1668	402814.602	2561750.298	635.258	EL PIER	1776	402819.134	2561750.000	647.455	LINE	1884	402778.129	2563857.969	632.360	EC
1450	399098.367	2557583.867	754.442	EL	1558	402731.130	2561285.221	640.307	FLUME	1669	402918.174	2561647.415	646.201	LINE ST	1777	402819.015	2561750.113	647.081	LINE	1885	403026.581	2563872.666	621.517	RIP
1451	399098.016	2557582.287	754.480	EL	1559	402993.073	2560350.660	663.584	NG	1670	402915.122	2561647.561	646.229	LINE	1778	402814.820	2561750.080	647.055	LINE	1886	403026.121	2563774.780	620.177	NG
1452	399097.917	2557580.274	755.144	EC	1560	402554.224	2561679.364	627.956	SSMH	1671	402914.733	2561647.510	646.090	LINE	1779	402814.713	2561750.115	647.303	LINE	1887	402977.840	2563768.440	618.729	NG
1453	399170.137	2557576.595	753.273	EL	1561	402522.056	2561663.126	627.583	WV	1672	402910.786	2561647.569	646.047	LINE	1780	402811.535	2561750.234	647.289	LINE	1888	402951.627	2563764.191	618.028	NG
1454	399170.297	2557578.865	752.351	EC	1562	402519.381	2561654.524	626.956	WM	1673	402910.144	2561647.652	646.408	LINE	1781	402811.379	2561750.338	647.053	LINE	1889	402920.828	2563765.499	617.317	NG
1455	399170.436	2557580.529	752.370	EL	1563	402520.422	2561660.414	627.323	IRR	1674	402907.198	2561647.768	646.349	LINE	1782	402807.349	2561750.320	647.092	LINE	1890	402893.610	2563766.129	617.397	NG
1456	399170.262	2557582.115	752.990	EC1	1564	402543.992	2561664.655	628.933	IRR	1675	402907.017	2561647.743	646.075	LINE	1783	402807.199	2561750.256	647.192	LINE	1891	402861.311	2563765.710	617.339	NG
1457	399218.622	2557574.259	751.741	EL	1565	403125.019	2561672.184	626.528	IRR	1676	402902.769	2561647.762	646.000	LINE	1784	402803.810	2561750.345	647.178	LINE	1892	402840.573	2563766.443	617.829	NG
1458	399218.469	2557576.058	750.872	EL	1566	403262.167	2561672.216	627.610	SSMH	1677	402902.005	2561647.877	646.540	LINE	1785	402912.563	2561750.461	636.511	EL PIER	1893	402812.692	2563777.319	619.277	NG
1459	399218.718	2557578.208	750.833	EL	1567	403271.638	2561677.591	627.073	VAULT	1678	402899.353	2561647.995	646.543	LINE	1786	402880.299	2561749.312	632.899	EL PIER	1894	402803.269	2563774.465	624.354	NG
1460	399218.932	2557580.050	751.564	EC1	1568	402921.082	2561703.829	624.118	CGMP	1679	402898.984	2561647.989	646.052	LINE	1787	402851.213	2561750.296	632.898	EL PIER	1895	402756.177	2563757.674	613.060	NG
1461	399291.510	2557576.100	749.614	EC1	1569	403196.450	2561138.849	667.821	LINE ST LOW WIRE	1680	402895.330	2561648.042	645.999	LINE	1788	402817.839	2561751.244	632.897	EL PIER	1896	402782.013	2563696.329	612.987	NG
1462	399291.989	2557574.744	748.871	EL	1570	403149.045	2561162.060	662.143	LINE LOW WIRE	1681	402896.808	2561648.536	646.933	LINE	1789	402813.366	2561752.526	633.374	EL PIER	1897	402838.768	2563712.377	614.946	NG
1463	399291.448	2557572.375	748.874	EL	1571	403149.045	2561162.060	662.143	LINE LOW WIRE	1682	402891.398	2561648.193	646.704	LINE	1790	402849.167	2561651.927	633.248	EL PIER	1898	402875.665	2563714.050	614.245	NG
1464	399291.273	2557570.523	749.760	EC	1572	403090.105	2561141.192	664.501	LINE LOW WIRE	1683	402891.042	2561648.201	646.040	LINE	1791	402879.644	2561651.386	633.142	EL PIER	1899	402923.566	2563715.410	614.222	NG
1465	399338.756	2557574.430	748.474	EC1	1573	403023.613	2561142.654	677.915	LINE LOW WIRE	1684	402886.782	2561648.198	645.967	LINE	1792	402911.427	2561650.544	634.290	EL PIER	1900	402965.635	2563717.067	615.220	NG
1466	399339.236	2557572.526	747.514	EL	1574	402481.695	2561148.613	651.554	LINE ST	1685	402886.485	2561648.337	646.874	LINE	1793	402920.000	2561657.236	630.581	EC ST	1901	403015.352	2563721.657	614.217	NG
1467	399338.843	2557570.400	747.467	EL	1575	402424.525	2561148.947	650.011	LINE	1686	402883.529	2561648.395	646.837	LINE	1794	402920.126	2561553.939	640.907	EC	1902	403019.955	2563671.876	613.212	NG
1468	399338.756	2557568.843	748.237	EL	1576	403018.110	2561143.570	687.994	LINE ST LOW WIRE	1687	402883.284	2561648.412	646.051	LINE	1795	402918.827	2561551.689	640.966	EC	1903	403019.852	2563686.133	614.672	NG
1469	399388.856	2557571.771	746.756	EC1	1577	402906.593	2561146.154	682.847	LINE LOW WIRE	1688	402878.818	2561648.380	645.978	LINE	1796	402918.597	2561549.607	645.061	LINE ST	1904	402986.867	2563658.136	614.671	NG
1470	399388.620	2557570.124	745.887	EL	1578	402756.240	2561149.331	691.828	LINE LOW WIRE	1689	402881.244	2561649.008	647.307	LINE	1797	402918.730	2561549.739	650.639	LINE	1905	402989.301	2563660.218	615.179	NG
1471	399388.796	2557567.829	745.958	EL	1580	402754.964	2561145.925	676.868	LINE ST LOW WIRE	1690	402875.600	2561648.605	647.045	LINE	179									

POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION	POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION	POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION	POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION
1966	402989.131	2563519.926	614.483	GB	2074	402909.335	2563261.051	617.866	CONC	2182	402350.961	2559145.299	664.441	NG	2312	402981.534	2563920.073	647.837	LINE
1967	403021.964	2563526.746	612.364	GB	2075	402883.811	2563361.928	618.223	CONC	2183	402358.438	2559142.934	664.778	NG	2313	402980.077	2563853.342	648.282	LINE
1968	403039.182	2563502.824	604.389	WE ST	2076	402940.945	2563260.207	617.818	CONC	2184	402359.391	2559147.824	665.722	NG	2314	402979.965	2563845.769	648.390	LINE ST
1969	403008.334	2563484.522	604.337	WE	2077	402853.401	2563463.024	618.585	CONC	2185	402360.230	2559150.034	665.247	NG	2315	402979.128	2563808.768	648.780	LINE
1970	402979.936	2563468.621	604.252	WE	2078	402912.098	2563361.611	618.176	CONC	2186	402350.355	2559148.363	665.120	NG	2316	402978.197	2563811.331	649.169	LINE
1971	402963.131	2563495.835	604.059	WE	2079	402942.643	2563360.497	618.112	CONC	2187	402351.013	2559153.615	665.259	NG	2317	402982.438	2563760.113	651.202	LP
1973	402941.584	2563496.223	604.102	WE	2080	402963.008	2563360.099	618.065	CONC	2188	402357.020	2559151.642	665.246	LINE ST	2318	402983.640	2563762.064	648.907	EOC ST
1974	402959.620	2563517.891	611.133	NG	2081	402884.666	2563461.790	618.515	CONC	2189	402356.239	2559148.409	665.574	LINE	2319	402983.574	2563762.019	644.453	EOC
1975	402966.169	2563552.959	608.768	NG	2082	402912.402	2563461.006	618.439	CONC	2190	402371.504	2559144.873	664.995	LINE RC	2320	402974.931	2563762.266	643.504	EOC
1976	402918.102	2563553.269	608.113	NG	2083	402944.259	2563460.272	618.345	CONC	2191	402368.509	2559145.866	665.011	EC ST	2321	402970.346	2563762.359	643.560	EOC
1977	402887.594	2563554.640	610.740	NG	2084	402964.385	2563459.730	618.281	CONC	2192	402369.171	2559148.662	664.903	EC	2322	402962.376	2563762.542	644.057	EOC
1978	402856.351	2563547.626	613.509	NG	2085	402745.667	2561618.311	630.640	NG	2193	402361.918	2559150.282	664.976	EC RC	2323	402955.404	2563762.643	642.568	EOC
1979	402836.880	2563550.319	614.251	NG	2086	402667.105	2561487.672	624.582	FLST ST	2195	402365.804	2559147.933	664.851	SIGN	2324	402918.366	2563763.469	642.594	EOC
1980	402809.782	2563549.317	613.922	CP 60D	2087	402665.868	2561490.784	626.090	WINGW ST	2196	402378.896	2559476.189	662.621	LINE ST	2325	402911.046	2563763.739	644.288	EOC
1981	402815.514	2563549.693	614.793	NG	2088	402665.497	2561490.349	626.049	WINGW	2197	402404.574	2559479.060	664.636	SIGN	2326	402911.125	2563763.657	650.453	EOC
1982	402826.350	2563591.470	614.609	NG	2089	402666.991	2561488.621	626.314	WINGW RC	2198	402425.852	2559467.663	665.760	LINE	2327	402902.578	2563763.688	650.337	EOC ST
1983	402871.322	2563596.800	613.284	NG	2090	402667.298	2561489.411	627.130	HDWL ST	2199	402427.961	2559482.305	665.633	LINE	2328	402902.518	2563763.681	644.355	EOC
1984	402895.265	2563587.432	613.449	NG	2091	402667.380	2561485.870	627.127	HDWL RC	2200	402594.134	2560050.407	656.984	LP	2329	402895.113	2563763.861	642.911	EOC
1985	402896.962	2563612.859	613.093	GB ST	2092	402667.893	2561485.917	627.082	HDWL RC	2201	402580.662	2560052.296	656.258	EP ST	2330	402858.837	2563764.681	644.773	EOC
1986	402932.768	2563610.320	613.342	GB	2093	402666.197	2561485.182	626.134	WINGW ST	2202	402593.355	2560050.350	657.102	EP	2331	402851.556	2563764.775	644.242	EOC
1987	402925.233	2563590.966	610.044	NG	2094	402667.106	2561485.974	626.319	WINGW	2203	402613.377	2560083.545	656.915	EP	2332	402843.140	2563764.827	643.465	EOC
1988	403030.045	2563541.154	614.267	NG	2095	402667.107	2561486.811	626.337	WINGW	2204	402623.096	2560129.891	656.589	EP	2333	402838.123	2563765.067	643.996	EOC
1989	402971.691	2563657.180	627.073	CONC	2096	402665.519	2561485.170	626.013	WINGW	2205	402636.556	2560170.843	656.359	EP	2334	402830.115	2563765.291	644.547	EOC
1990	402951.713	2563657.753	627.581	CONC	2097	402422.610	2561351.500	637.175	WALL ST	2206	402613.988	2560171.371	654.751	EP	2335	402830.138	2563765.213	649.022	EOC
1991	402919.963	2563658.408	629.454	CONC	2098	402424.552	2561355.031	637.144	WALL	2207	402625.804	2560163.883	655.448	LINE ST	2336	402985.531	2563853.356	648.062	EOC ST
1992	402892.404	2563659.284	630.732	CONC	2099	402424.613	2561355.045	637.854	WALL	2208	402625.389	2560160.411	655.519	LINE	2337	402985.441	2563853.354	643.772	EOC
1993	402860.866	2563660.315	620.761	CONC	2100	402426.640	2561358.365	638.105	WALL	2209	402645.674	2560157.680	658.351	LINE RC	2338	402977.157	2563853.562	642.530	EOC
1994	402840.052	2563660.670	631.169	CONC	2101	402426.603	2561358.480	638.516	WALL	2210	402639.664	2560160.675	656.818	SIGN	2339	402973.453	2563853.633	642.577	EOC
1995	402971.518	2563557.922	619.214	CONC	2102	402432.163	2561369.555	638.495	WALL	2211	402631.461	2560161.449	655.954	SIGN	2340	402964.021	2563853.580	643.213	EOC
1996	402950.845	2563558.538	619.213	CONC	2103	402432.205	2561369.785	639.150	WALL	2212	402652.461	2560174.018	658.988	POST	2341	402956.854	2563854.027	641.773	EOC
1997	402918.741	2563559.479	619.212	CONC	2104	402436.116	2561379.533	639.207	WALL	2213	402654.260	2560209.011	658.820	POST	2342	402953.416	2563854.104	641.765	EOC
1998	402891.159	2563560.828	619.211	CONC	2105	402438.748	2561388.471	639.231	WALL	2214	402655.474	2560210.365	658.703	EC ST	2343	402920.367	2563854.967	641.849	EOC
1999	402859.074	2563561.010	619.209	CONC	2106	402439.168	2561389.555	639.911	WALL	2215	402655.318	2560207.876	658.689	EC	2344	402913.364	2563855.217	643.366	EOC
2000	402838.658	2563560.748	619.208	CONC	2107	402442.455	2561404.924	639.861	WALL	2216	402652.439	2560207.854	658.728	EC RC	2345	402913.385	2563855.032	649.507	EOC
2001	402836.673	2563462.319	619.208	CONC	2108	402445.986	2561431.694	639.916	WALL	2217	402651.342	2560175.522	659.189	EC ST	2346	402904.477	2563855.252	649.489	EOC ST
2002	402856.746	2563461.981	619.210	CONC	2109	402446.014	2561432.267	639.280	WALL	2218	402651.253	2560173.019	659.198	EC	2347	402904.459	2563855.251	643.392	EOC
2003	402888.635	2563461.574	619.209	CONC	2110	402447.526	2561450.833	639.243	WALL	2219	402654.229	2560172.907	659.169	EC RC	2348	402897.459	2563855.353	641.920	EOC
2004	402916.242	2563461.099	619.208	CONC	2111	402447.526	2561451.303	638.576	WALL	2220	402643.433	2560213.527	656.085	EP ST	2349	402890.835	2563855.087	641.905	EOC
2005	402833.525	2563362.598	619.199	CONC	2112	402448.191	2561461.268	638.593	WALL	2221	402668.285	2560228.283	656.995	EP	2350	402853.069	2563856.147	643.161	EOC
2006	402854.010	2563362.280	619.198	CONC	2113	402448.157	2561461.589	637.941	WALL	2222	402679.077	2560251.127	657.307	EP	2351	402845.277	2563856.243	642.559	EOC
2007	402947.921	2563460.957	619.206	CONC	2114	402449.372	2561476.026	637.917	WALL	2223	402685.401	2560279.745	657.501	EP	2352	402840.508	2563856.433	642.653	EOC
2008	402886.112	2563361.660	619.259	CONC	2115	402449.339	2561476.346	637.319	WALL	2224	402686.457	2560311.666	657.024	EL	2353	402832.221	2563856.603	643.672	EOC
2009	402851.763	2563261.894	619.182	CONC	2116	402449.994	2561490.032	637.307	WALL	2225	402685.282	2560312.352	656.941	EP	2354	402832.557	2563856.574	648.075	EOC
2010	402911.722	2563361.544	619.196	CONC	2117	402449.944	2561490.341	636.623	WALL	2226	402685.319	2560372.657	655.895	EP	2355	402992.535	2563932.085	629.634	EL
2011	402967.510	2563460.920	619.207	CONC	2118	402450.714	2561507.720	636.623	WALL	2227	402703.919	2560372.053	659.517	SIGN	2356	402992.663	2563932.113	629.314	EL
2012	402883.682	2563261.317	619.191	CONC	2119	402450.609	2561507.946	635.951	WALL	2228	402703.593	2560460.999	659.257	SIGN	2357	402985.392	2564054.777	646.531	LINE ST
2013	402849.280	2563161.305	619.184	CONC	2120	402450.684	2561512.372	635.991	WALL	2250	402917.884	2561647.404	646.165	EC ST	2358	402985.315	2564051.798	646.490	LINE
2014	402945.199	2563360.855	619.201	CONC	2121	402450.479	2561512.633	635.324	WALL	2251	402918.342	2561647.217	641.429	EC	2359	402981.024	2564051.817	646.462	LINE
2015	402844.583	2562960.682	633.847	CONC	2122	402450.301	2561515.566	635.309	WALL	2252	402910.979	2561647.528	639.640	EC	2360	402921.033	2564055.630	646.632	LINE ST
2016	402878.142	2563060.596	633.814	CONC	2123	402450.210	2561516.003	634.595	WALL	2253	402874.905	2561648.512	639.471	EC	2361	402921.075	2564052.621	646.429	LINE
2017	402908.681	2563160.564	630.743	CONC	2124	402449.901	2561519.526	633.977	W										

POINT NAME	NORTHING	EASTING	ELEVATION	DESCRIPTION
50006	401849.268	2558014.555	642.791	lpf 3/8
50007	403076.110	2557406.567	685.524	lps 5/8 w/cap
50008	401246.501	2557440.749	685.645	lps 5/8 w/cap
50009	402596.067	2560919.287	629.814	Tcp60d
50010	401576.127	2557986.448	654.141	lps 5/8 w/cap
50011	401640.599	2557968.304	646.860	Pkf sack ls 1703
50012	401678.387	2557844.633	646.635	Pks
50013	401517.154	2557758.463	654.635	Tcp 60d
50014	401468.584	2558063.864	662.280	lps 5/8 w/cap
50015	401514.737	2558185.907	656.204	lps 3/8 w/cap
50016	401376.562	2558056.865	662.521	Tcp 60d
50017	401200.999	2558124.356	661.786	Pks
50018	401193.485	2558253.800	658.509	Tcp 60e
50019	401143.306	2558370.666	662.825	Tcp 60e
50020	401232.952	2557953.005	666.916	lps 5/8 w/cap
50021	401690.655	2557967.431	644.605	lpf 3/8
50022	402073.035	2558389.165	646.958	Pks
50023	401856.237	2558396.094	641.393	Tcp 60d
50024	401715.063	2558315.764	638.981	Tcp 60d
50025	401604.769	2558278.139	628.082	Tcp 60d
50026	401674.660	2558264.537	628.033	Tcp 60d
50027	401527.225	2558299.114	624.608	Pks
50028	401587.778	2558362.190	630.405	Tcp 60d
50029	401523.544	2558413.299	628.990	Tcp 60d
50030	401466.343	2558355.935	633.866	Tcp 60d
50031	401640.116	2558445.676	628.900	Tcp 60d
50032	401633.802	2558556.241	623.523	Tcp 60d
50400	401955.474	2558211.102	634.520	lpf 3/8 sack pls 1763

Project Name: 203192 I-44_US75
 Description:
 Horizontal Alignment Name: **CRL I-44 SURVEY (A001)**
 Description:
 Style: Default

STATION	NORTHING	EASTING
Element: Linear		
P.O.B. STA. (305)	120+52.12	402411.8845 2557420.4360
P.C. STA. (377)	130+35.62	402427.3327 2558403.8142
Tangent Direction: N 89°06'00.00" E		
Tangent Length: 983.50		
Element: Circular		
P.C. STA. (377)	130+35.62	402427.3327 2558403.8142
P.I. STA. ()	133+88.01	402432.8677 2558756.1570
C.C. STA. (379)	405291.7692	2558358.8161
P.T. STA. (378)	137+36.87	402523.6263 2559096.6552
Radius: 2864.79		
Delta: 14°01'29.98" Left		
Degree of Curvature(Arc): 2°00'00.00"		
Length: 701.25		
Tangent: 352.39		
Chord: 699.50		
Middle Ordinate: 21.43		
External: 21.59		
Tangent Direction: N 89°06'00.00" E		
Radial Direction: S 0°54'00.00" E		
Chord Direction: N 82°05'15.01" E		
Radial Direction: S 14°55'29.98" E		
Tangent Direction: N 75°04'30.02" E		

Project Name: 203192 I-44_US75
 Description:
 Horizontal Alignment Name: **CRL RAMP SE (A005)**
 Description:
 Style: Default

STATION	NORTHING	EASTING
Element: Circular		
P.C. STA. (315)	250+18.16	401294.8811 2557522.2327
P.I. STA. ()	253+82.15	401658.5020 2557538.6120
C.C. STA. (317)	401251.9099	2558476.1954
P.T. STA. (316)	257+13.67	401918.9636 2557792.8721
Radius: 954.93		
Delta: 41°43'50.11" Right		
Degree of Curvature(Arc): 5°59'59.99"		
Length: 695.51		
Tangent: 363.99		
Chord: 680.24		
Middle Ordinate: 62.62		
External: 67.02		
Tangent Direction: N 2°34'44.89" E		
Radial Direction: S 87°25'15.11" E		
Chord Direction: N 23°26'39.95" E		
Radial Direction: S 45°41'25.00" E		
Tangent Direction: N 44°18'35.00" E		
Element: Linear		
P.T. STA. (316)	257+13.67	401918.9636 2557792.8721
P.C. STA. (319)	260+36.72	402150.1299 2558018.5344
Tangent Direction: N 44°18'35.00" E		
Tangent Length: 323.05		

Project Name: 203192 I-44_US75
 Description:
 Horizontal Alignment Name: **CL S.Frontage Rd. (A012)**
 Description:
 Style: Default

STATION	NORTHING	EASTING
Element: Linear		
P.O.B. STA. (325)	121+97.95	401811.1813 2557432.7155
P.C. STA. (326)	125+65.90	401816.9608 2557800.6173
Tangent Direction: N 89°06'00.00" E		
Tangent Length: 367.95		
Element: Circular		
P.C. STA. (326)	125+65.90	401816.9608 2557800.6173
P.I. STA. ()	127+69.95	401820.1659 2558004.6417
C.C. STA. (328)	402226.1703	2557794.1890
P.T. STA. (327)	129+44.47	401985.0358 2558124.8669
Radius: 409.26		
Delta: 53°00'00.00" Left		
Degree of Curvature(Arc): 13°59'59.45"		
Length: 378.58		
Tangent: 204.05		
Chord: 365.22		
Middle Ordinate: 43.00		
External: 48.05		
Tangent Direction: N 89°06'00.00" E		
Radial Direction: S 0°54'00.00" E		
Chord Direction: N 62°36'00.00" E		
Radial Direction: S 53°54'00.00" E		
Tangent Direction: N 36°06'00.00" E		
Element: Linear		
P.T. STA. (316)	257+13.67	401918.9636 2557792.8721
P.C. STA. (319)	260+36.72	402150.1299 2558018.5344
Tangent Direction: N 44°18'35.00" E		
Tangent Length: 323.05		

Project Name: 203192 I-44_US75
 Description:
 Horizontal Alignment Name: **CL S.Frontage Rd. (A012)**
 Description:
 Style: Default

STATION	NORTHING	EASTING
Element: Circular		
P.C. STA. (333)	A 142+85.57	402465.9513 2559319.0705
P.I. STA. ()	A 143+47.44	402481.8870 2559378.8563
C.C. STA. (335)	402158.3799	2559401.0527
P.T. STA. (334)	A 144+07.79	402474.2662 2559440.2584
Radius: 318.31		
Delta: 22°00'00.00" Right		
Degree of Curvature(Arc): 17°59'59.98"		
Length: 122.22		
Tangent: 61.87		
Chord: 121.47		
Middle Ordinate: 5.85		
External: 5.96		
Tangent Direction: N 75°04'30.00" E		
Radial Direction: S 14°55'30.00" E		
Chord Direction: N 86°04'30.00" E		
Radial Direction: S 7°04'30.00" W		
Tangent Direction: S 82°55'30.00" E		
Element: Linear		
P.T. STA. (334)	A 144+07.79	402474.2662 2559440.2584
P.C. STA. (336)	A 145+37.74	402458.2607 2559569.2170
Tangent Direction: S 82°55'30.00" E		
Tangent Length: 129.95		
Element: Circular		
P.C. STA. (336)	A 145+37.74	402458.2607 2559569.2170
P.I. STA. ()	A 145+99.61	402450.6399 2559630.6190
C.C. STA. (338)	402774.1470	2559608.4227
P.T. STA. (337)	A 146+59.96	402466.5756 2559690.4049
Radius: 318.31		
Delta: 22°00'00.00" Left		
Degree of Curvature(Arc): 17°59'59.98"		
Length: 122.22		
Tangent: 61.87		
Chord: 121.47		
Middle Ordinate: 5.85		
External: 5.96		
Tangent Direction: S 82°55'30.00" E		
Radial Direction: S 7°04'30.00" W		
Chord Direction: N 86°04'30.00" E		
Radial Direction: S 14°55'30.00" E		
Tangent Direction: N 75°04'30.00" E		
Element: Linear		
P.T. STA. (337)	A 146+59.96	402466.5756 2559690.4049
P.C. STA. (339)	A 147+39.02	402486.9379 2559766.7977
Tangent Direction: N 75°04'30.00" E		
Tangent Length: 79.06		
Element: Circular		
P.C. STA. (339)	A 147+39.02	402486.9379 2559766.7977
P.I. STA. ()	A 147+89.44	402499.9226 2559815.5122
C.C. STA. (341)	402794.5093	2559684.8155
P.T. STA. (340)	A 148+39.02	402527.3254 2559857.8300
Radius: 318.31		
Delta: 18°00'00.00" Left		
Degree of Curvature(Arc): 17°59'59.98"		
Length: 100.00		
Tangent: 50.42		
Chord: 99.59		
Middle Ordinate: 3.92		
External: 3.97		
Tangent Direction: N 75°04'30.00" E		
Radial Direction: S 14°55'30.00" E		
Chord Direction: N 66°04'30.00" E		
Radial Direction: S 32°55'30.00" E		
Tangent Direction: N 57°04'30.00" E		
Element: Linear		
P.T. STA. (340)	A 148+39.02	402527.3254 2559857.8300
P.C. STA. (342)	A 150+24.29	402628.0272 2560013.3424
Tangent Direction: N 57°04'30.00" E		
Tangent Length: 185.27		

Project Name: 203192 I-44_US75
 Description:
 Horizontal Alignment Name: **U.S.75**
 Description: Continued
 Style: Default

STATION	NORTHING	EASTING
Element: Linear		
P.O.B. STA. (301)	170+00.00	393277.9729 2557607.1560
P.O.E. STA. (305)	261+35.82	402411.8845 2557420.4360
Tangent Direction: N 1°10'15.98" W		
Tangent Length: 9135.82		

Project Name: 203192 I-44_US75
 Description:
 Horizontal Alignment Name: **CRL I-44 SURVEY (A002)**
 Description:
 Style: Default

STATION	NORTHING	EASTING
Element: Linear		
P.T. STA. (378)	137+36.87	402523.6263 2559096.6552
P.O.E. STA. (380)	145+26.37	402726.9655 2559859.5205
Tangent Direction: N 75°04'30.02" E		
Tangent Length: 789.50		
Element: Circular		
P.C. STA. (381)	145+26.37	402714.5193 2559862.8669
P.I. STA. ()	148+79.08	402805.3603 2560203.6740
C.C. STA. (383)	399946.3764	2560600.7060
P.T. STA. (382)	152+28.25	402810.8228 2560556.3377
Radius: 2864.79		
Delta: 14°02'15.34" Right		
Degree of Curvature(Arc): 2°00'00.00"		
Length: 701.88		
Tangent: 352.71		
Chord: 700.13		
Middle Ordinate: 21.47		
External: 21.63		
Tangent Direction: N 75°04'30.02" E		
Radial Direction: S 14°55'29.98" E		
Chord Direction: N 82°05'37.69" E		
Radial Direction: S 0°53'14.64" E		
Tangent Direction: N 89°06'45.36" E		
Element: Linear		
P.T. STA. (382)	152+28.25	402810.8228 2560556.3377
P.I. STA. (9011)	159+14.76	402821.4550 2561242.7650
Tangent Direction: N 89°06'45.37" E		
Tangent Length: 686.51		
Element: Linear		
P.I. STA. (9011)	159+14.76	402821.4550 2561242.7650
P.I. STA. (384)	164+31.66	402832.0121 2561759.5570
Tangent Direction: N 88°49'46.97" E		
Tangent Length: 516.90		
Element: Linear		
P.I. STA. (384)	164+31.66	402832.0121 2561759.5570
P.O.E. STA. (393)	188+55.71	402881.5205 2564183.0992
Tangent Direction: N 88°49'46.98" E		
Tangent Length: 2424.05		

Project Name: 203192 I-44_US75
 Description:
 Horizontal Alignment Name: **CRL I-44 SURVEY (A002)**
 Description:
 Style: Default

STATION	NORTHING	EASTING
Element: Circular		
P.C. STA. (319)	260+36.72	402150.1299 2558018.5344
P.I. STA. ()	262+32.76	402290.4143 2558155.4788
C.C. STA. (321)	401749.8963	2558428.5298
P.T. STA. (320)	264+14.50	402317.4015 2558349.6569
Radius: 572.96		
Delta: 37°46'40.42" Right		
Degree of Curvature(Arc): 9°59'59.86"		
Length: 377.78		
Tangent: 196.04		
Chord: 370.97		
Middle Ordinate: 30.86		
External: 32.61		
Tangent Direction: N 44°18'35.00" E		
Radial Direction: S 45°41'25.00" E		
Chord Direction: N 63°11'55.21" E		
Radial Direction: S 7°54'44.58" E		
Tangent Direction: N 82°05'15.42" E		
Element: Linear		
P.T. STA. (320)	264+14.50	402317.4015 2558349.6569
P.C. STA. (322)	268+33.02	402375.0145 2558764.1925
Tangent Direction: N 82°05'15.37" E		
Tangent Length: 418.52		
Element: Circular		
P.C. STA. (322)	268+33.02	402375.0145 2558764.1925
P.I. STA. ()	270+13.46	402399.8531 2558942.9107
C.C. STA. (324)	405291.7692	2558358.8161
P.T. STA. (323)	271+93.44	402446.3252 2559117.2595
Radius: 2944.79		
Delta: 7°00'45.35" Left		
Degree of Curvature(Arc): 1°56'44.40"		
Length: 360.42		
Tangent: 180.44		
Chord: 360.20		
Middle Ordinate: 5.51		
External: 5.52		
Tangent Direction: N 82°05'15.37" E		
Radial Direction: S 7°54'44.63" E		
Chord Direction: N 78°34'52.69" E		
Radial Direction: S 14°55'29.98" E		
Tangent Direction: N 75°04'30.02" E		
Element: Linear		
P.T. STA. (323)	271+93.44	402446.3252 2559117.2595
P.O.E. STA. (325)	274+14.50	402474.2662 2559440.2584
Tangent Direction: N 75°04'30.02" E		
Tangent Length: 2424.05		

Project Name: 203192 I-44_US75
 Description:
 Horizontal Alignment Name: **CL S.Frontage Rd. (A012)**
 Description:
 Style: Default

STATION	NORTHING	EASTING
Element: Linear		
P.T. STA. (327)	129+44.47	401985.0358 2558124.8669
P.C. STA. (329)	130+59.37	402077.8739 2558192.5656
Tangent Direction: N 36°06'00.00" E		
Tangent Length: 114.90		
Element: Circular		
P.C. STA. (329)	130+59.37	402077.8739 2558192.5656
P.I. STA. ()	132+92.10	402265.9155 2558329.6879
C.C. STA. (331)	401740.2879	2558655.5115
P.T. STA. (330)	135+01.50	402305.0776 2558559.0970
Radius: 572.96		
Delta: 44°12'45.00" Right		
Degree of Curvature(Arc): 9°59'59.86"		
Length: 442.13		
Tangent: 232.73		
Chord: 431.24		
Middle Ordinate: 42.12		
External: 45.46		
Tangent Direction: N 36°06'00.00" E		
Radial Direction: S 53°54'00.00" E		
Chord Direction: N 58°12'22.50" E		
Radial Direction: S 9°41'15.00" E		
Tangent Direction: N 80°18'45.00" E		
Element: Linear		
P.T. STA. (330)	135+01.50	402305.0776 2558559.0970
EQUATION BACK STA.: ()		

Project Name: 203192 I-44_US75

Description:
Horizontal Alignment Name: **CL S.Frontage Rd. (A012)**
Description: Continued
Style: Default
STATION NORTHING EASTING

Element: Circular
P.C. STA. (342) A 150+24.29 402628.0272 2560013.3424
P.I. STA. () A 151+88.79 402717.4377 2560151.4180
C.C. STA. (344) 402147.0944 2560324.7696
P.T. STA. (343) A 153+44.67 402719.9857 2560315.8949
Radius: 572.96
Delta: 32°02'15.00" Right
Degree of Curvature(Arc): 9°59'59.86"
Length: 320.38
Tangent: 164.50
Chord: 316.22
Middle Ordinate: 22.25
External: 23.15
Tangent Direction: N 57°04'30.00" E
Radial Direction: S 32°55'30.00" E
Chord Direction: N 73°05'37.50" E
Radial Direction: S 0°53'15.00" E
Tangent Direction: N 89°06'45.00" E

Element: Linear
P.T. STA. (343) A 153+44.67 402719.9857 2560315.8949
P.C. STA. (345) A 154+81.37 402722.1030 2560452.5785
Tangent Direction: N 89°06'45.00" E
Tangent Length: 136.70

Element: Circular
P.C. STA. (345) A 154+81.37 402722.1030 2560452.5785
P.I. STA. () B 155+82.82 402723.5218 2560544.1618
C.C. STA. (347) 402602.7494 2560454.4274
EQUATION BACK STA.: () A 154+81.37 402722.1030 2560452.5786
EQUATION AHEAD STA.: () B 154+91.23 402722.1030 2560452.5786
P.T. STA. (346) B 156+47.48 402635.4263 2560569.2357
Radius: 119.37
Delta: 75°00'00.00" Right
Degree of Curvature(Arc): 47°59'57.40"
Length: 156.25
Tangent: 91.59
Chord: 145.33
Middle Ordinate: 24.67
External: 31.09
Tangent Direction: N 89°06'45.00" E
Radial Direction: S 0°53'15.00" E
Chord Direction: S 53°23'15.00" E
Radial Direction: S 74°06'45.00" W
Tangent Direction: S 15°53'15.00" E

Element: Linear
P.T. STA. (346) B 156+47.48 402635.4263 2560569.2357
P.C. STA. (348) B 157+41.99 402544.5293 2560595.1069
Tangent Direction: S 15°53'15.00" E
Tangent Length: 94.51

Element: Circular
P.C. STA. (348) B 157+41.99 402544.5293 2560595.1069
P.I. STA. () B 158+70.87 402420.5674 2560630.3891
C.C. STA. (350) 402588.0993 2560748.1871
P.T. STA. (349) B 159+58.66 402429.3055 2560758.9778
Radius: 159.16
Delta: 78°00'00.00" Left
Degree of Curvature(Arc): 35°59'55.88"
Length: 216.67
Tangent: 128.89
Chord: 200.33
Middle Ordinate: 35.47
External: 45.64
Tangent Direction: S 15°53'15.00" E
Radial Direction: S 74°06'45.00" W
Chord Direction: S 54°53'15.00" E
Radial Direction: S 3°53'15.00" E
Tangent Direction: N 86°06'45.00" E

Element: Linear
P.T. STA. (349) B 159+58.66 402429.3055 2560758.9778
P.C. STA. (351) B 166+56.39 402476.6097 2561455.0984
Tangent Direction: N 86°06'45.00" E
Tangent Length: 697.73

Project Name: 203192 I-44_US75

Description:
Horizontal Alignment Name: **CL S.Frontage Rd. (A012)**
Description: Continued
Style: Default
STATION NORTHING EASTING

Element: Circular
P.C. STA. (351) B 166+56.39 402476.6097 2561455.0984
P.I. STA. () B 168+07.43 402486.8496 2561605.7881
C.C. STA. (355) 402635.4035 2561444.3077
P.T. STA. (353) B 168+98.06 402637.8687 2561603.4487
Radius: 159.16
Delta: 87°00'00.00" Left
Degree of Curvature(Arc): 35°59'55.88"
Length: 241.67
Tangent: 151.04
Chord: 219.12
Middle Ordinate: 43.71
External: 60.26
Tangent Direction: N 86°06'45.00" E
Radial Direction: S 3°53'15.00" E
Chord Direction: N 42°36'45.00" E
Radial Direction: N 89°06'45.00" E
Tangent Direction: N 0°53'15.00" W

Element: Linear
P.T. STA. (353) B 168+98.06 402637.8687 2561603.4487
P.O.E. STA. (357) B 172+77.96 403017.7231 2561597.5643
Tangent Direction: N 0°53'15.00" W
Tangent Length: 379.90

Project Name: 203192 I-44_US75

Description:
Horizontal Alignment Name: **CL N.Frontage Rd. (A015)**
Description:
Style: Default
STATION NORTHING EASTING

Element: Linear
P.O.B. STA. (9004) 132+77.17 402780.4141 2558593.1320
P.C. STA. (365) 143+12.05 402796.4418 2559627.8904
Tangent Direction: N 89°06'45.36" E
Tangent Length: 1034.88

Element: Circular
P.C. STA. (365) 143+12.05 402796.4418 2559627.8904
P.I. STA. () 144+84.73 402799.1162 2559800.5500
C.C. STA. (366) 403751.2572 2559613.1010
P.T. STA. (364) 146+53.72 402862.0881 2559961.3388
Radius: 954.93
Delta: 20°30'00.36" Left
Degree of Curvature(Arc): 5°59'59.99"
Length: 341.67
Tangent: 172.68
Chord: 339.85
Middle Ordinate: 15.24
External: 15.49
Tangent Direction: N 89°06'45.36" E
Radial Direction: S 0°53'14.64" E
Chord Direction: N 78°51'45.18" E
Radial Direction: S 21°23'15.00" E
Tangent Direction: N 88°36'45.00" E

Element: Linear
P.T. STA. (364) 146+53.72 402862.0881 2559961.3388
P.C. STA. (362) 153+07.77 403100.6018 2560570.3450
Tangent Direction: N 68°36'45.00" E
Tangent Length: 654.05

Element: Circular
P.C. STA. (362) 153+07.77 403100.6018 2560570.3450
P.I. STA. () 154+80.45 403163.5743 2560731.1352
C.C. STA. (363) 402211.4327 2560918.5828
P.T. STA. (361) 156+49.44 403166.2482 2560903.7963
Radius: 954.93
Delta: 20°30'01.00" Right
Degree of Curvature(Arc): 5°59'59.99"
Length: 341.67
Tangent: 172.68
Chord: 339.85
Middle Ordinate: 15.24
External: 15.49
Tangent Direction: N 68°36'45.00" E
Radial Direction: S 21°23'15.00" E
Chord Direction: N 78°51'45.50" E
Radial Direction: S 0°53'14.00" E
Tangent Direction: N 88°06'46.00" E

Element: Linear
P.T. STA. (361) 156+49.44 403166.2482 2560903.7963
P.I. STA. (360) 159+83.73 403171.4245 2561238.0494
Tangent Direction: N 89°06'46.00" E
Tangent Length: 334.29

Element: Linear
P.I. STA. (360) 159+83.73 403171.4245 2561238.0494
P.C. STA. (358) 161+81.66 403174.4893 2561435.9573
Tangent Direction: N 89°06'46.00" E
Tangent Length: 197.93

Non-collinear

Element: Circular
P.C. STA. (358) 161+81.66 403174.4893 2561435.9573
P.I. STA. () 163+40.91 403176.9554 2561595.1882
C.C. STA. (359) 403015.3484 2561438.4220
P.T. STA. (357) 164+31.76 403017.7231 2561597.5643
Radius: 159.16
Delta: 90°01'56.60" Right
Degree of Curvature(Arc): 35°59'55.88"
Length: 250.10
Tangent: 159.25
Chord: 225.15
Middle Ordinate: 46.65
External: 65.99
Tangent Direction: N 89°06'45.73" E
Radial Direction: S 0°53'14.27" E
Chord Direction: S 45°52'15.97" E
Radial Direction: S 89°08'42.33" W
Tangent Direction: S 0°51'17.67" E

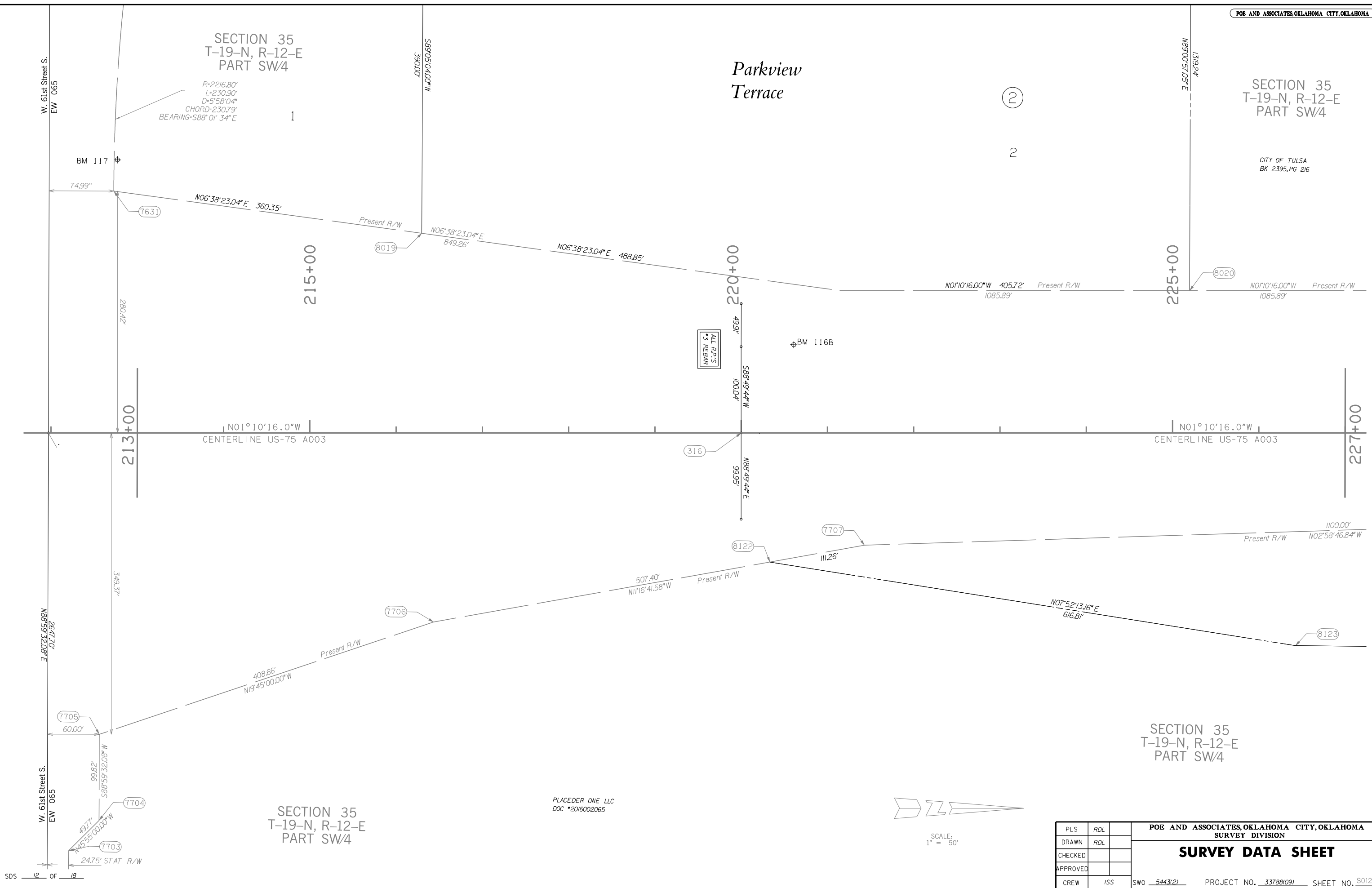
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DRAWN				SURVEY DIVISION	
CHECKED				SURVEY DATA SHEET	
APPROVED					
CREW	POE	SWO	5443/21	PROJECT NO. 33788(09)	SHEET NO. S011

SECTION 35
T-19-N, R-12-E
PART SW/4

SECTION 35
T-19-N, R-12-E
PART SW/4

CITY OF TULSA
BK 2395, PG 216

Parkview
Terrace



$R=2216.80'$
 $L=230.90'$
 $D=5^{\circ}58'04''$
CHORD=230.79'
BEARING= $S88^{\circ}01'34''E$

$N06^{\circ}38'23.04''E$ 360.35'

$N06^{\circ}38'23.04''E$ 849.26'

$N06^{\circ}38'23.04''E$ 488.85'

$N01^{\circ}10'16.00''W$ 405.72'
1085.89'

$N01^{\circ}10'16.00''W$ 1085.89'

$N01^{\circ}10'16.0''W$
CENTERLINE US-75 A003

$N01^{\circ}10'16.0''W$
CENTERLINE US-75 A003

$507.40'$
 $N116^{\circ}41.58''W$

$N07^{\circ}52'13.16''E$
616.81'

$408.66'$
 $N19^{\circ}45'00.00''W$

SECTION 35
T-19-N, R-12-E
PART SW/4

SECTION 35
T-19-N, R-12-E
PART SW/4

PLACEDER ONE LLC
DOC *2016002065



SCALE:
1" = 50'

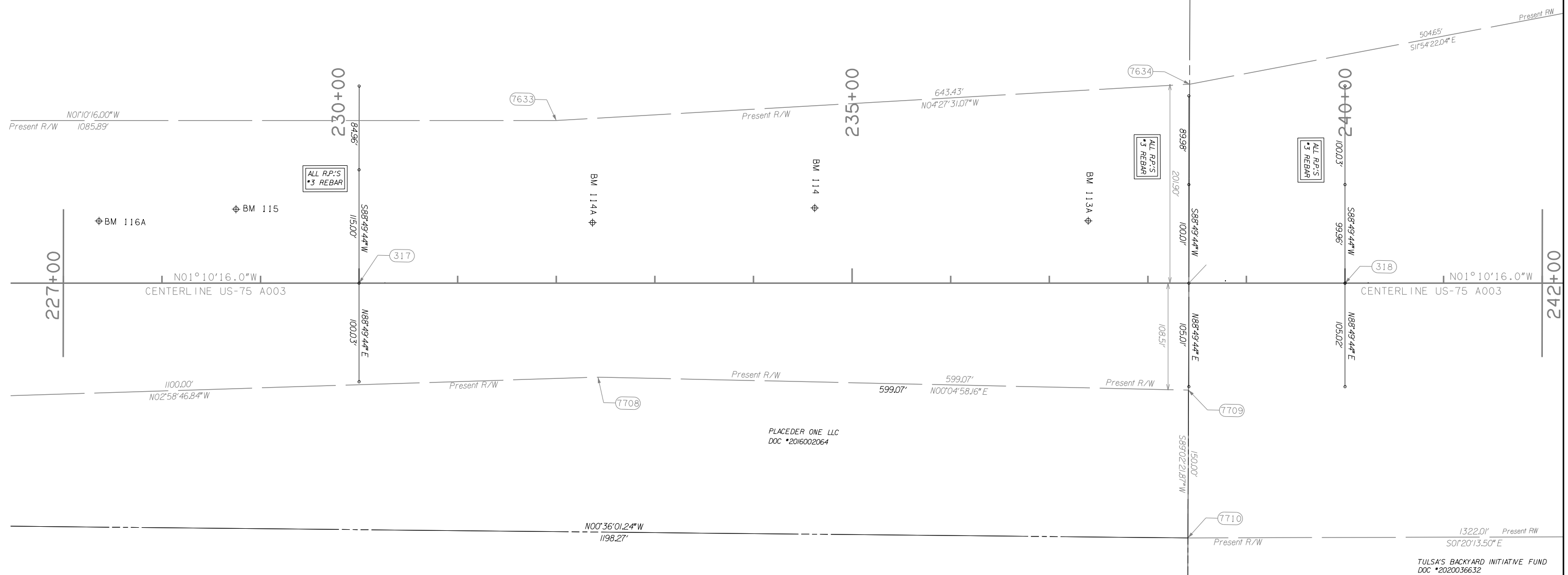
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DRAWN	RDL	SURVEY DIVISION	
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APPROVED			
CREW	ISS		
		SWO 5443/21	PROJECT NO. 33788/091
		SHEET NO. S012	

SECTION 35
T-19-N, R-12-E
PART NW/4

City of Tulsa
Deed Book 2395 Page 216

SECTION 35
T-19-N, R-12-E
PART SW/4

CITY OF TULSA
BK 2395, PG 216

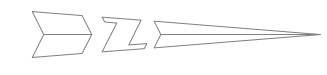


SECTION 35
T-19-N, R-12-E
PART SW/4

PLACEDER ONE LLC
DOC #2016002065

SECTION 35
T-19-N, R-12-E
PART NW/4

TULSA'S BACKYARD INITIATIVE FUND
DOC #2020036632



SCALE:
1" = 50'

PLS	RDL	POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA	
DRAWN	RDL	SURVEY DIVISION	
CHECKED		SURVEY DATA SHEET	
APPROVED			
CREW	ISS		
		SWO 5443/21	PROJECT NO. 33788/09
		SHEET NO. S013	

SECTION 35
T-19-N, R-12-E
PART NW/4

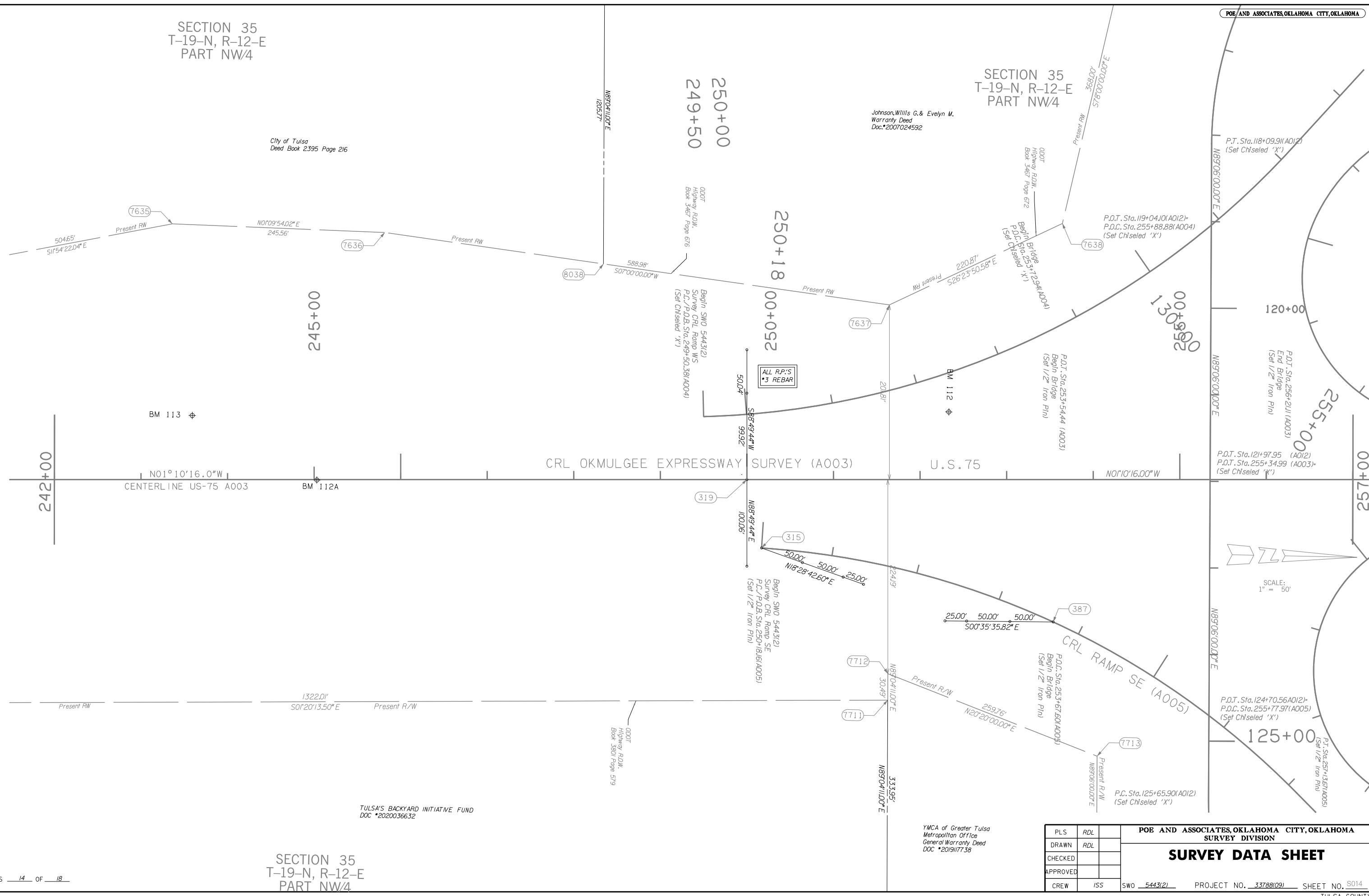
SECTION 35
T-19-N, R-12-E
PART NW/4

City of Tulsa
Deed Book 2395 Page 216

Johnson, Willis G. & Evelyn M.
Warranty Deed
Doc. #2007024592

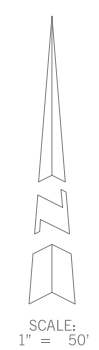
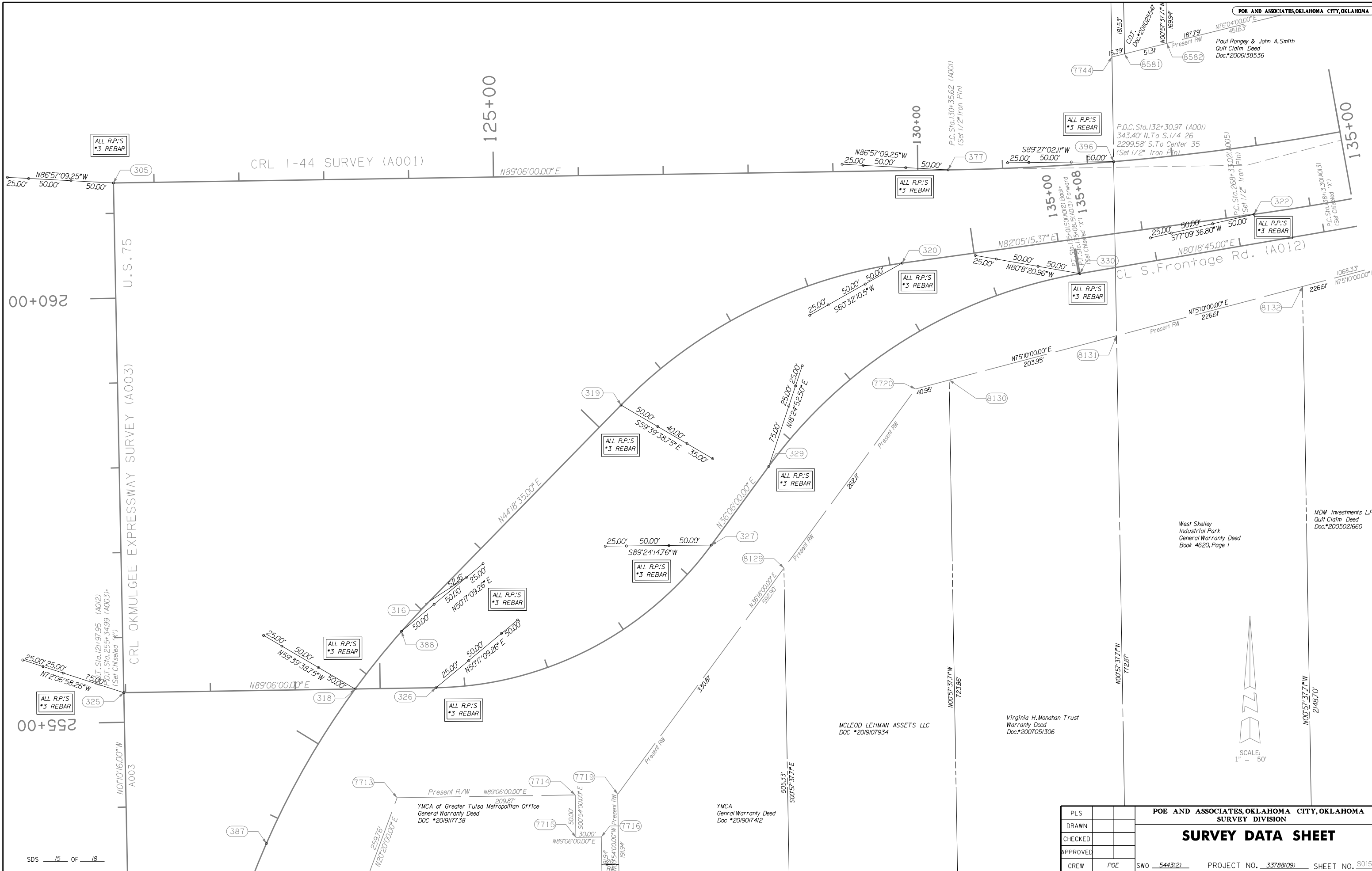
TULSA'S BACKYARD INITIATIVE FUND
DOC #2020036632

SECTION 35
T-19-N, R-12-E
PART NW/4

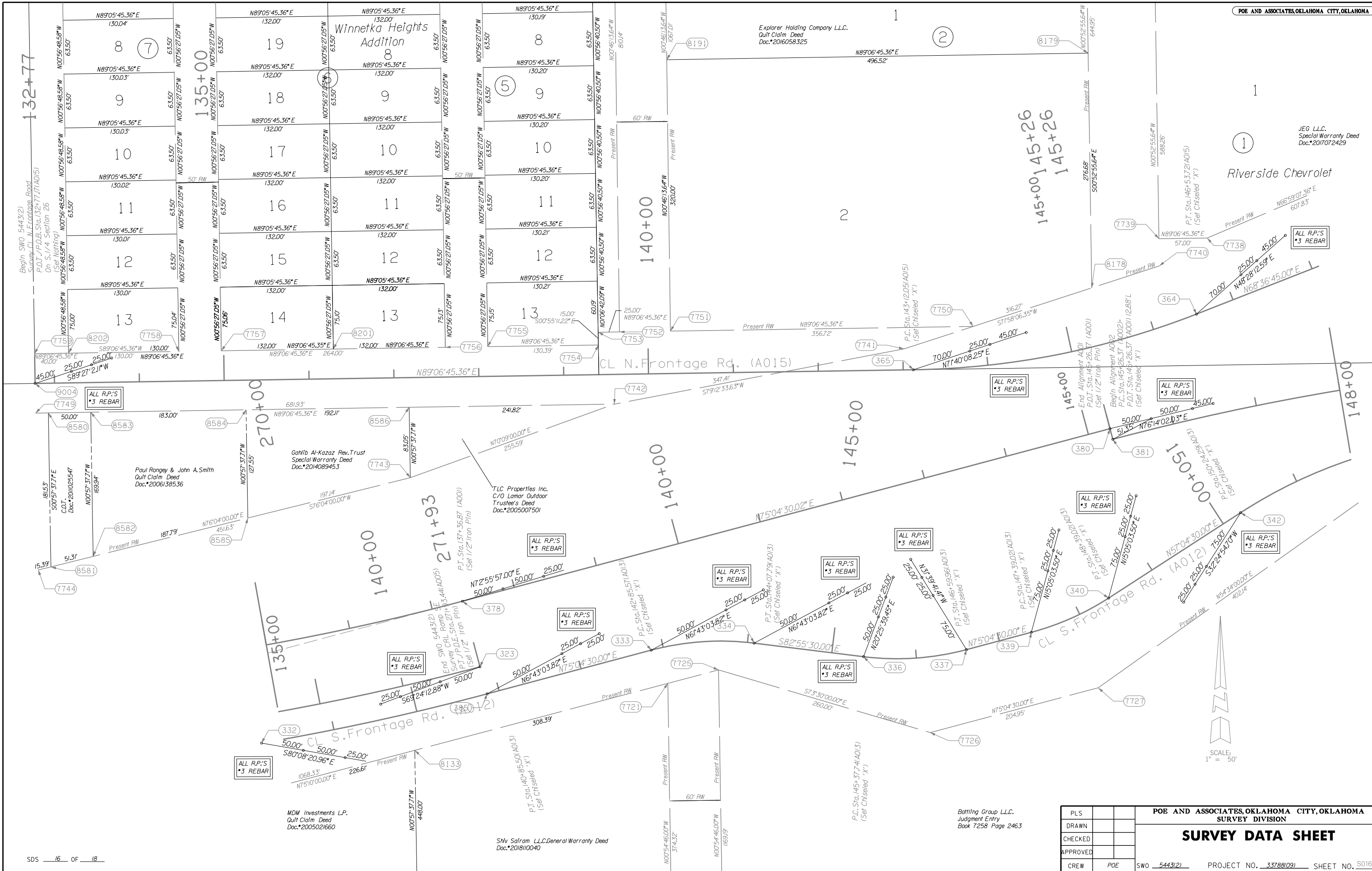


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DRAWN	RDL	SURVEY DIVISION	
CHECKED		SURVEY DATA SHEET	
APPROVED			
CREW	ISS		
		SWO 5443/21	PROJECT NO. 33788/09
		SHEET NO. S014	

YMCA of Greater Tulsa
Metropolitan Office
General Warranty Deed
DOC #201917738



PLS		POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA	
DRAWN		SURVEY DIVISION	
CHECKED		SURVEY DATA SHEET	
APPROVED			
CREW	POE	SWO 5443/21	PROJECT NO. 33788/09
			SHEET NO. S015



Explorer Holding Company LLC.
Quit Claim Deed
Doc.*2016058325

JEG LLC.
Special Warranty Deed
Doc.*2017072429

Riverside Chevrolet

Paul Rongey & John A. Smith
Quit Claim Deed
Doc.*2006138536

GaHib Al-Kazaz Rev.Trust
Special Warranty Deed
Doc.*2014089453

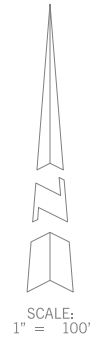
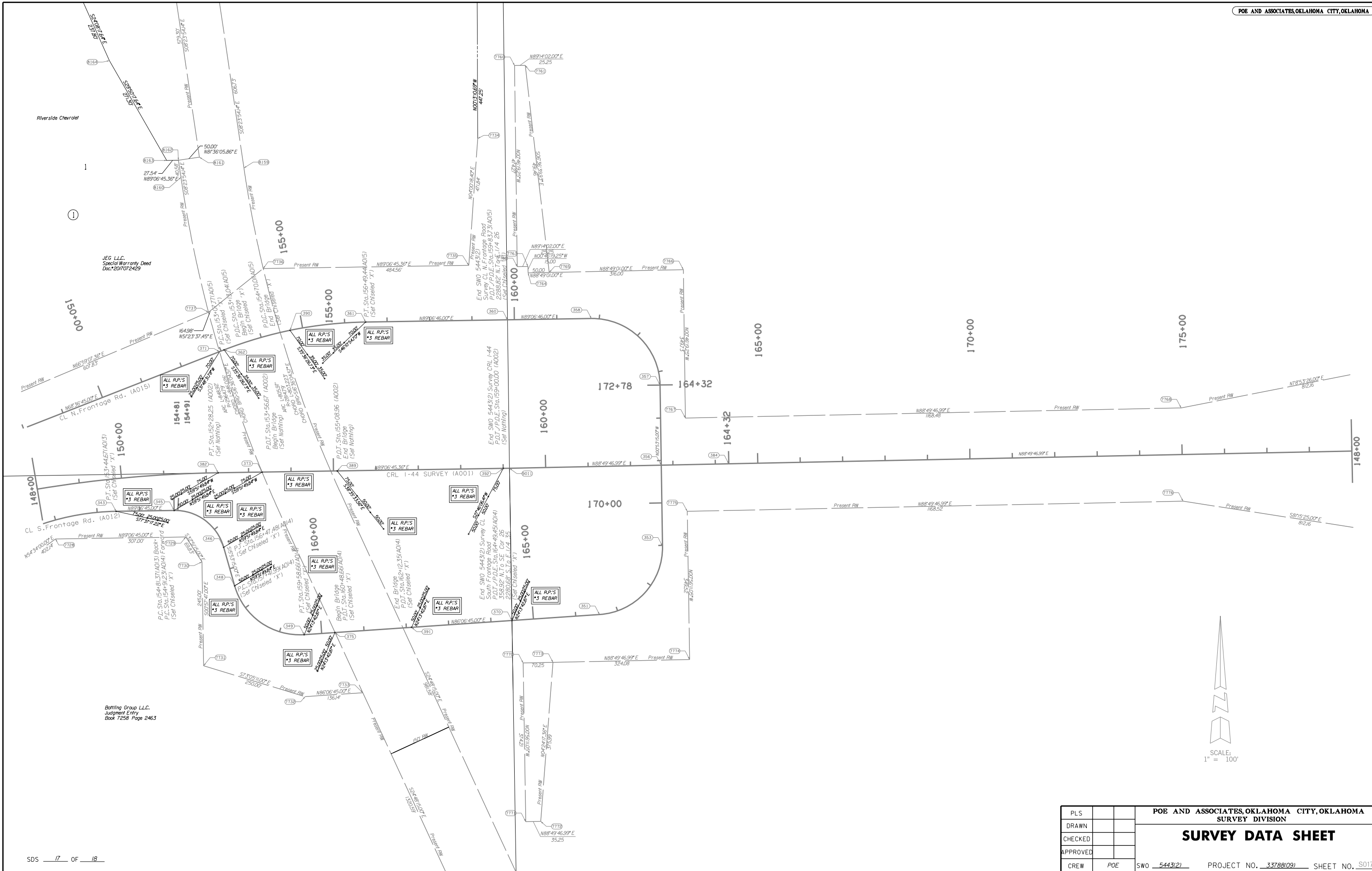
TLC Properties Inc.
C/O Lamar Outdoor
Trustee's Deed
Doc.*2005007501

MDM Investments L.P.
Quit Claim Deed
Doc.*2005021660

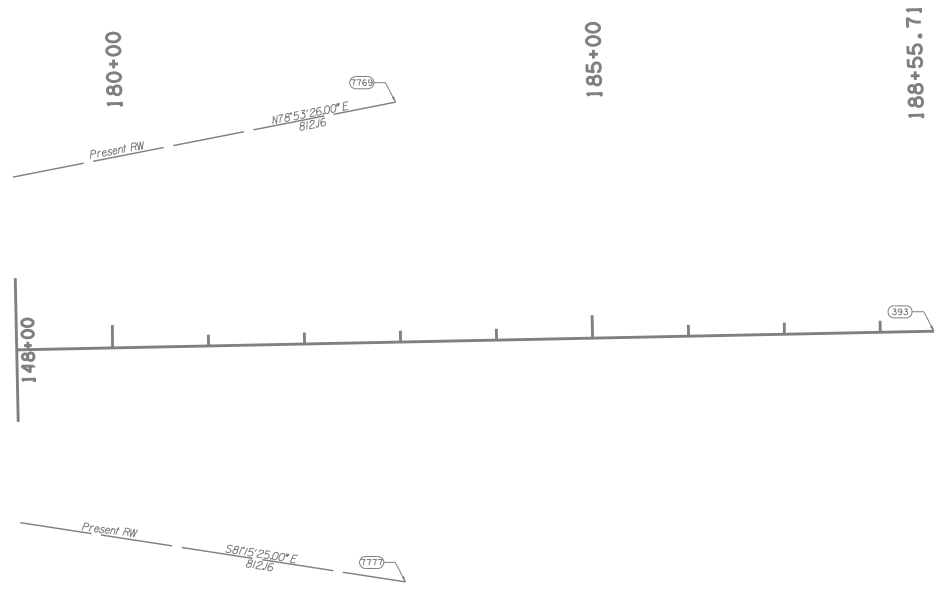
Shiv Salram LLC.General Warranty Deed
Doc.*2018110040

Bottling Group LLC.
Judgment Entry
Book 7258 Page 2463

PLS		POE AND ASSOCIATES, OKLAHOMA CITY, OKLAHOMA	
DRAWN		SURVEY DIVISION	
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APPROVED			
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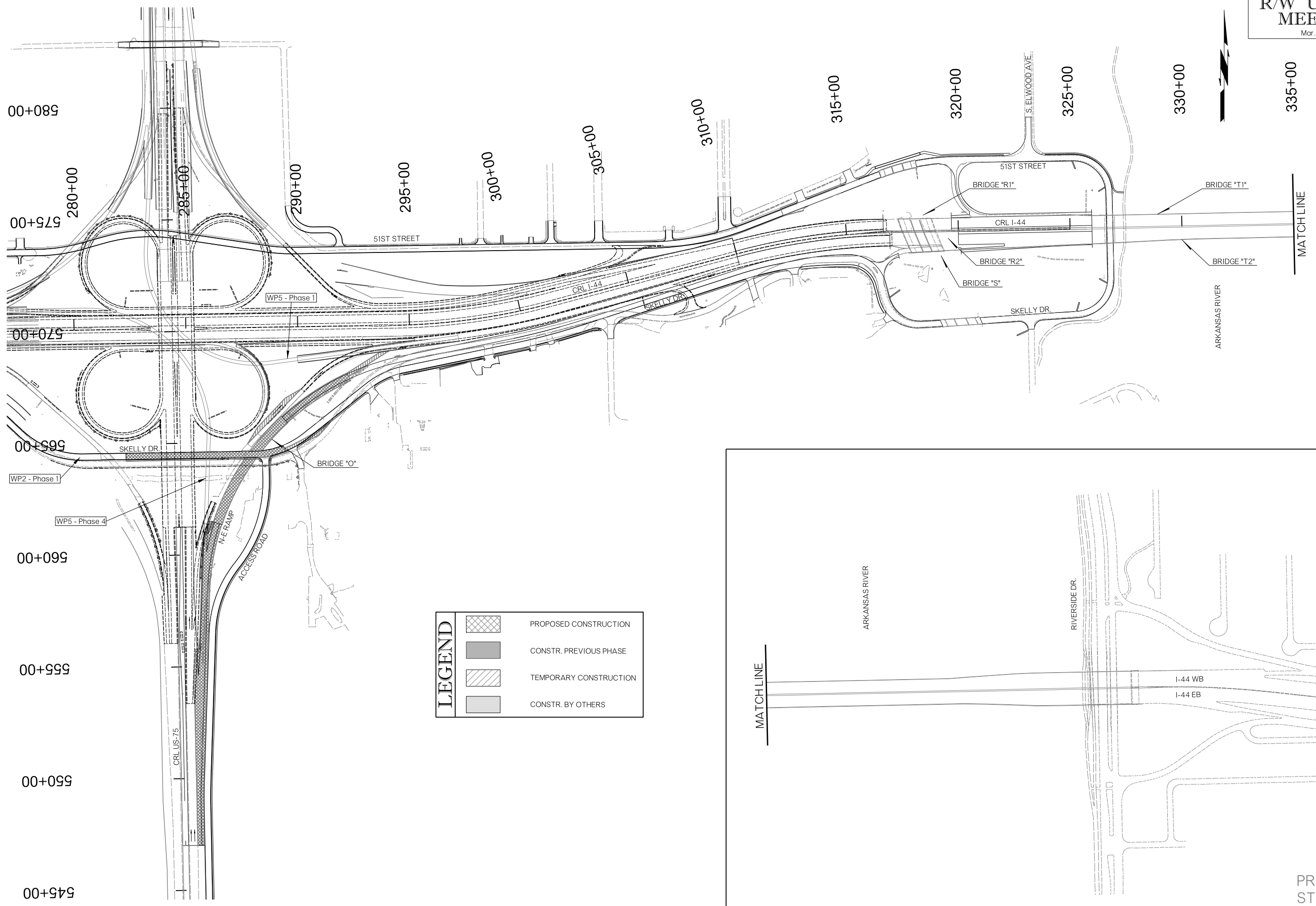


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DRAWN		SURVEY DIVISION	
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CREW	POE	SWO 5443/21	PROJECT NO. 33788/09 SHEET NO. S017

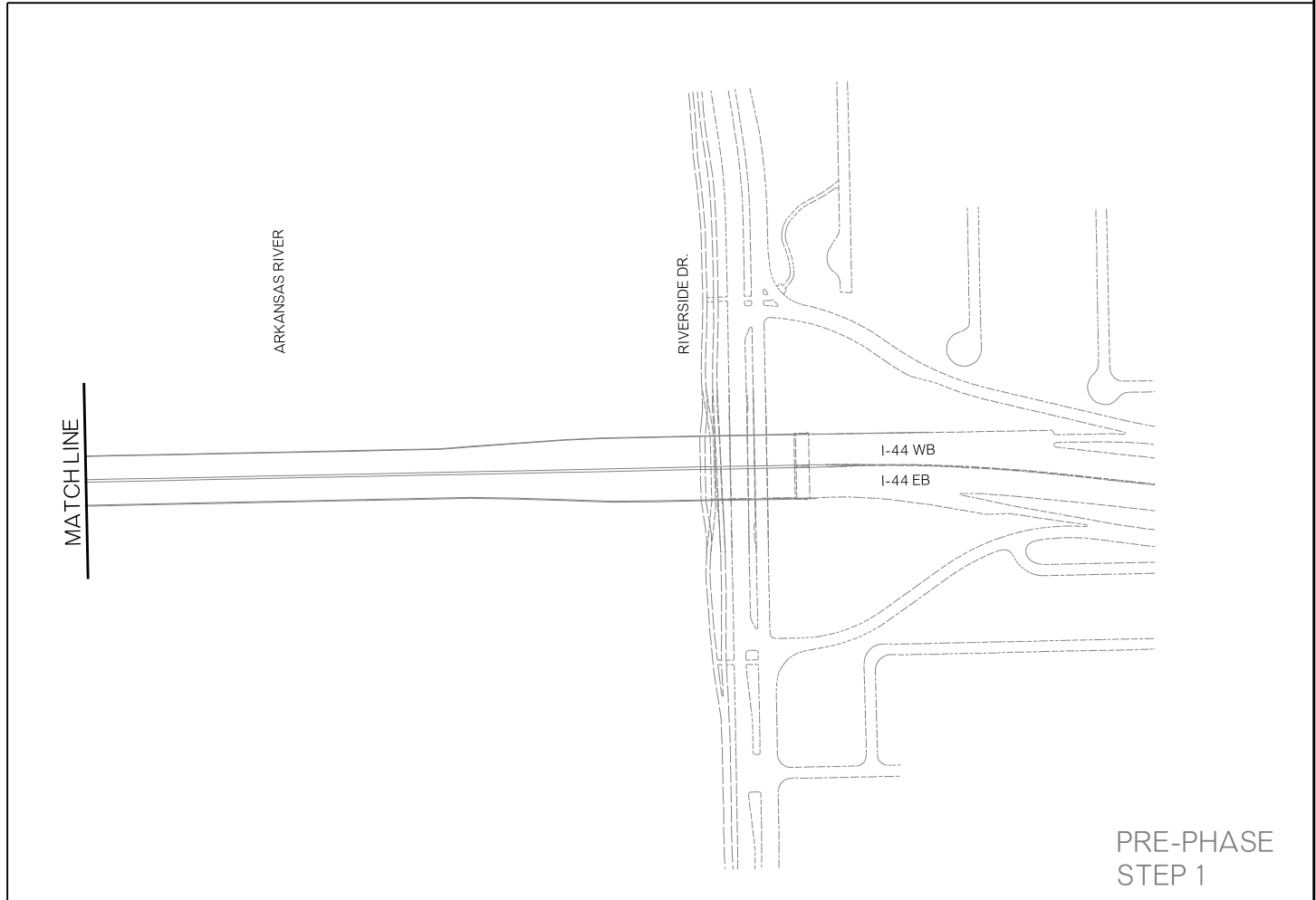


P.O.T. 5161.88+55.71 @ 1+44
 END SURVEY (CALC. POINT)

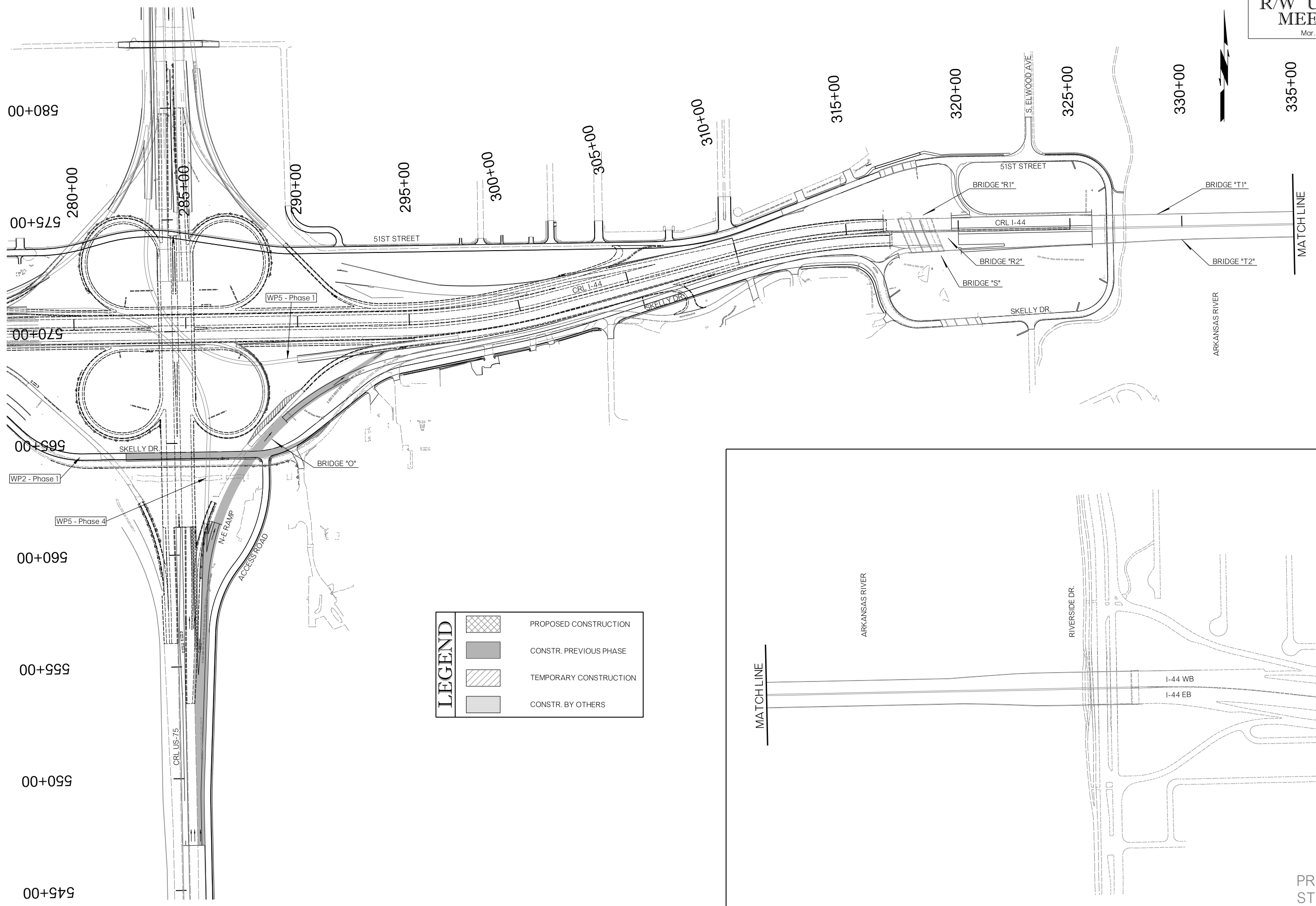
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DRAWN			SURVEY DIVISION	
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CREW	POE	SWO 5443/21	PROJECT NO. 3788/091	SHEET NO. S018



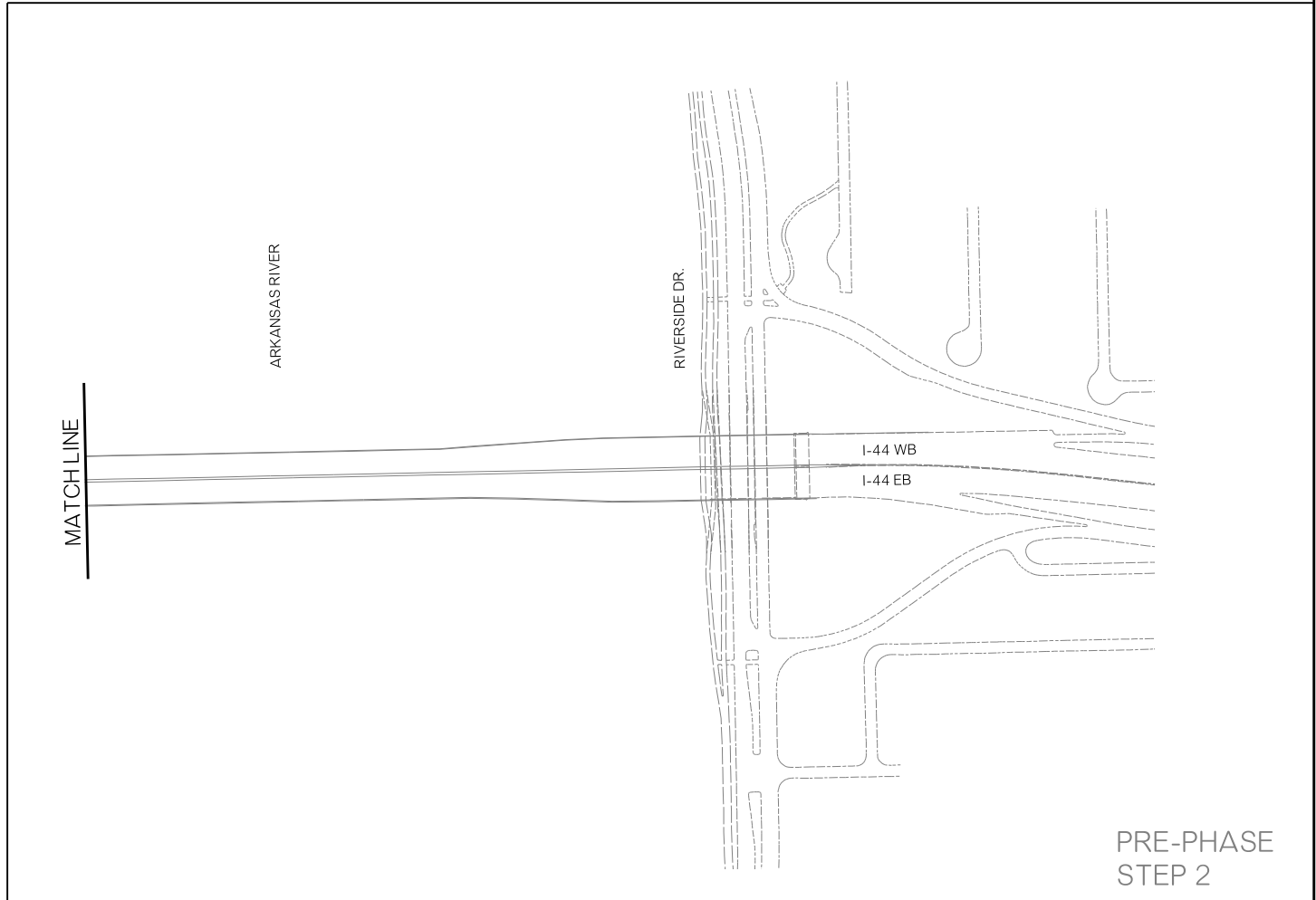
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	CONSTR. PREVIOUS PHASE
	TEMPORARY CONSTRUCTION
	CONSTR. BY OTHERS



3/17/2021 H:\203192 US75-I44\Plan Sheets\33788(09)-Sequence 1.dgn

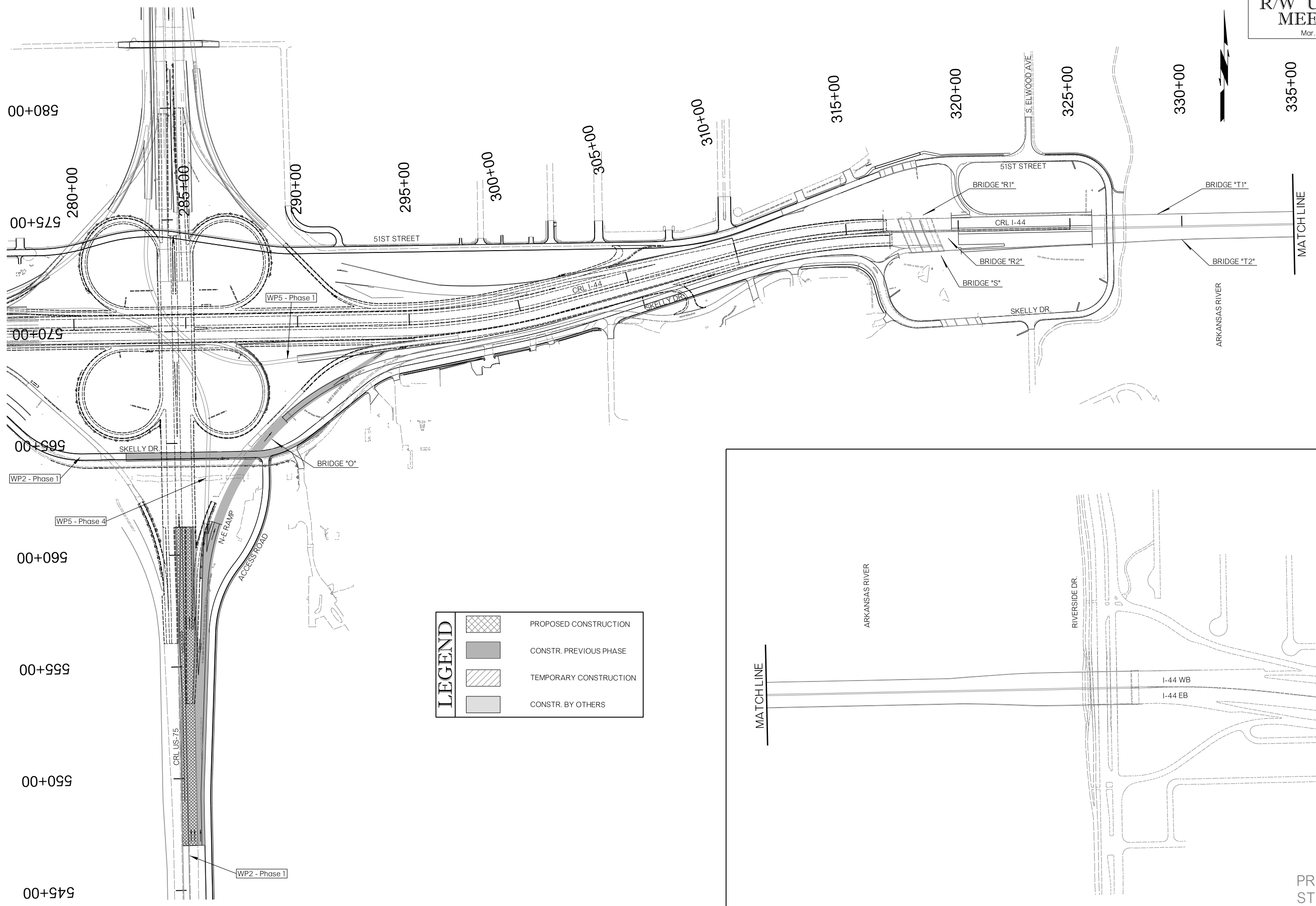


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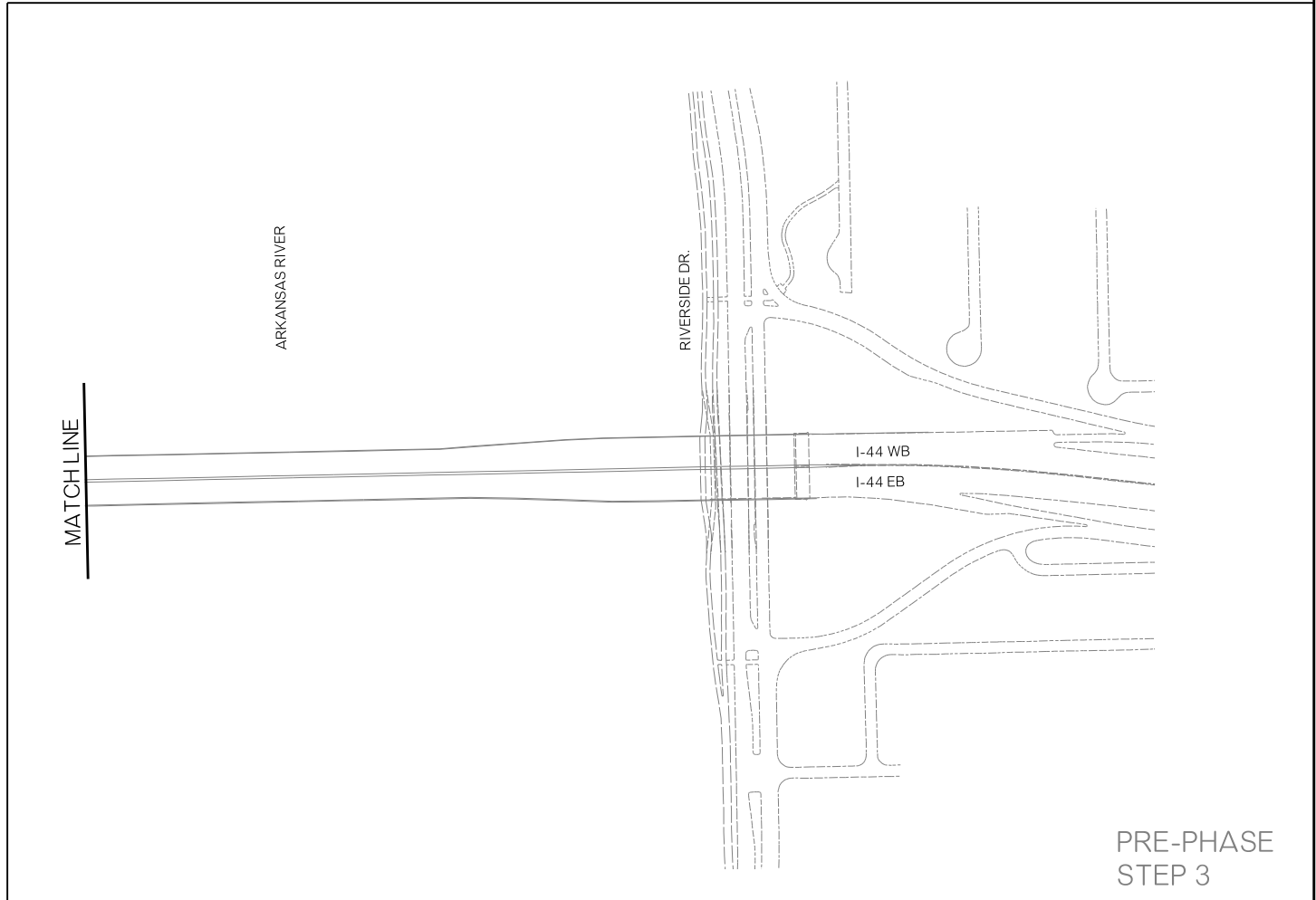


PRE-PHASE
STEP 2

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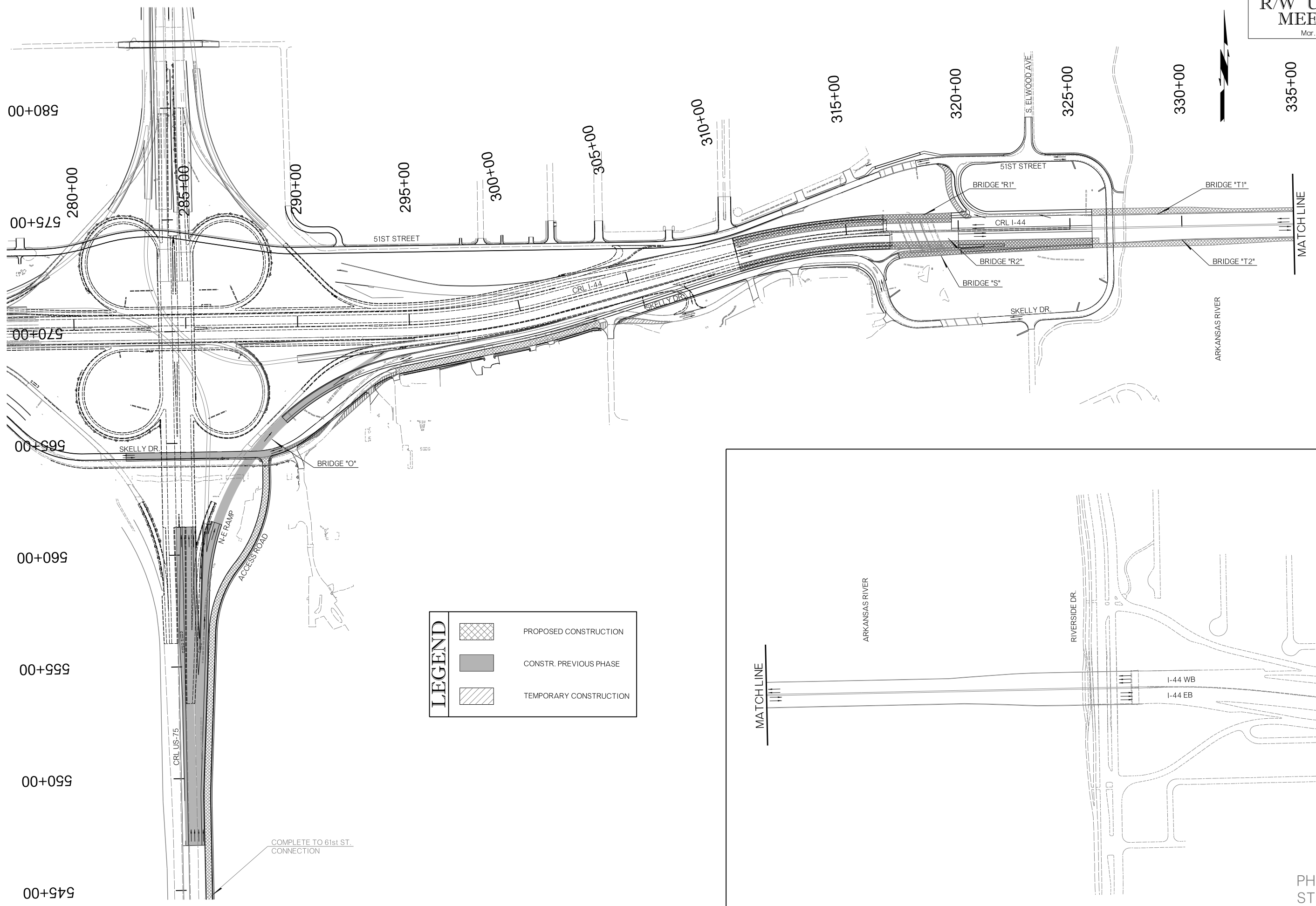


LEGEND	
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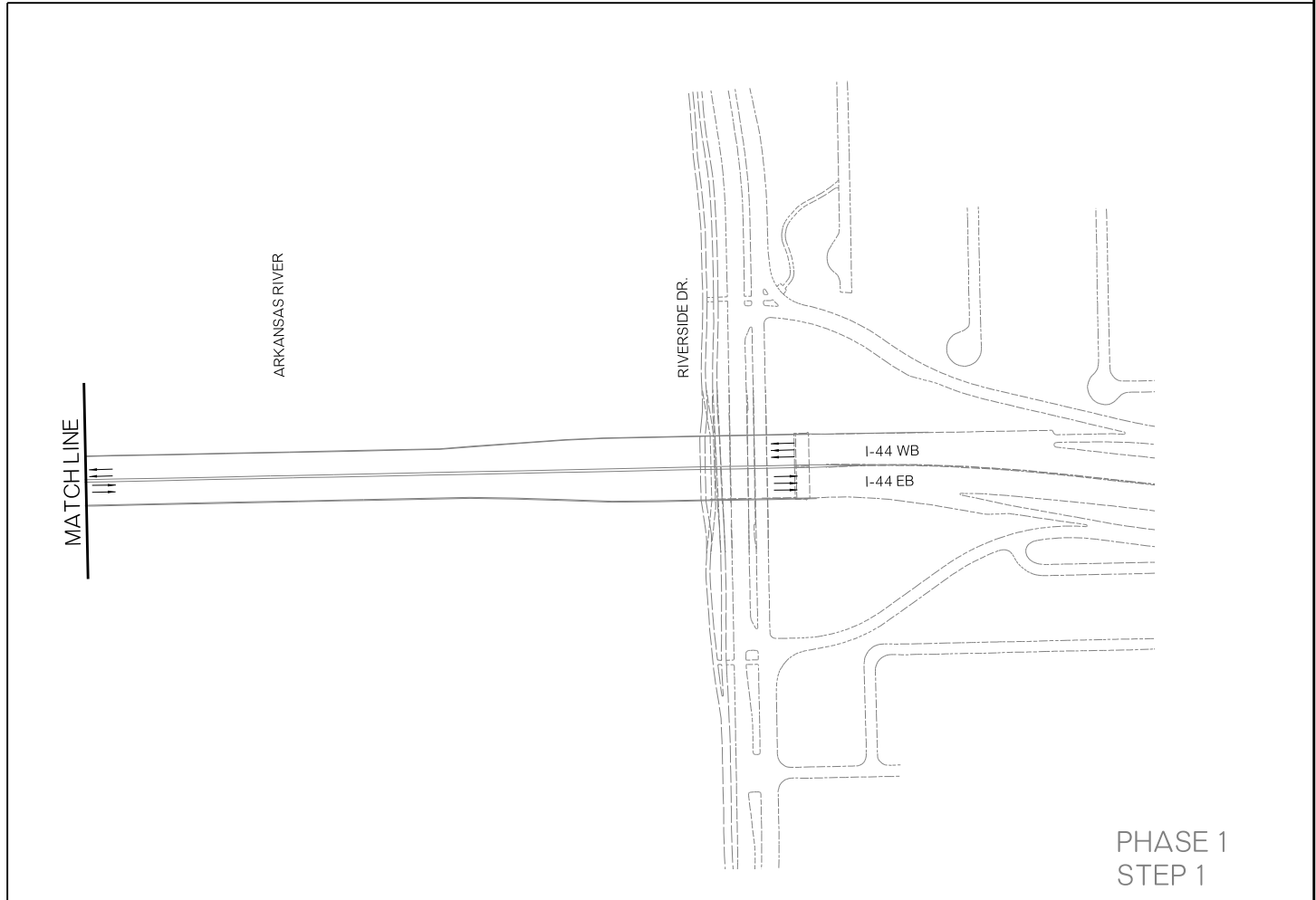


PRE-PHASE
STEP 3

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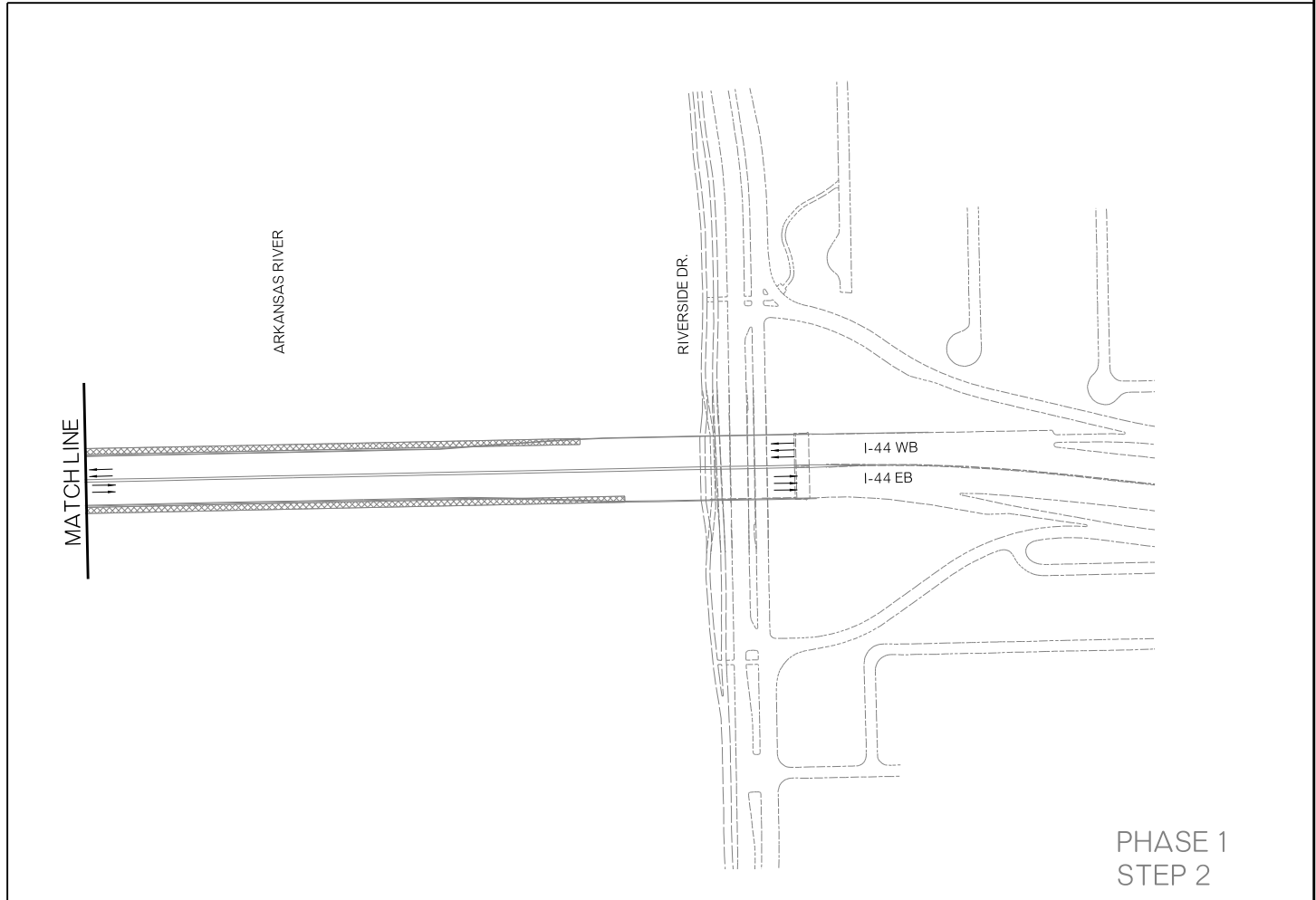
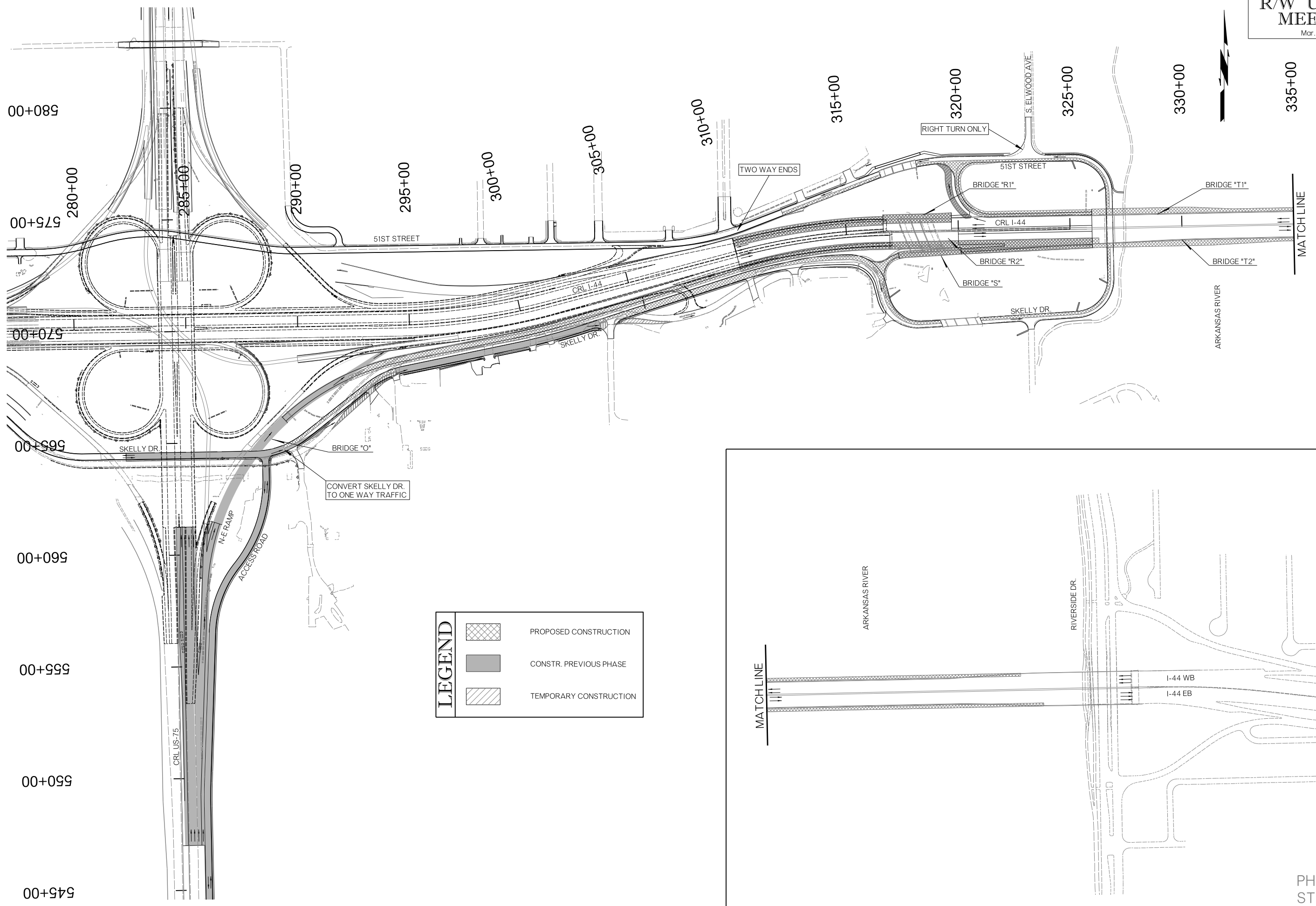


LEGEND	
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	TEMPORARY CONSTRUCTION



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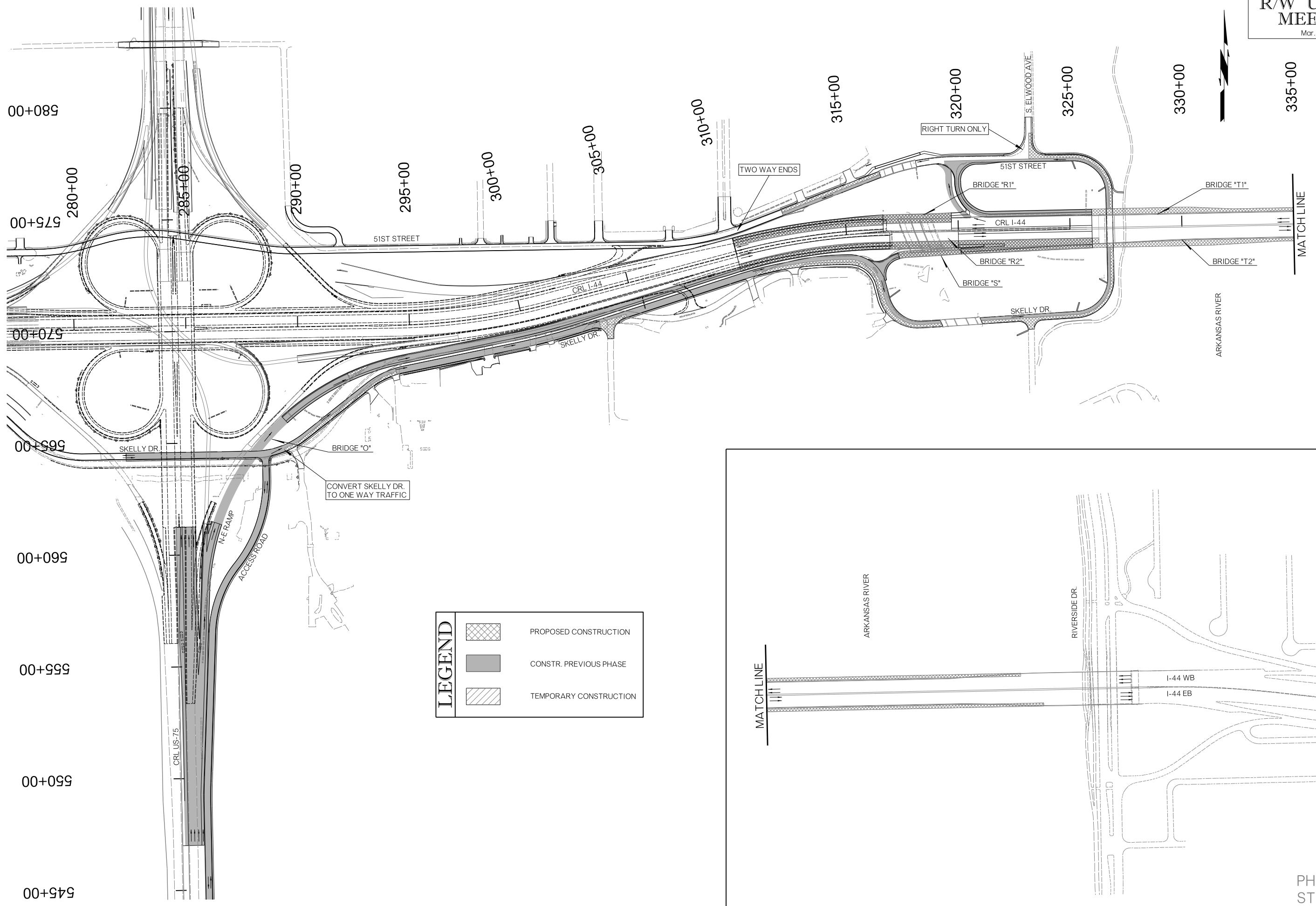
PHASE 1
STEP 1



LEGEND	
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	CONSTR. PREVIOUS PHASE
	TEMPORARY CONSTRUCTION

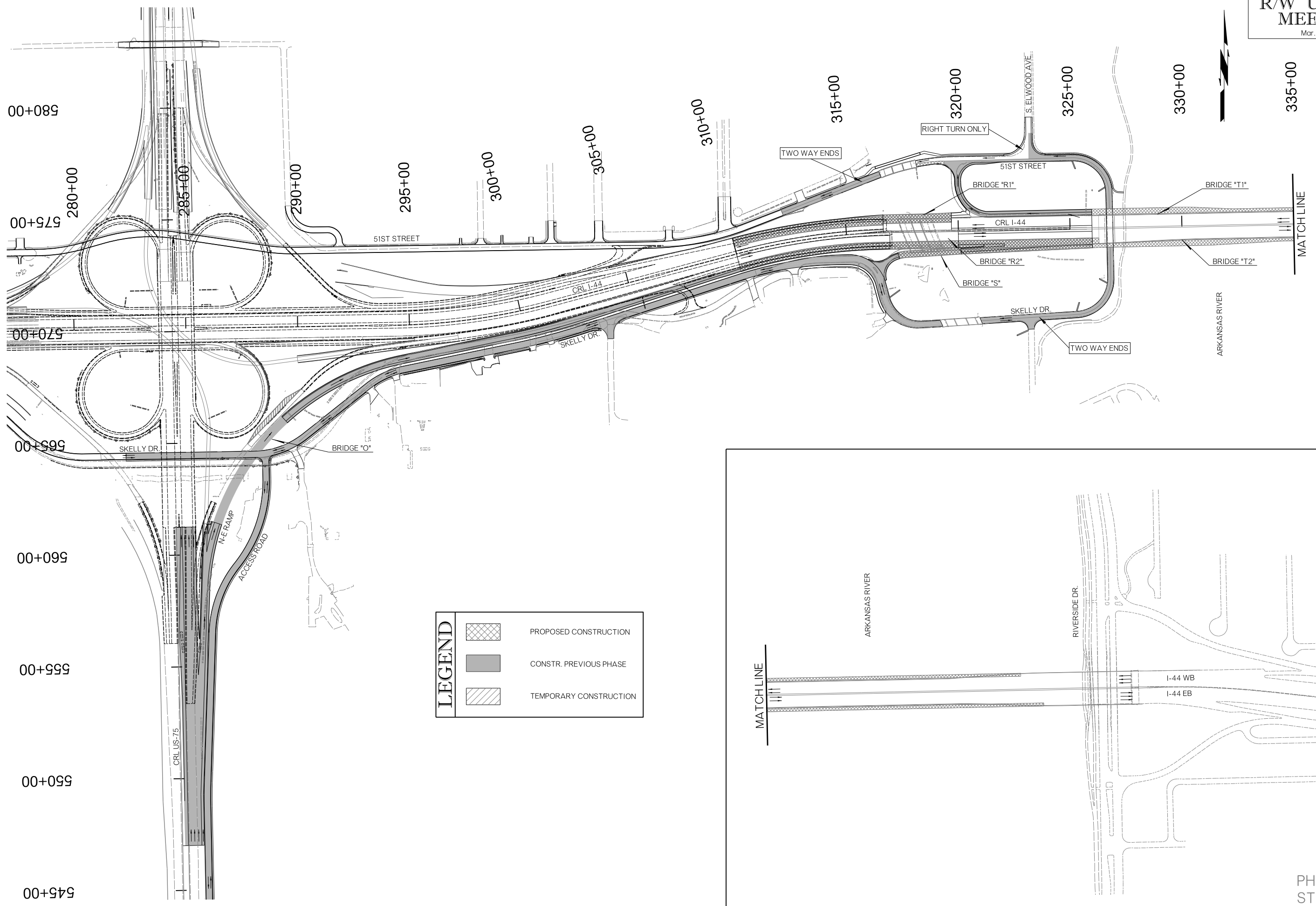
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PHASE 1
STEP 2

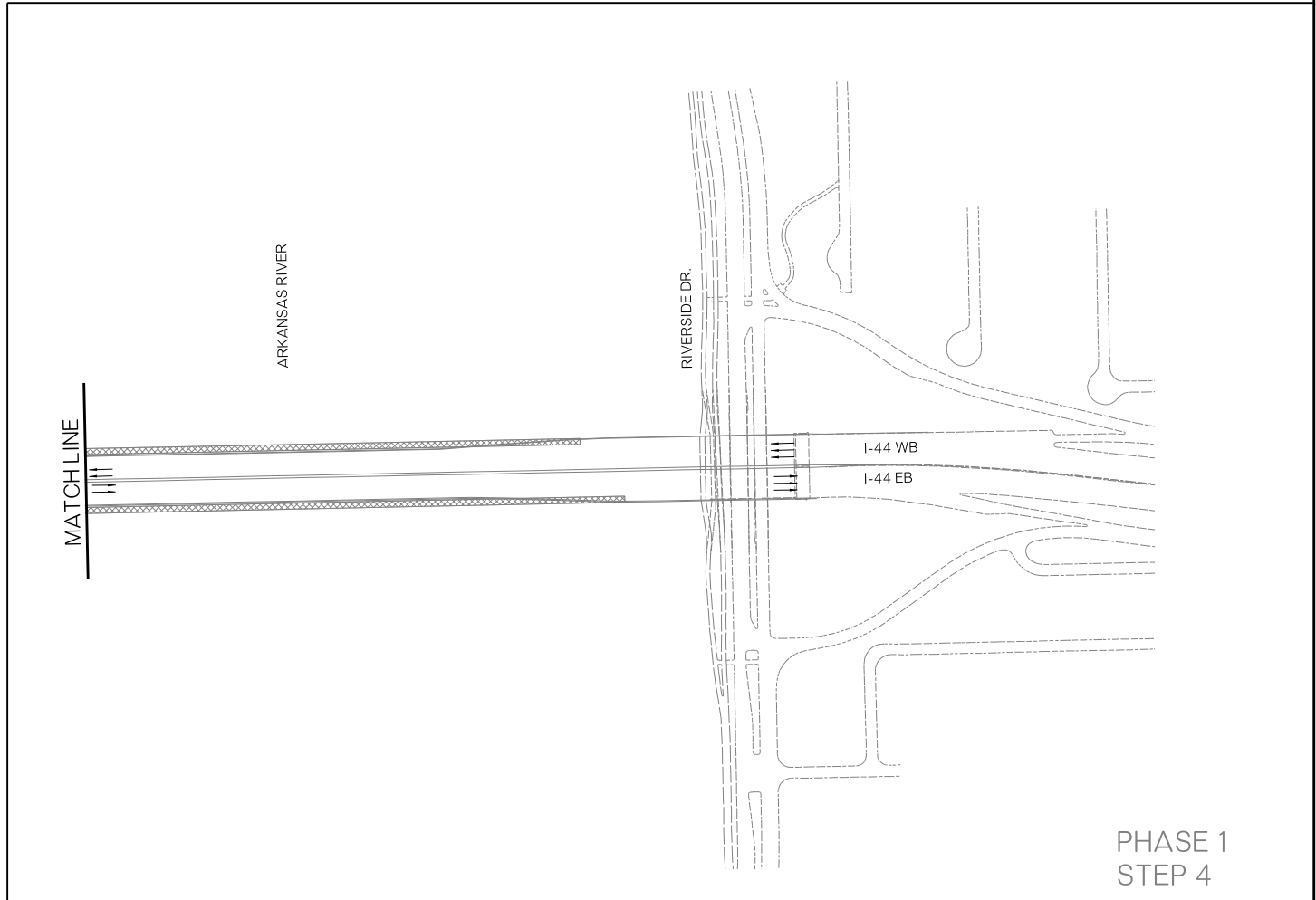


3/17/2021 H:\203192 US75-I44\Plan Sheets\33788(09)-Sequence 6.dgn

PHASE 1
STEP 3



LEGEND	
	PROPOSED CONSTRUCTION
	CONSTR. PREVIOUS PHASE
	TEMPORARY CONSTRUCTION

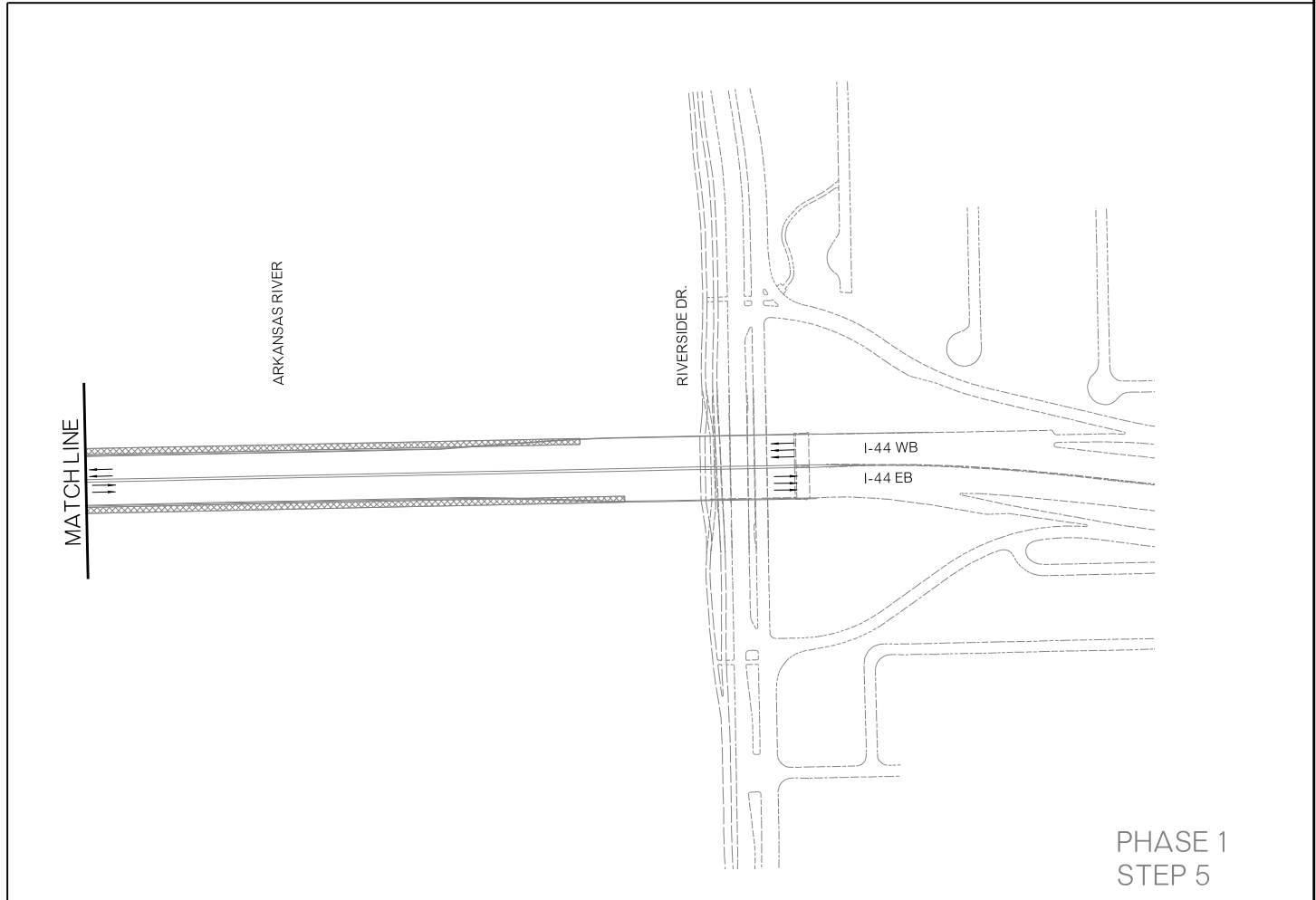
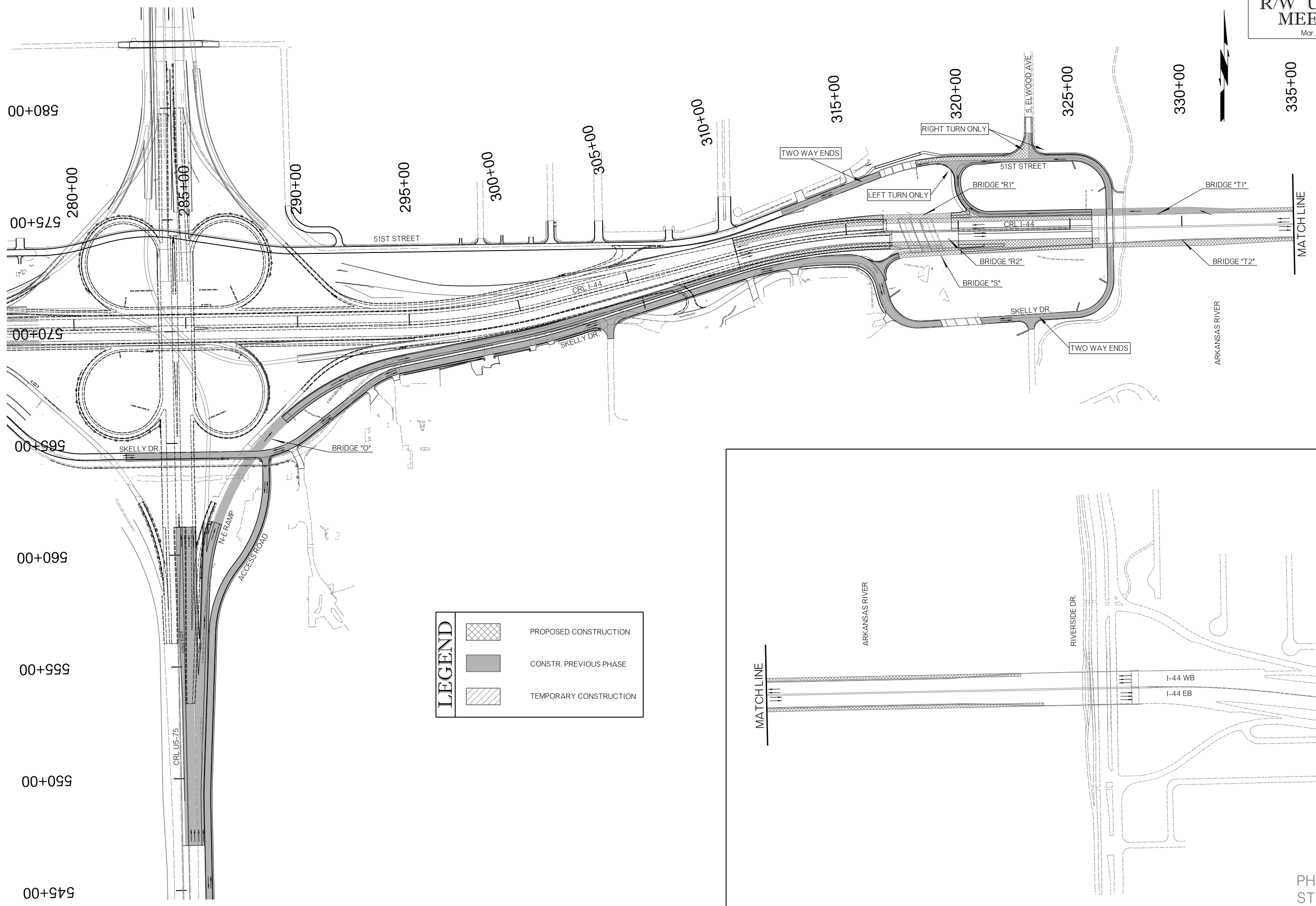


PHASE 1
STEP 4

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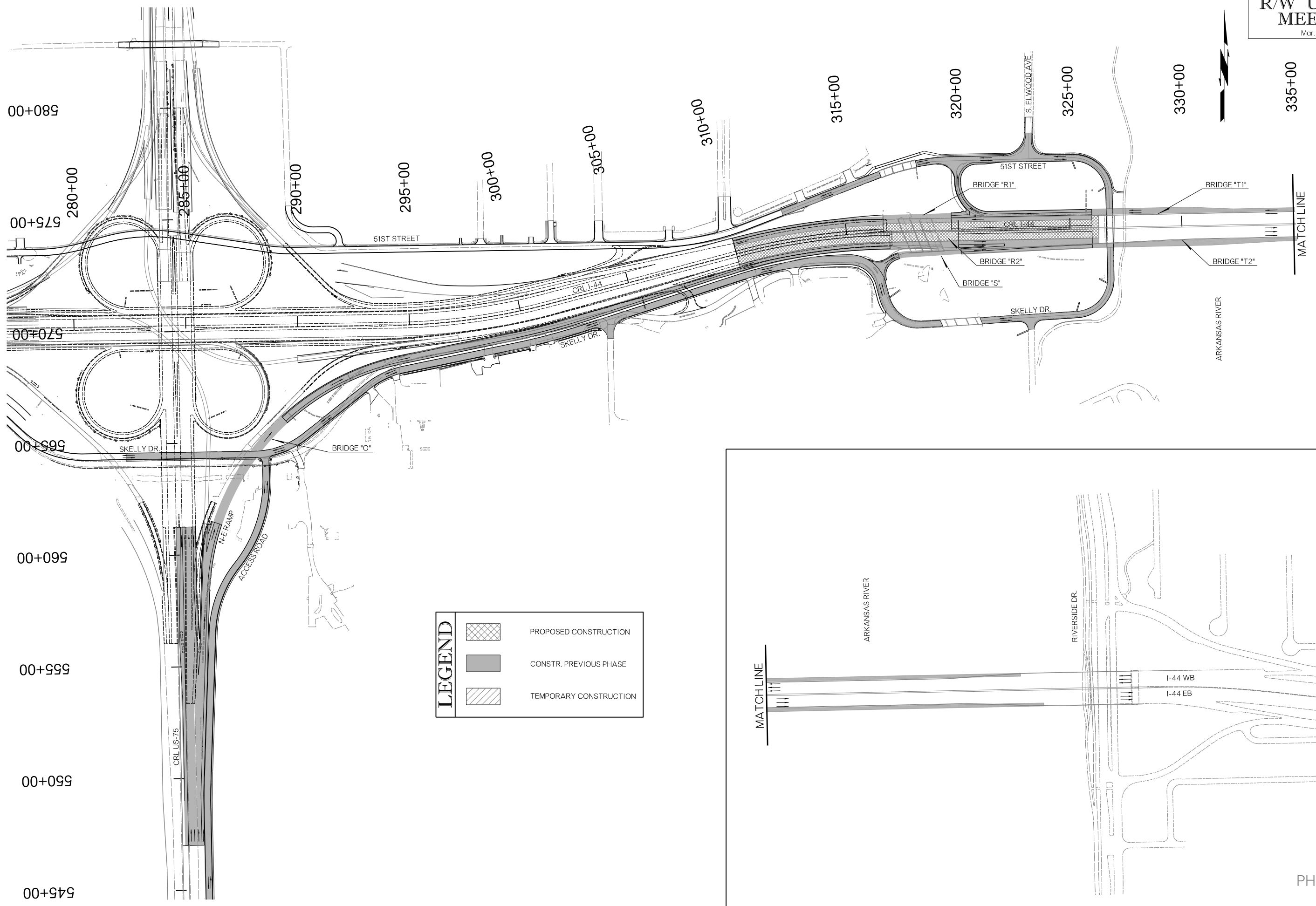
R/W UTILITY MEETING

Mar. 17, 2021

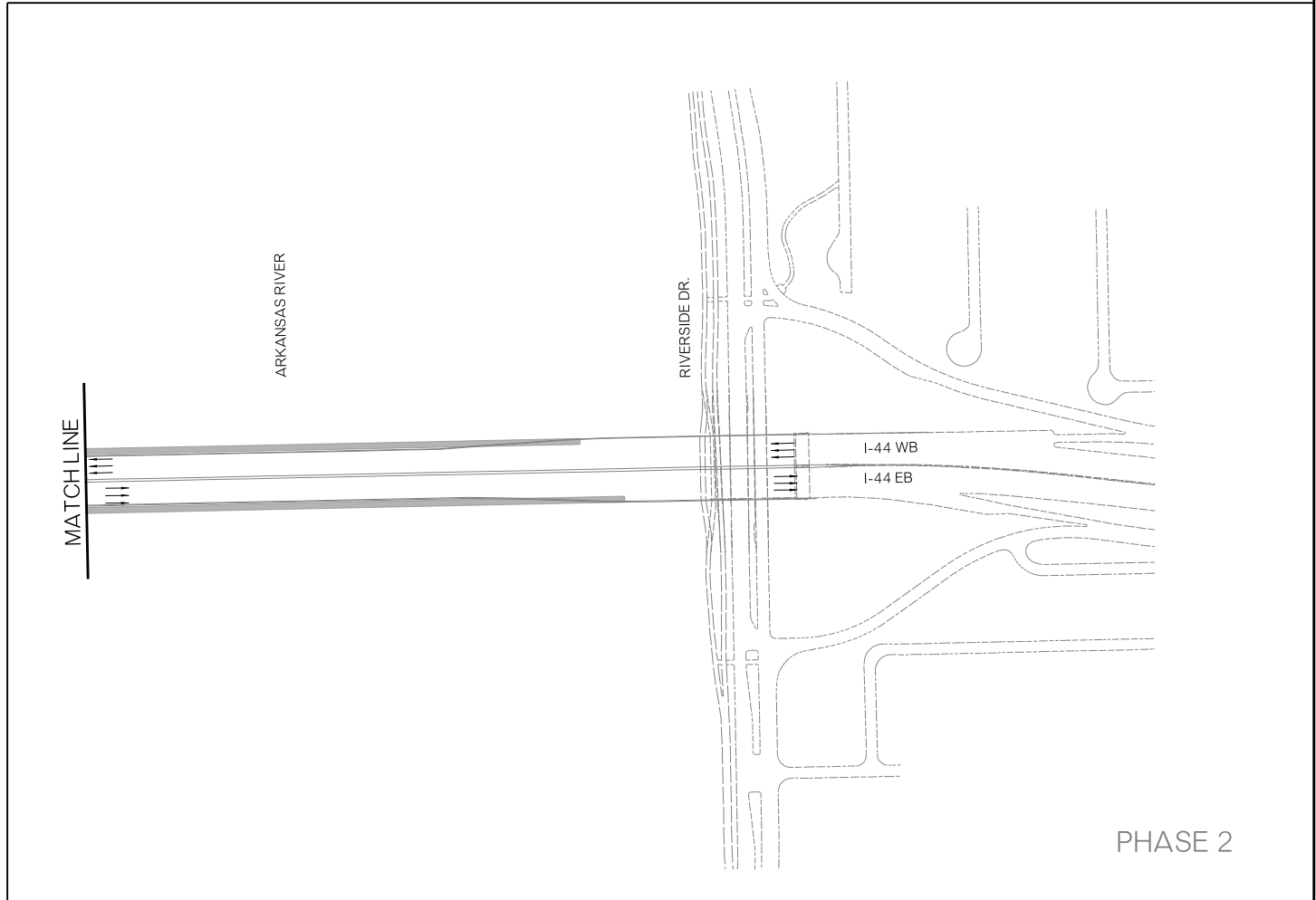


3/17/2021 H:\203192 US75-I44\Plan Sheets\33788(09)-Sequence 8.dgn

PHASE 1
STEP 5



LEGEND	
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	TEMPORARY CONSTRUCTION

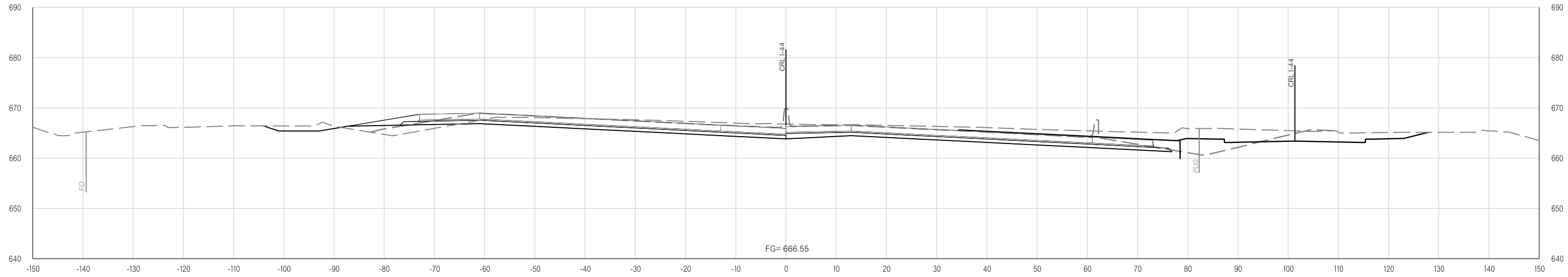


PHASE 2

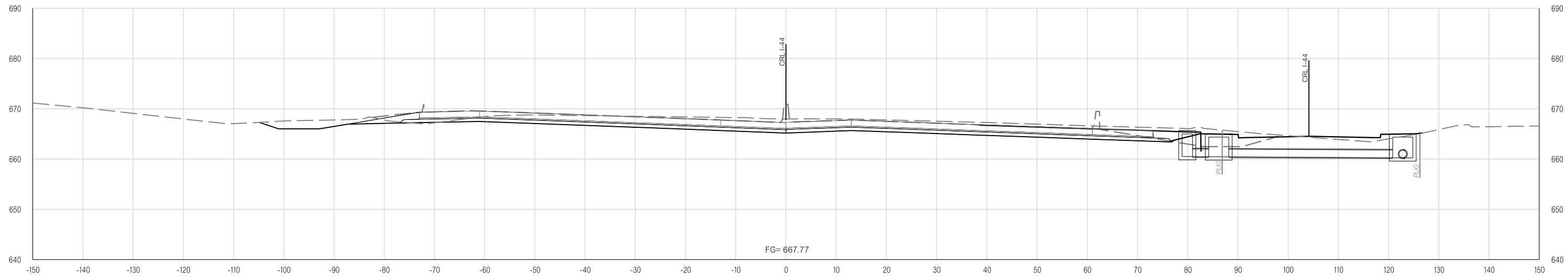
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Mar. 17, 2021

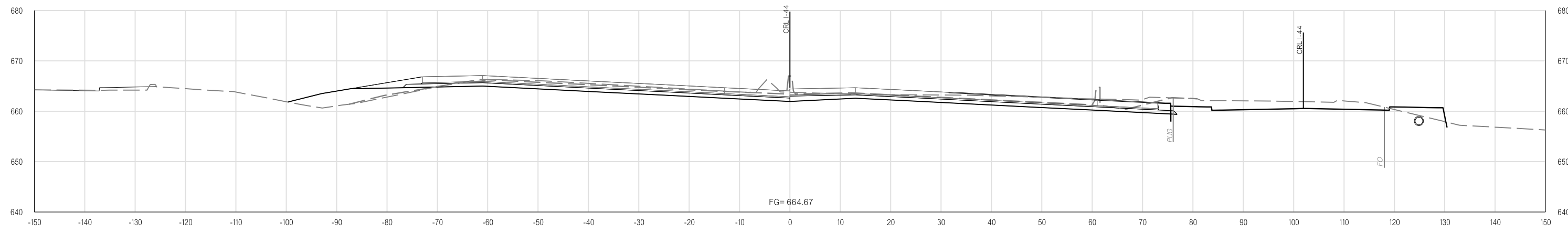


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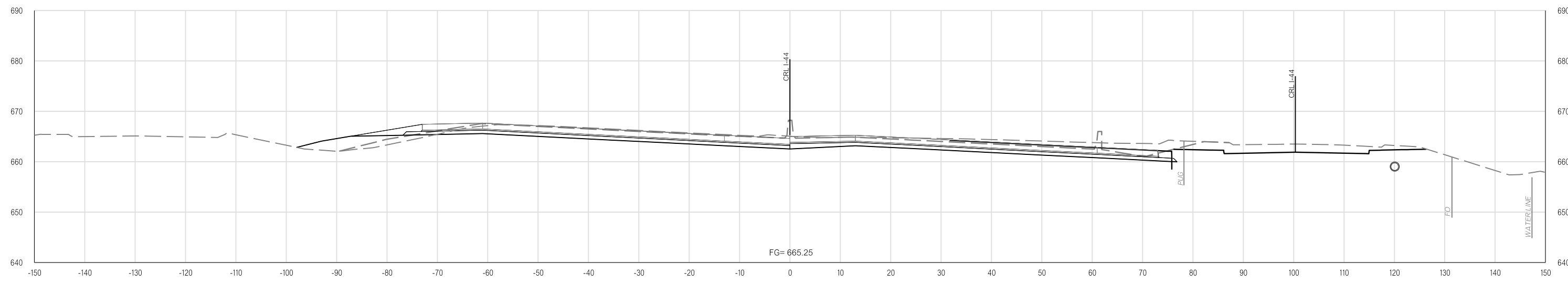


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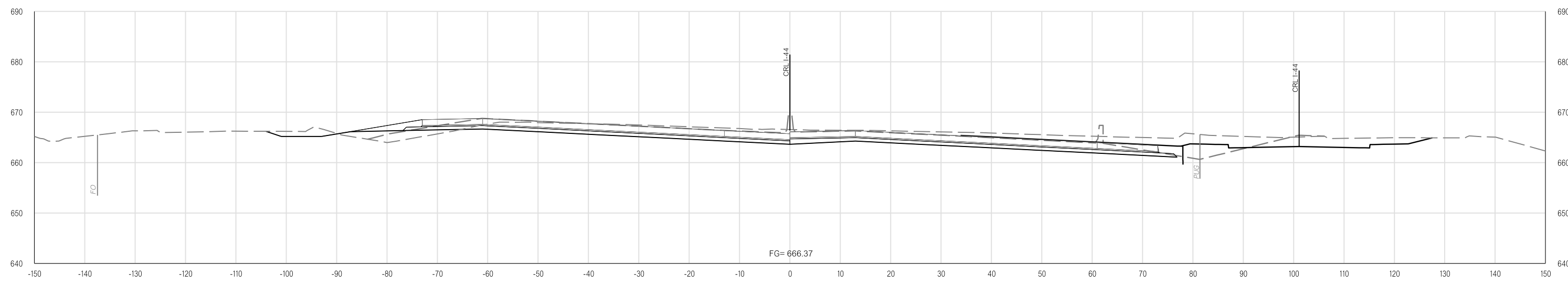
I-44



313+00.00



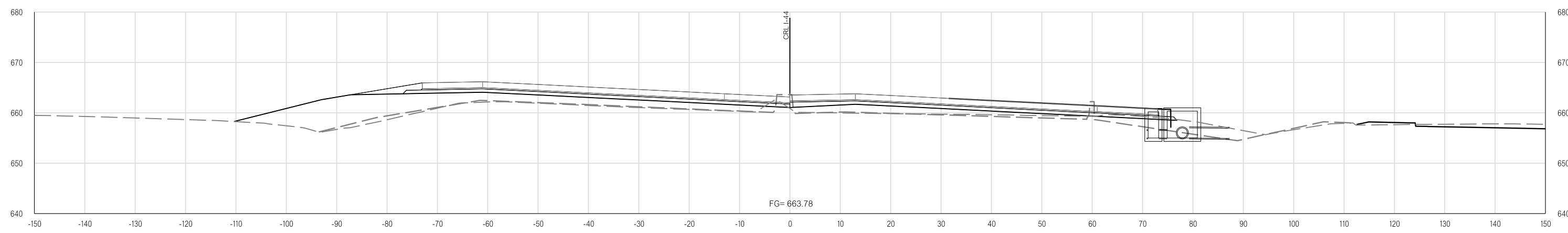
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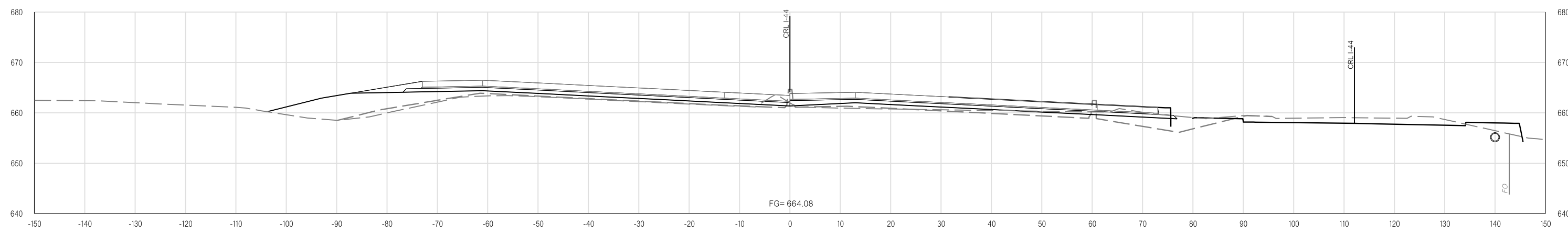
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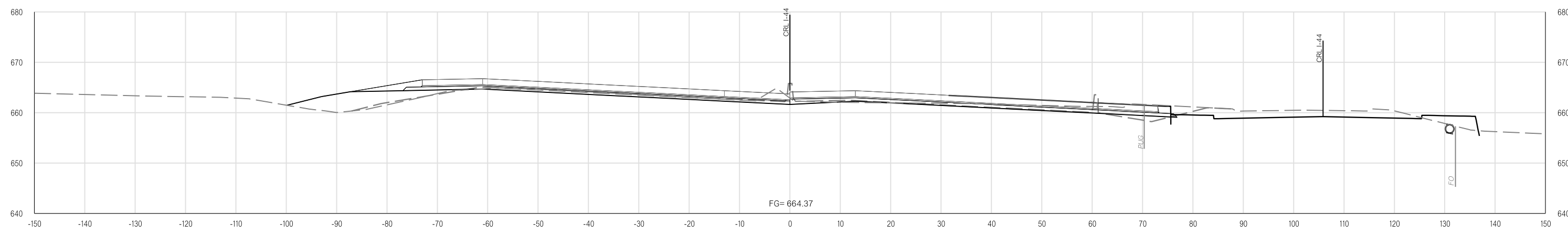
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 3/17/2021



315+99.38



315+00.00

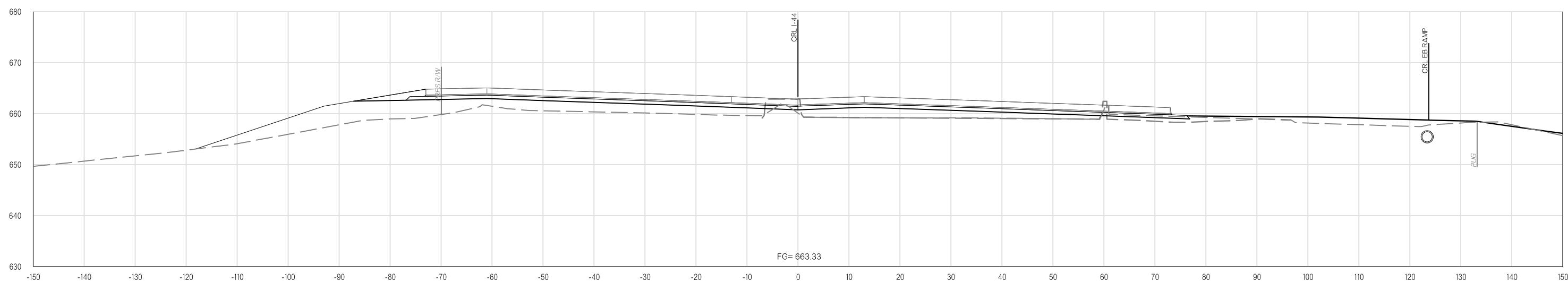


314+00.00

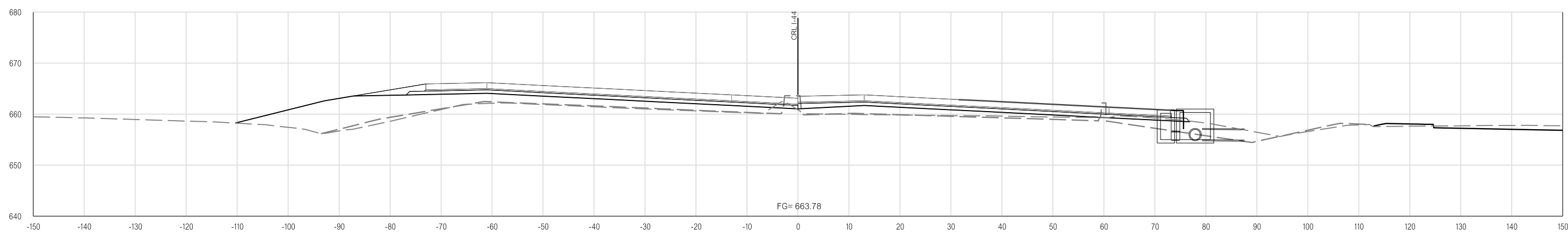
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 3/17/2021

R/W UTILITY MEETING

Mar. 17, 2021



317+00.00



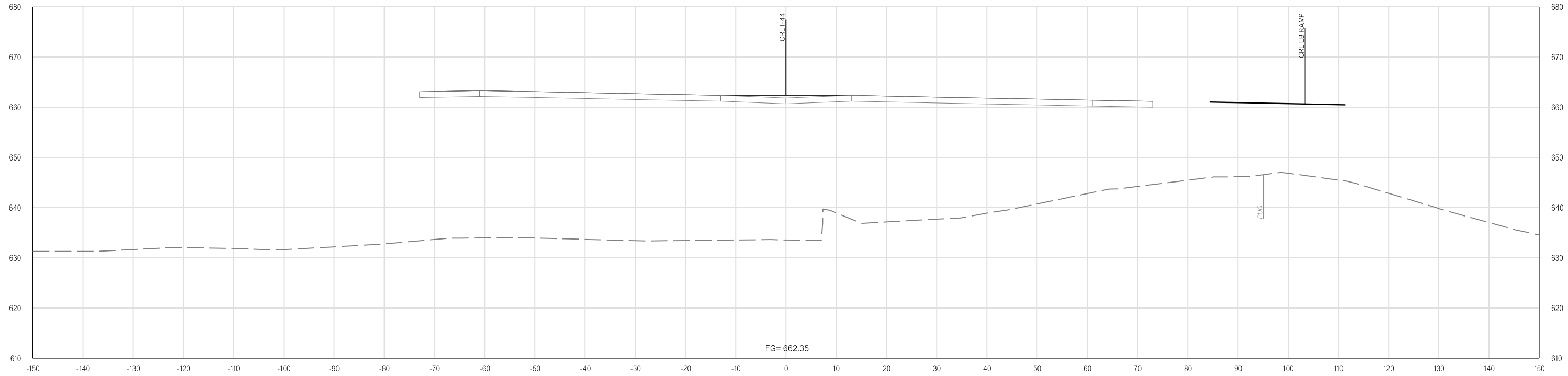
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I-44

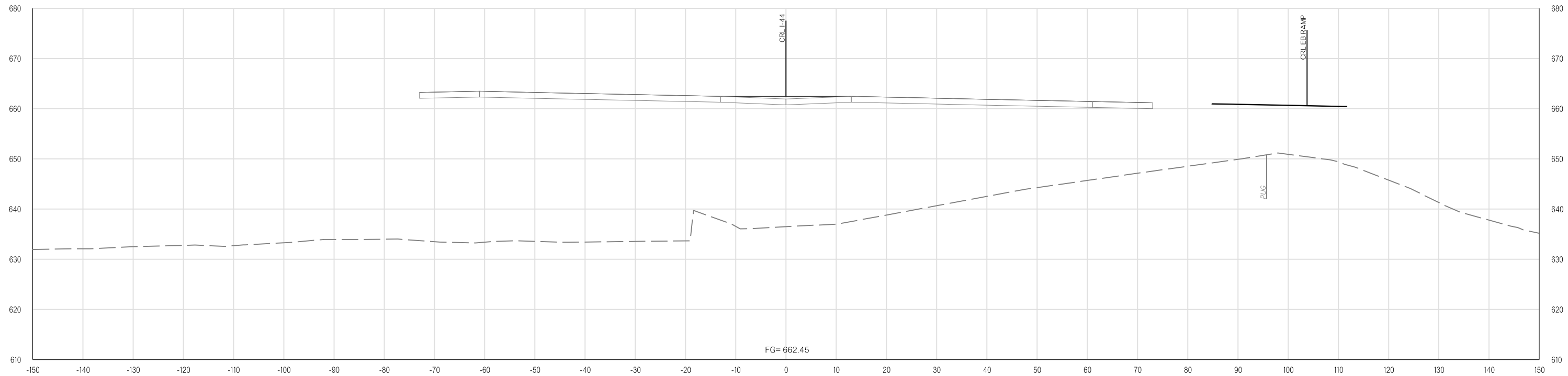
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3/17/2021

R/W UTILITY MEETING

Mar. 17, 2021

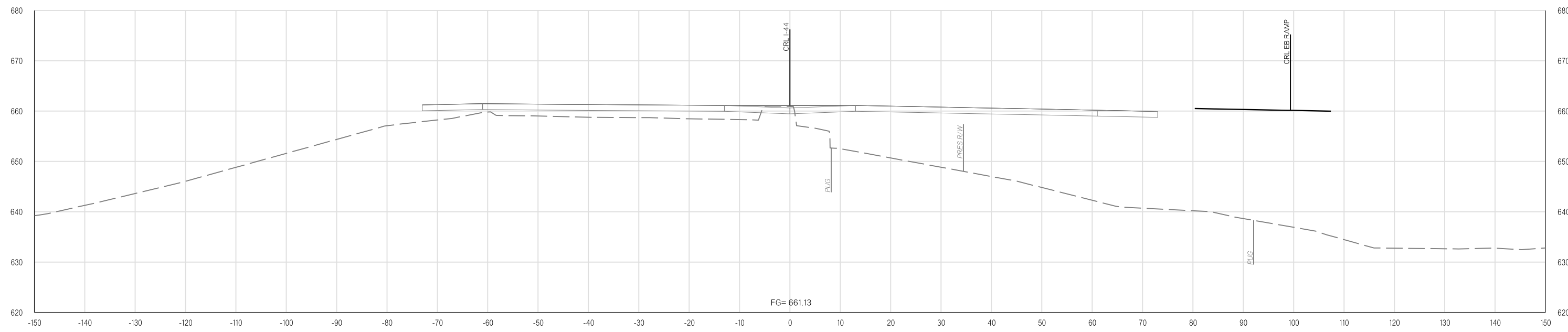
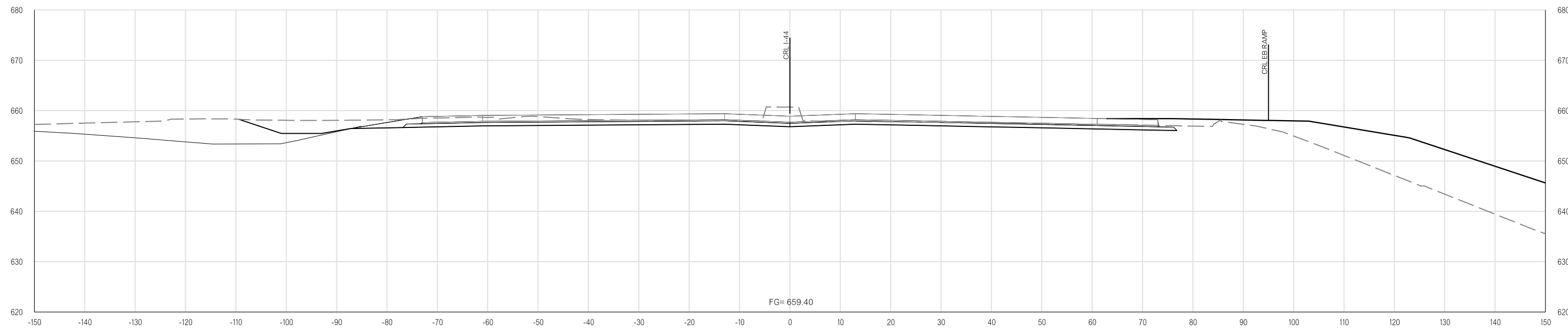


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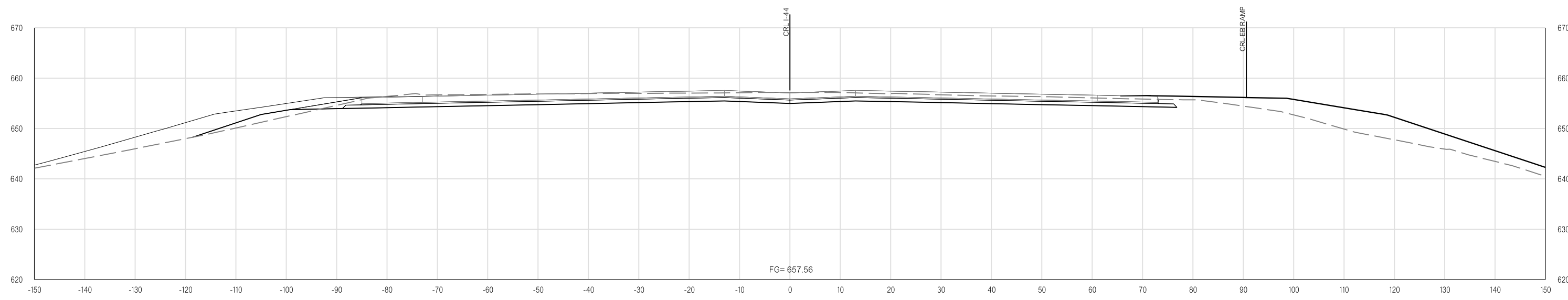


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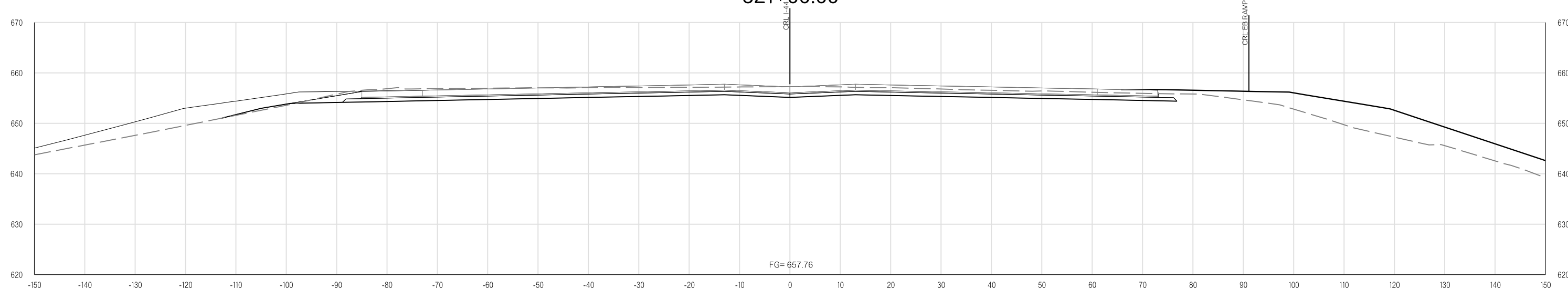
I-44



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 3/17/2021

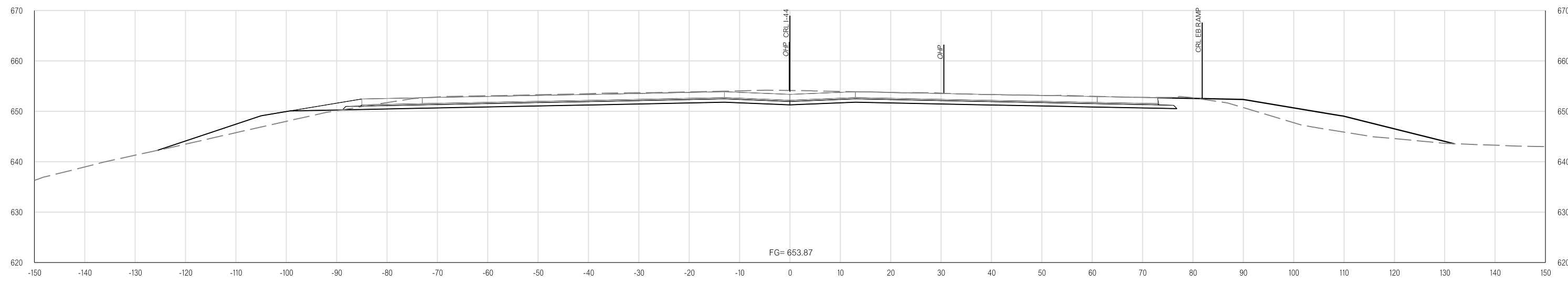


321+00.00

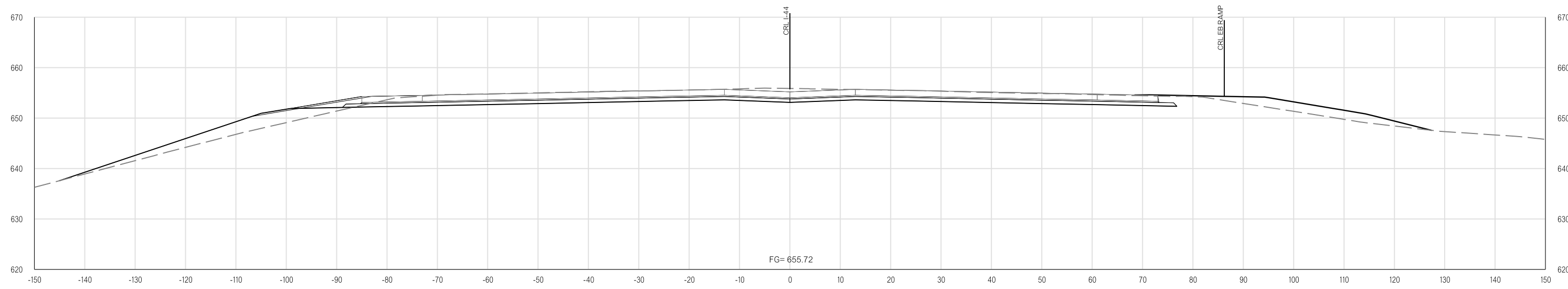


320+88.68

I-44



323+00.00



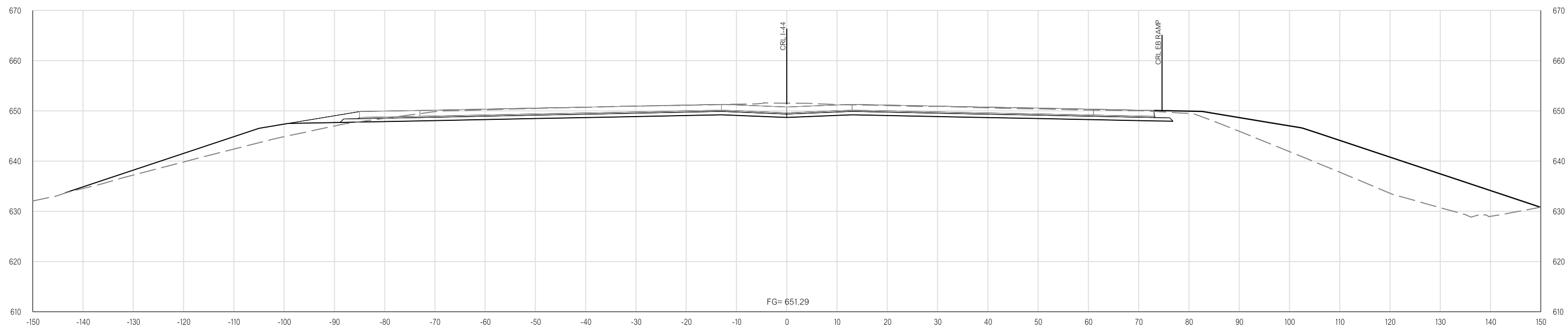
322+00.00

I-44

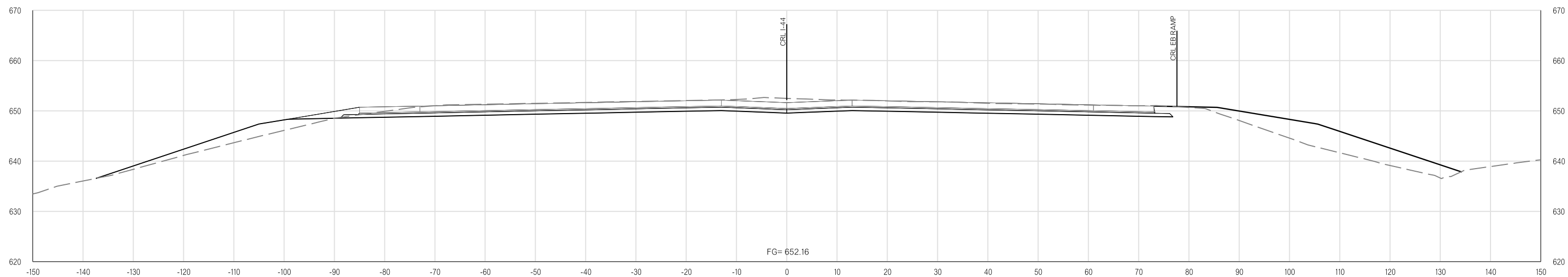
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325+00.00



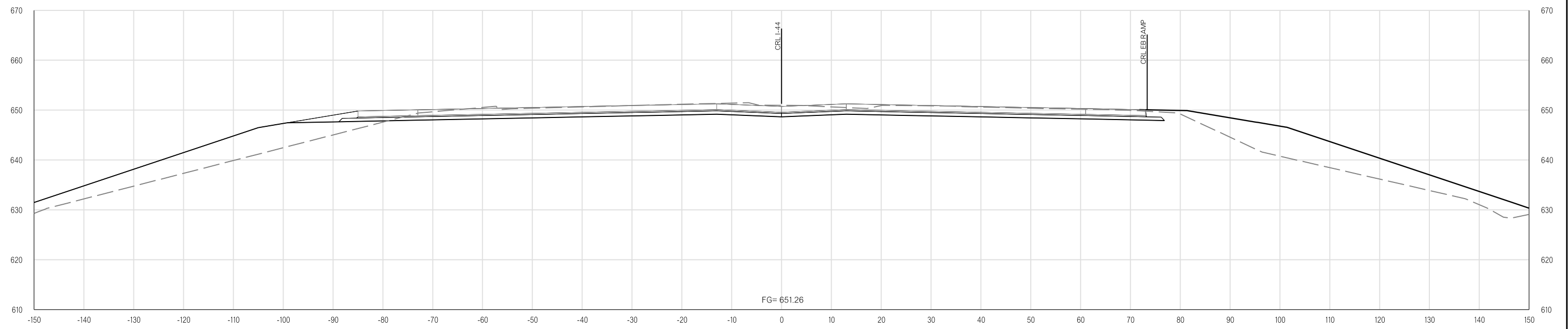
324+00.00

I-44

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3/17/2021

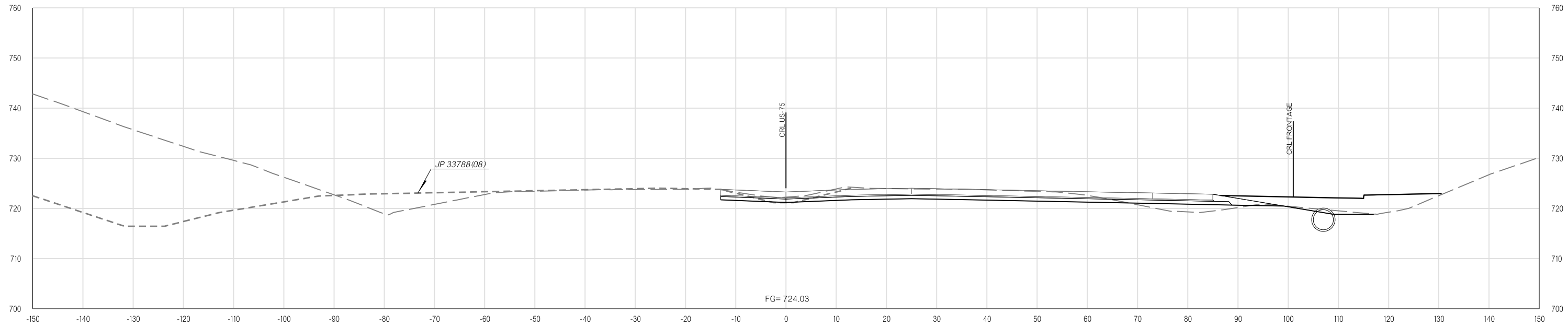


325+77.00

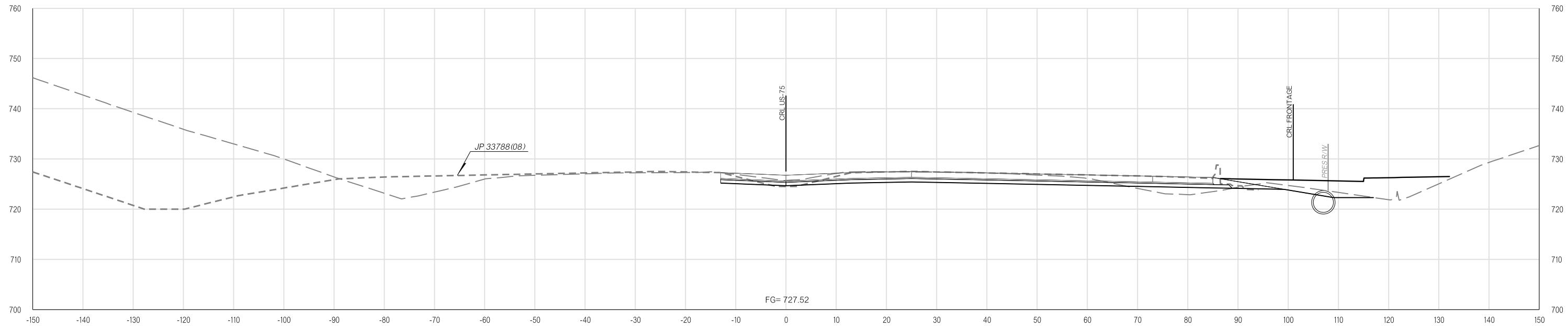
I-44

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548+00.00

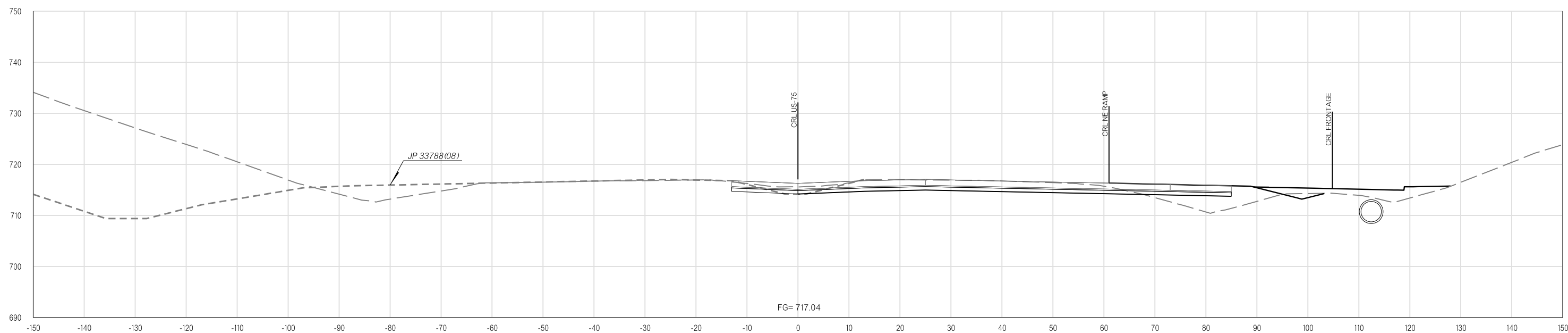


547+00.00

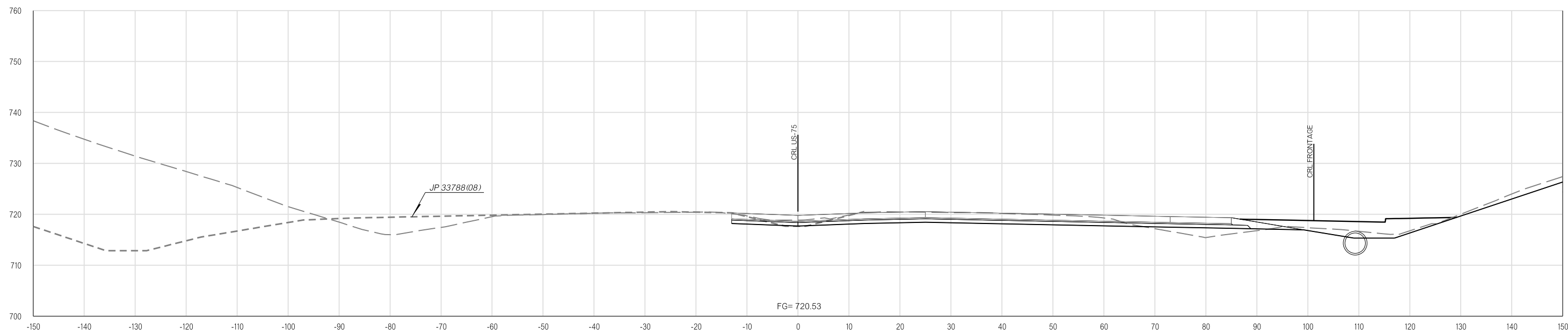
US-75

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550+00.00

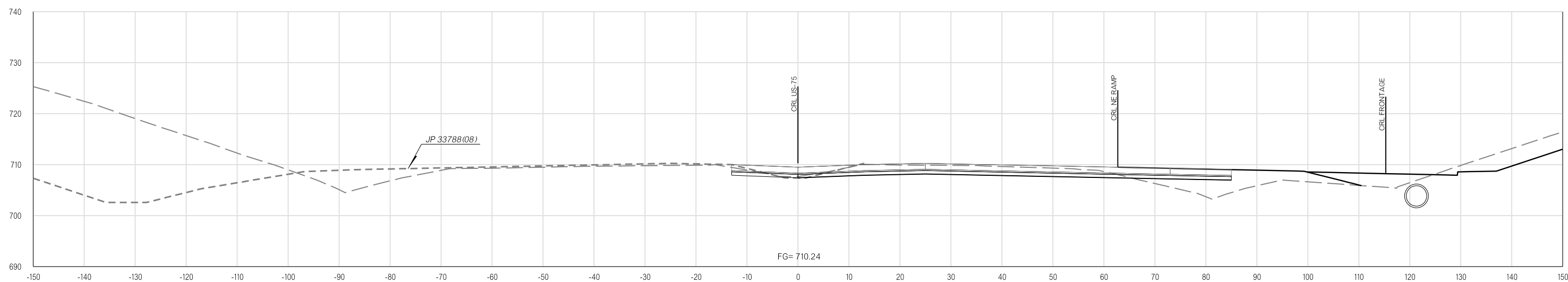


549+00.00

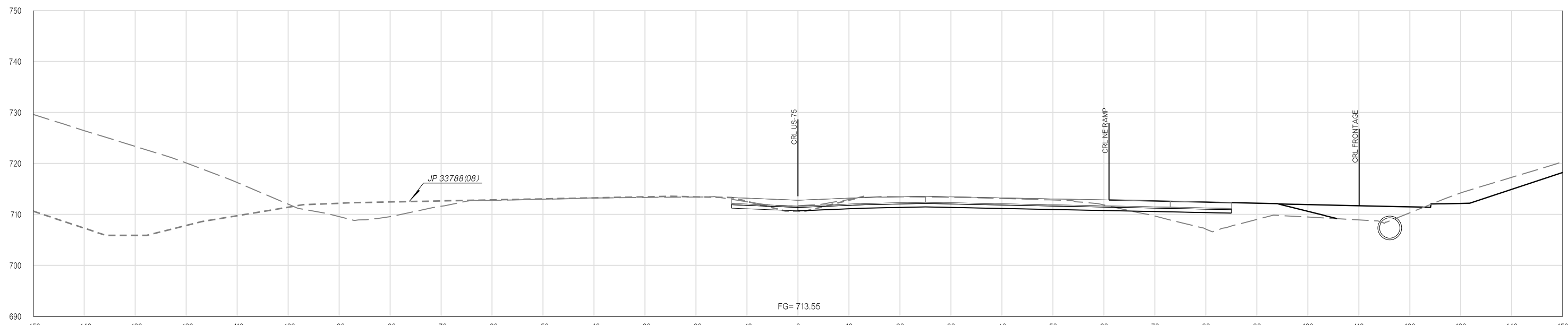
US-75

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552+00.00

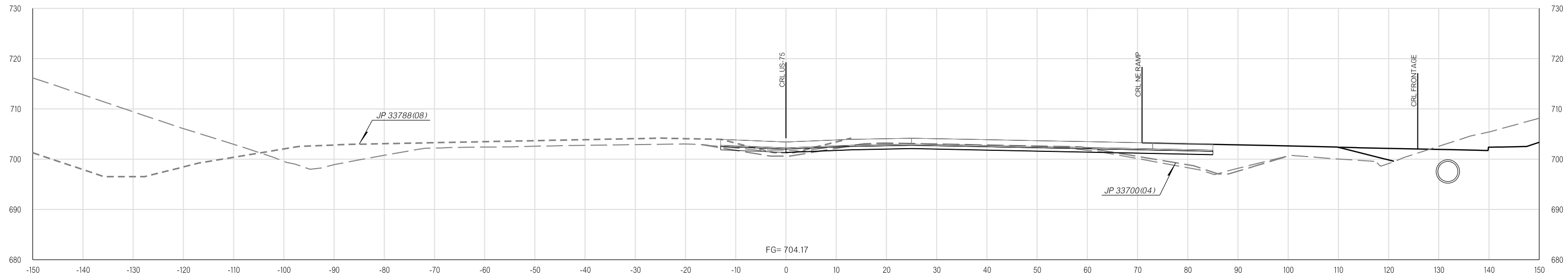


551+00.00

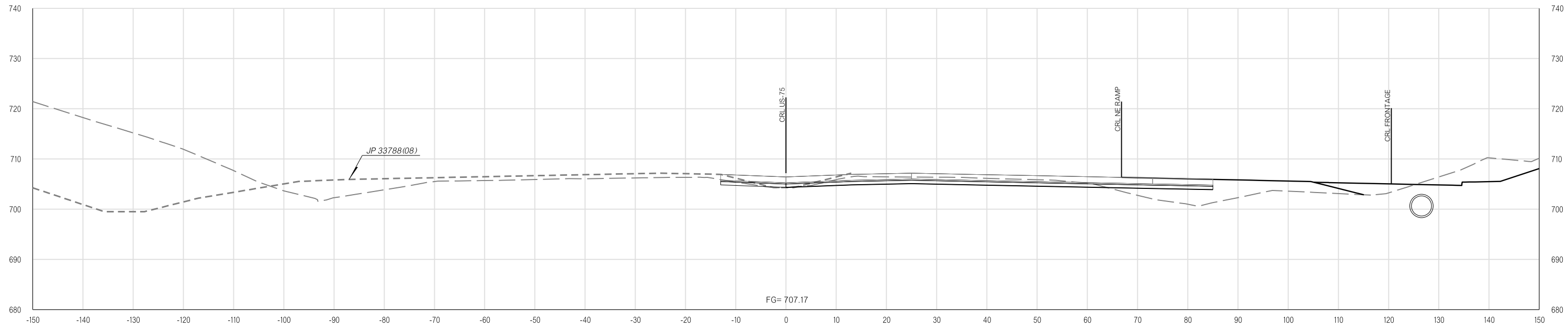
US-75

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554+00.00

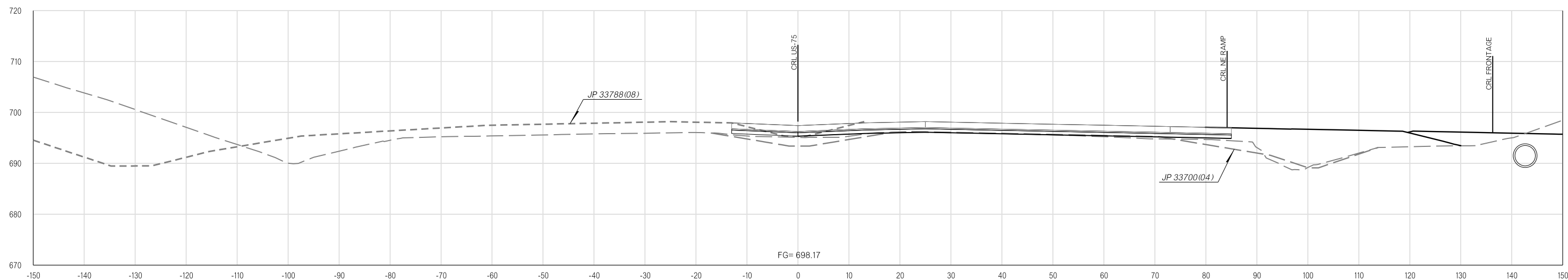


553+00.00

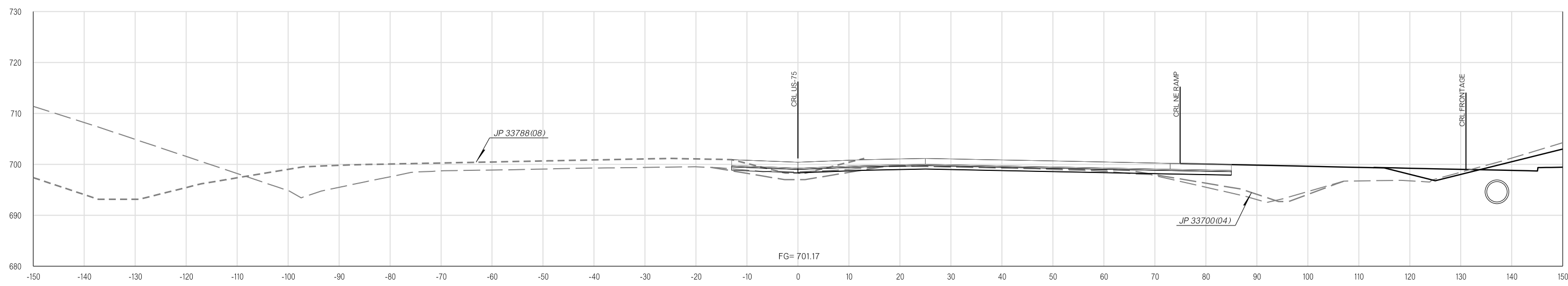
US-75

R/W UTILITY MEETING

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556+00.00

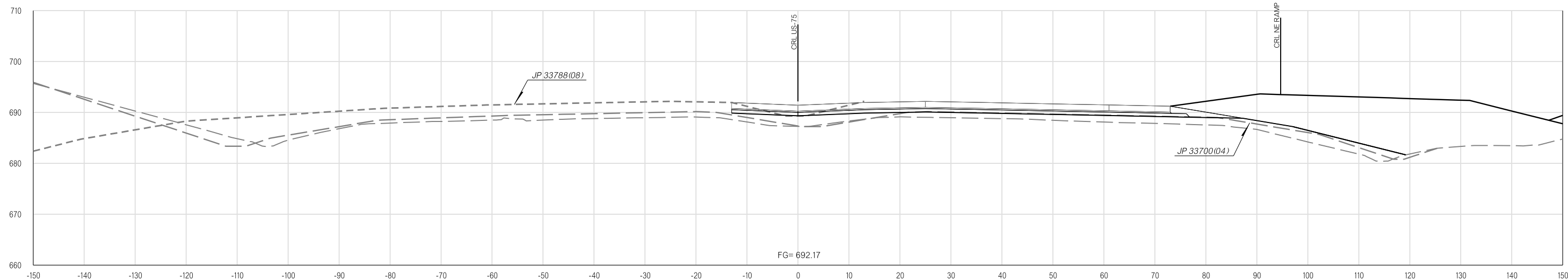


555+00.00

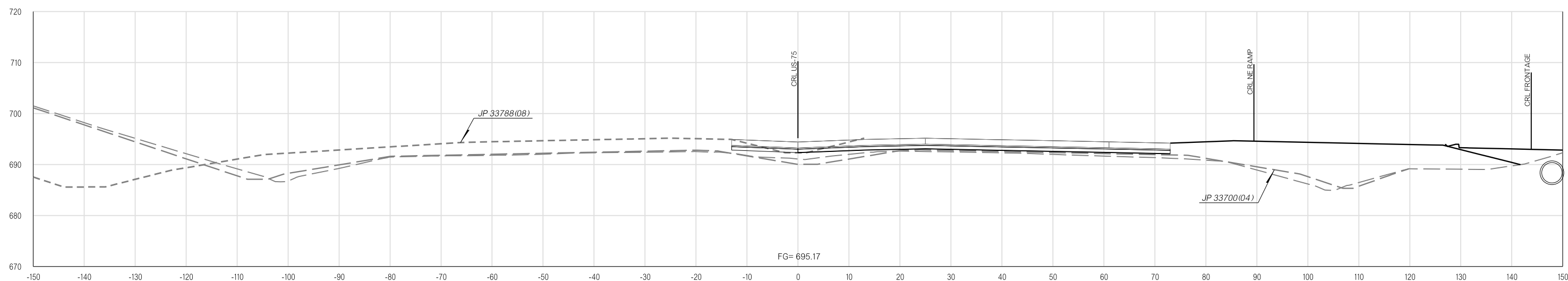
US-75

R/W UTILITY MEETING

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558+00.00



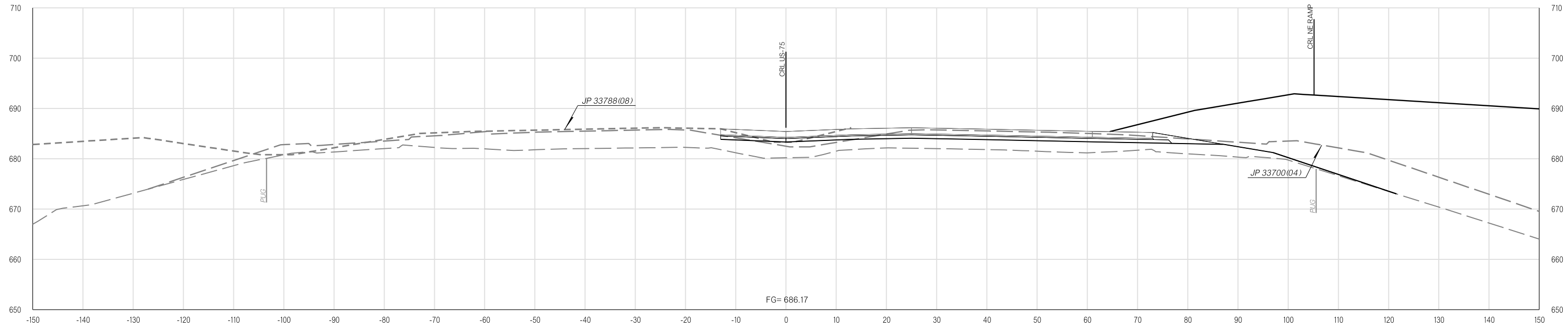
557+00.00

US-75

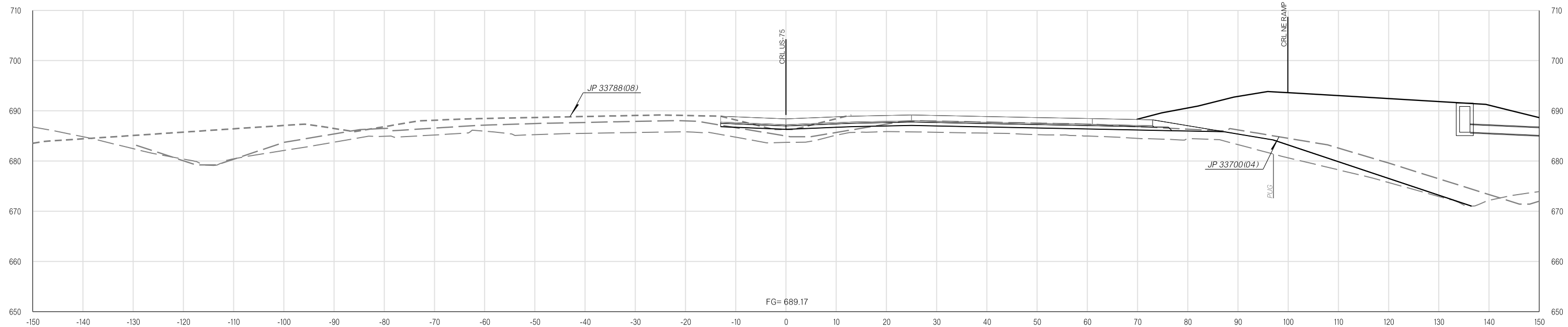
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560+00.00

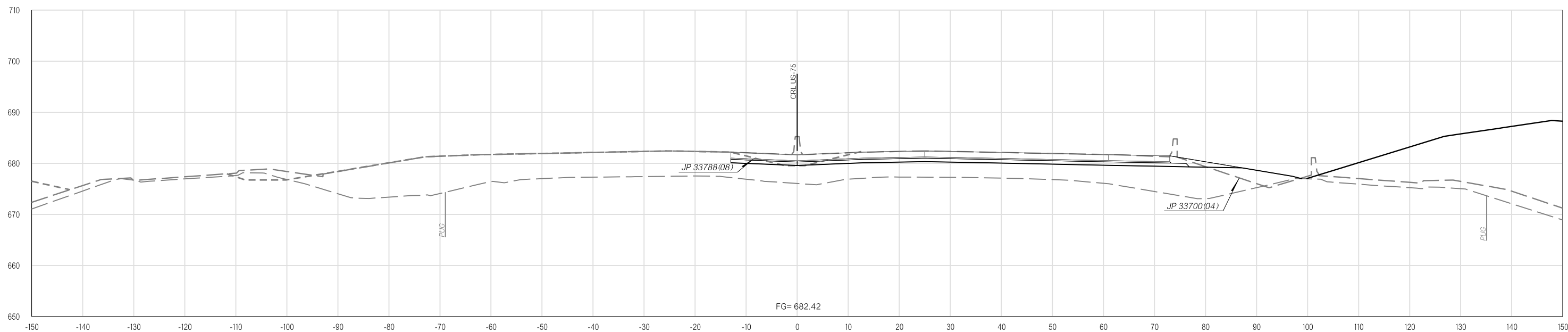


559+00.00

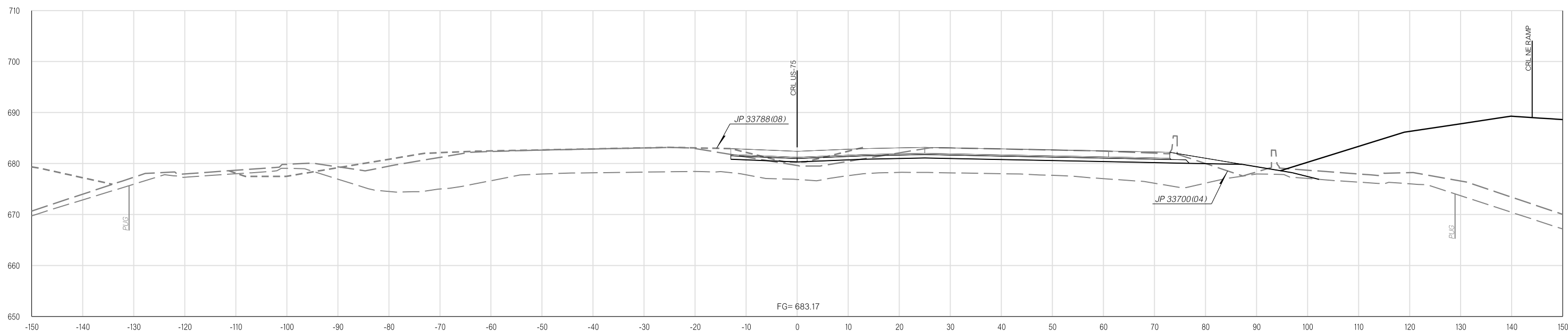
US-75

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561+25.00

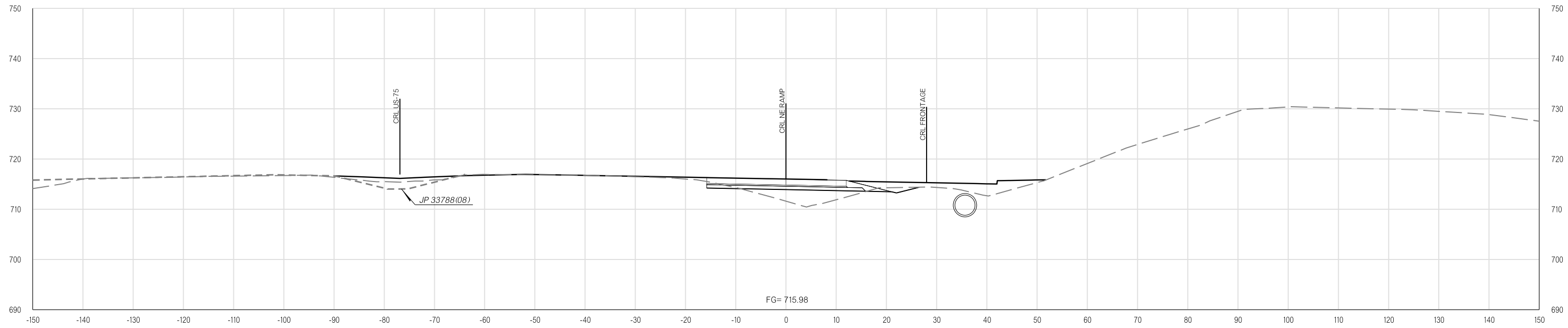


561+00.00

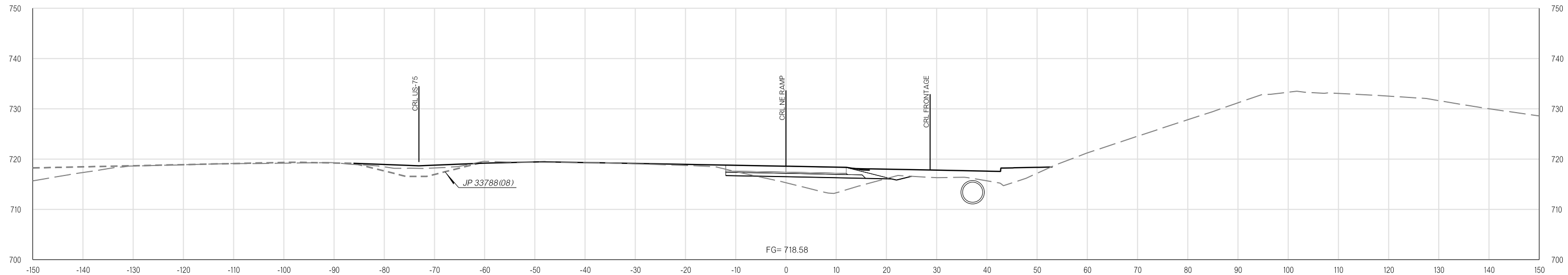
US-75

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150+00.00

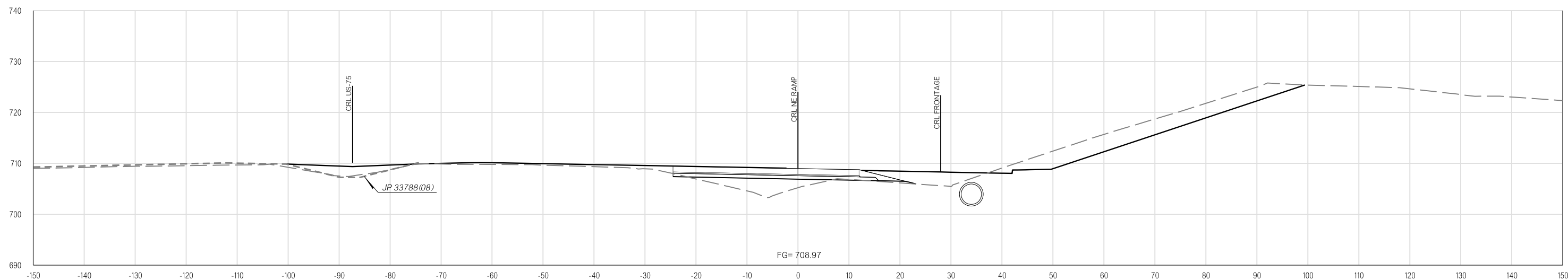


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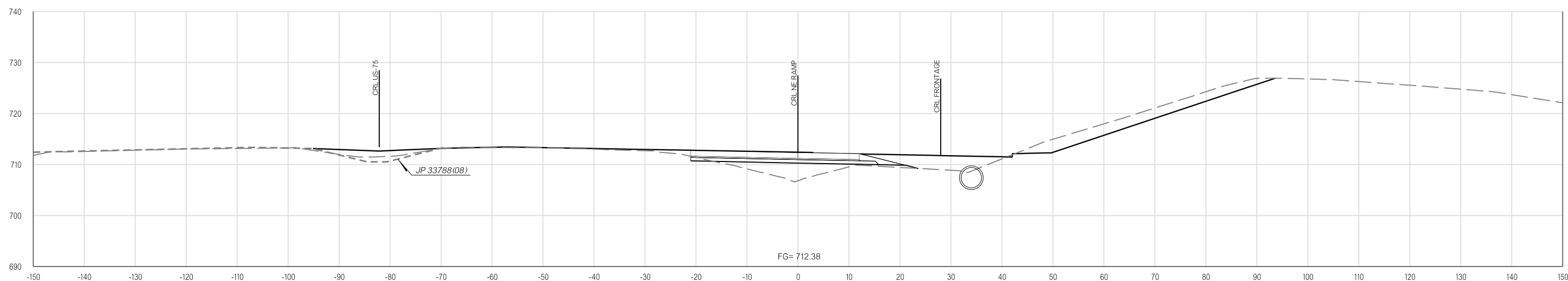
I-75 TO I-44 N-E RAMP

R/W UTILITY MEETING

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152+00.00

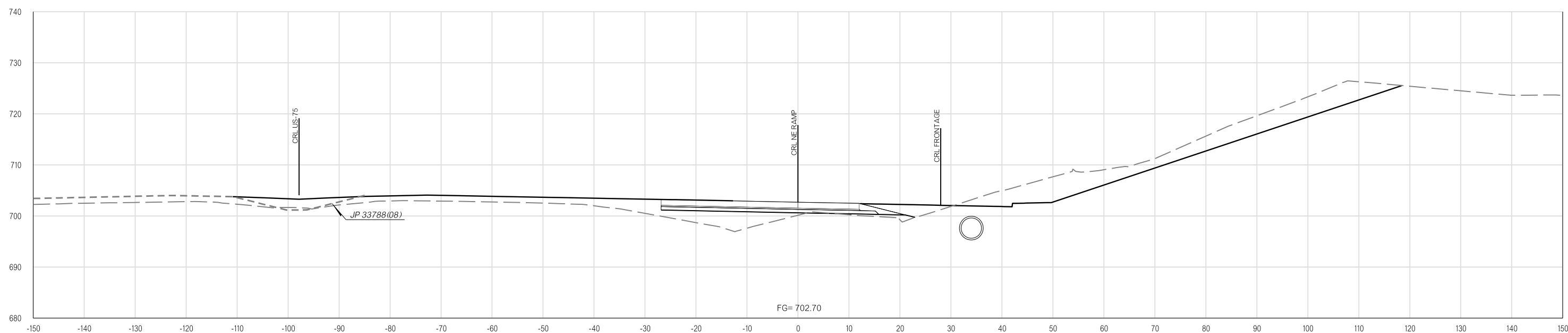


151+00.00

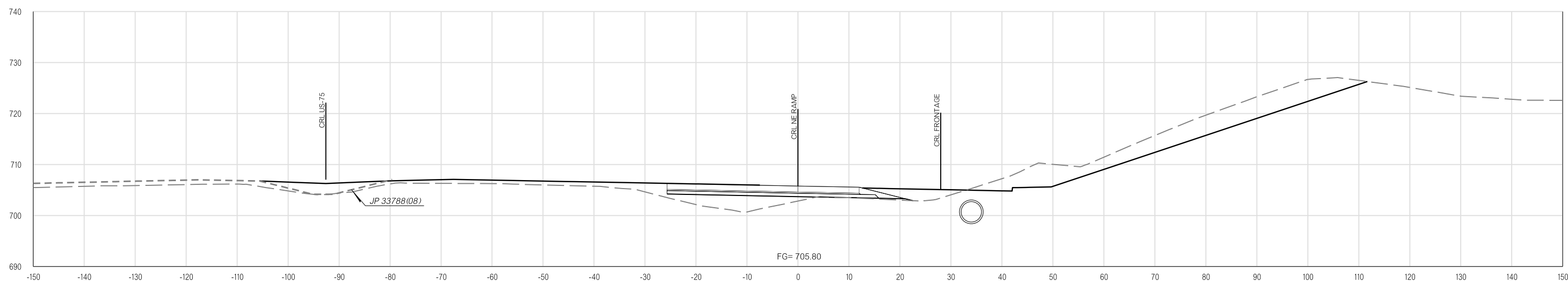
I-75 TO I-44 N-E RAMP

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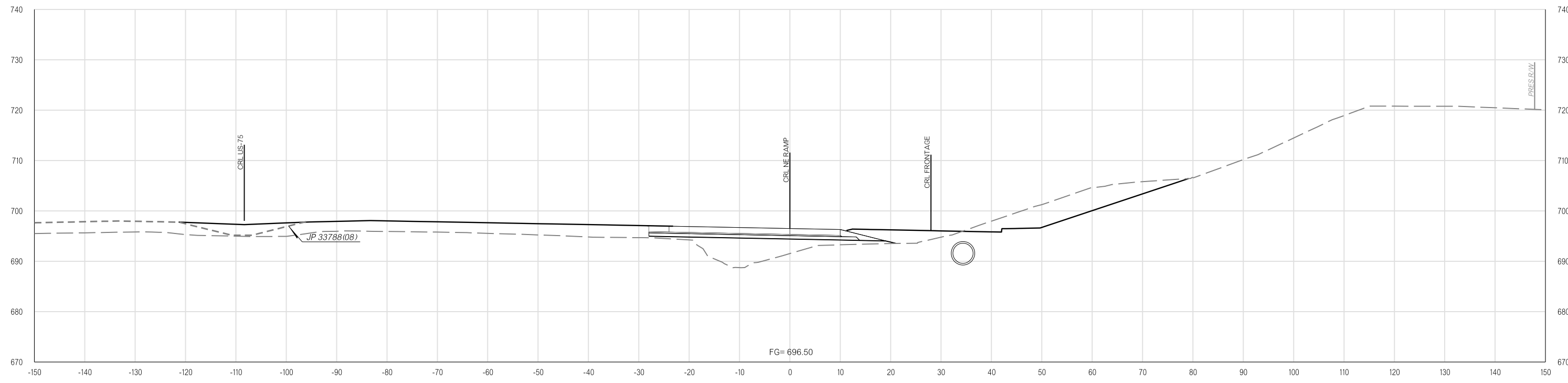


154+00.00

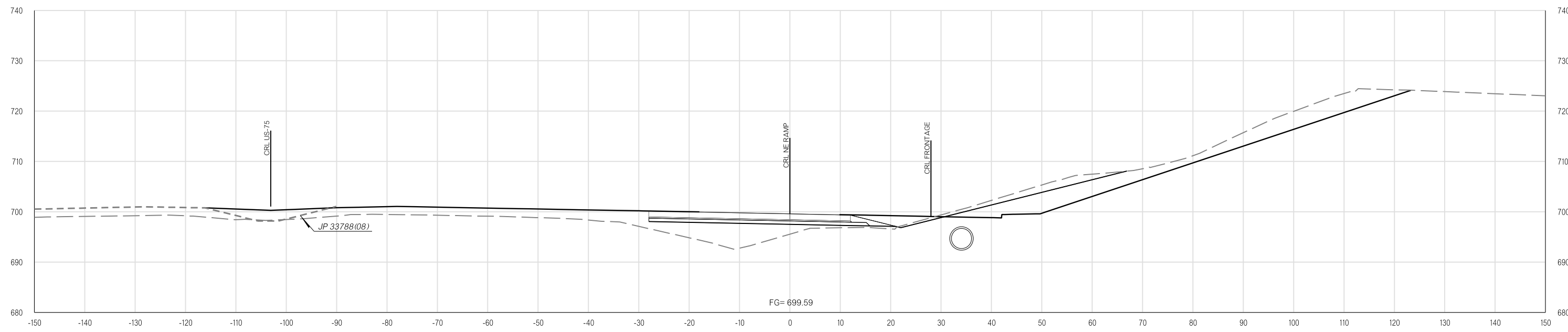


153+00.00

I-75 TO I-44 N-E RAMP



156+00.00



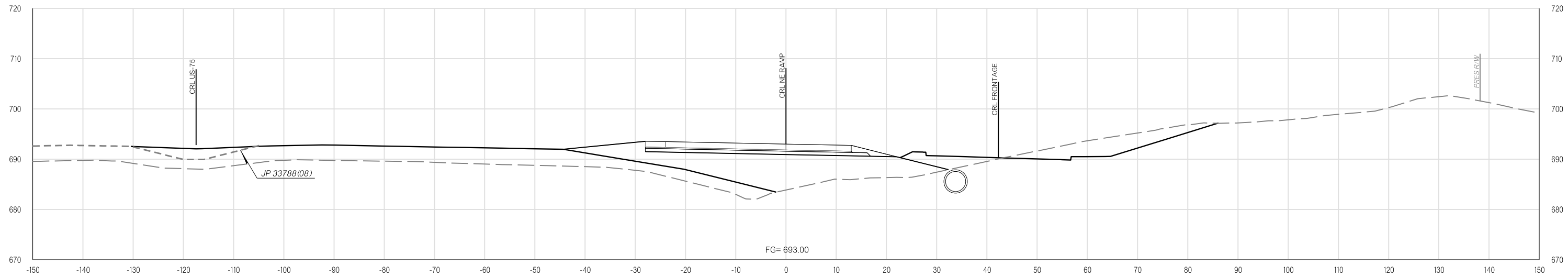
155+00.00

I-75 TO I-44 N-E RAMP

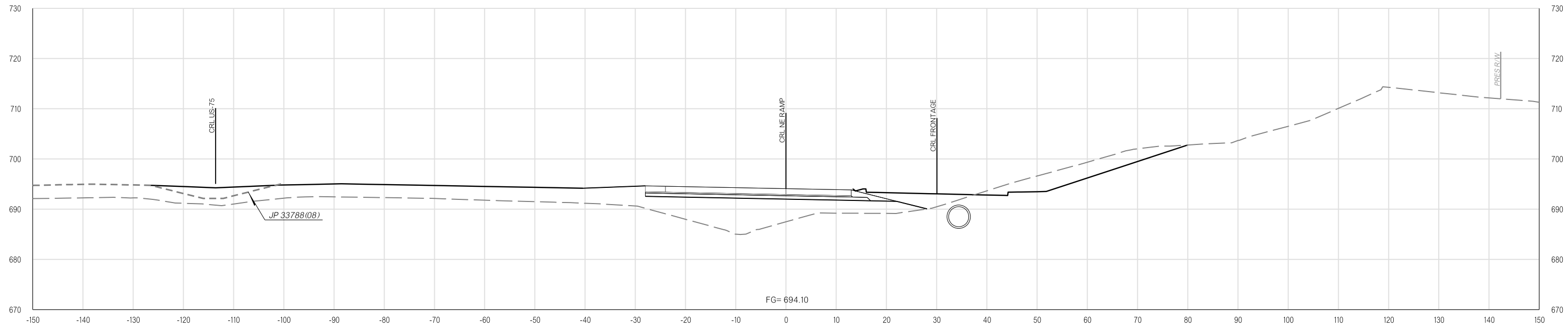
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157+74.17

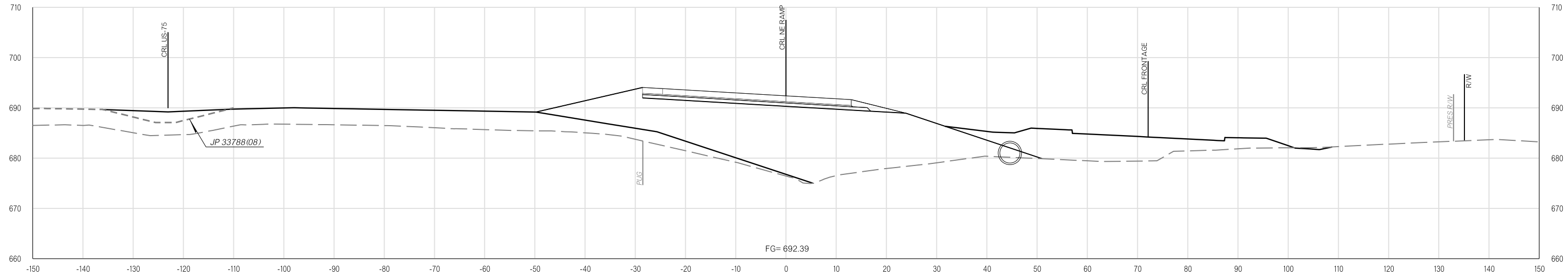


157+00.00

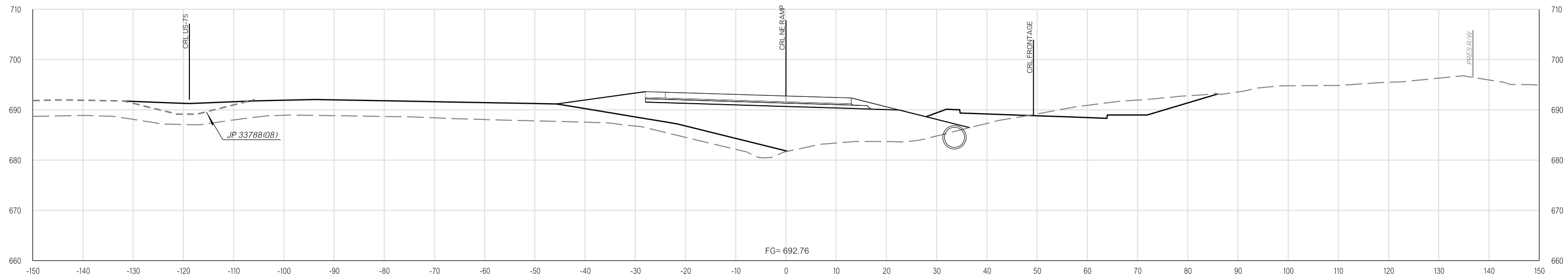
I-75 TO I-44 N-E RAMP

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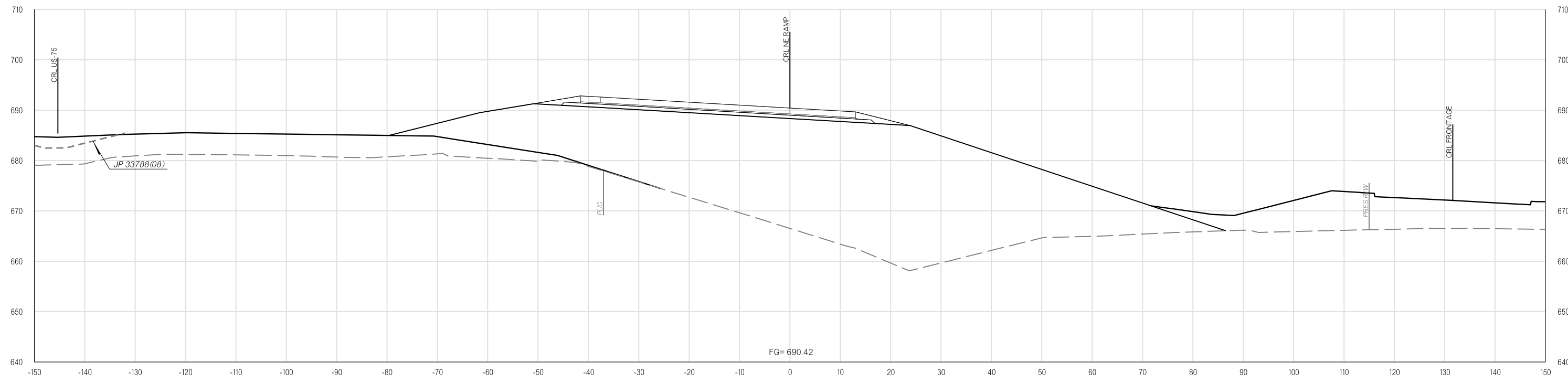


158+65.24

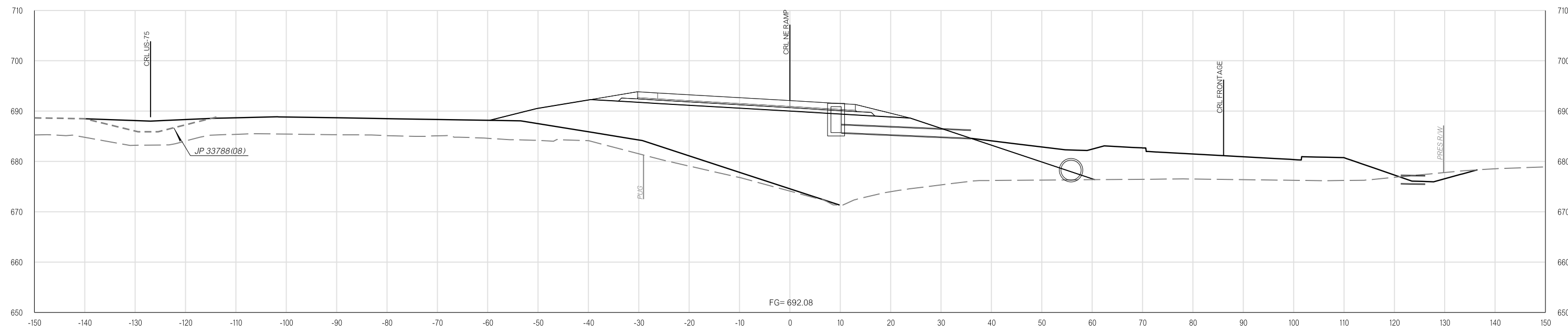


158+00.00

I-75 TO I-44 N-E RAMP

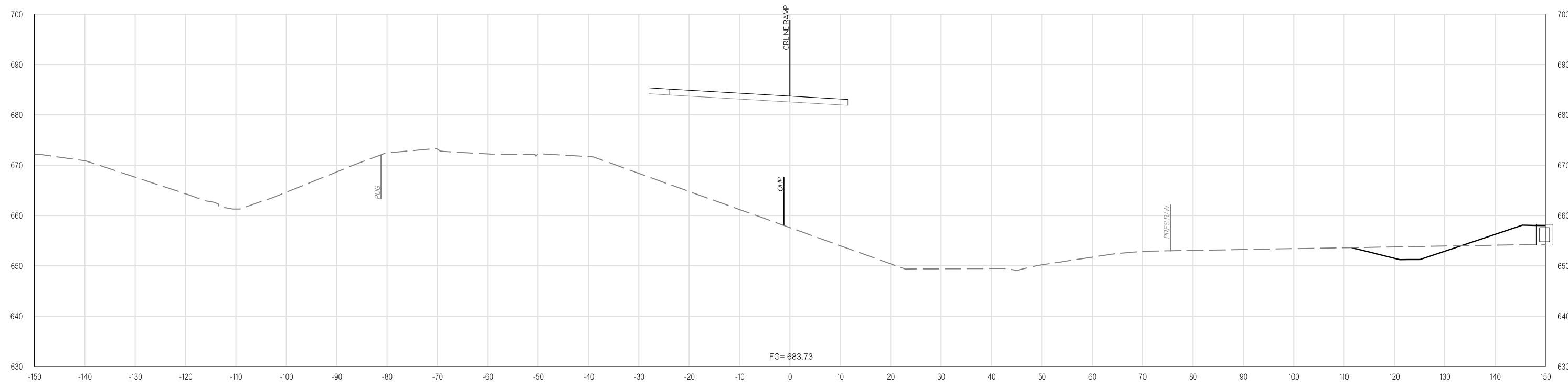


160+00.00

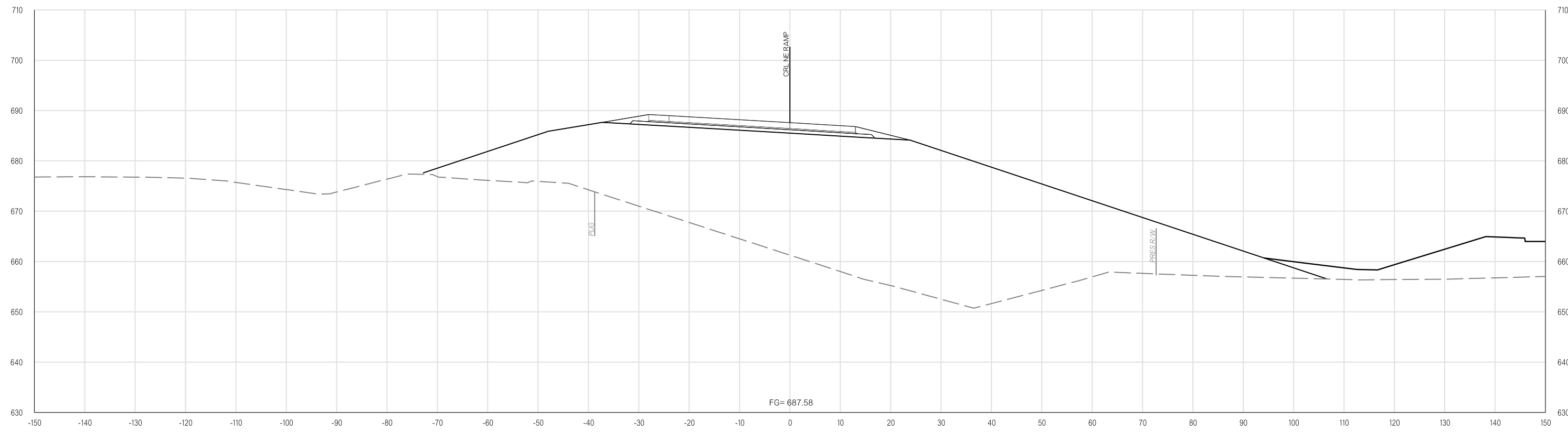


159+00.00

I-75 TO I-44 N-E RAMP



162+00.00



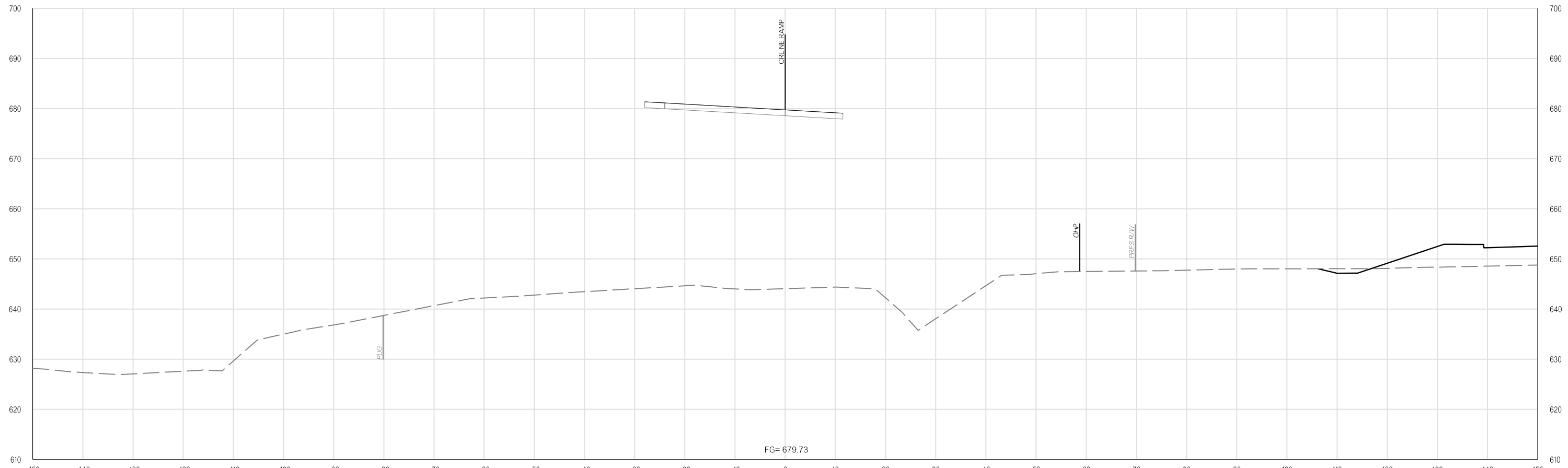
161+00.00

I-75 TO I-44 N-E RAMP

R/W UTILITY MEETING

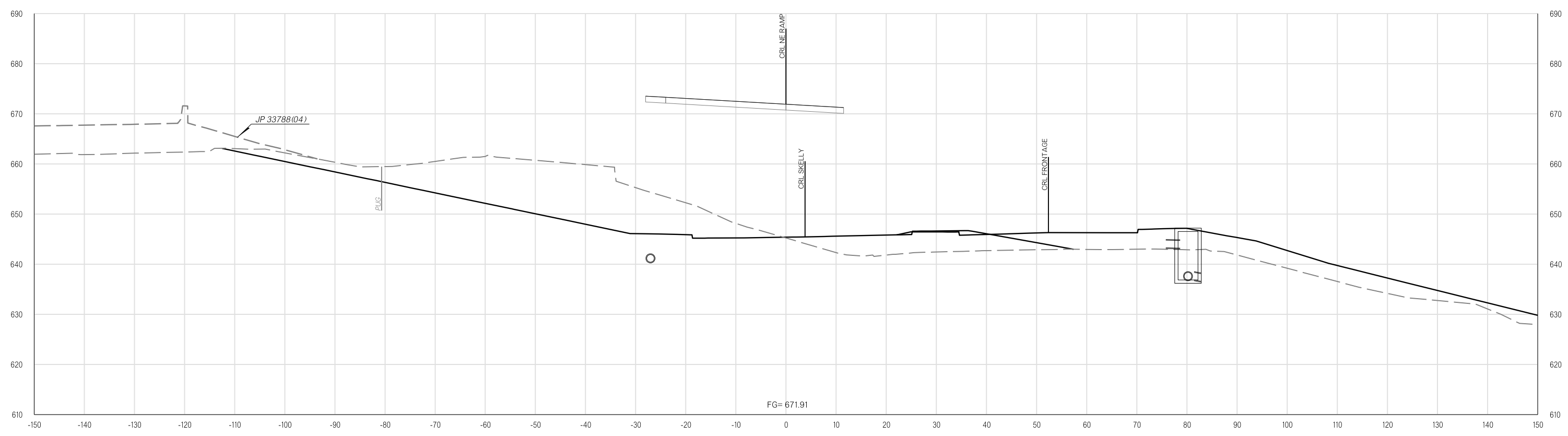
Mar. 17, 2021

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3/17/2021

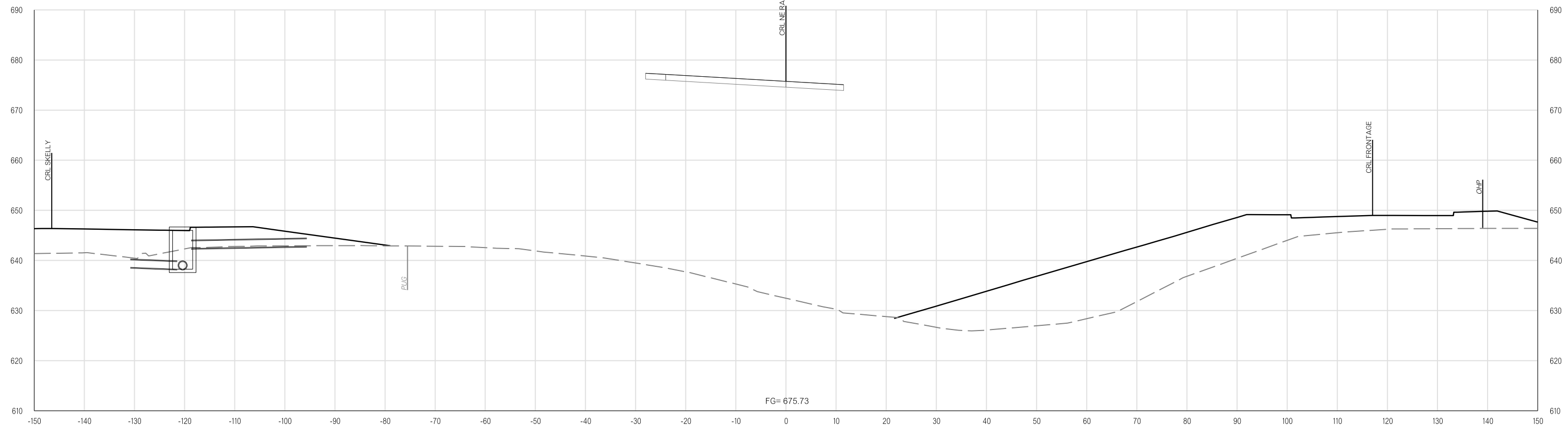


163+00.00

I-75 TO I-44 N-E RAMP

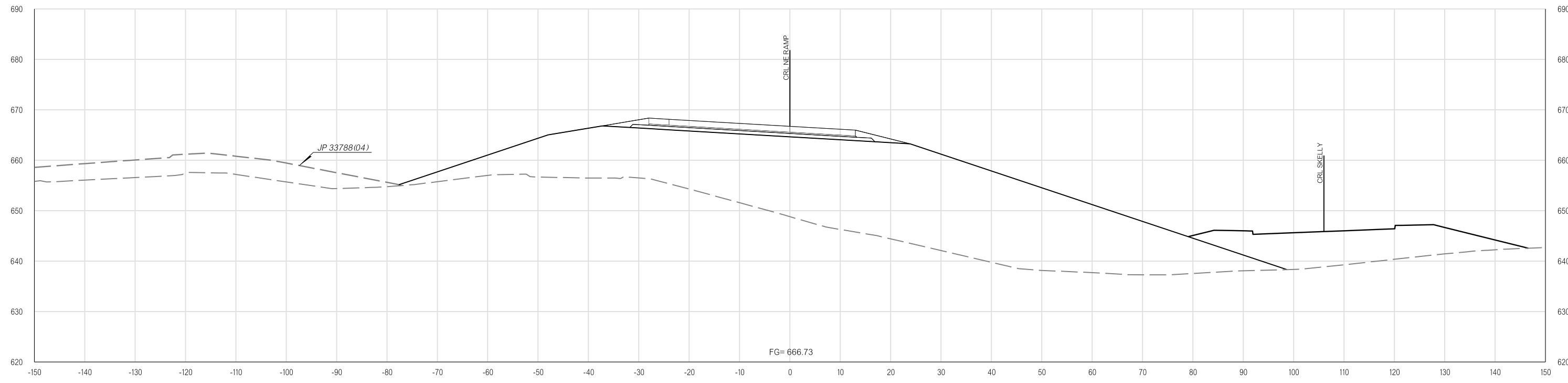


165+00.00

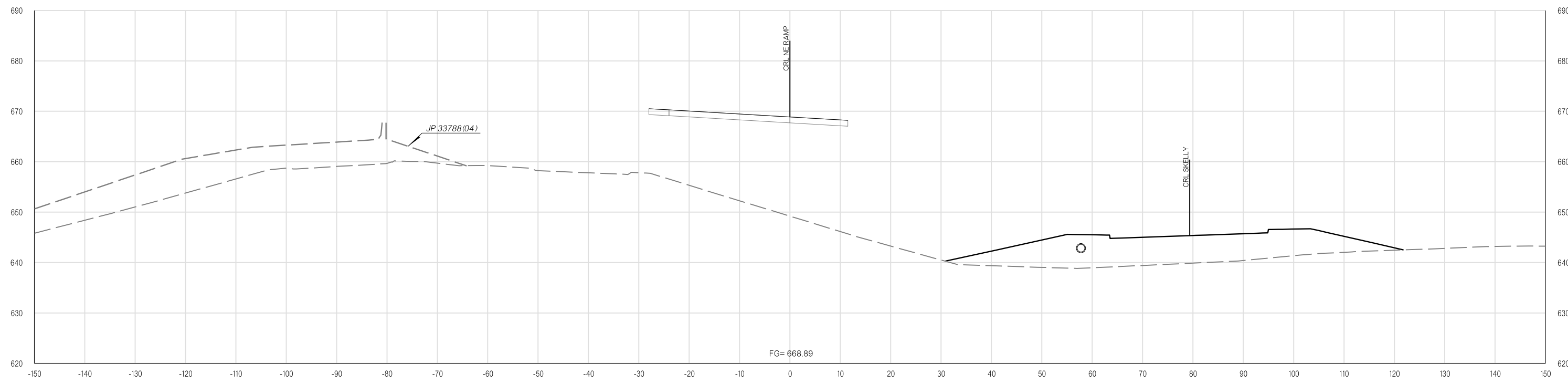


164+00.00

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3/17/2021



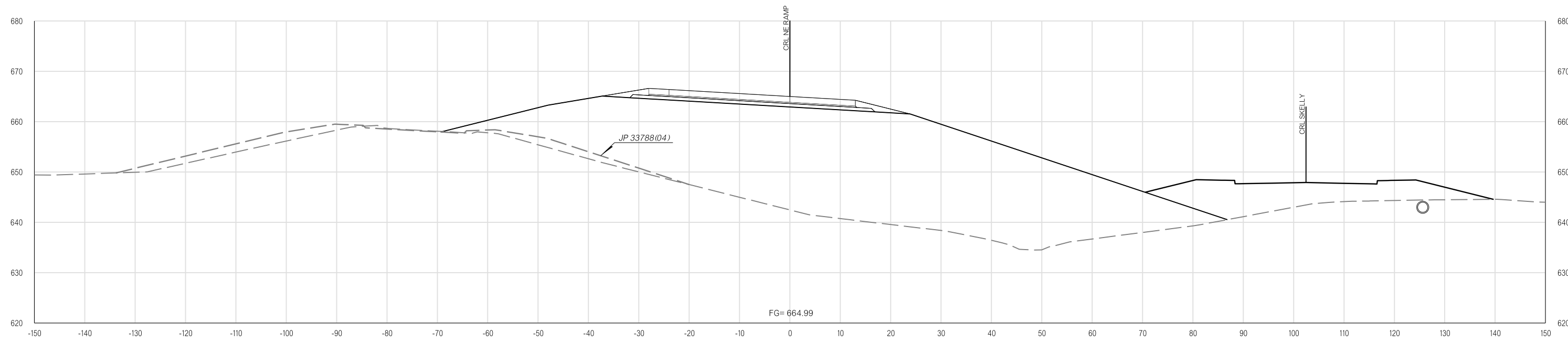
167+00.00



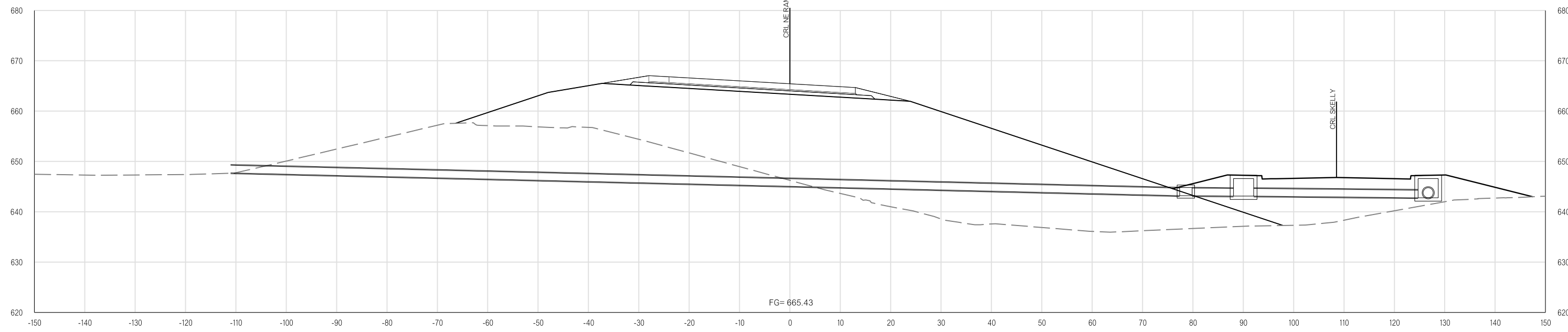
166+00.00

I-75 TO I-44 N-E RAMP

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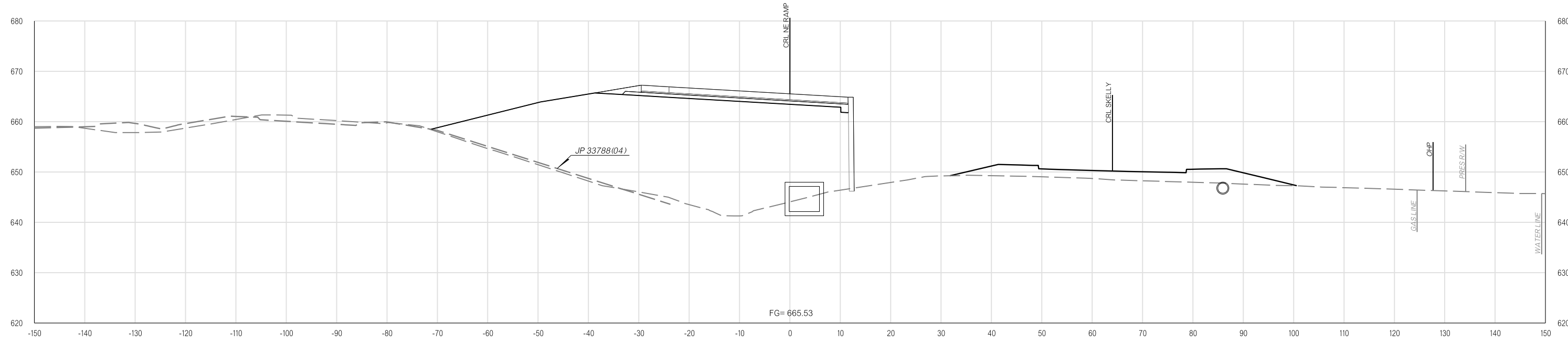


169+00.00

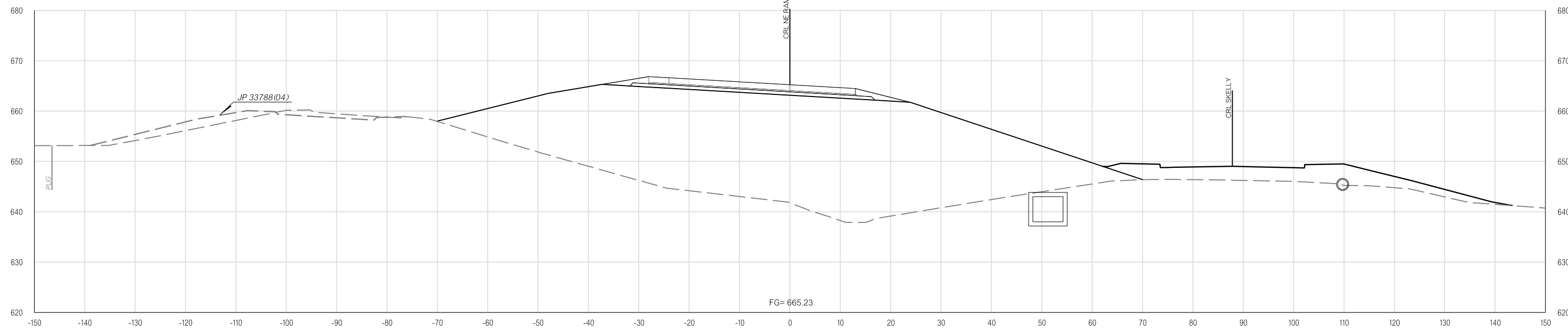


168+00.00

I-75 TO I-44 N-E RAMP

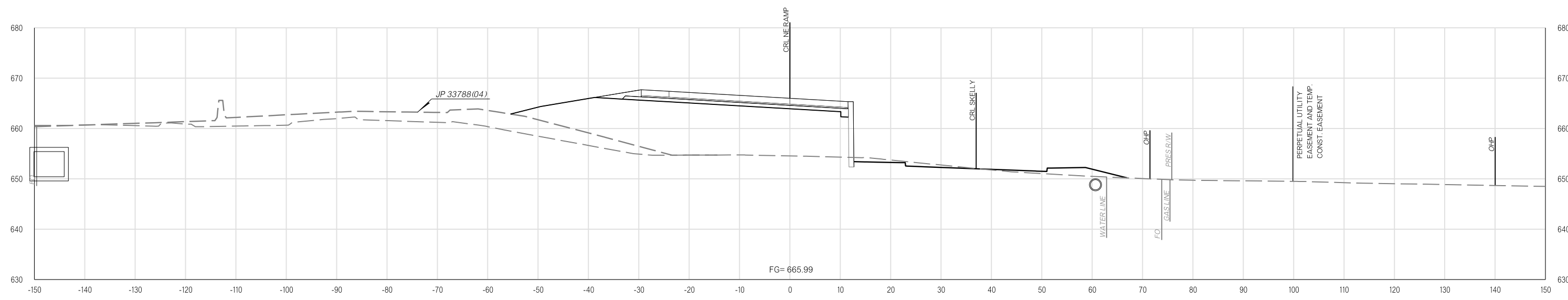


171+00.00

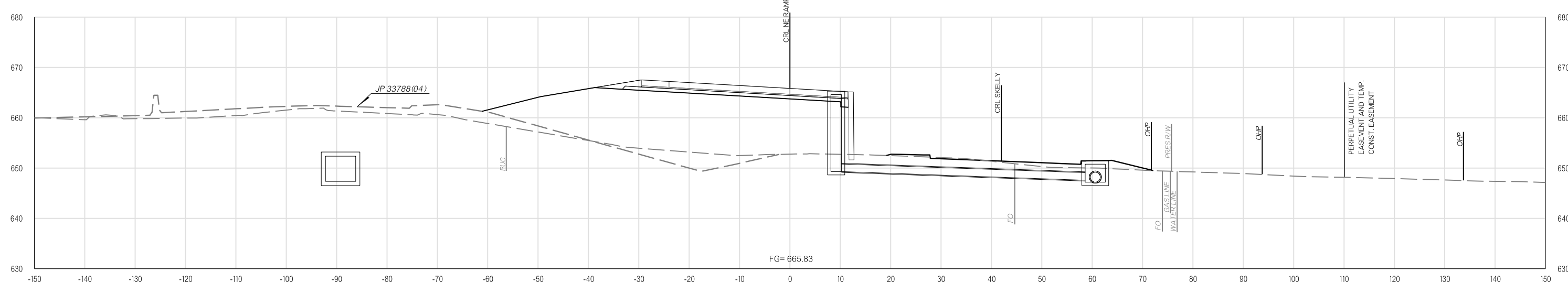


170+00.00

I-75 TO I-44 N-E RAMP



172+51.33



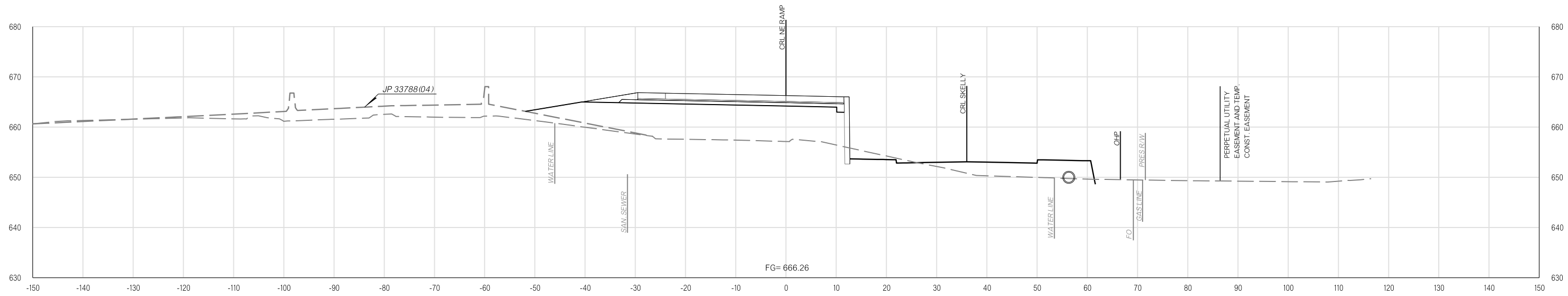
172+00.00

I-75 TO I-44 N-E RAMP

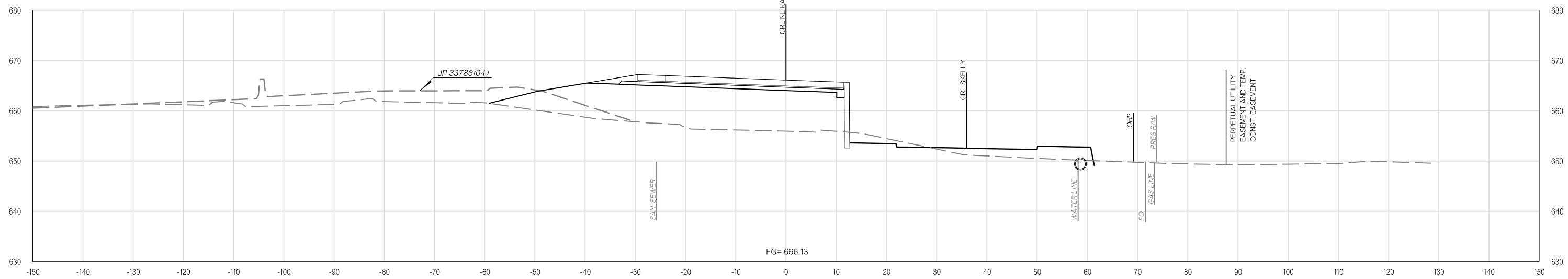
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173+42.40

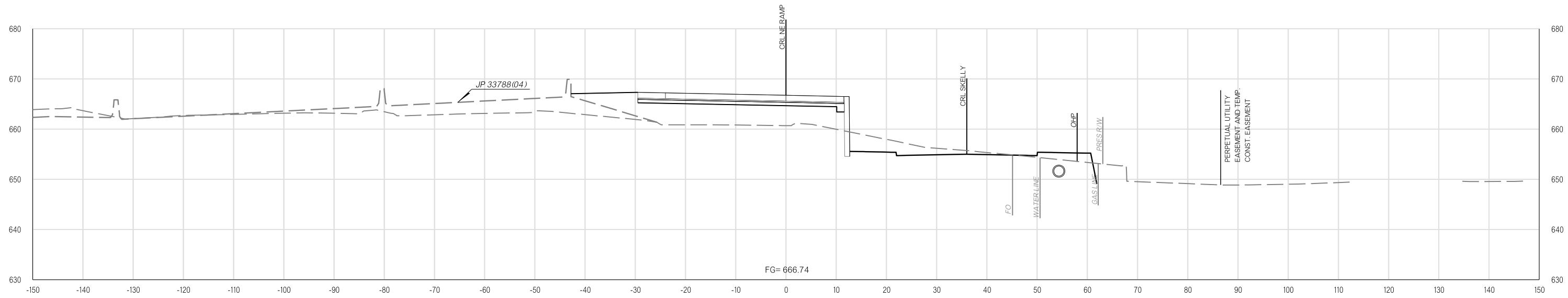


173+00.00

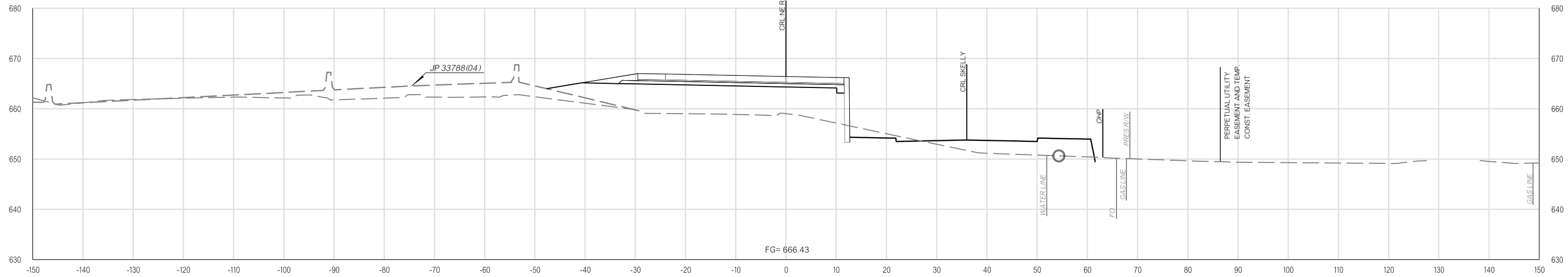
I-75 TO I-44 N-E RAMP

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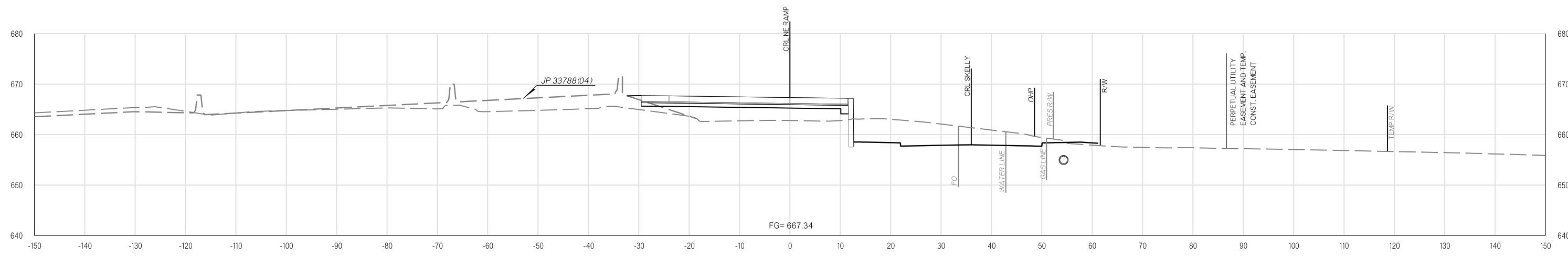


175+00.00

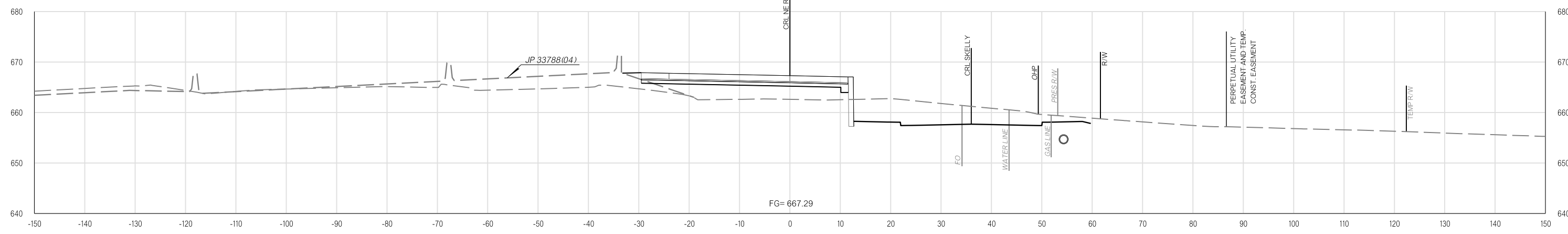


174+00.00

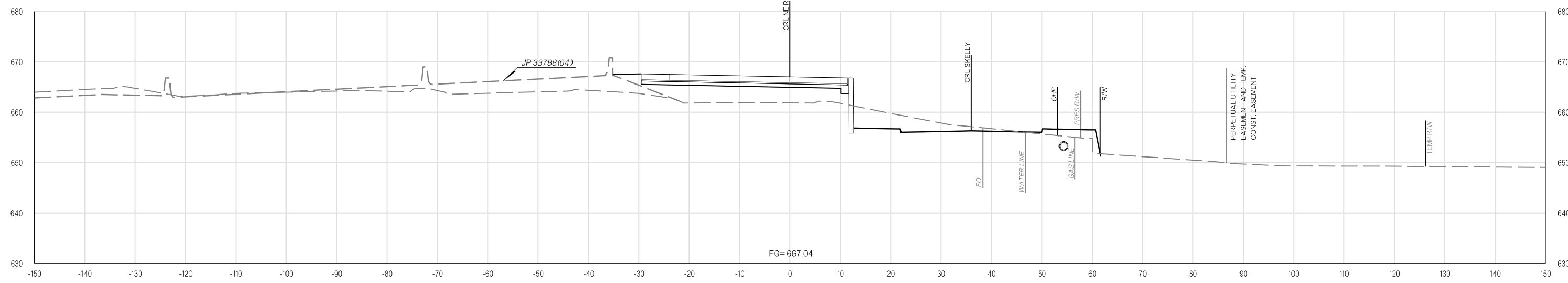
I-75 TO I-44 N-E RAMP



177+00.00



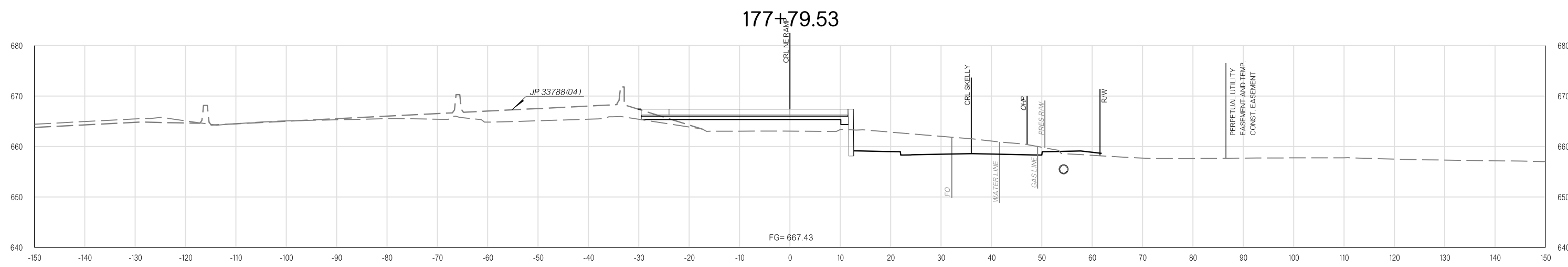
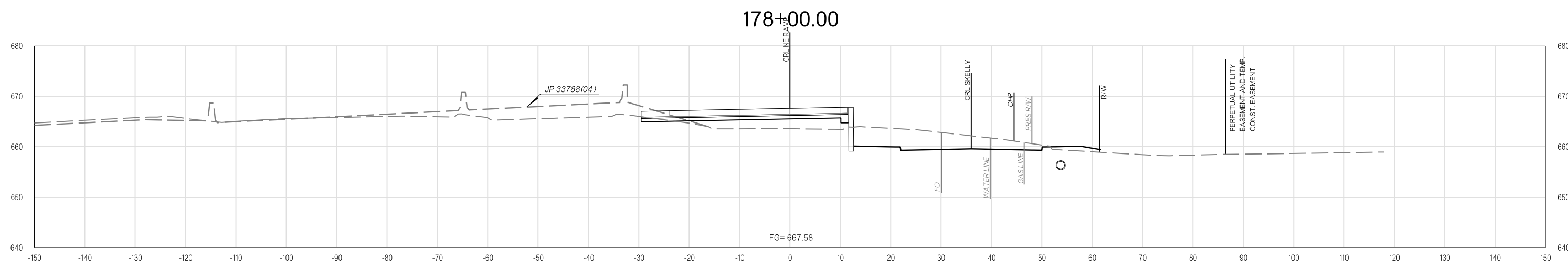
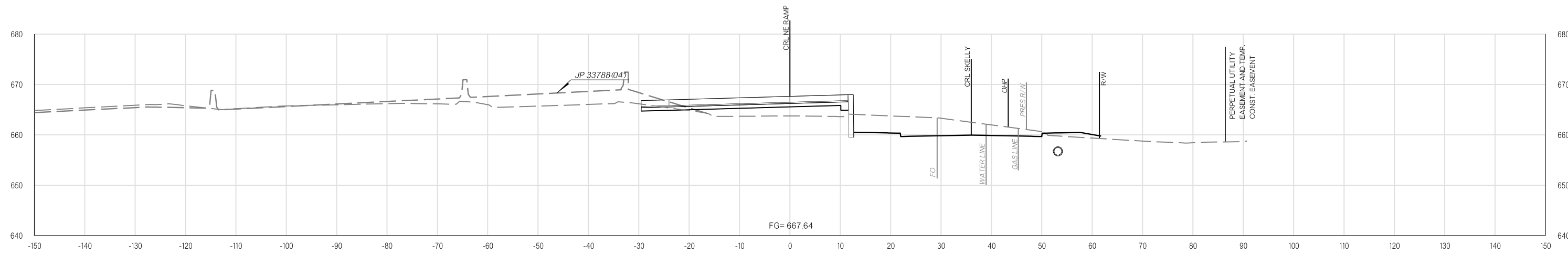
176+83.97



176+00.00

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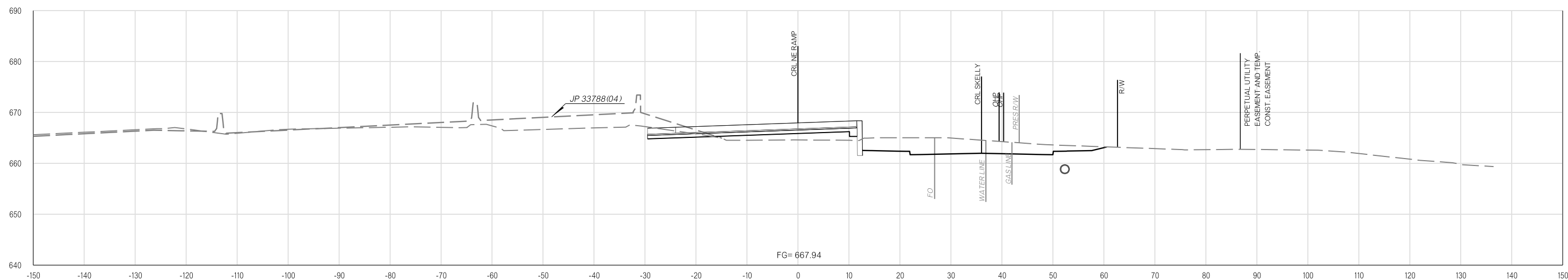
I-75 TO I-44 N-E RAMP



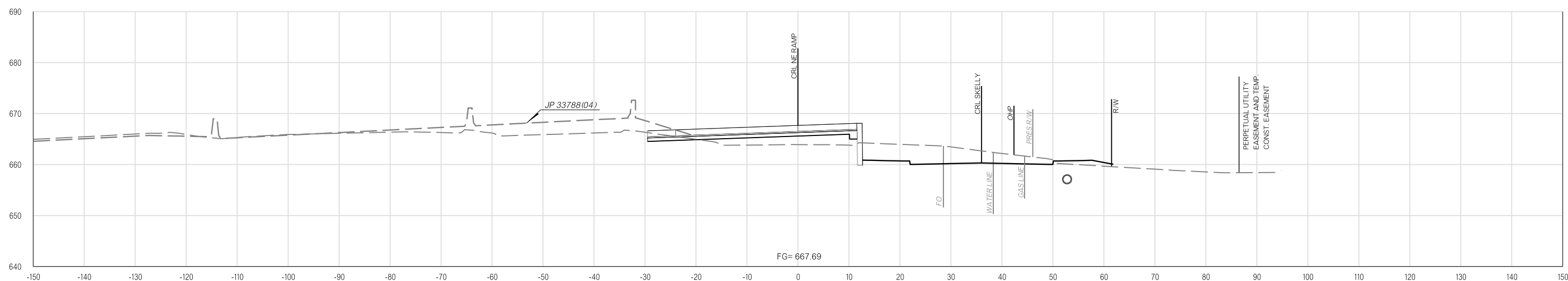
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179+00.00

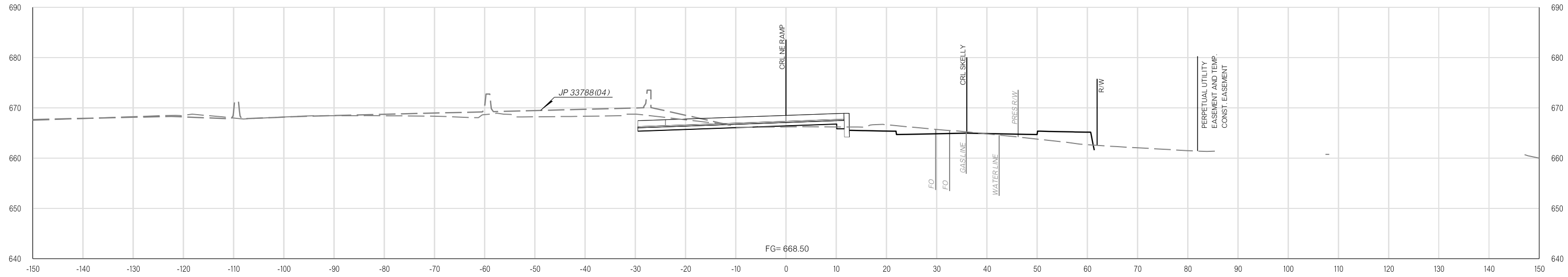


178+17.75

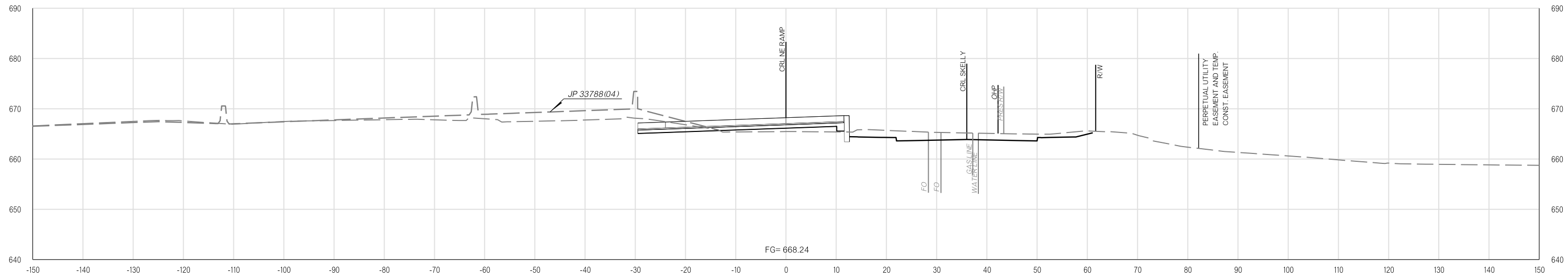
I-75 TO I-44 N-E RAMP

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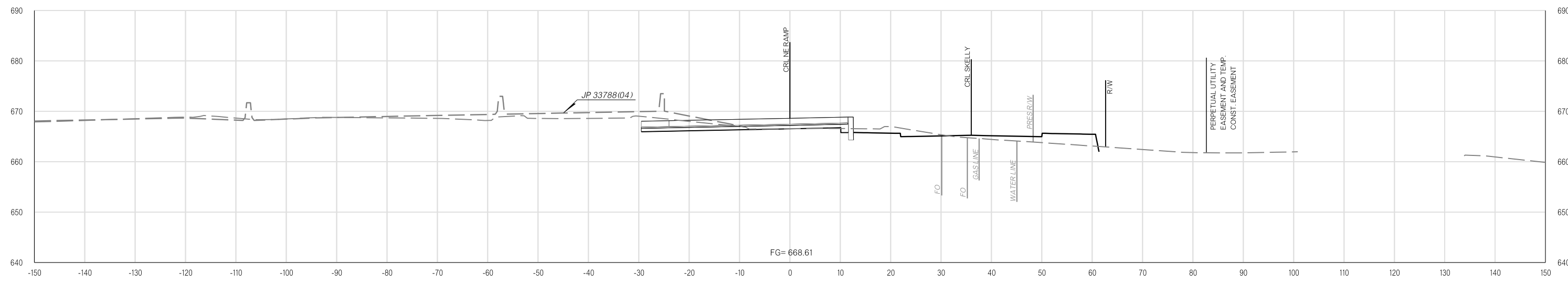


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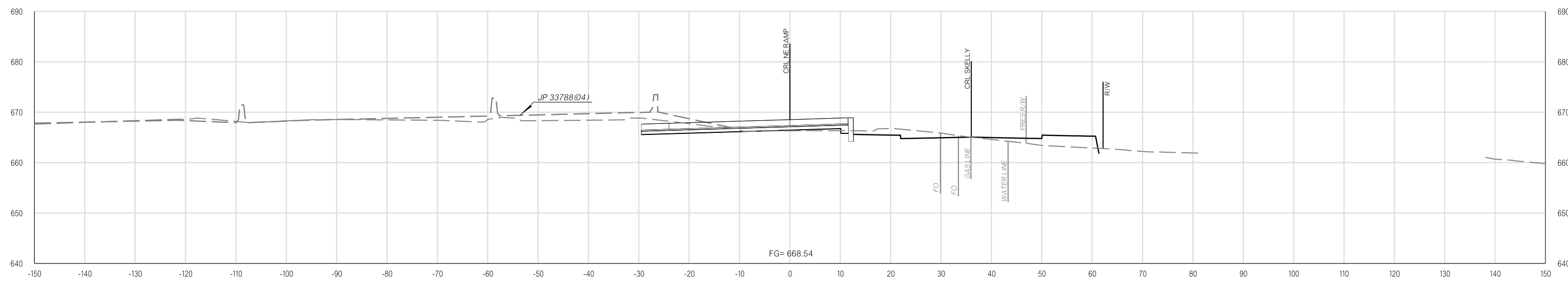


180+00.00

I-75 TO I-44 N-E RAMP



181+25.08

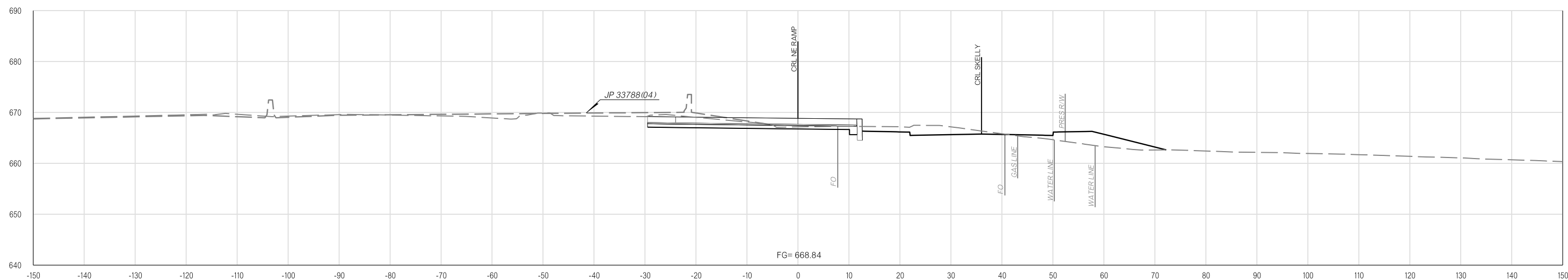


181+00.00

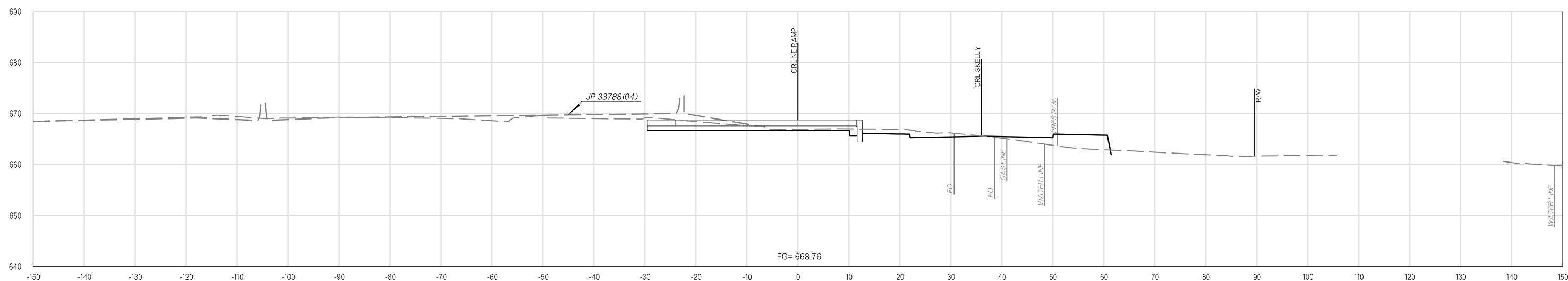
I-75 TO I-44 N-E RAMP

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182+00.00

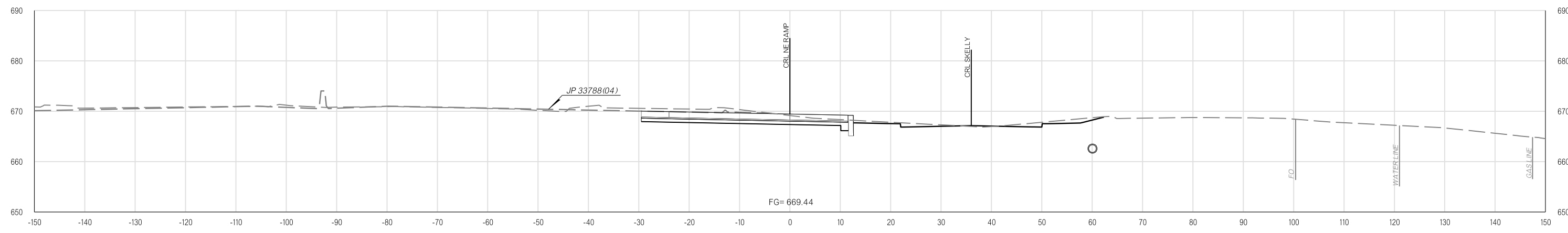


181+72.86

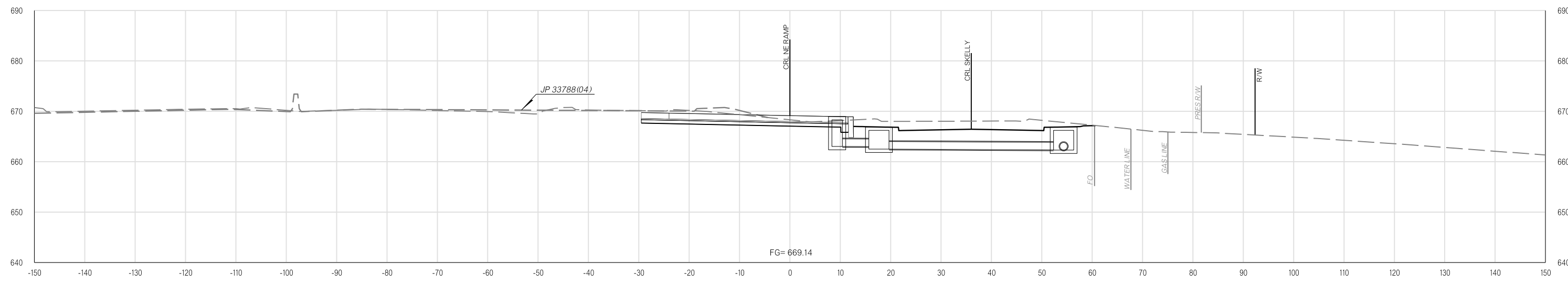
I-75 TO I-44 N-E RAMP

R/W UTILITY MEETING

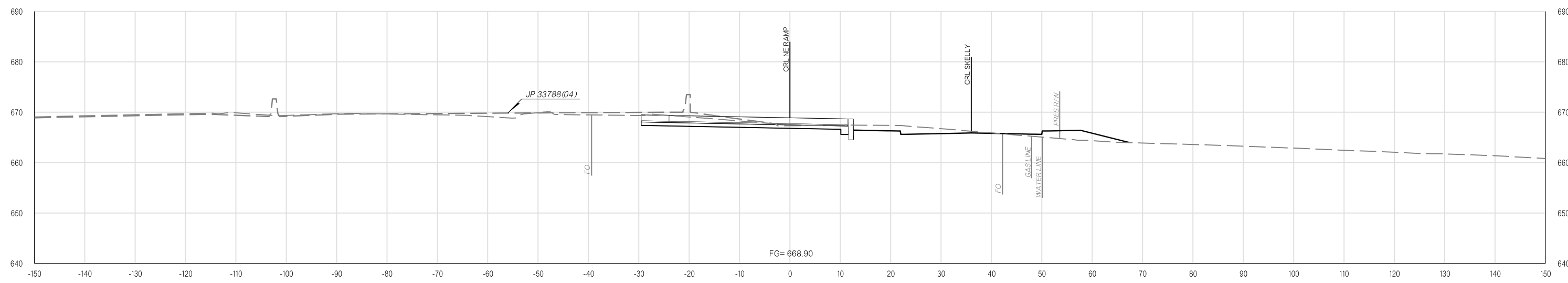
Mar. 17, 2021



184+00.00

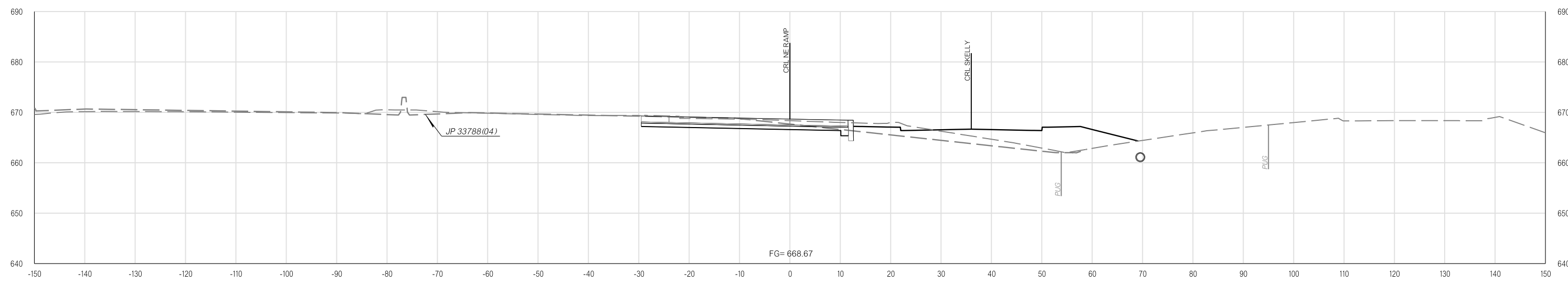


183+00.00

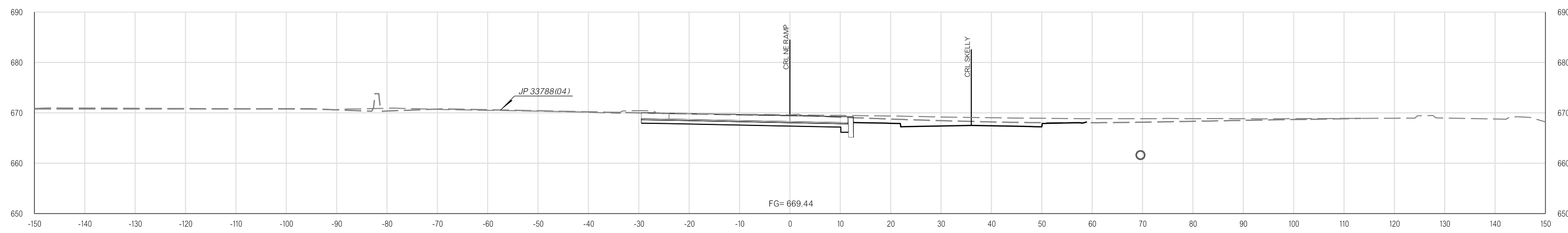


182+20.64

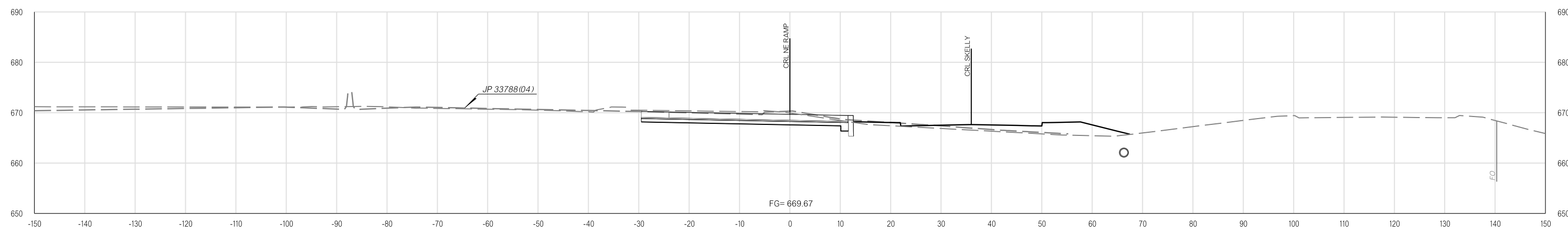
I-75 TO I-44 N-E RAMP



187+00.00



186+00.00

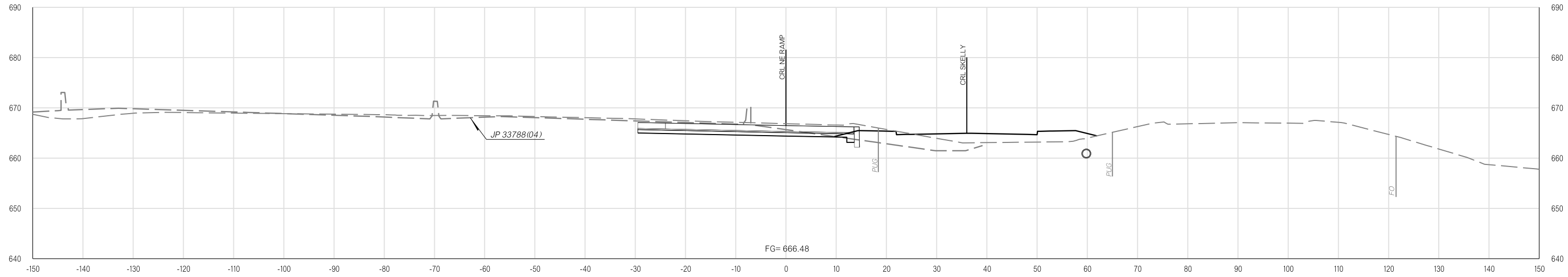


185+00.00

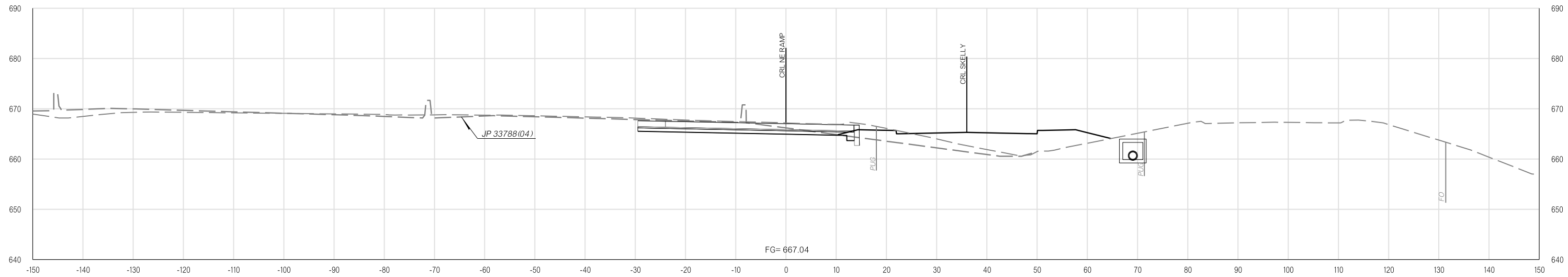
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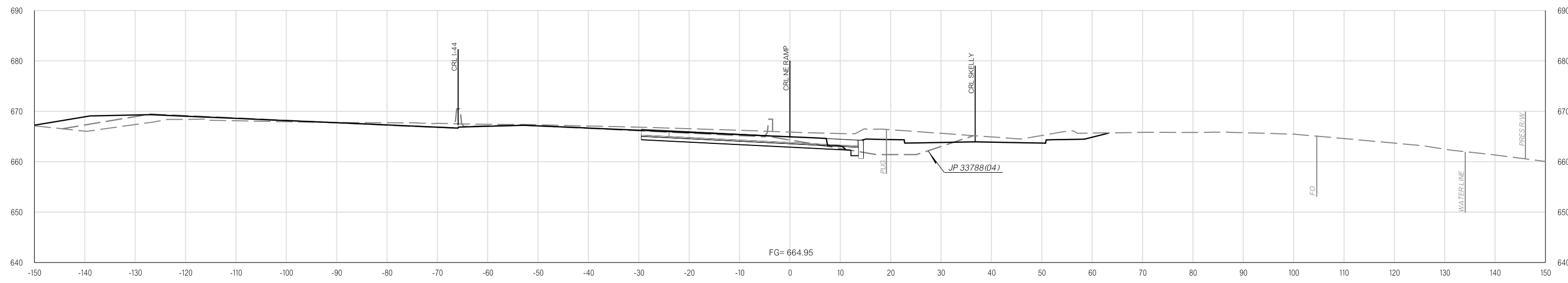


188+26.89

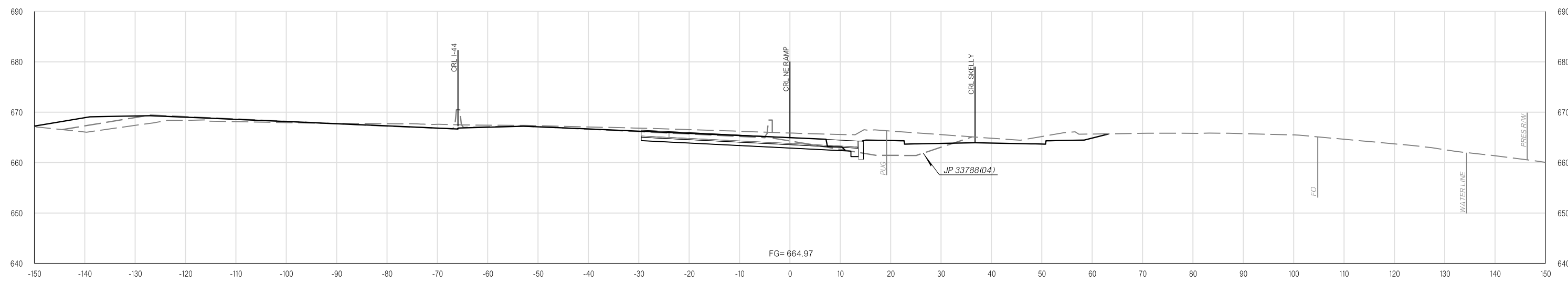


188+00.00

I-75 TO I-44 N-E RAMP

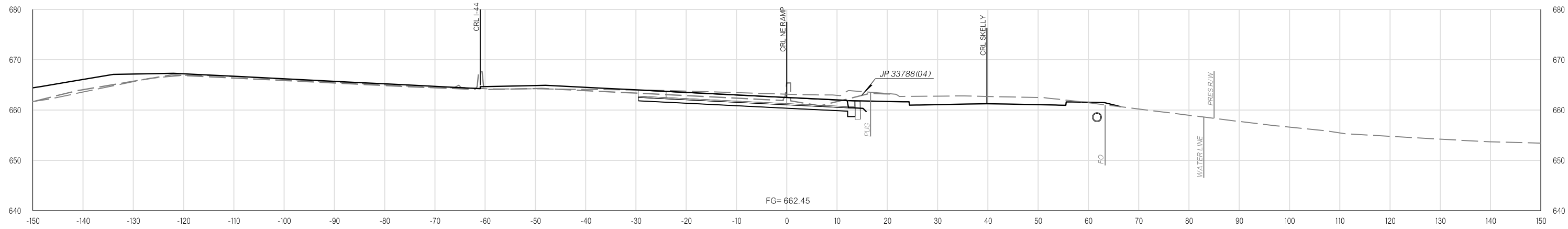


189+00.00

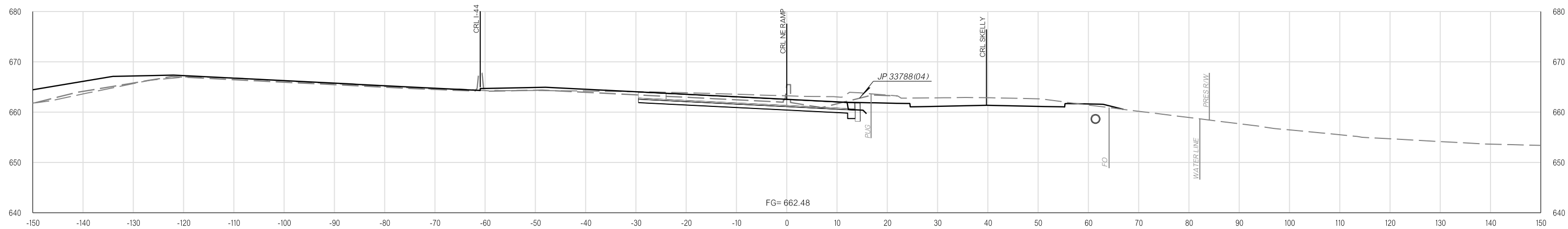


188+98.89

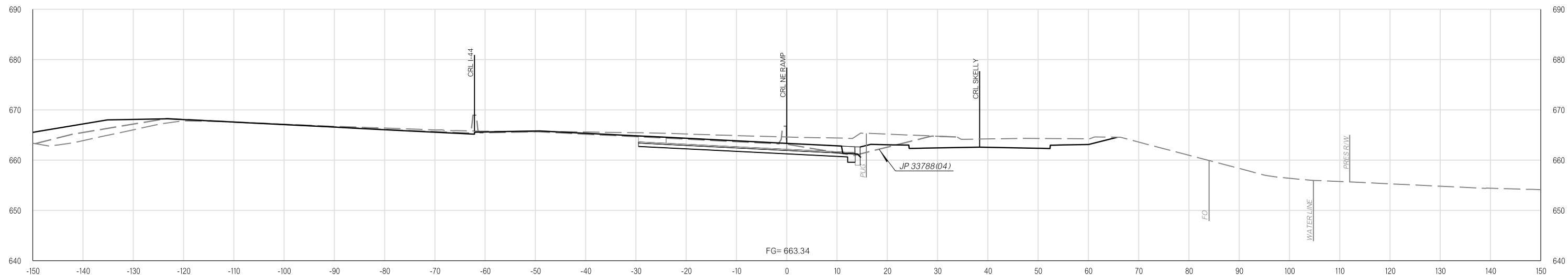
I-75 TO I-44 N-E RAMP



191+00.00



190+94.07

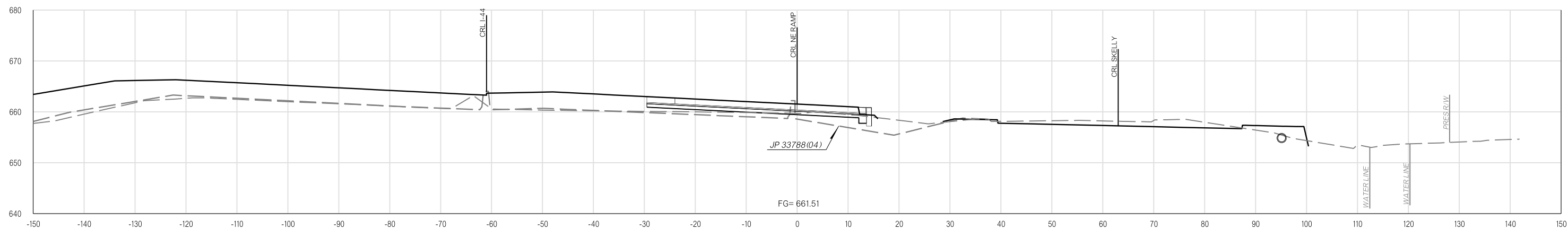


190+00.00

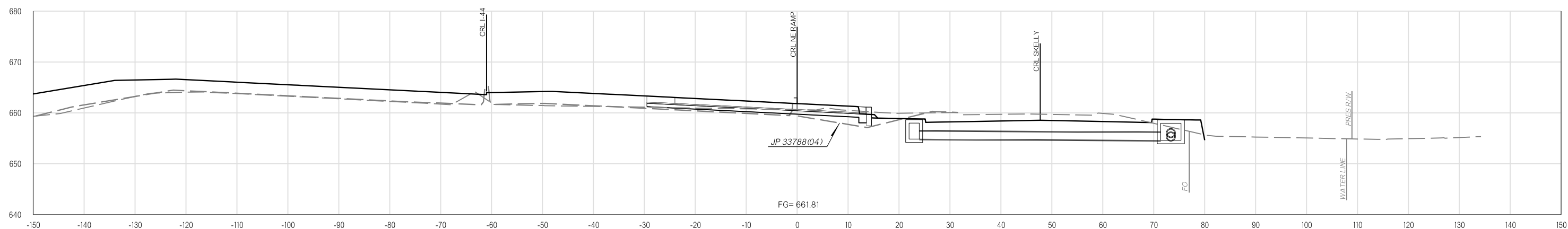
I-75 TO I-44 N-E RAMP

R/W UTILITY MEETING

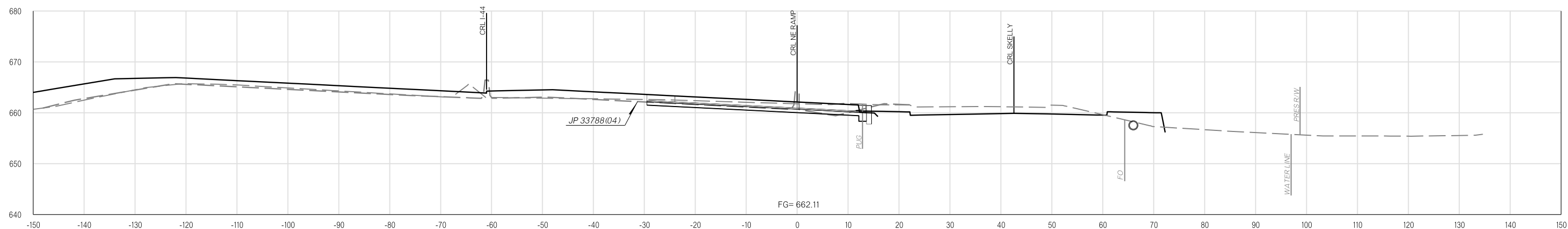
Mar. 17, 2021



194+00.00

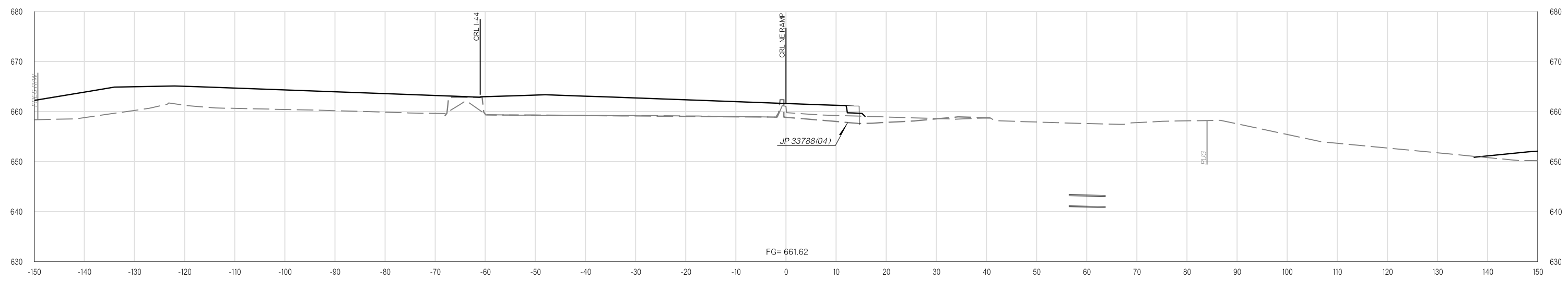


193+00.00

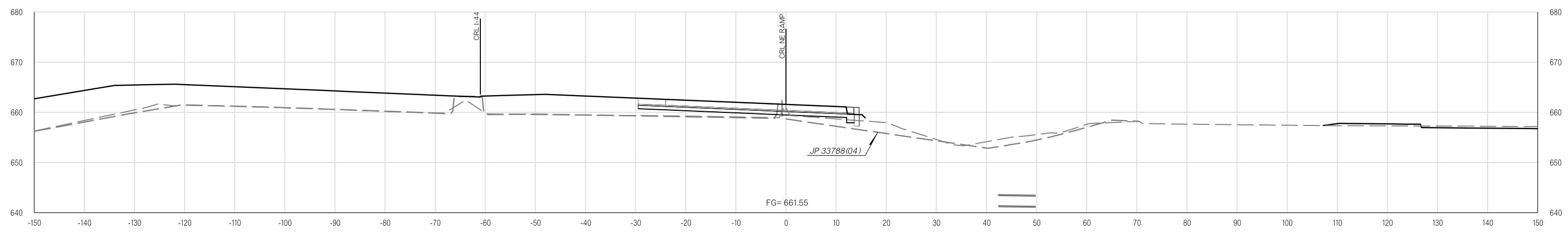


192+00.00

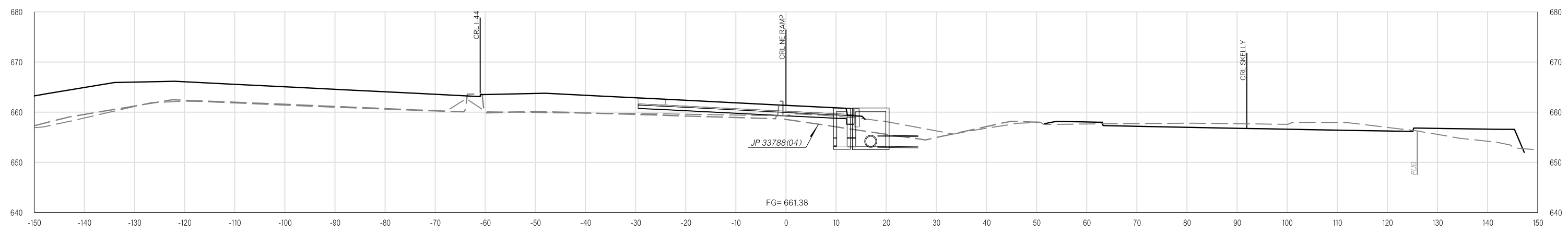
I-75 TO I-44 N-E RAMP



195+38.00



195+00.00

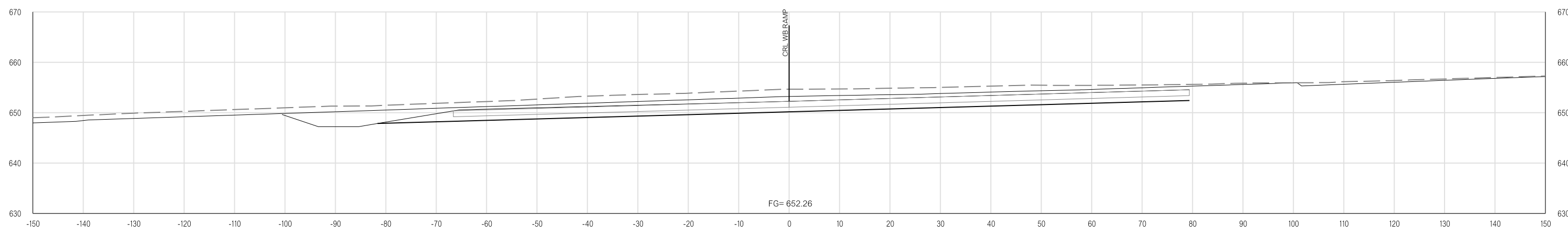
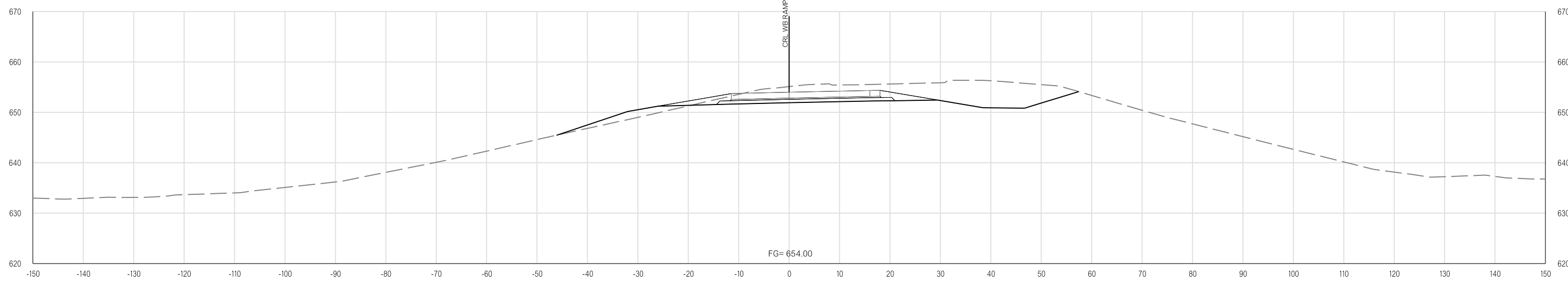


194+45.53

I-75 TO I-44 N-E RAMP

R/W UTILITY MEETING

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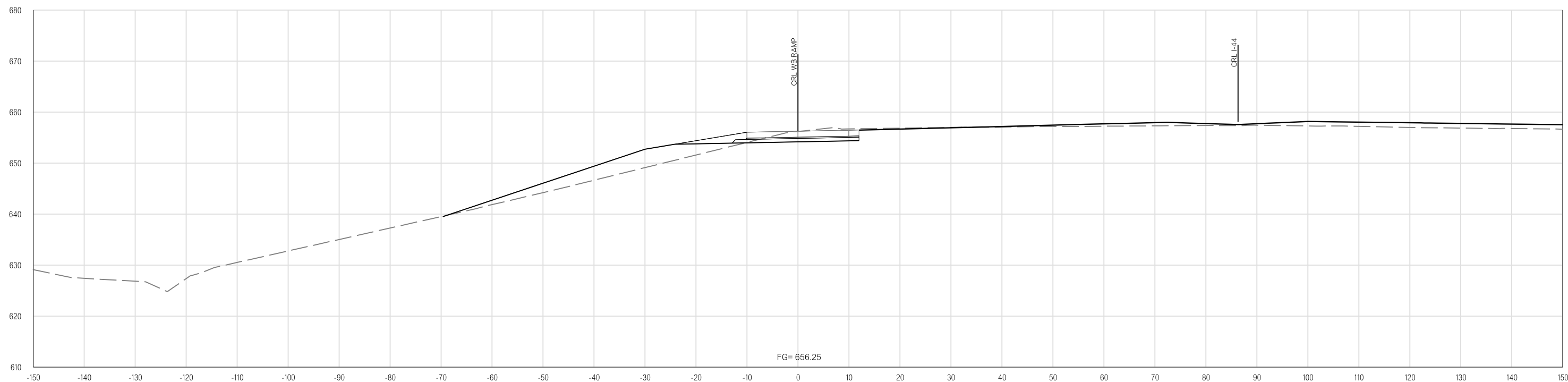


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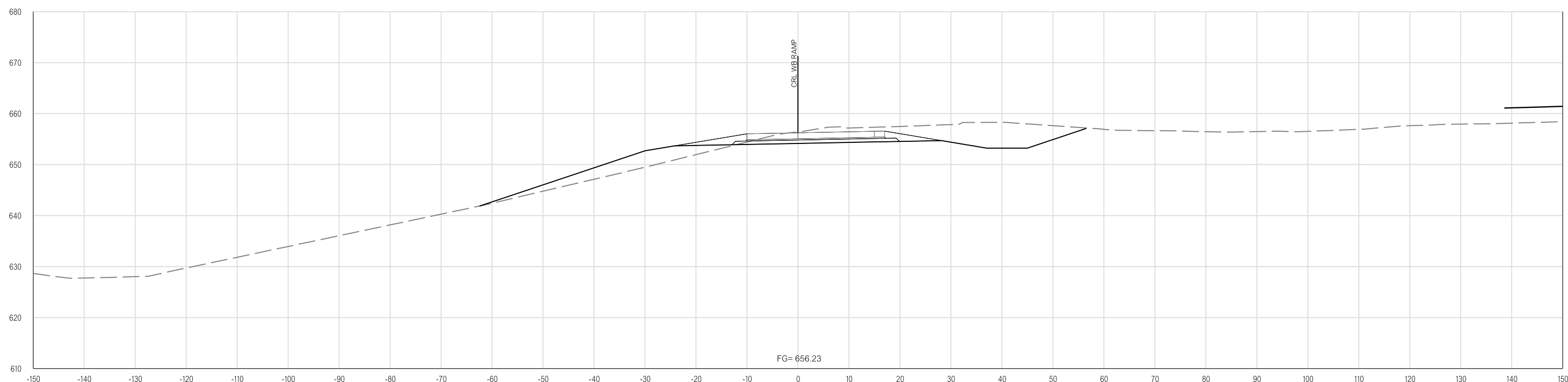
I-44 WB RAMP

R/W UTILITY MEETING

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21+00.00



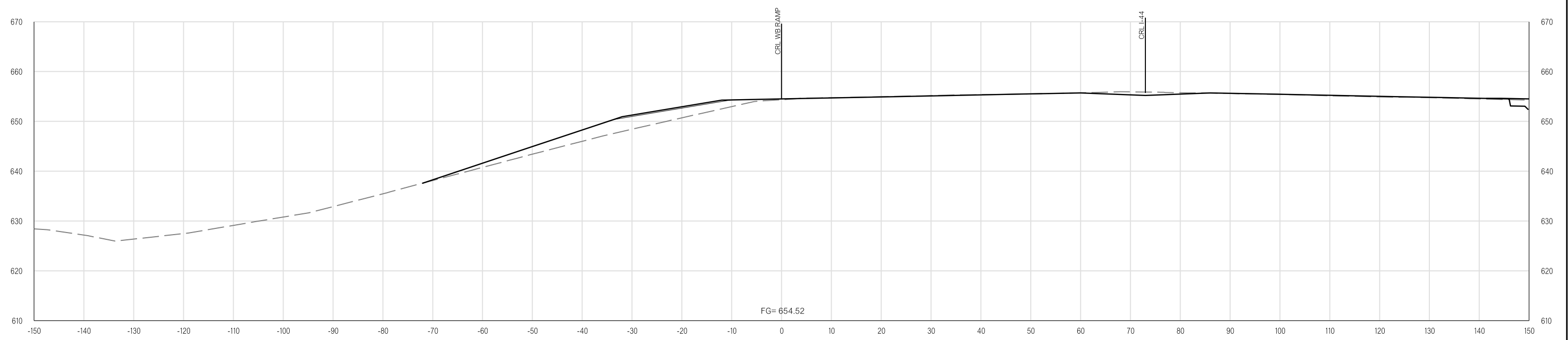
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I-44 WB RAMP

R/W UTILITY MEETING

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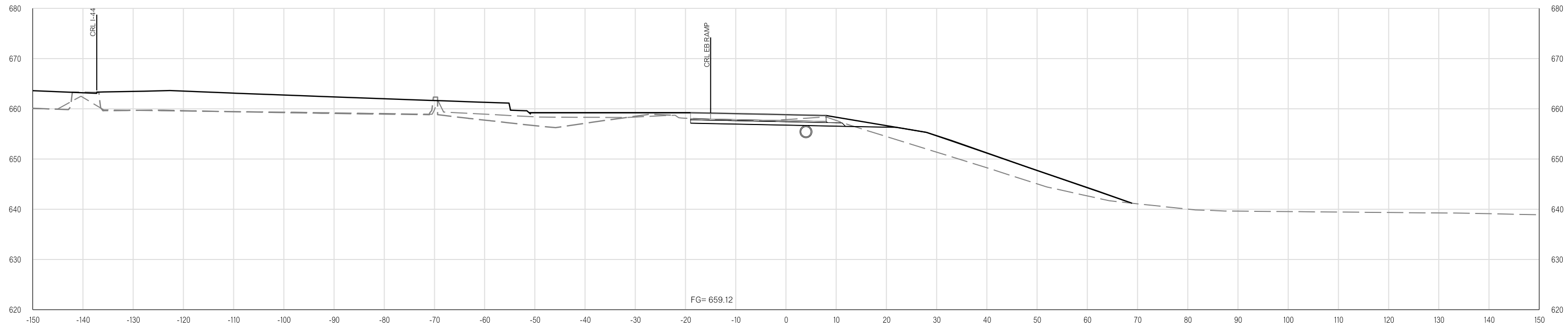


22+00.00

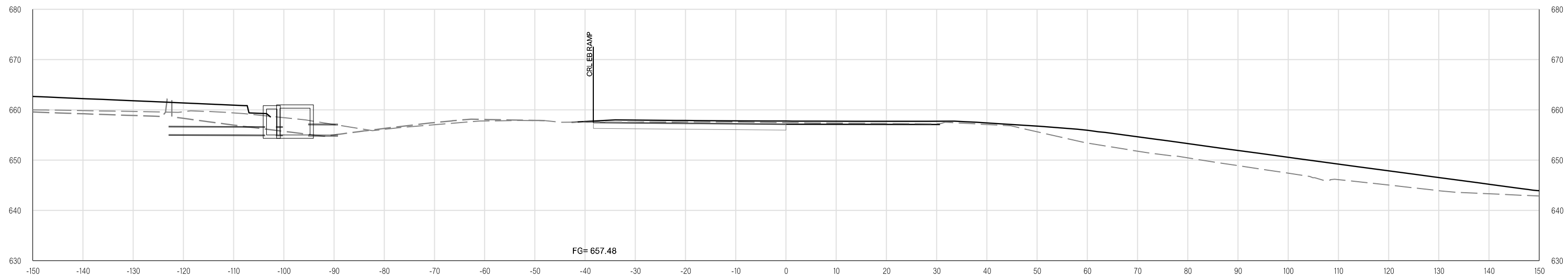
I-44 WB RAMP

R/W UTILITY MEETING

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117+00.00

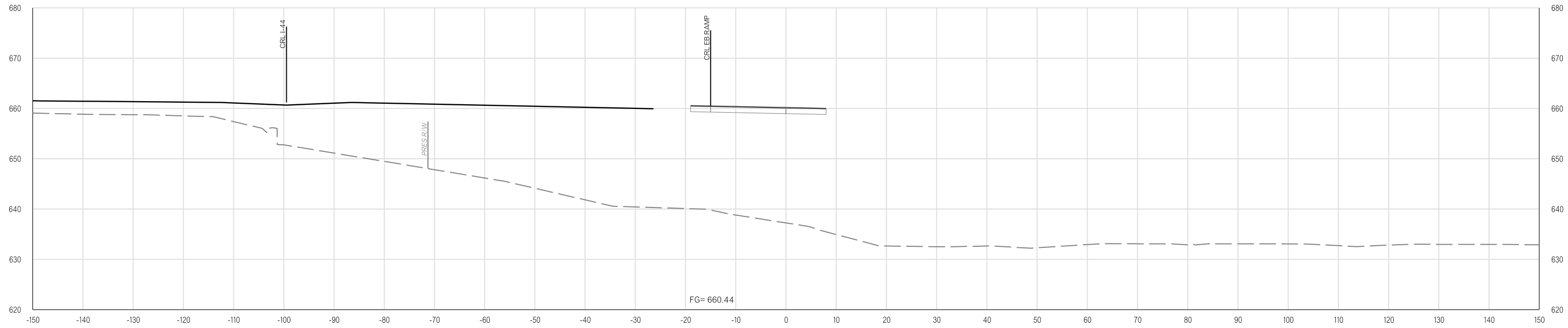


116+30.31

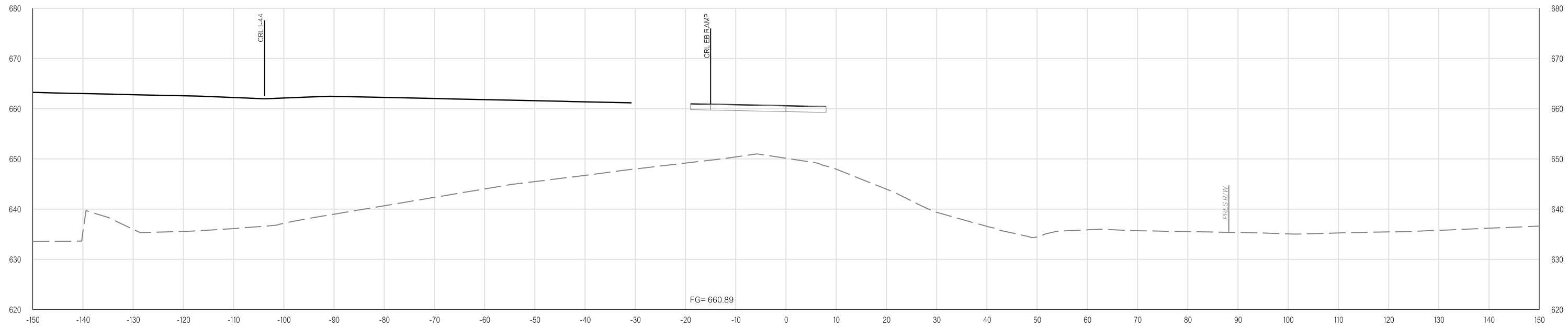
I-44 EB RAMP

R/W UTILITY MEETING

Mar. 17, 2021

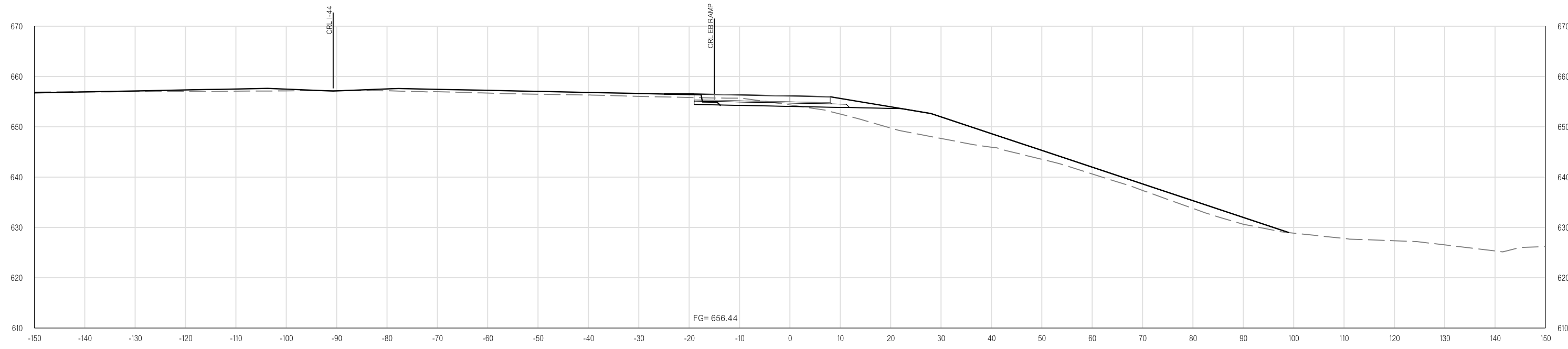


119+00.00

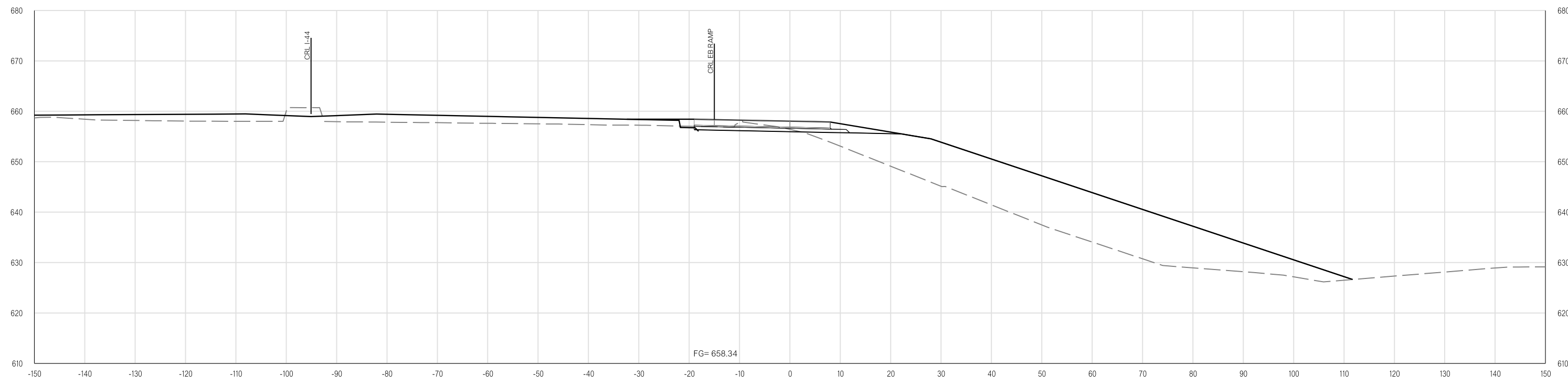


118+00.00

I-44 EB RAMP



121+00.00

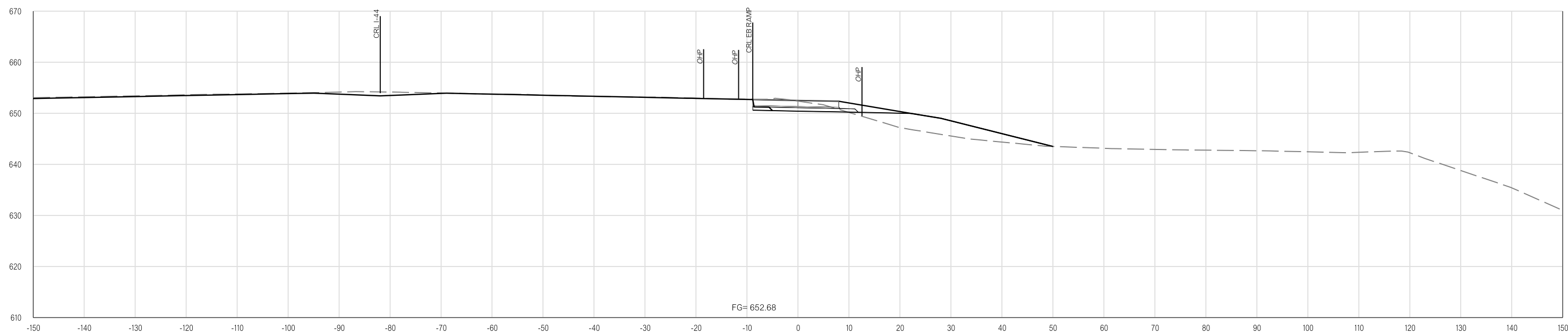


120+00.00

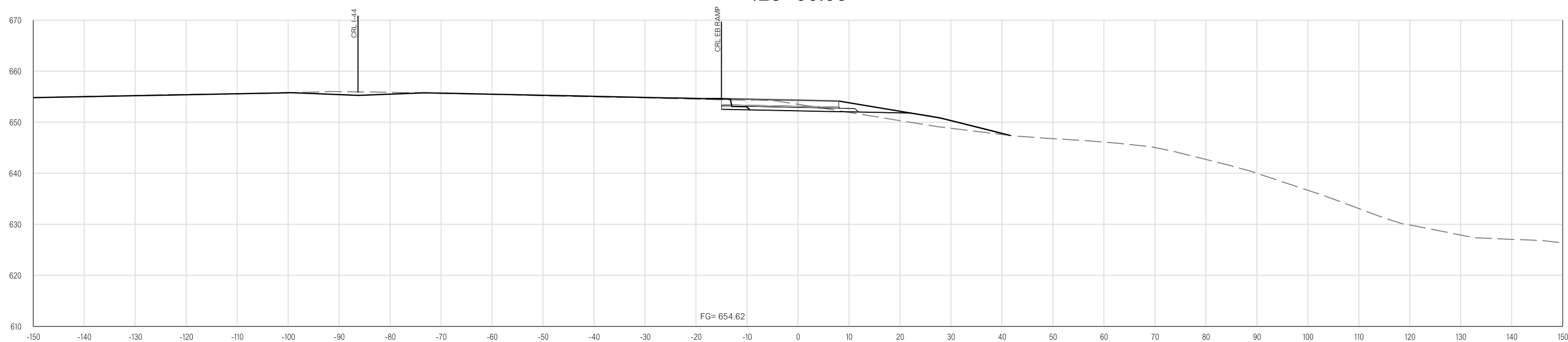
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R/W UTILITY MEETING

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123+00.00

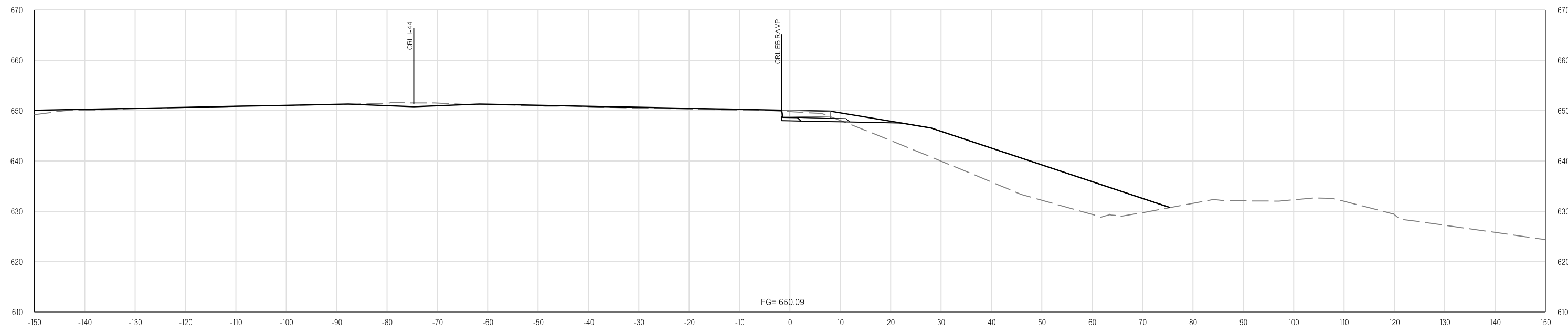


122+00.00

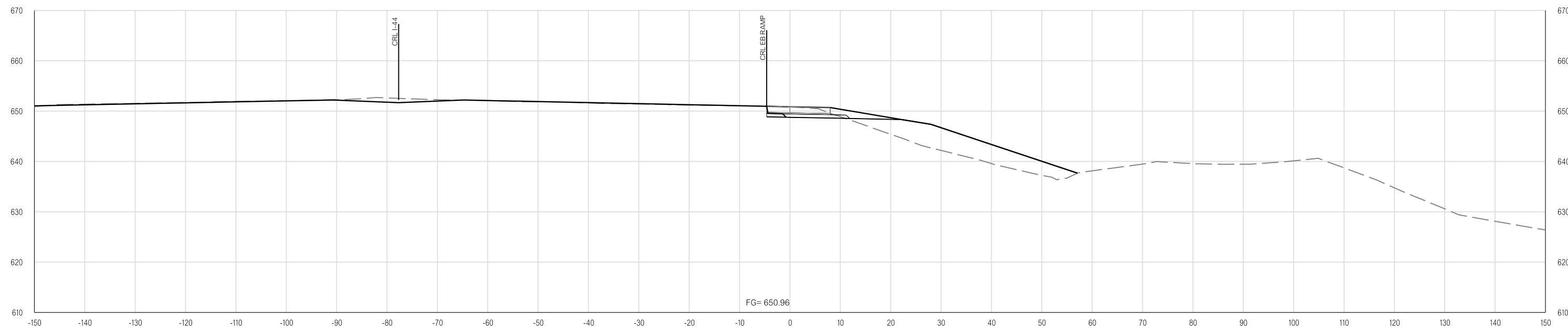
I-44 EB RAMP

R/W UTILITY MEETING

Mar. 17, 2021



125+00.00



124+00.00

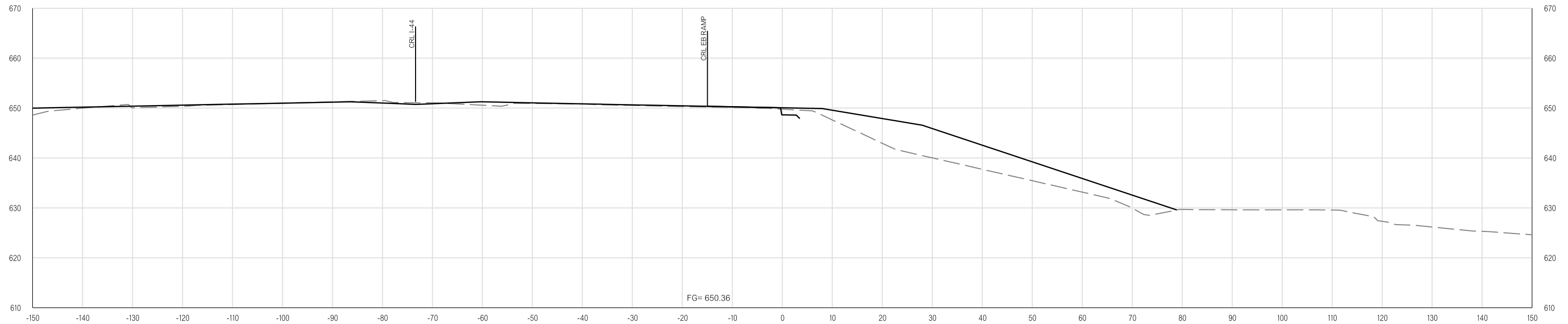
I-44 EB RAMP

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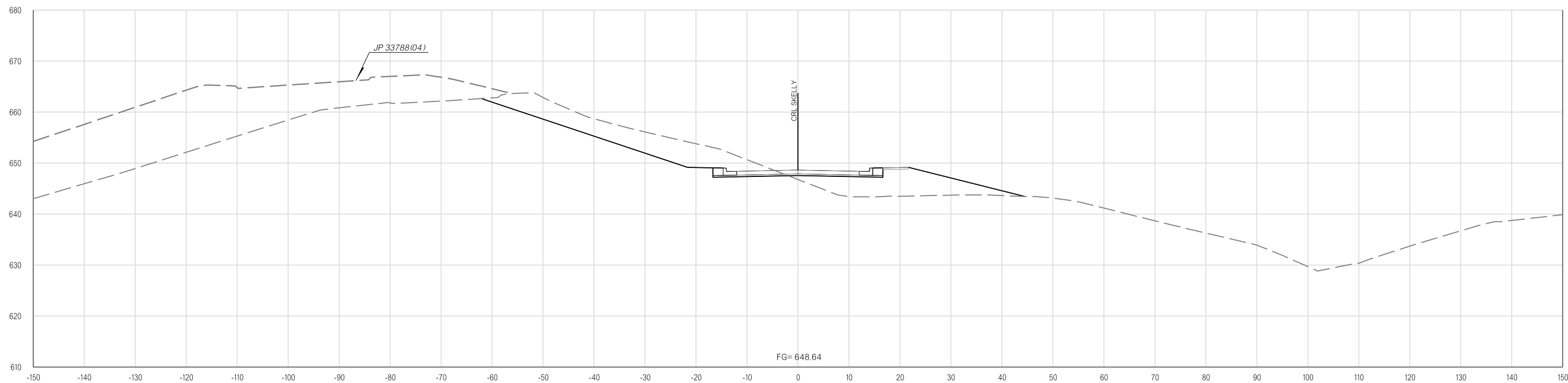


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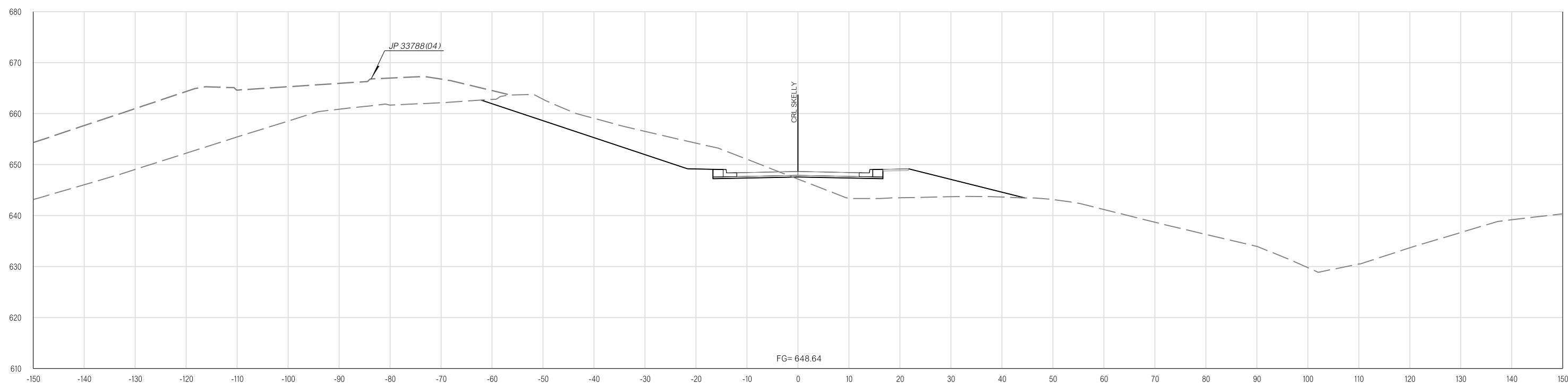
I-44 EB RAMP

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585+50.00

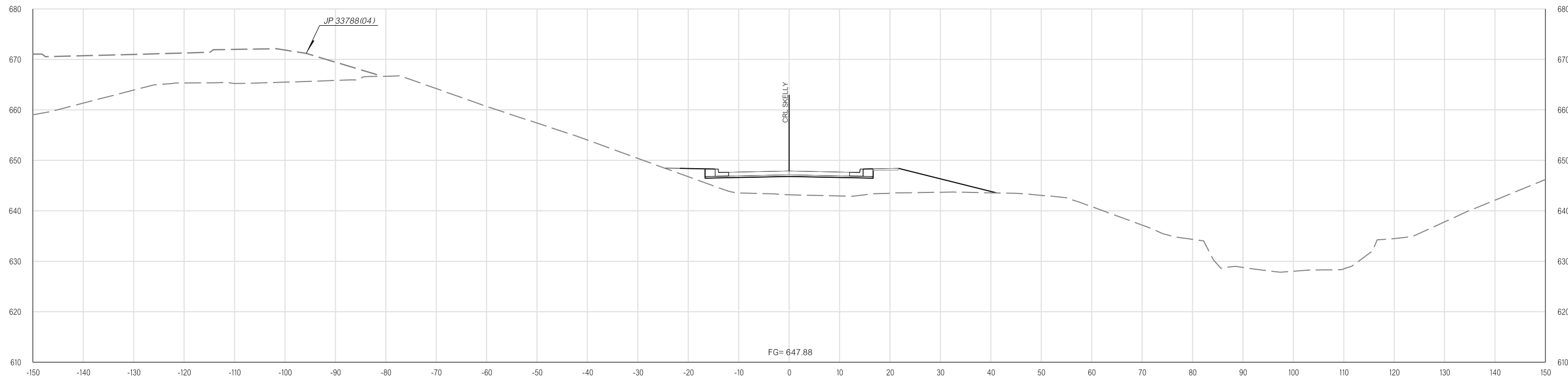


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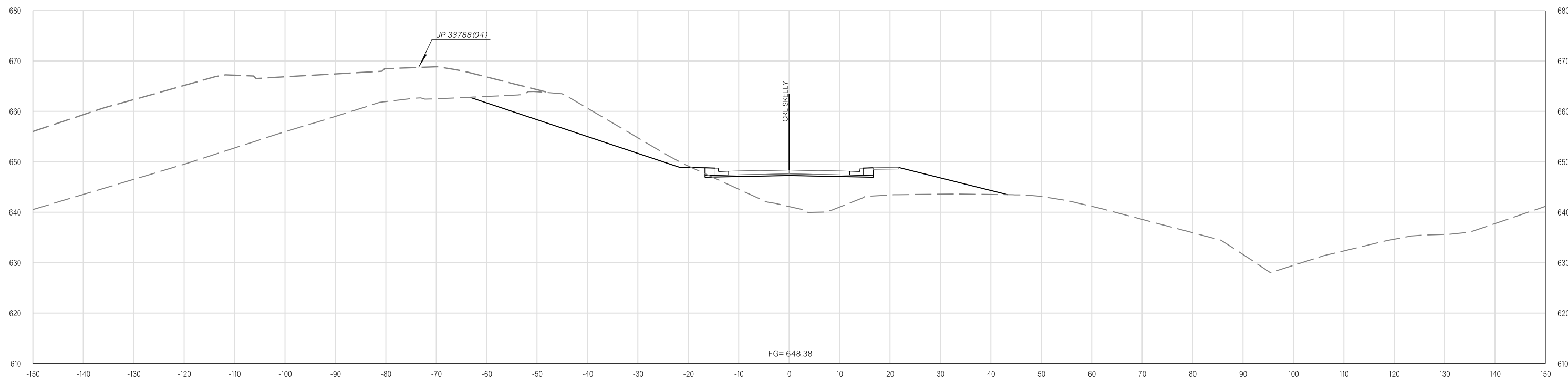
SKELLY DR.

R/W UTILITY MEETING

Mar. 17, 2021



587+00.00



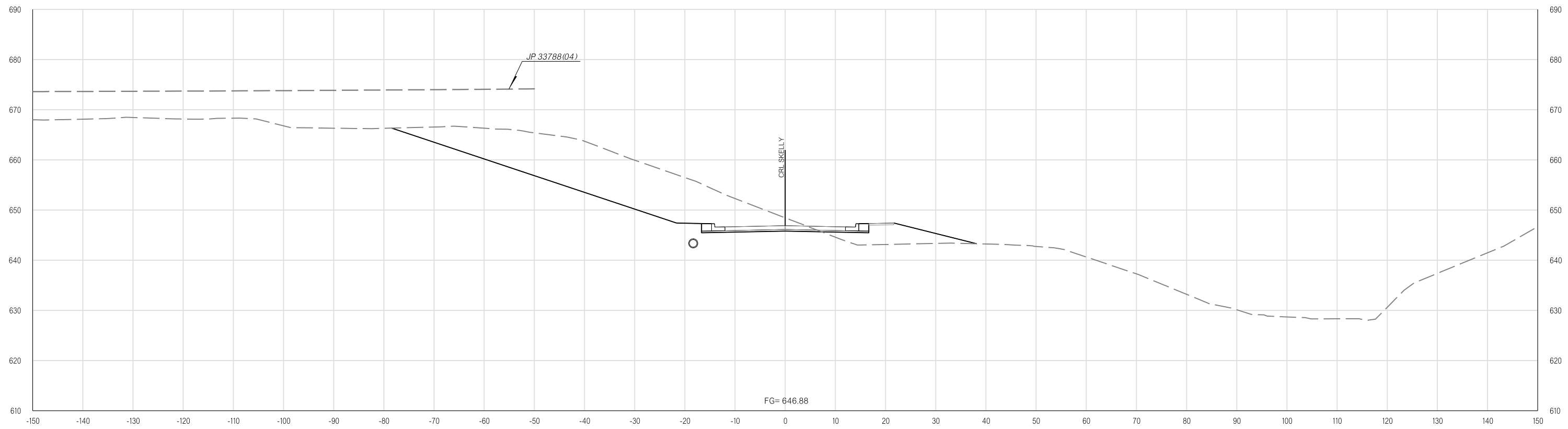
586+00.00

SKELLY DR.

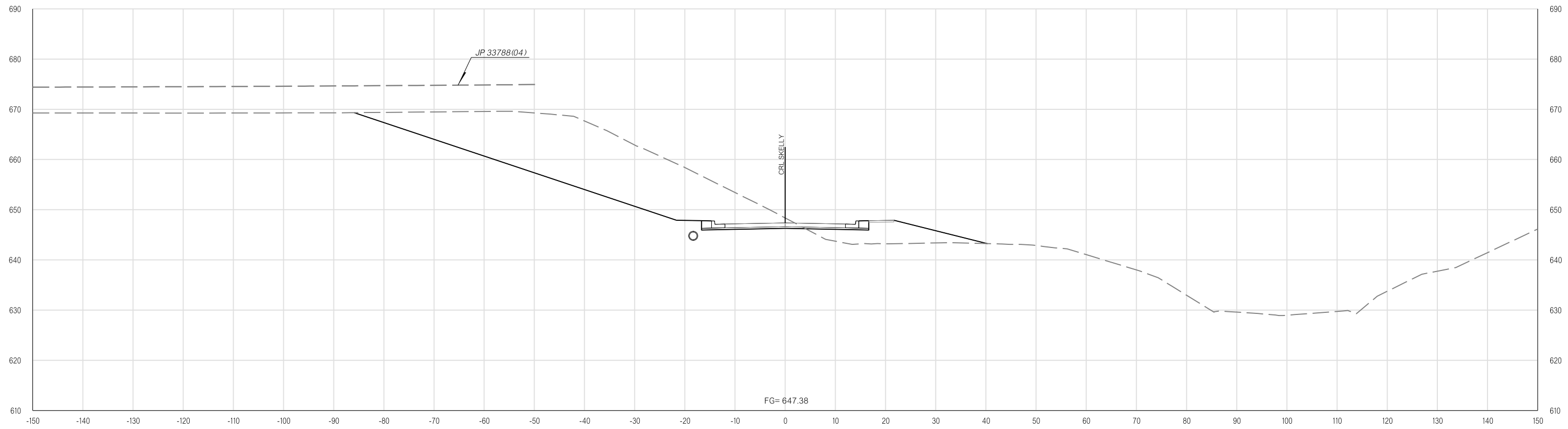
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R/W UTILITY MEETING

Mar. 17, 2021

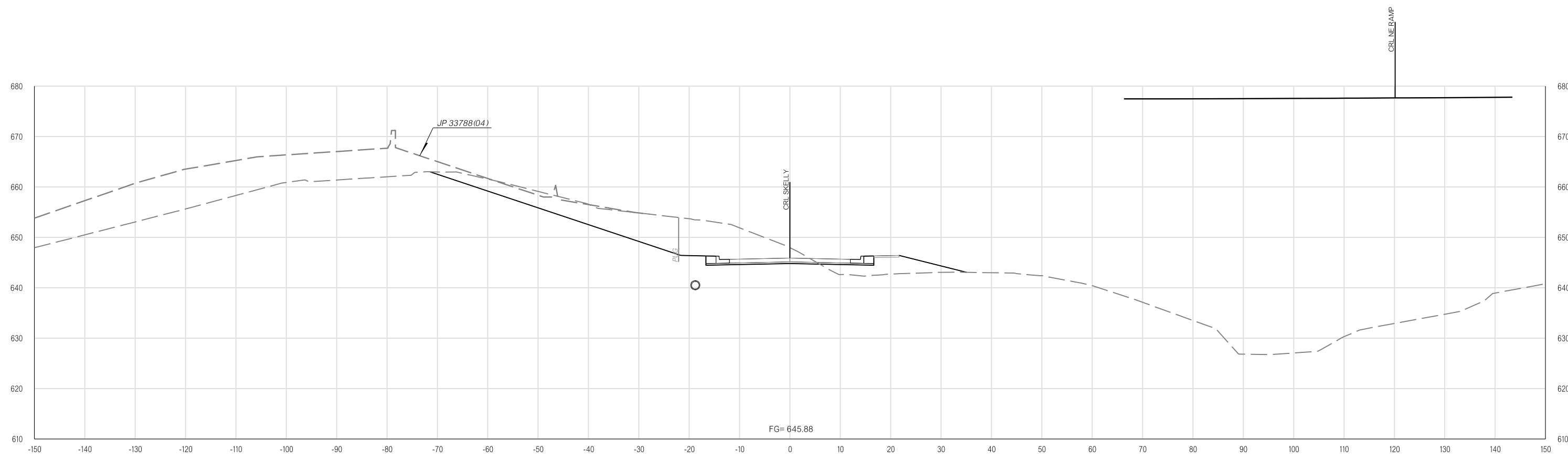


589+00.00

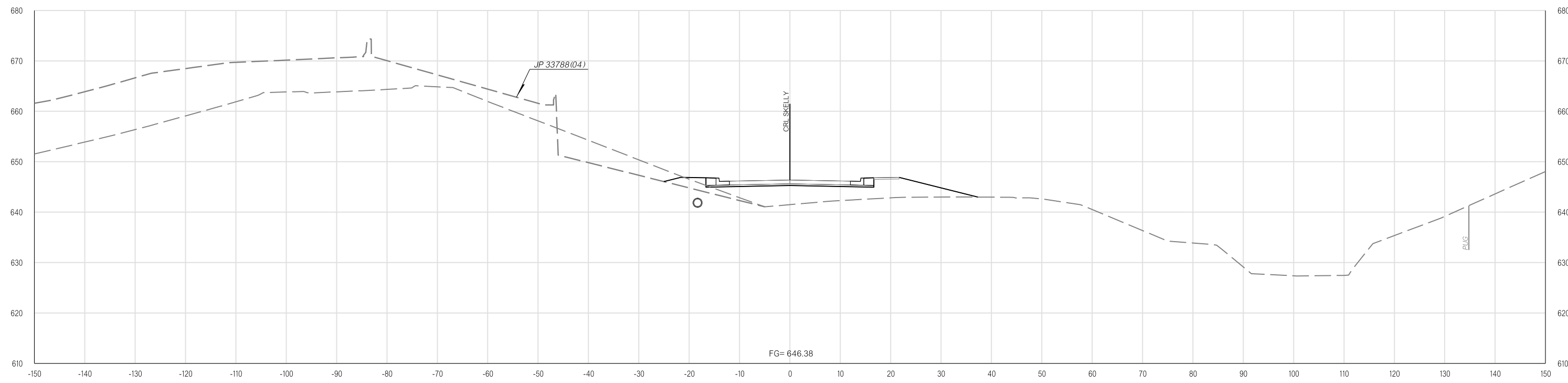


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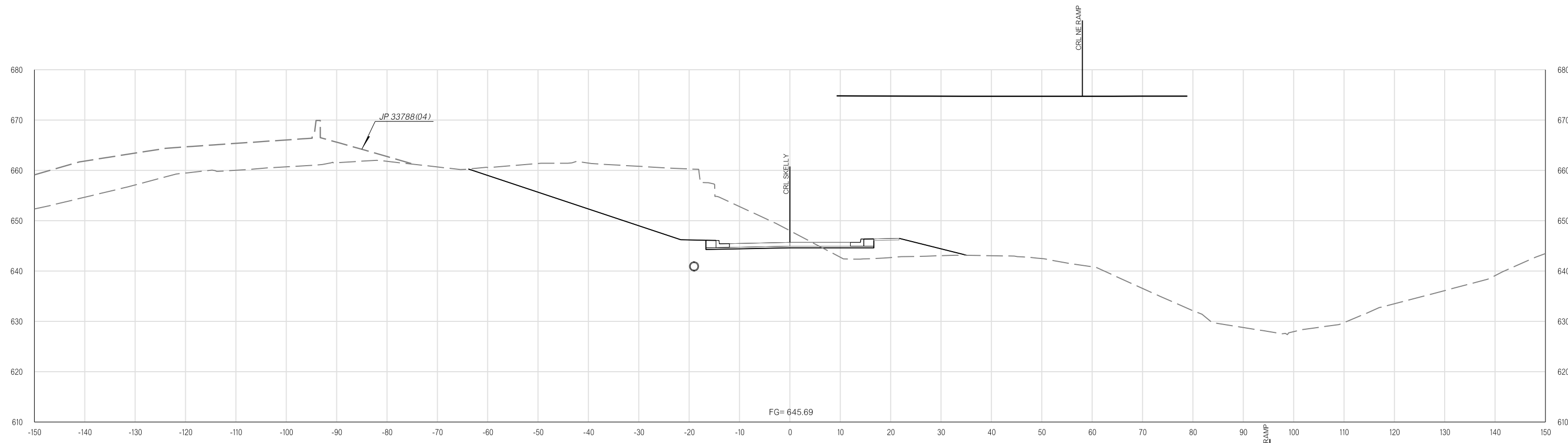
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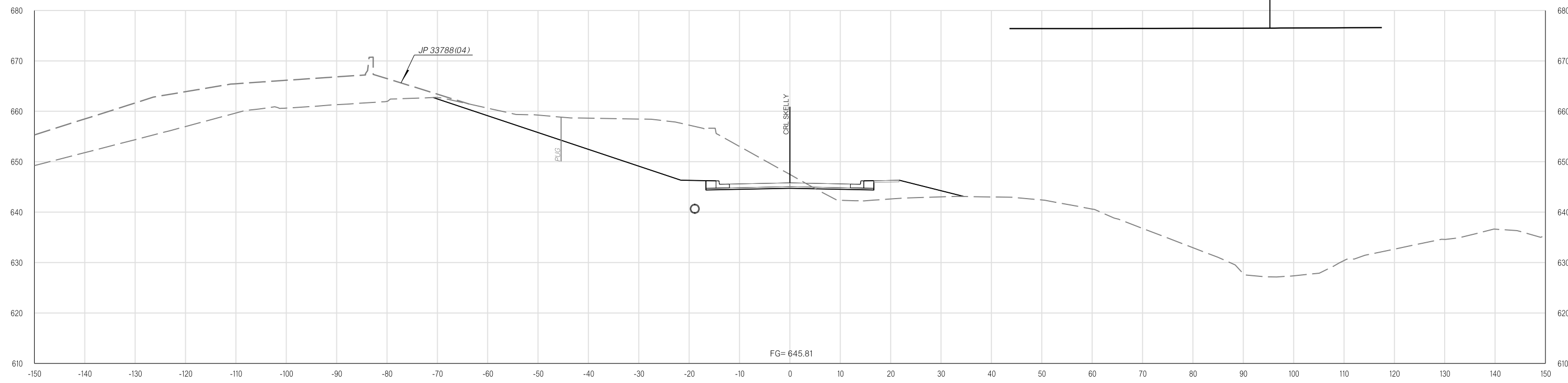
590+00.00

SKELLY DR.

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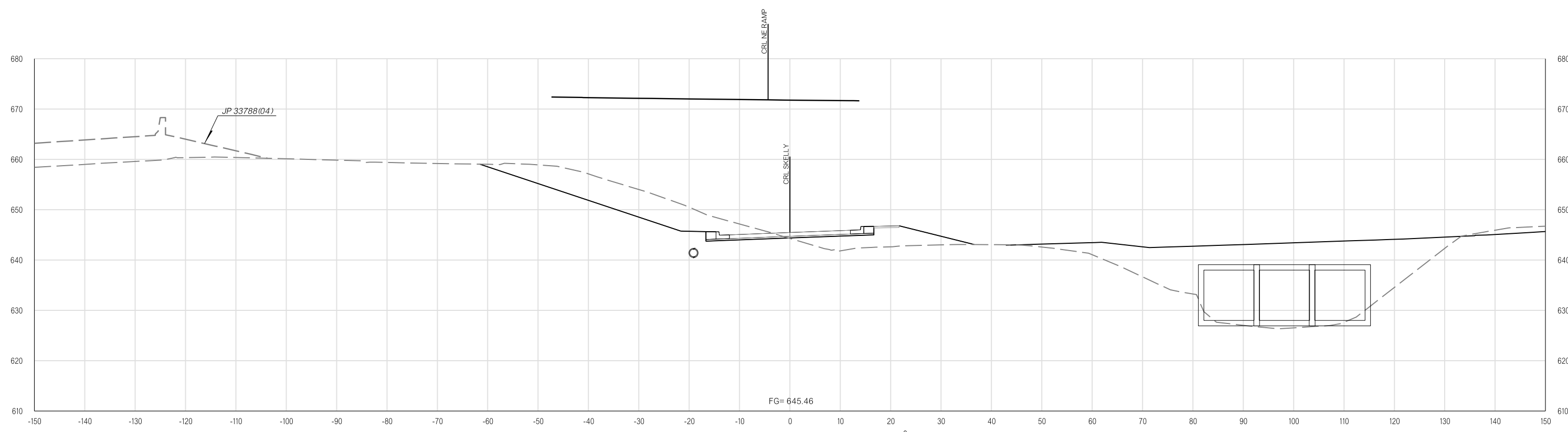


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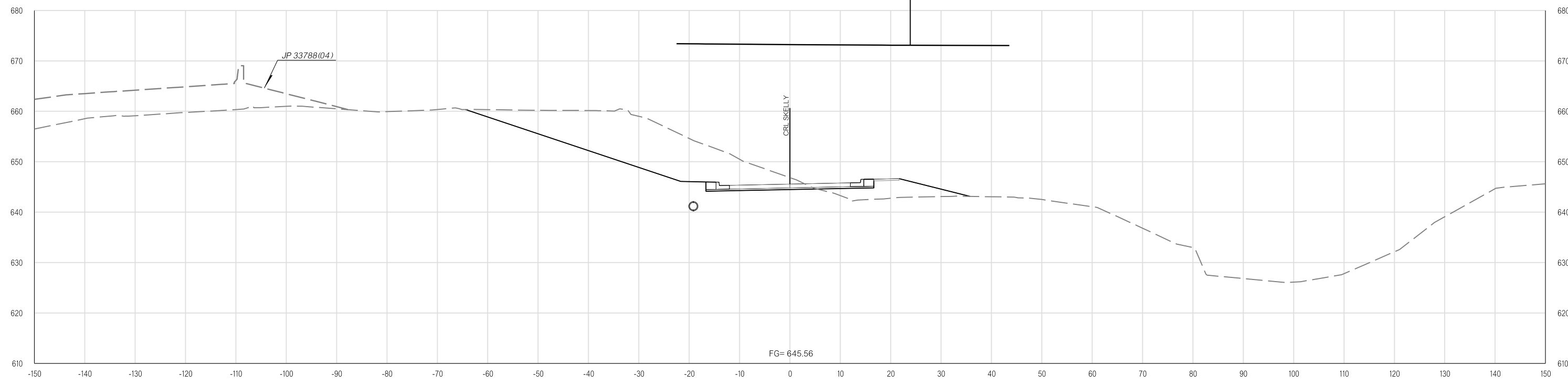


591+14.88

SKELLY DR.



591+84.48

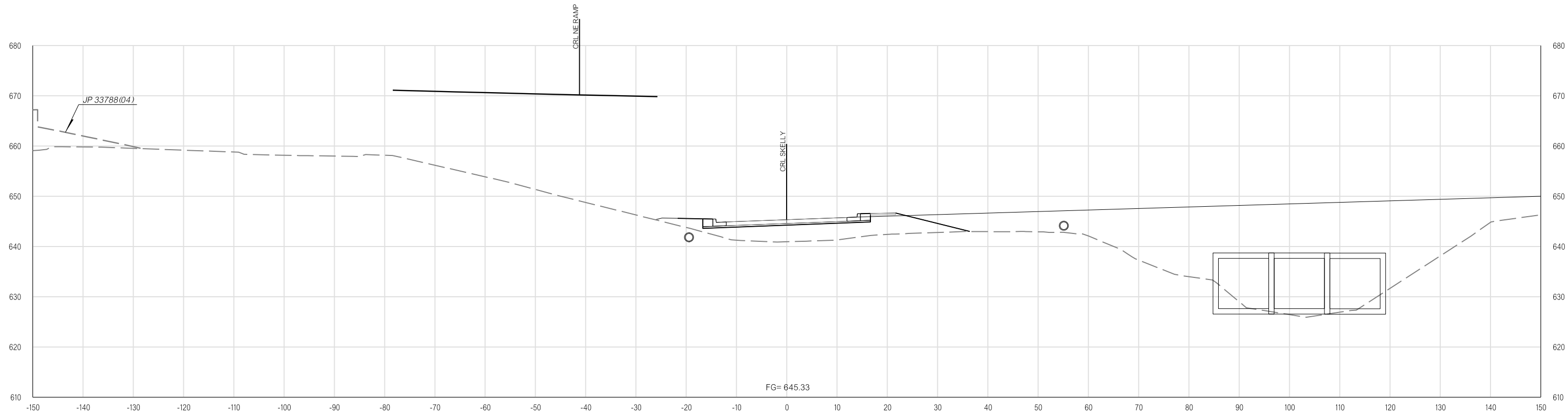


591+62.88

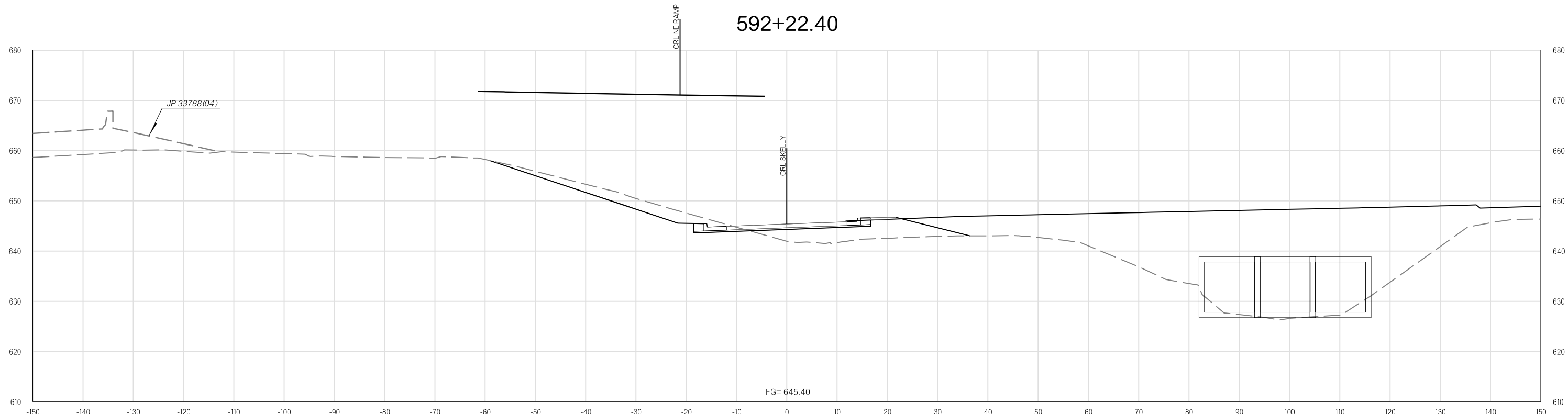
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R/W UTILITY MEETING

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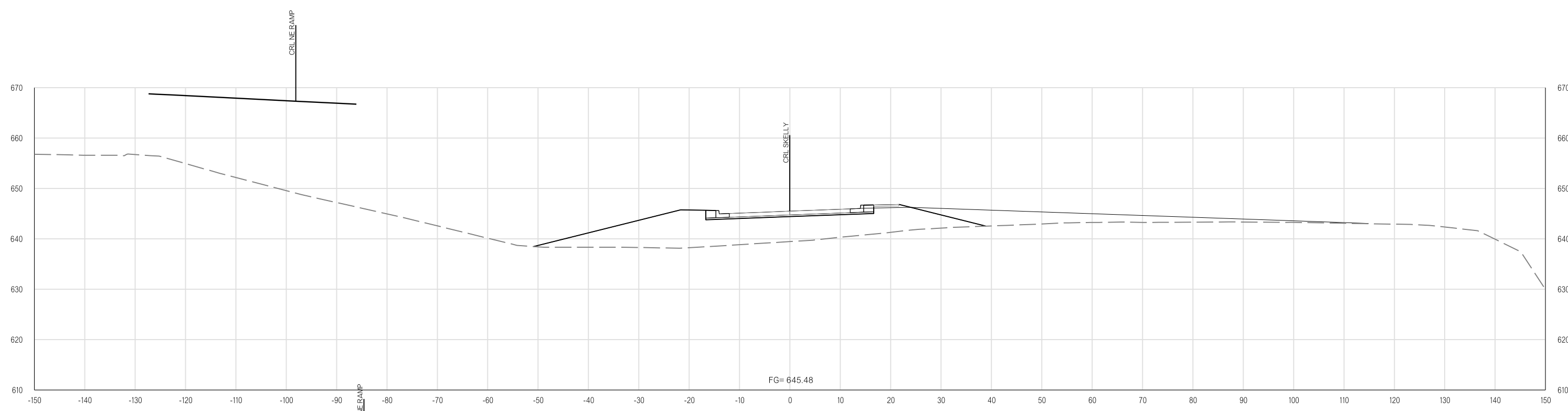


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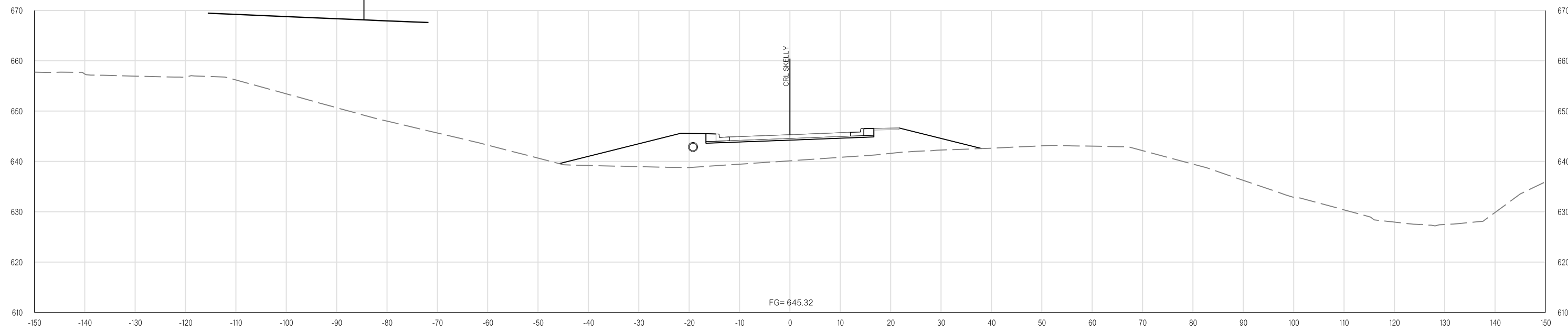


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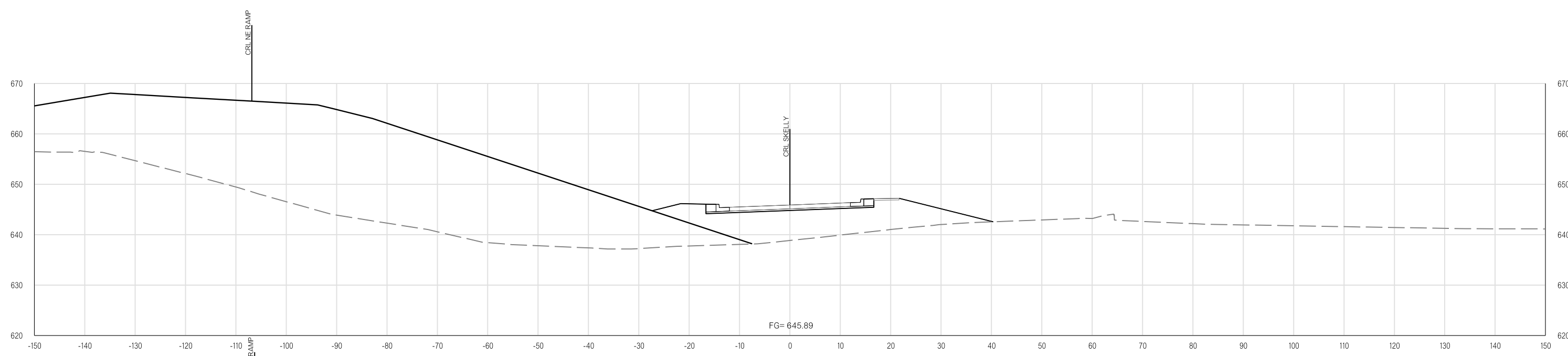


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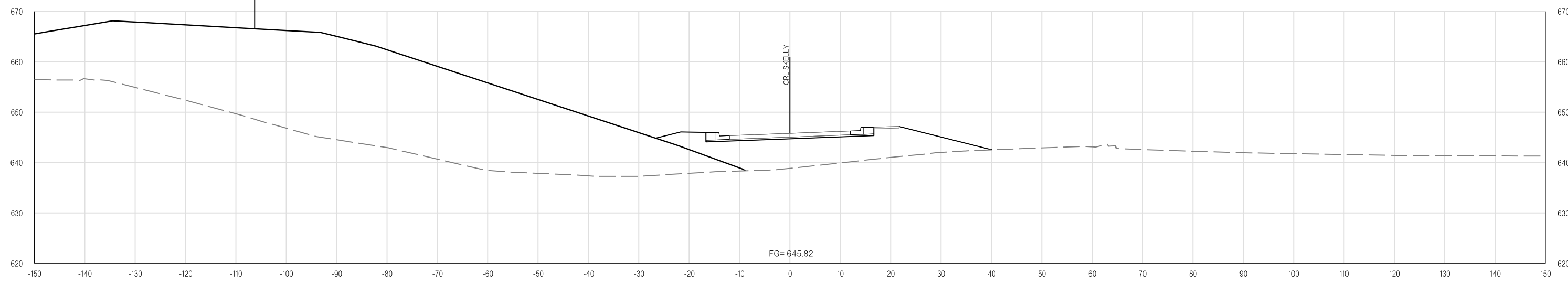


593+00.00

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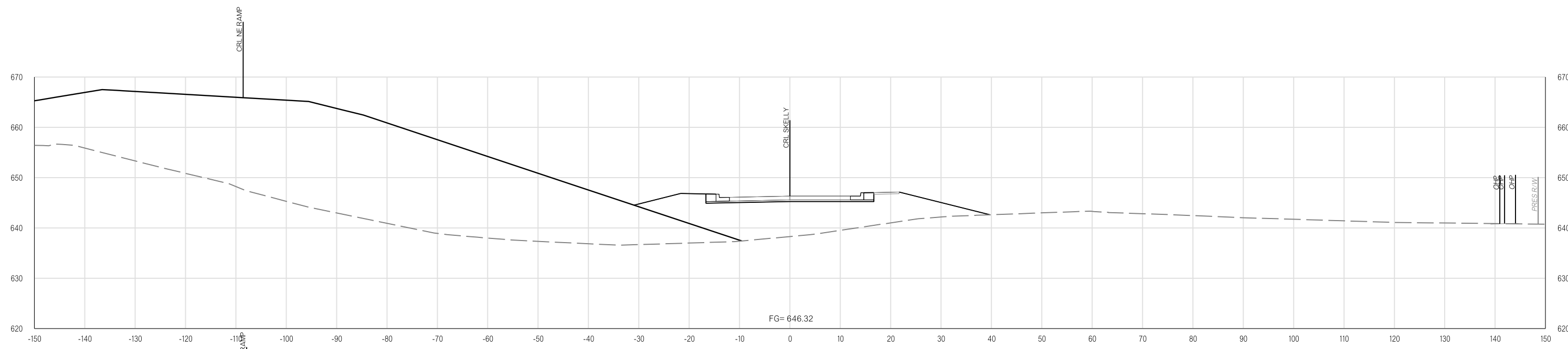


594+07.53

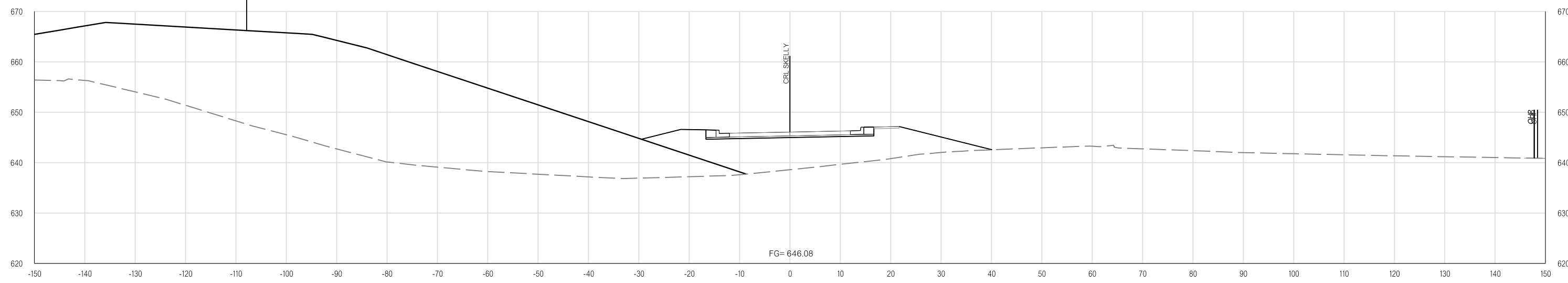


594+00.00

SKELLY DR.



594+53.13



594+29.13

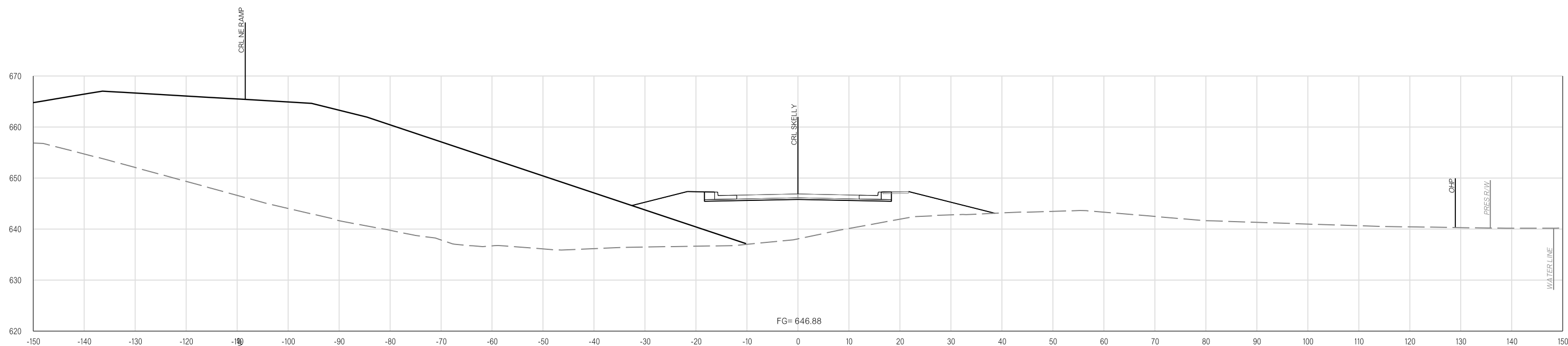
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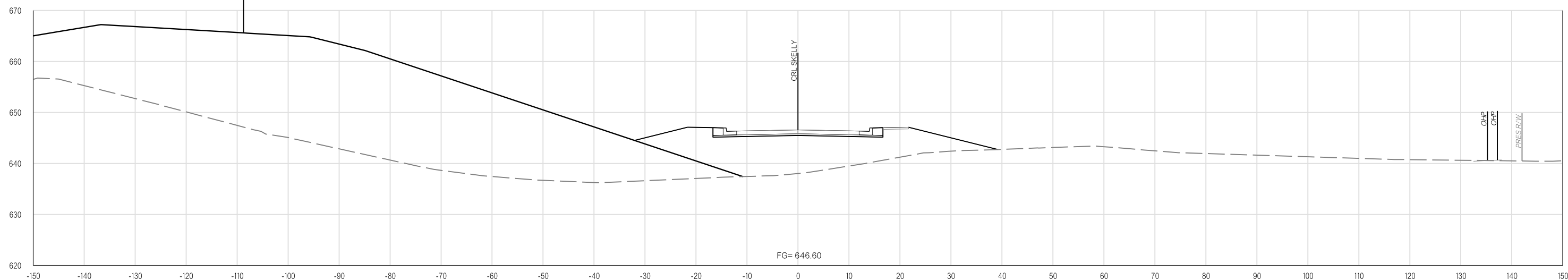
R/W UTILITY MEETING

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595+00.00

FG= 646.88



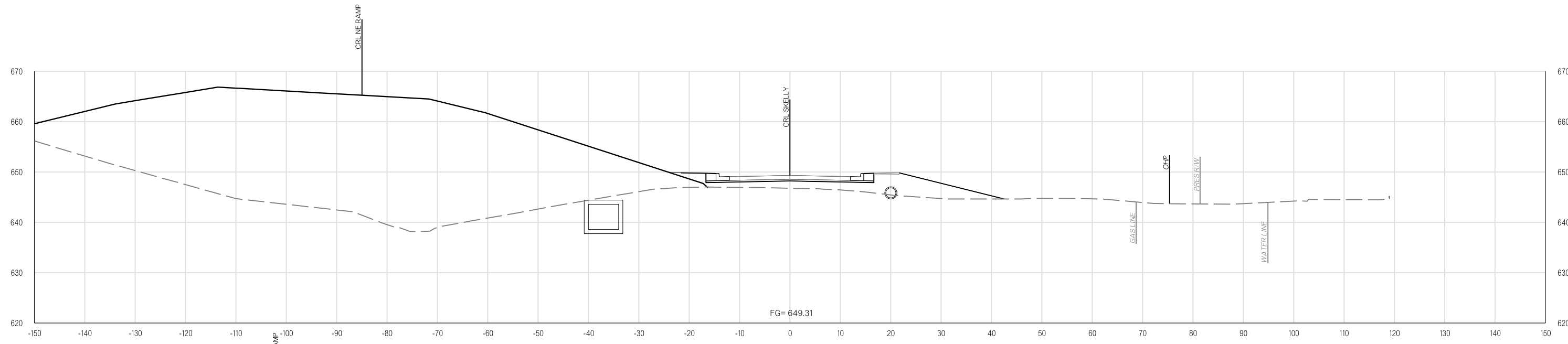
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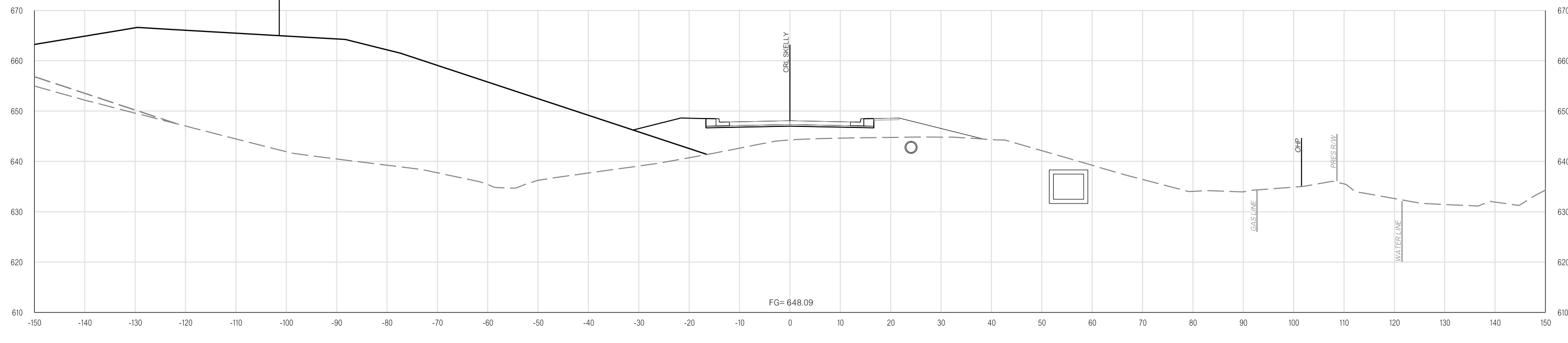
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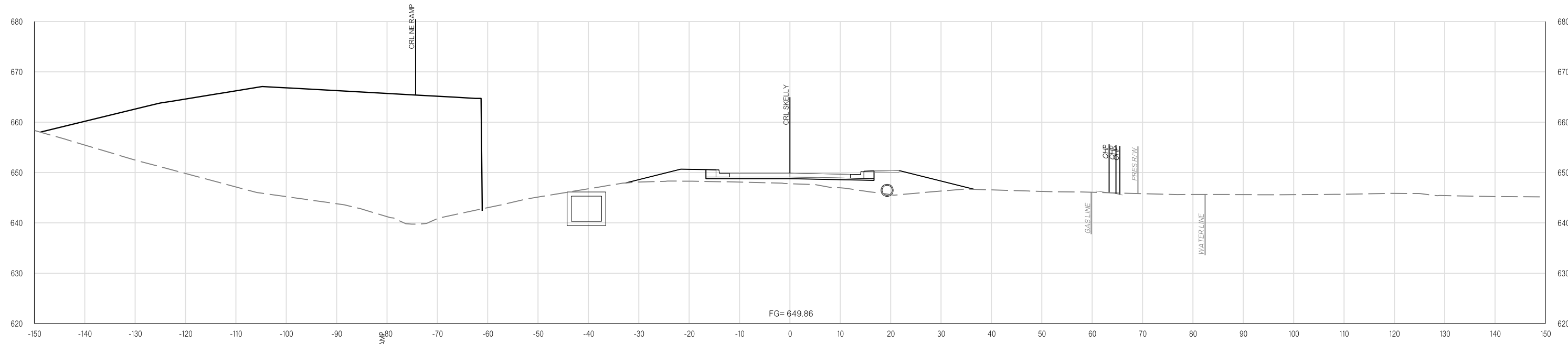
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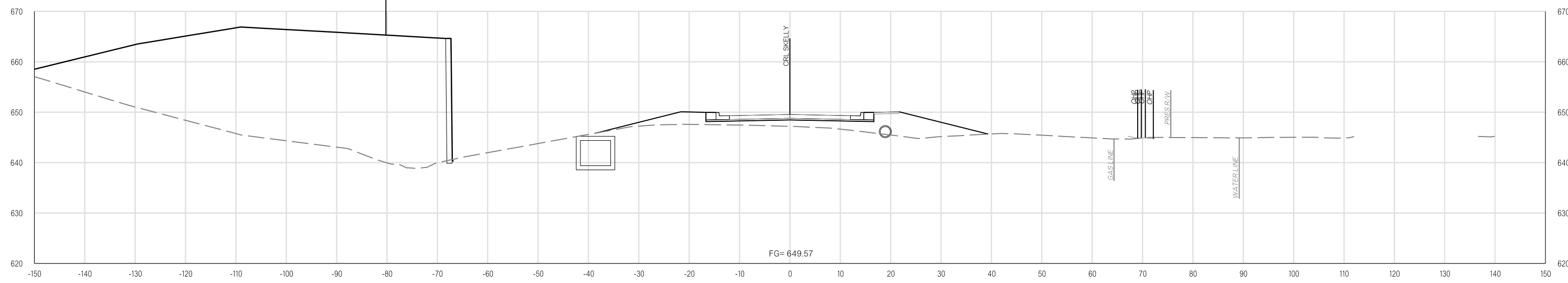
596+00.00

SKELLY DR.

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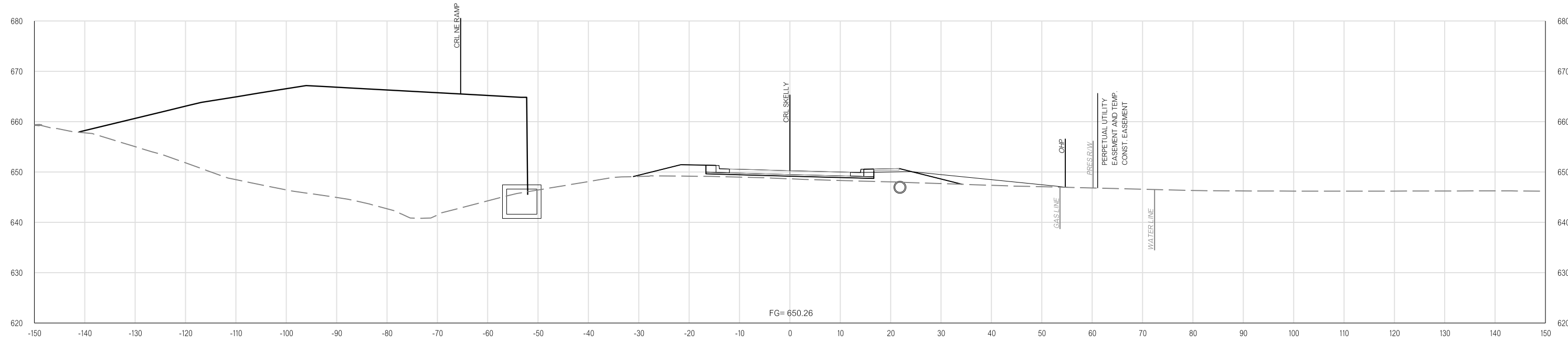
597+45.31



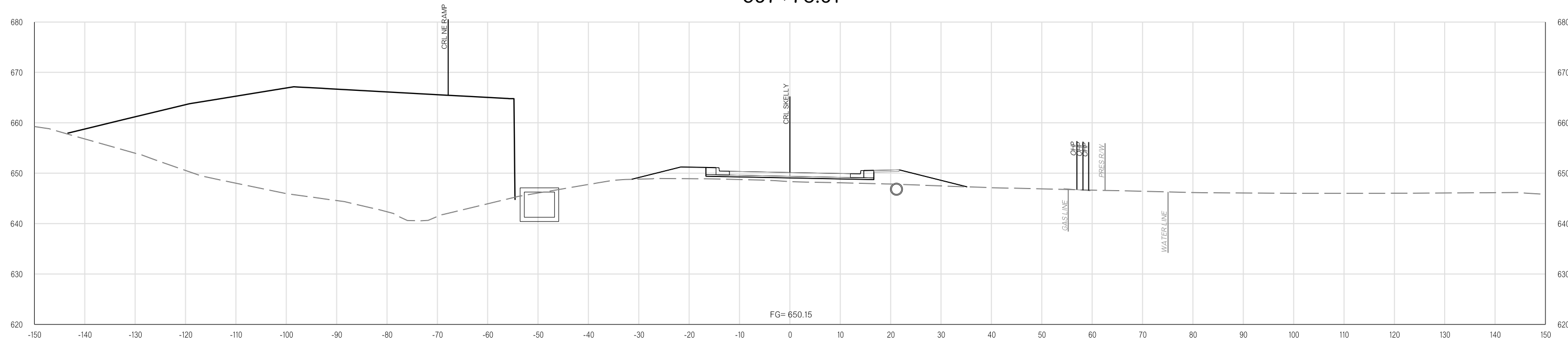
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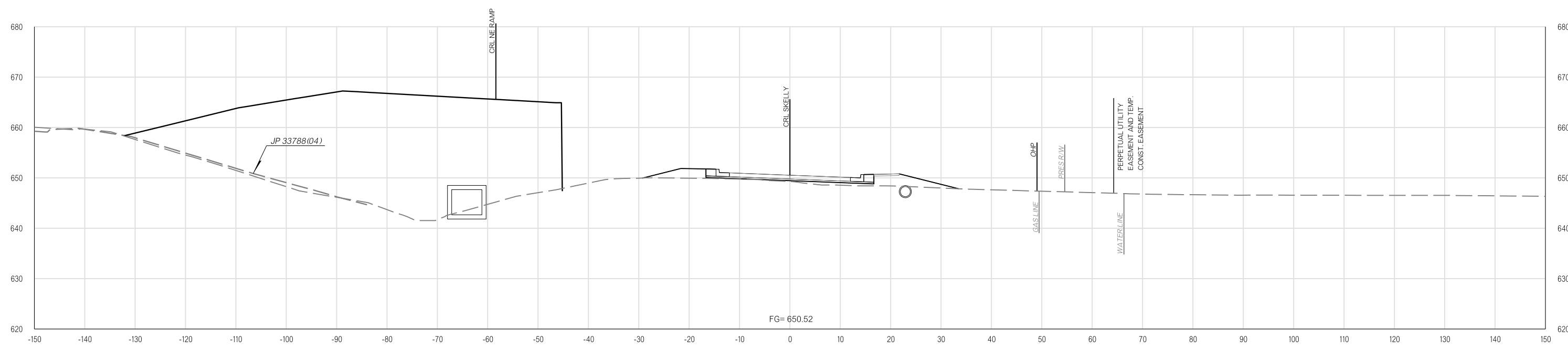
597+78.01



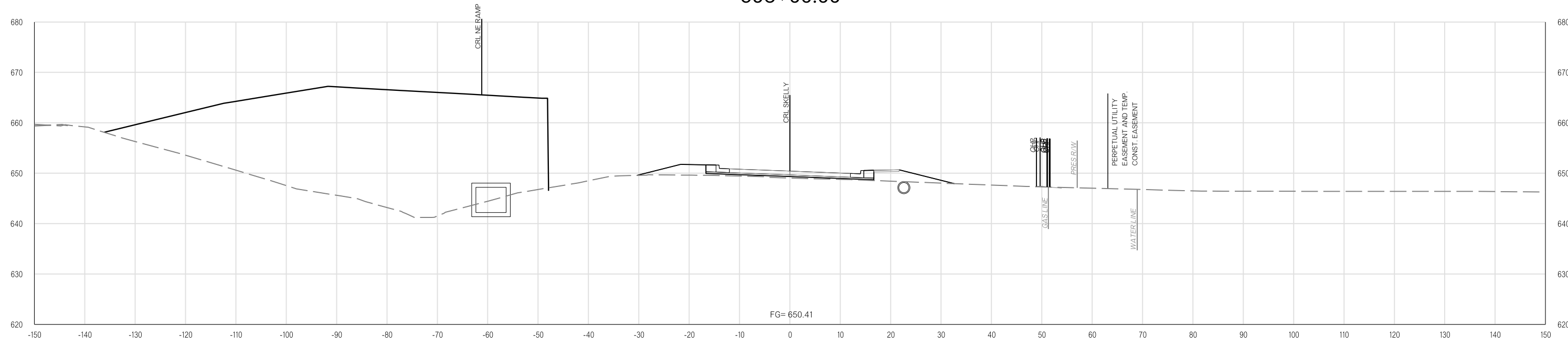
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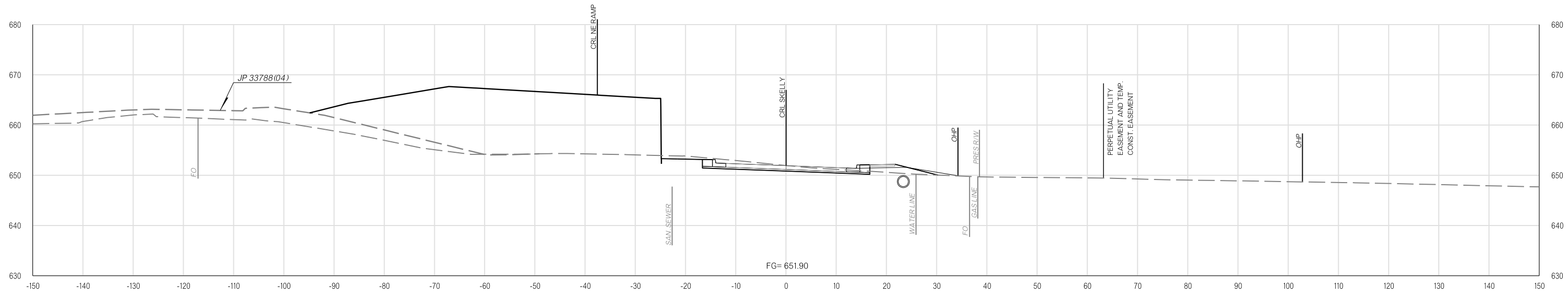


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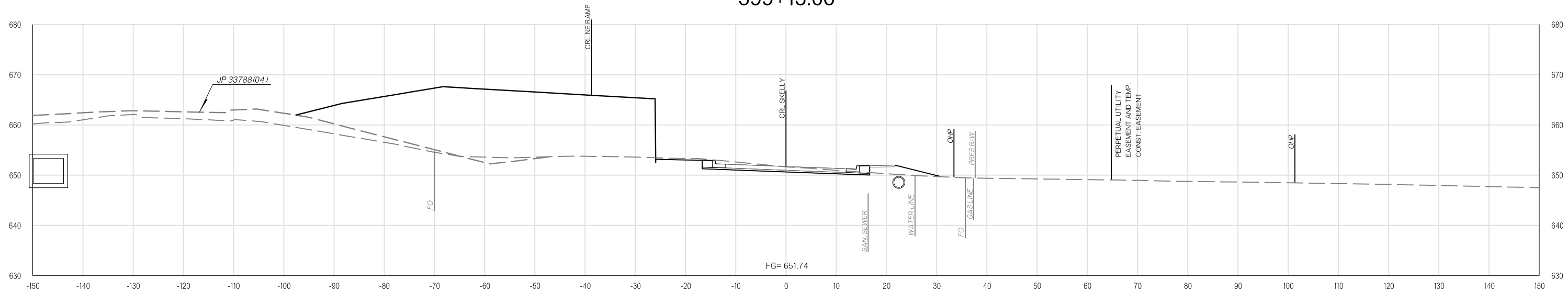


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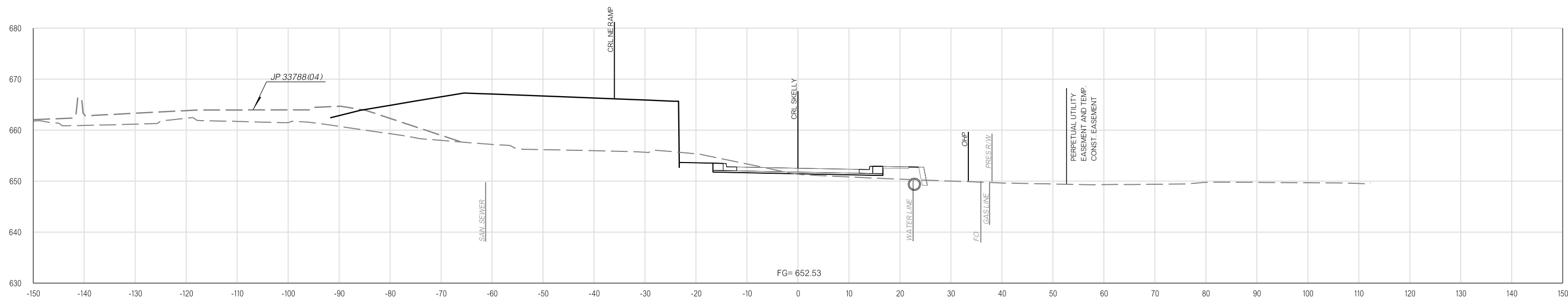


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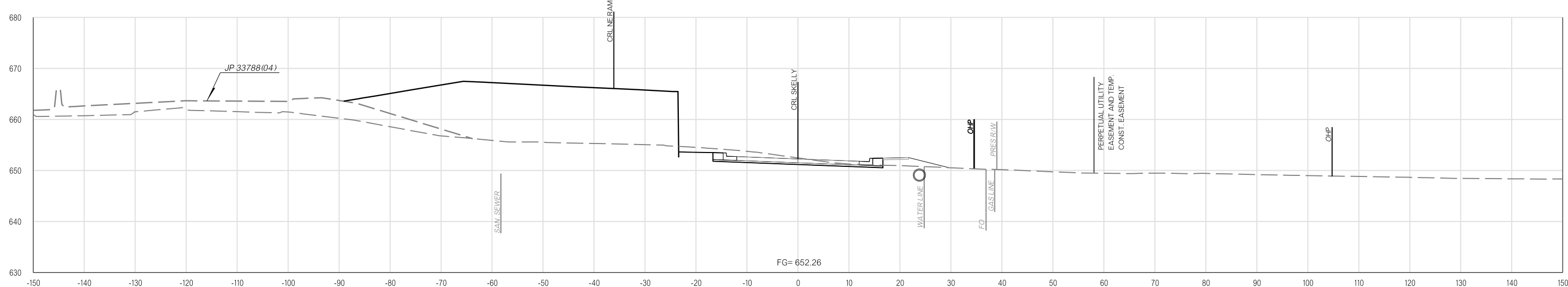
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599+64.71

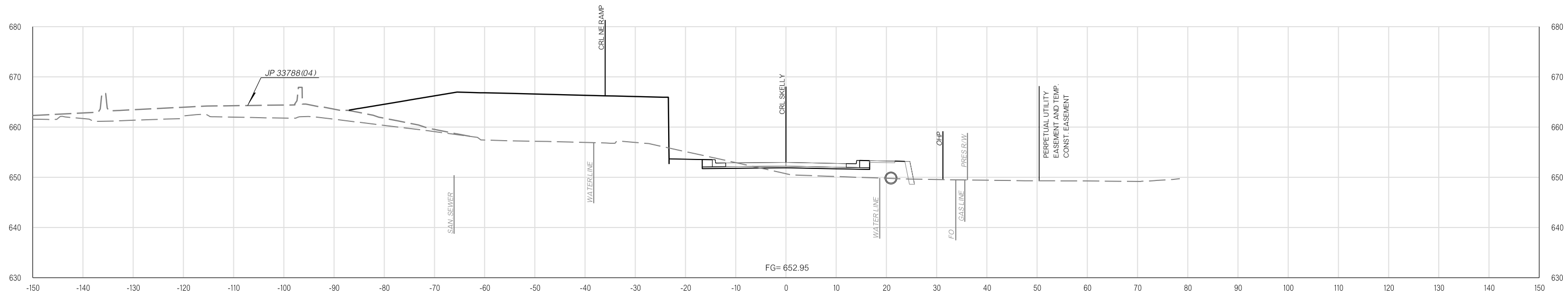


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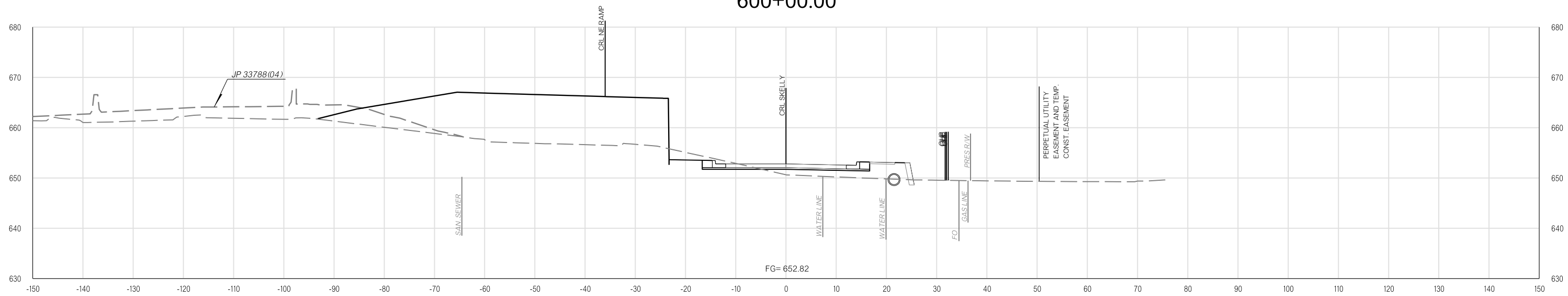
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R/W UTILITY MEETING

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600+00.00

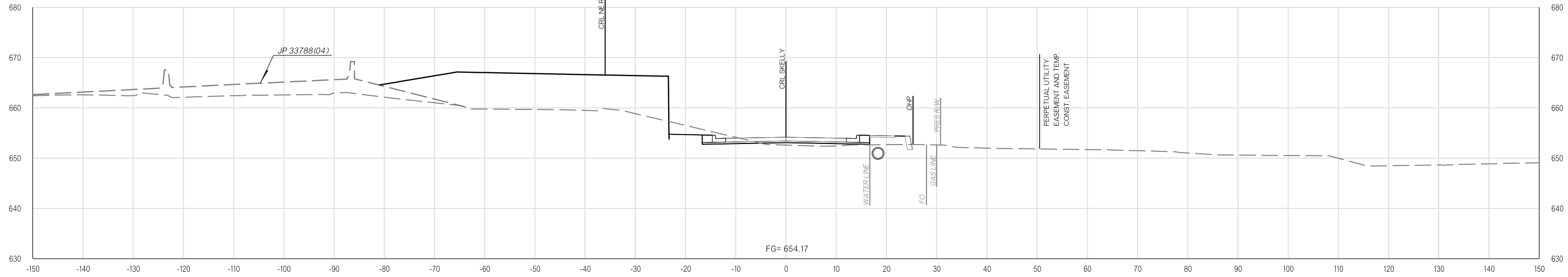


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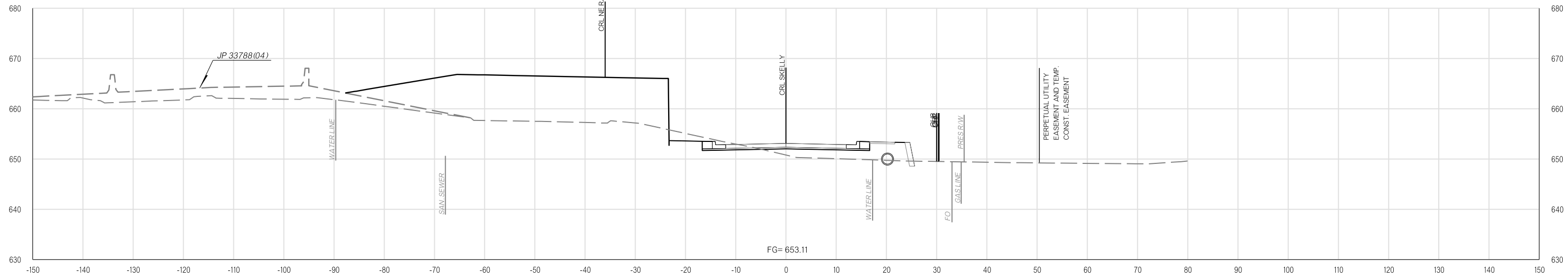
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R/W UTILITY MEETING

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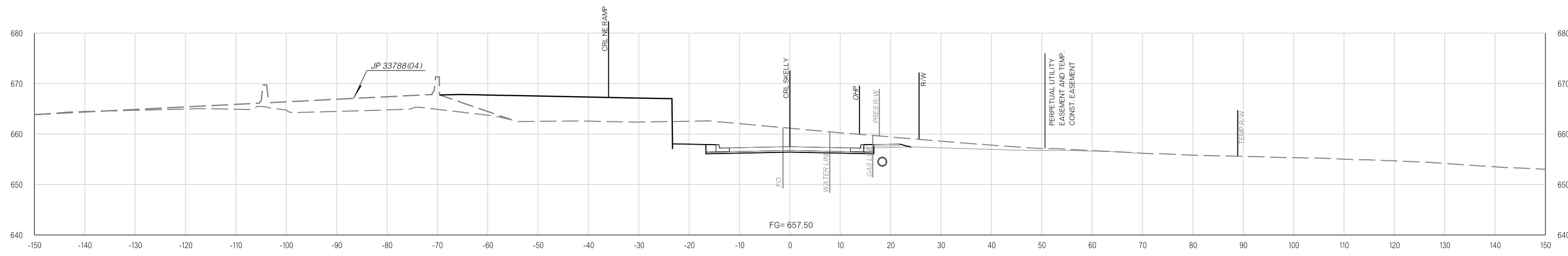


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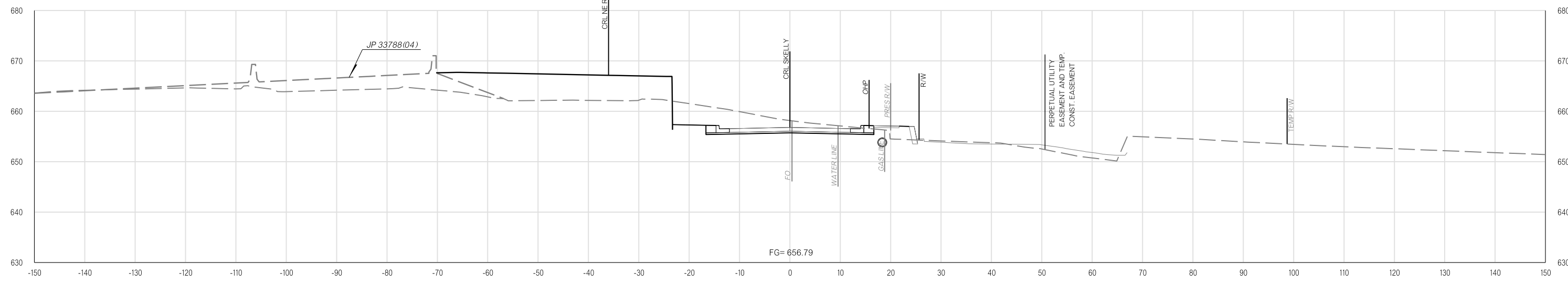


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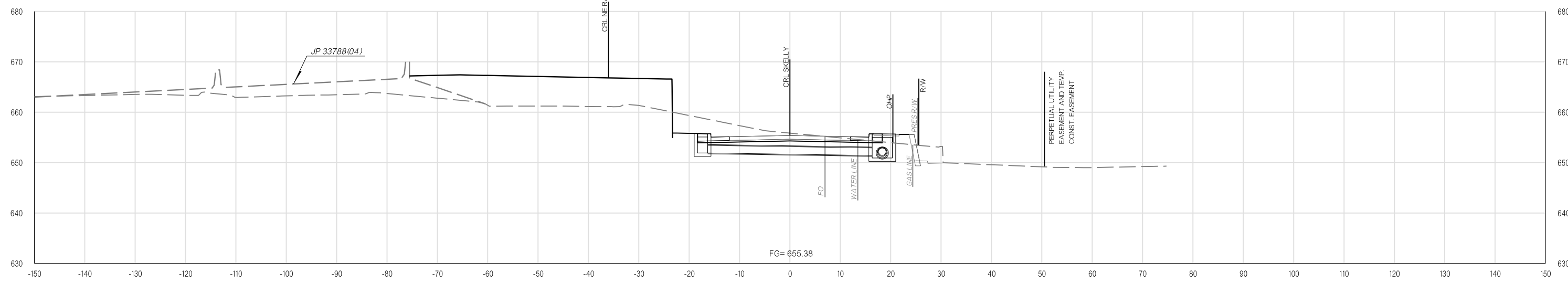
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603+41.72



603+00.00

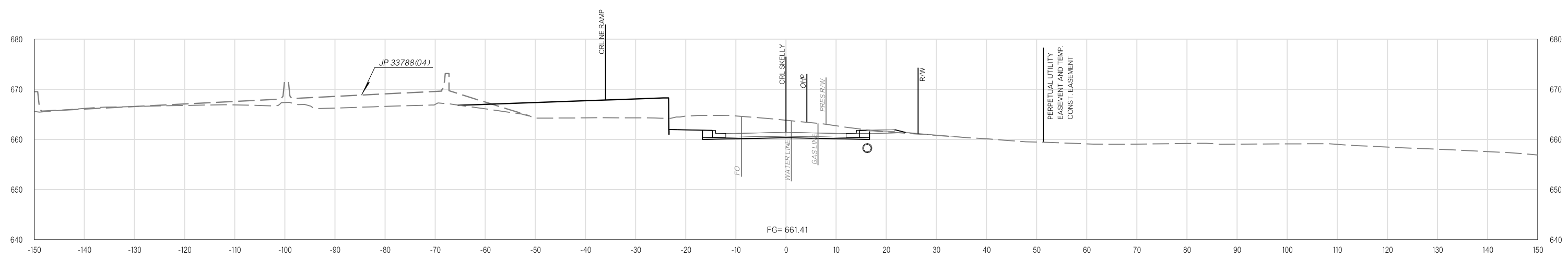


602+00.00

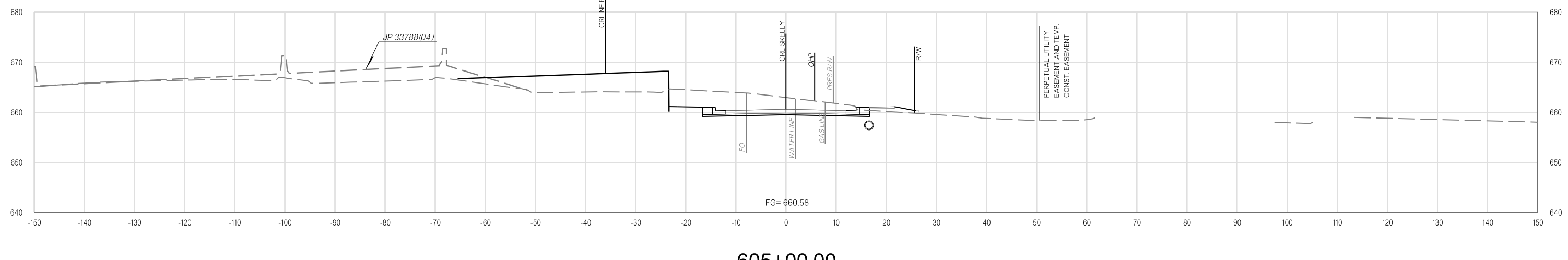
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R/W UTILITY MEETING

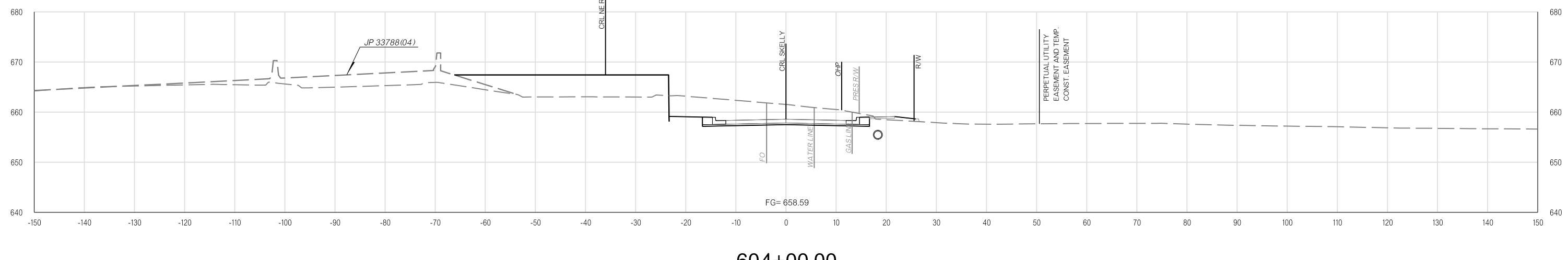
Mar. 17, 2021



605+41.67



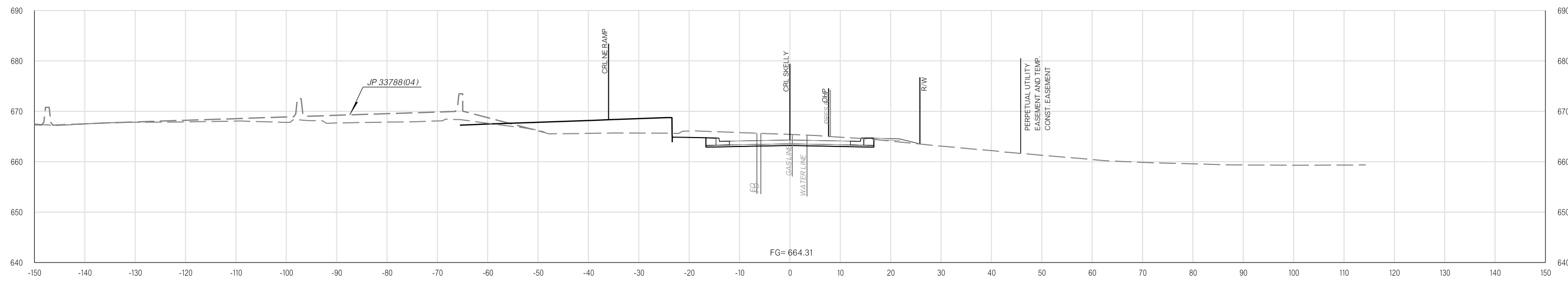
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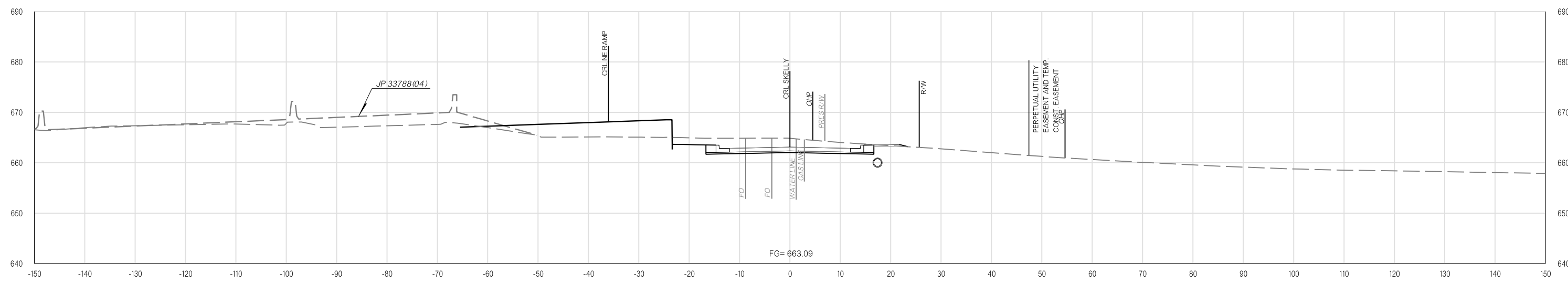
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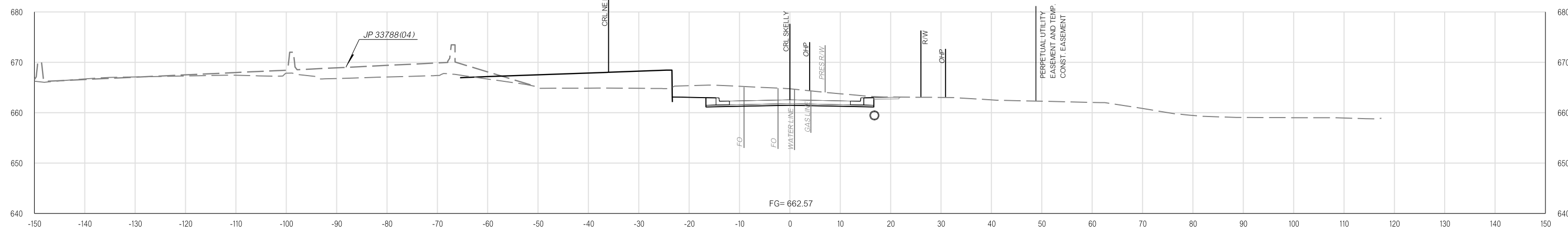
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607+00.00



606+26.33

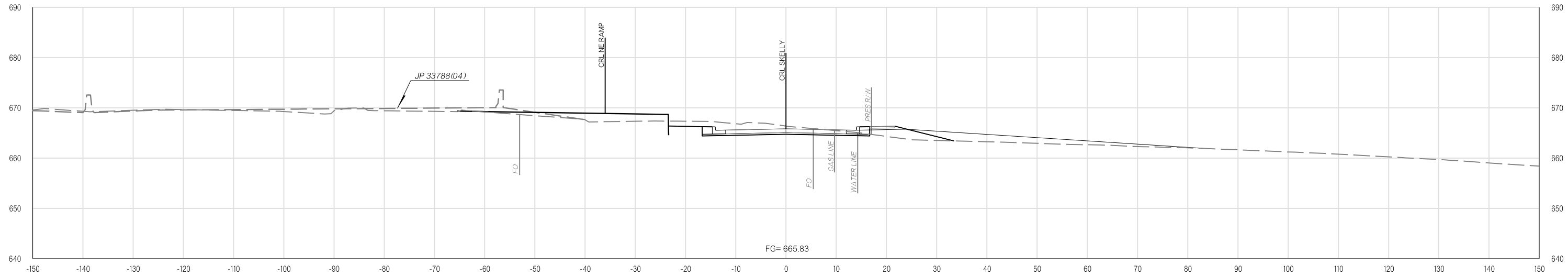


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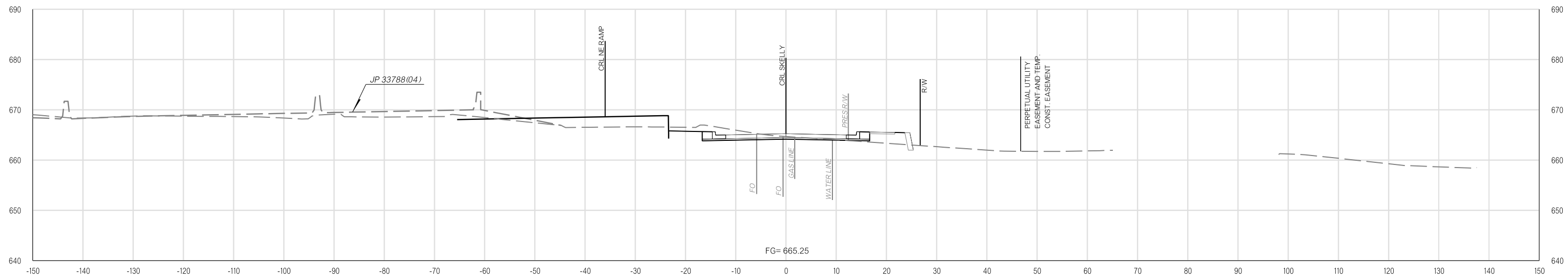
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R/W UTILITY MEETING

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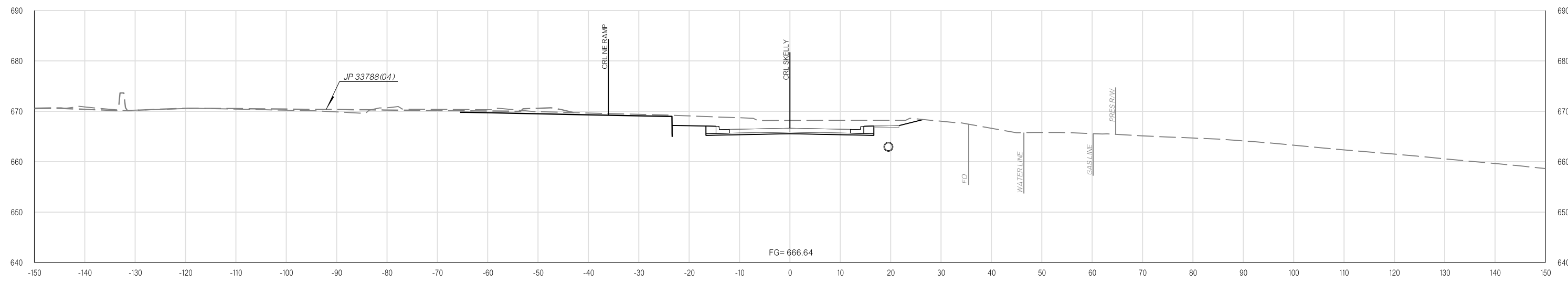


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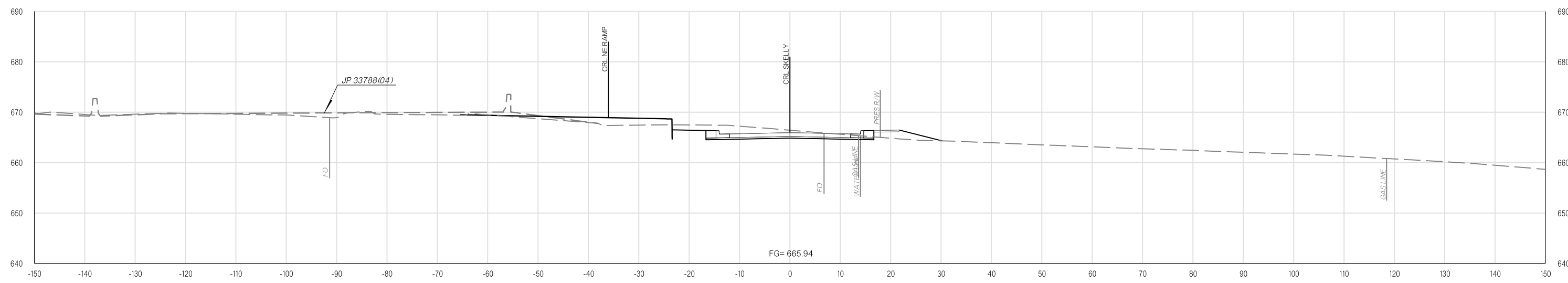


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610+00.00

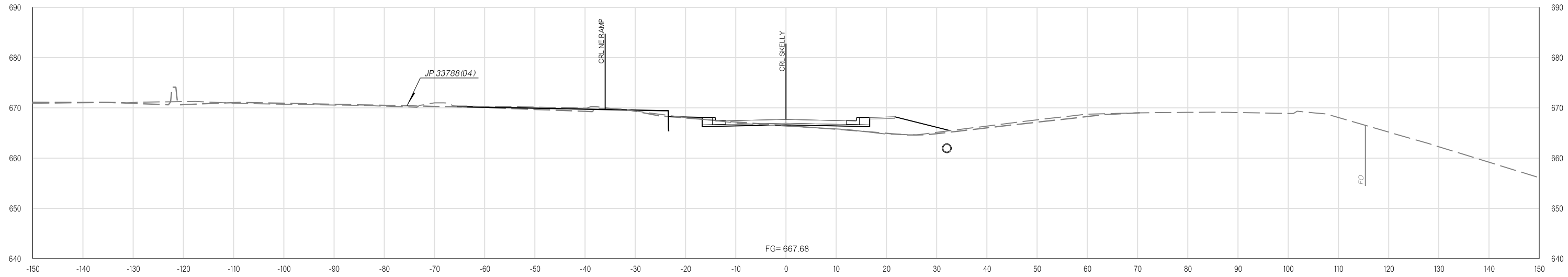


609+00.00

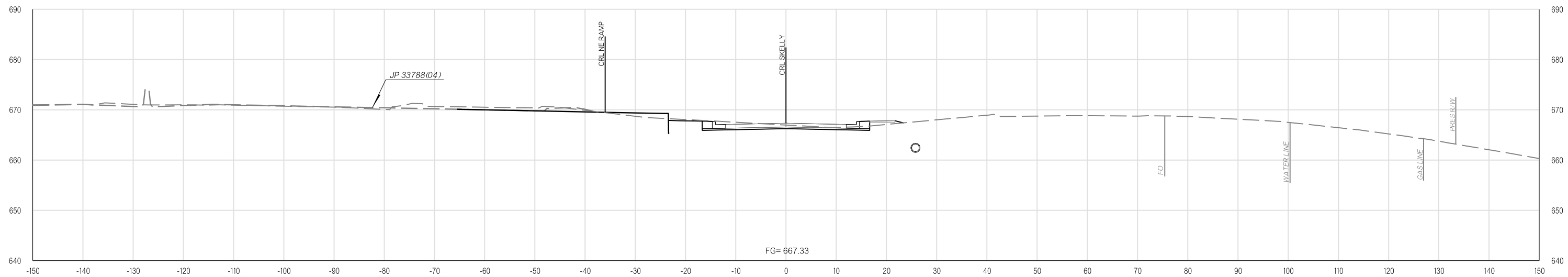
SKELLY DR.

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612+00.00

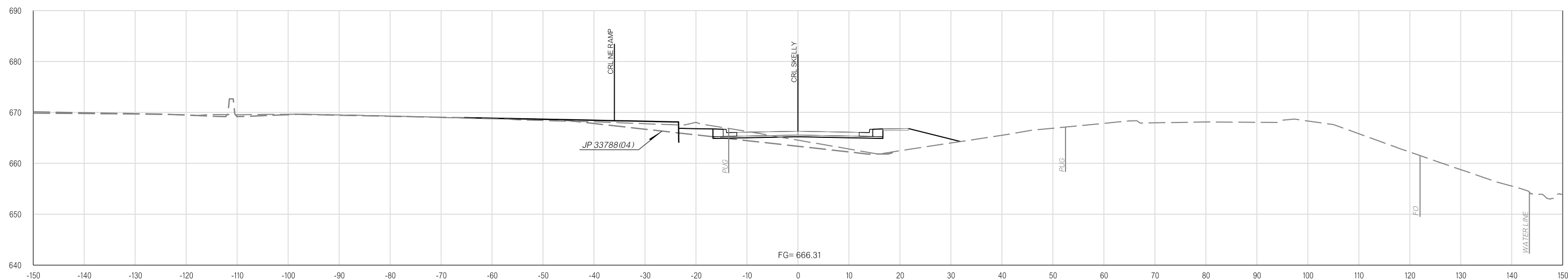


611+00.00

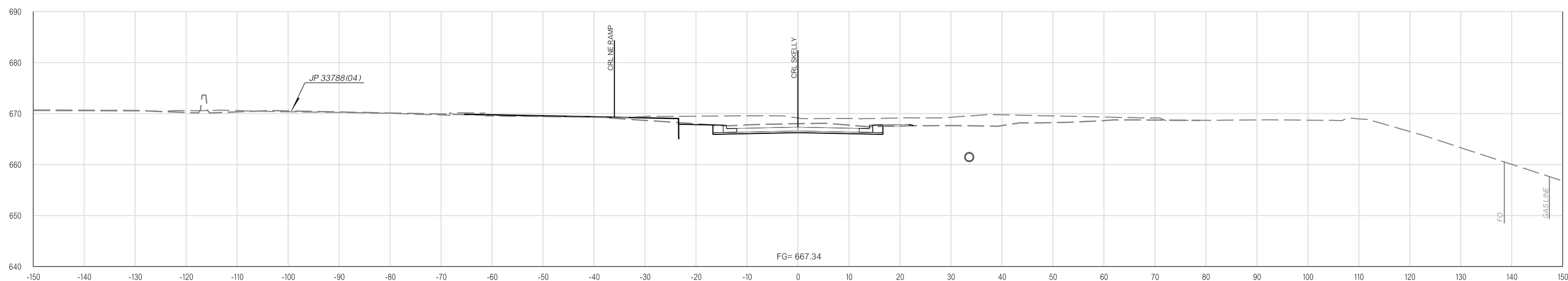
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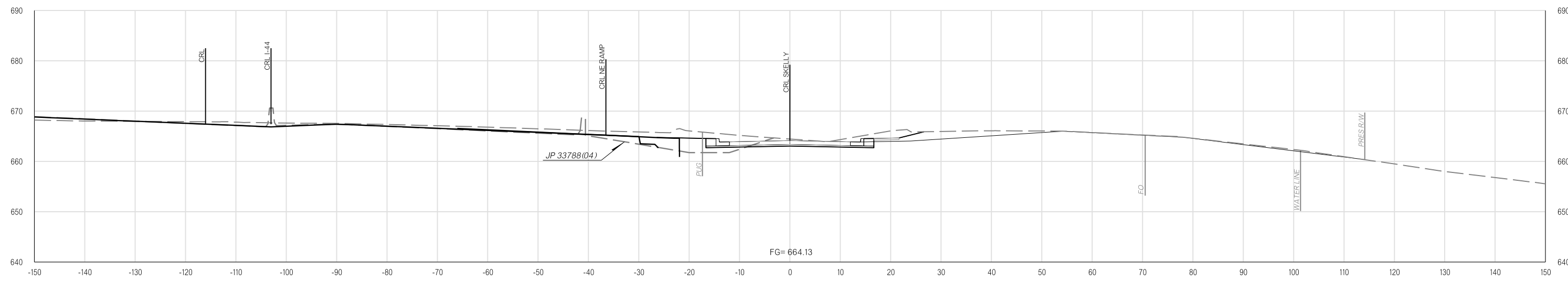


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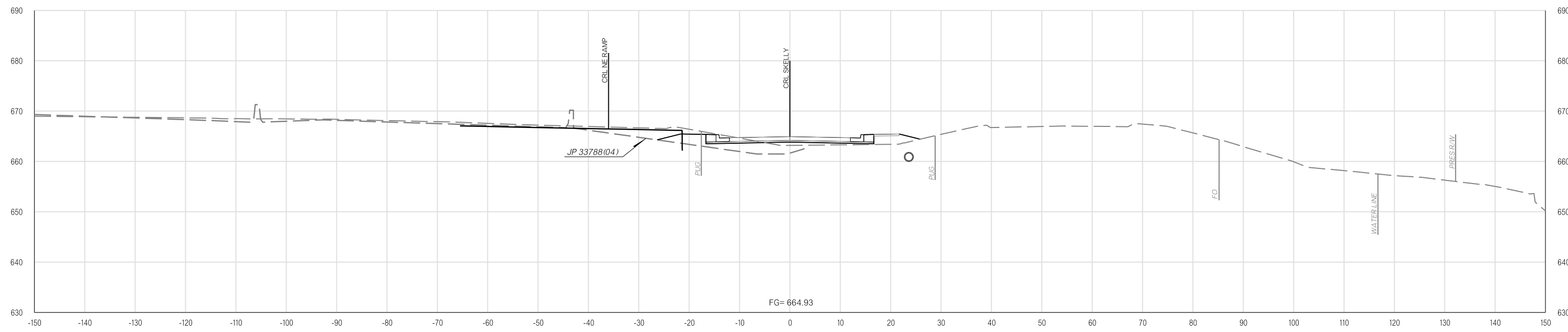


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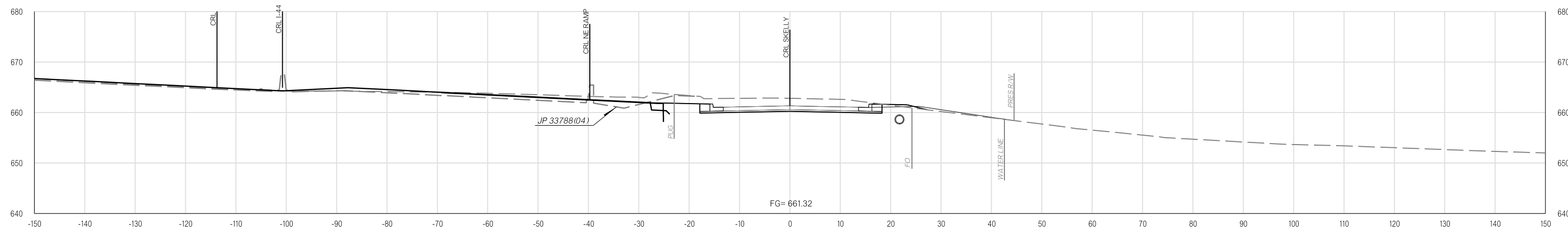


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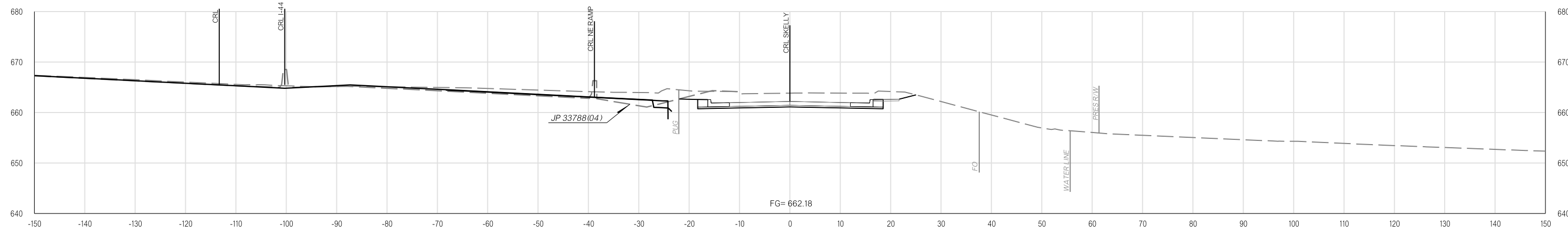


615+00.00

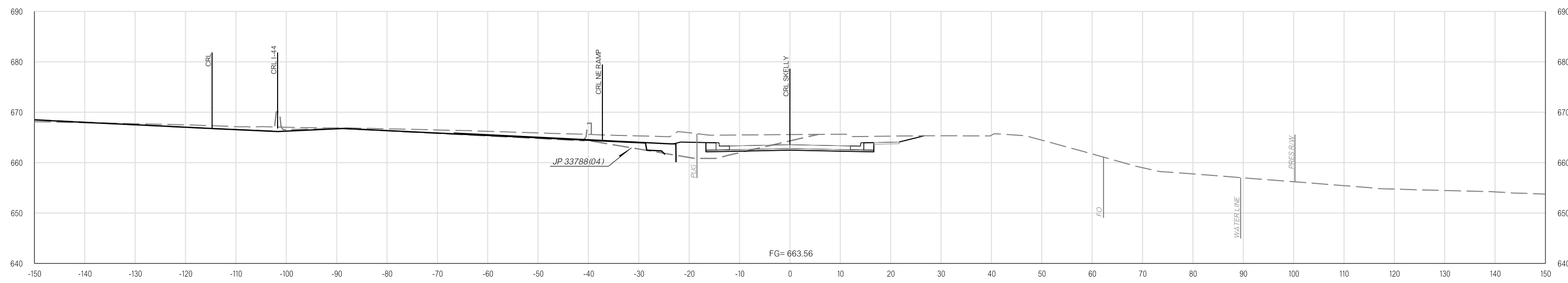
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617+62.88

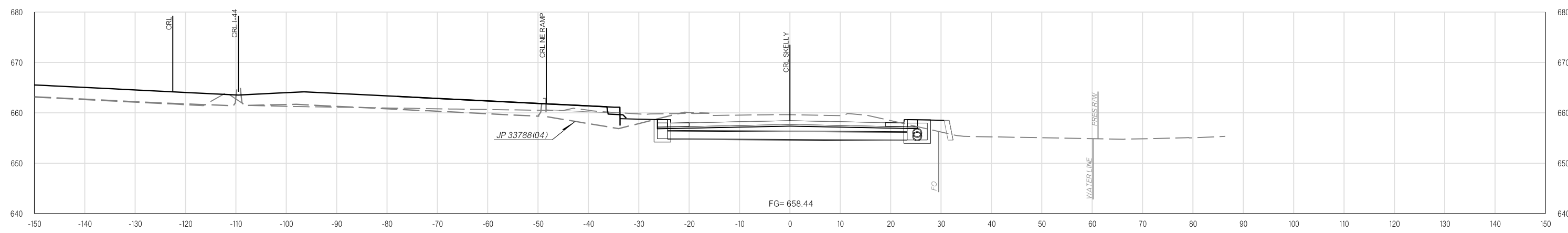


617+00.00

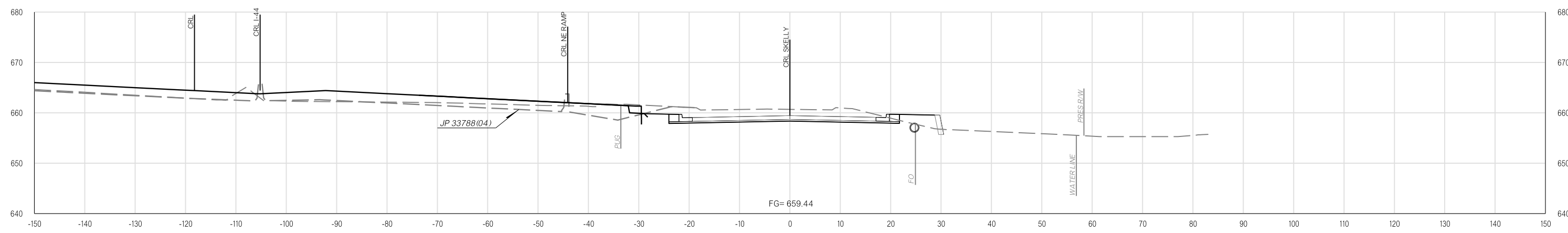


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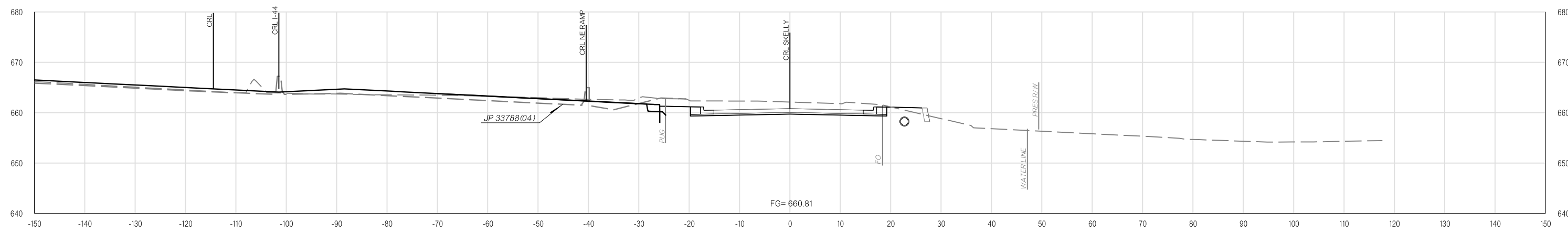
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619+72.73



619+00.00



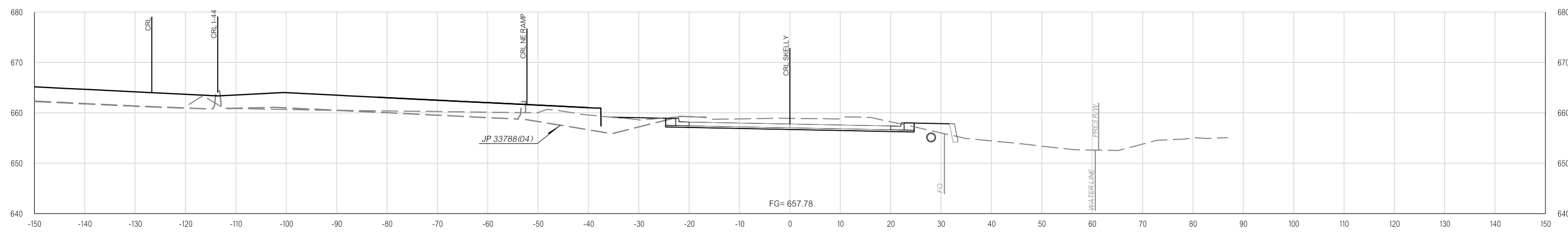
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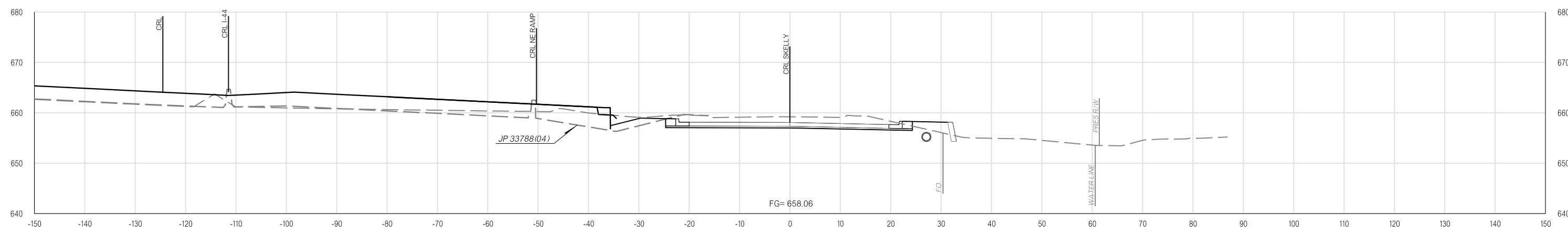
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R/W UTILITY MEETING

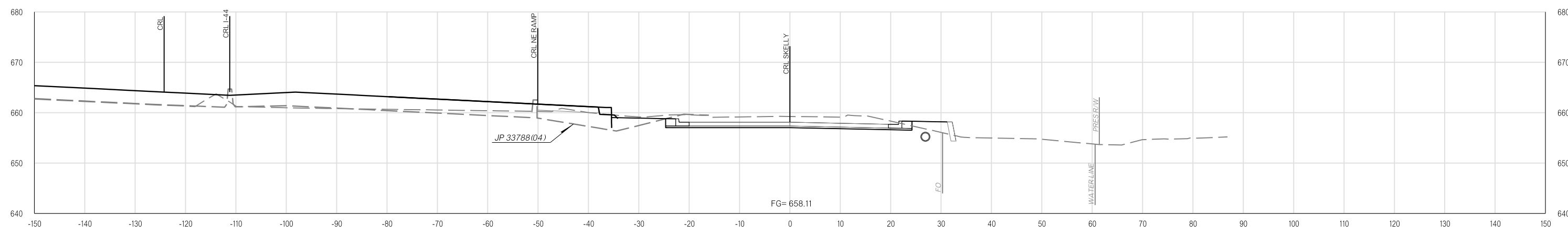
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620+20.73



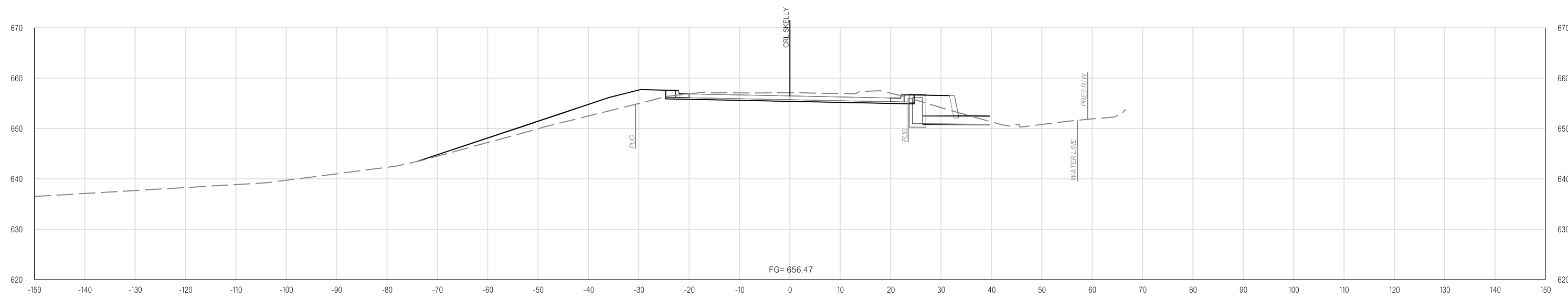
620+00.00



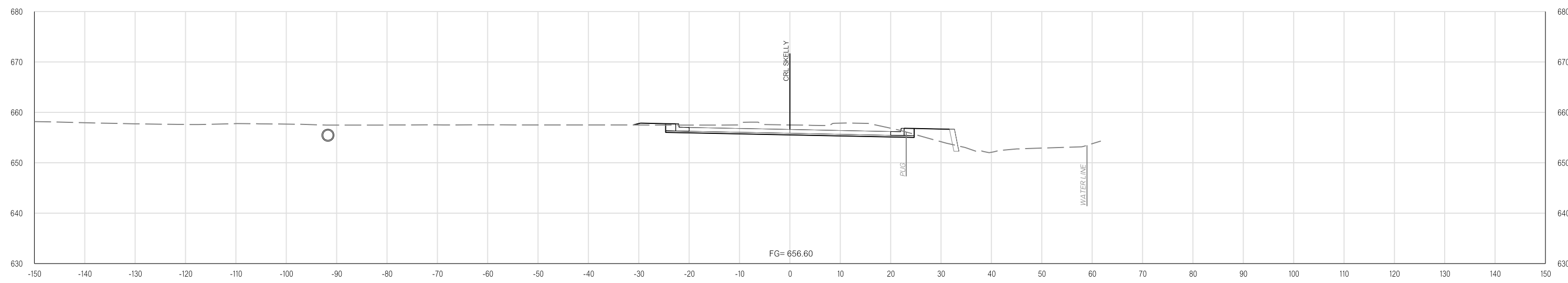
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SKELLY DR.

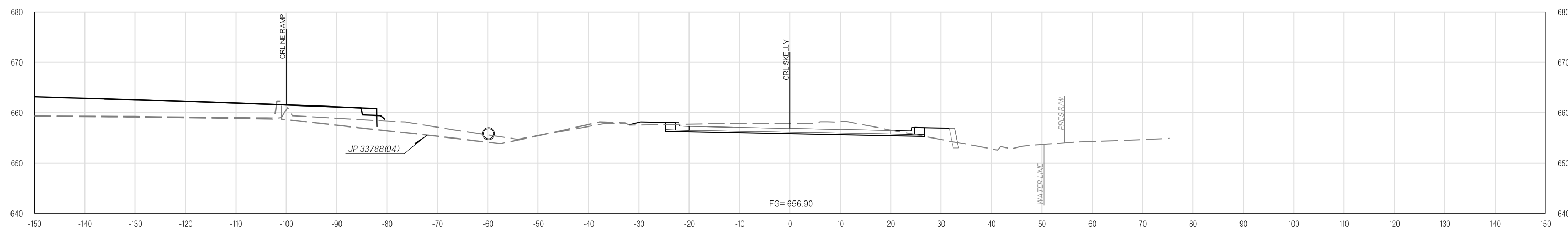
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621+97.74



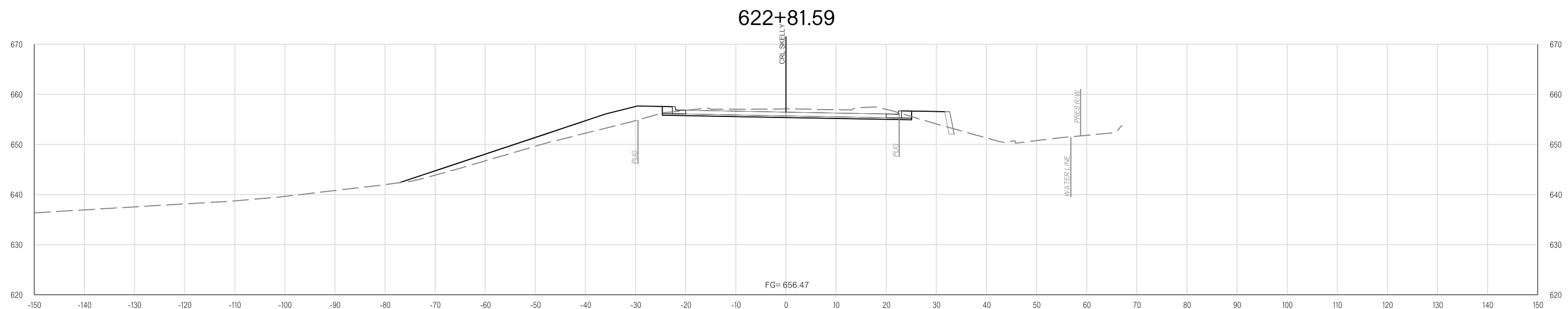
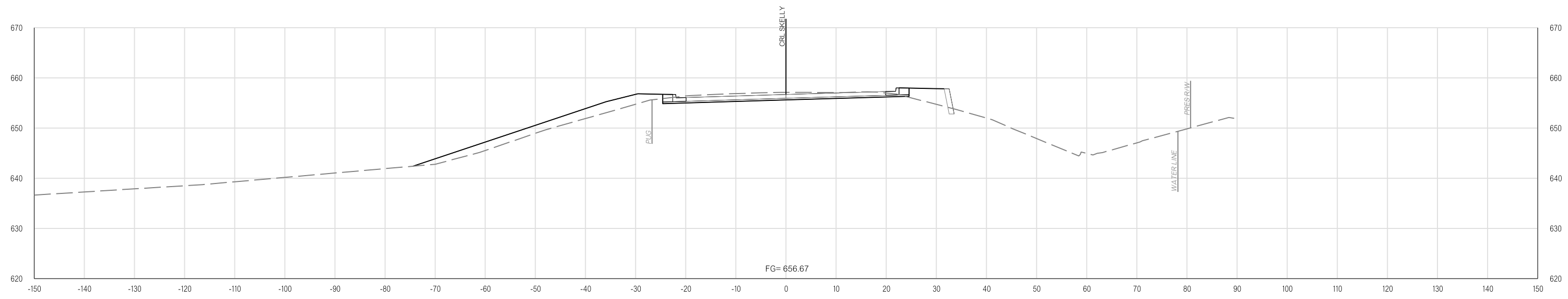
621+48.36



621+00.00

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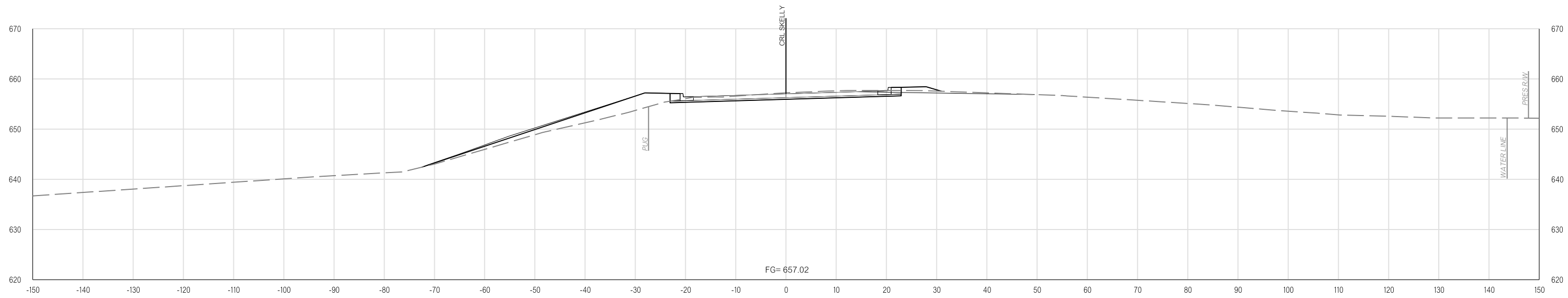
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622+00.00

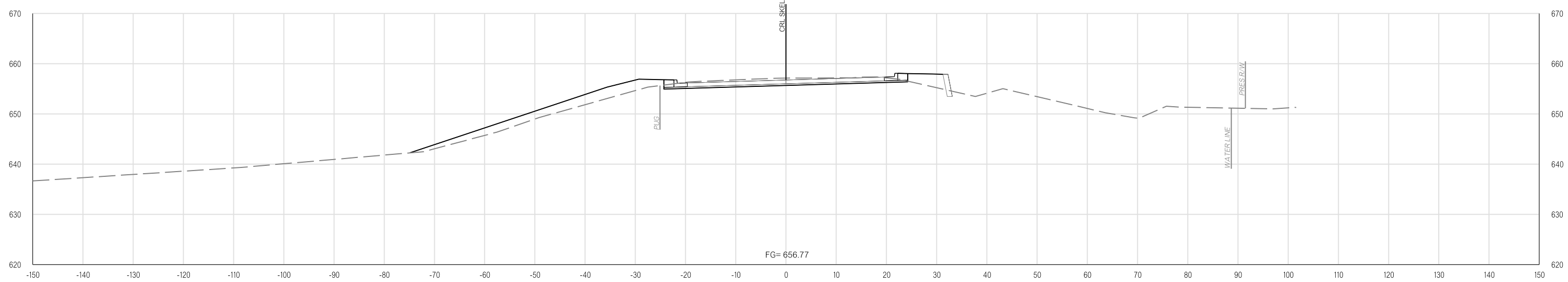
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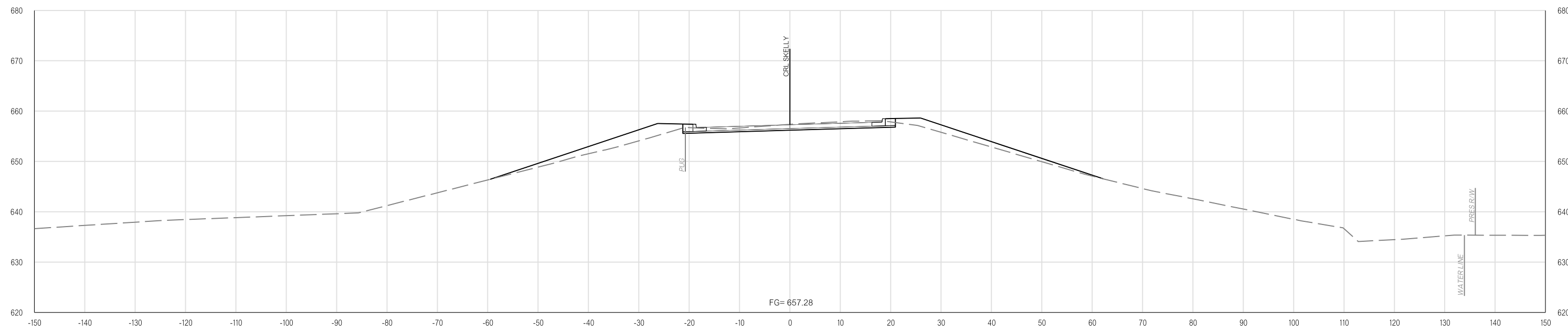
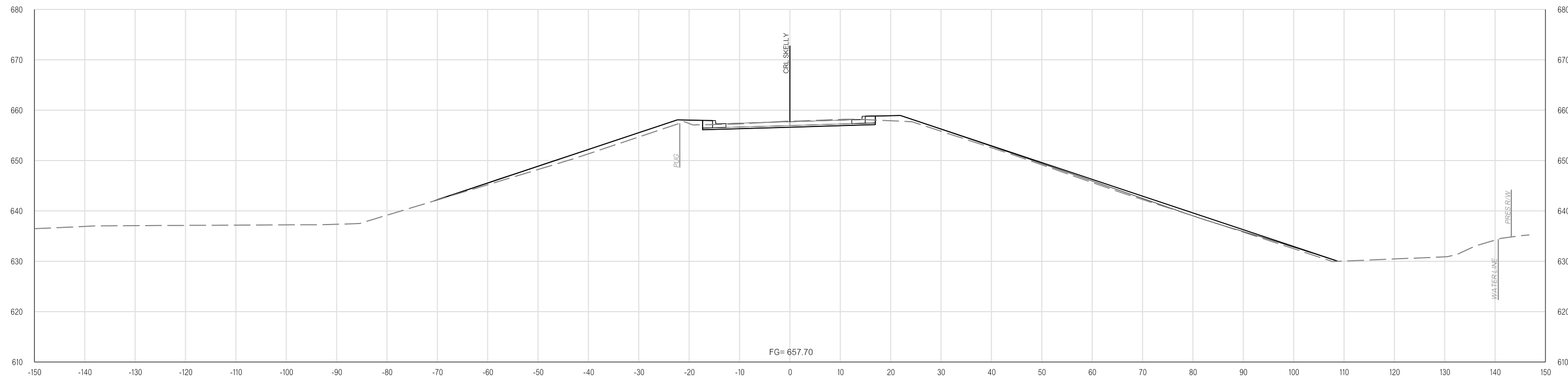


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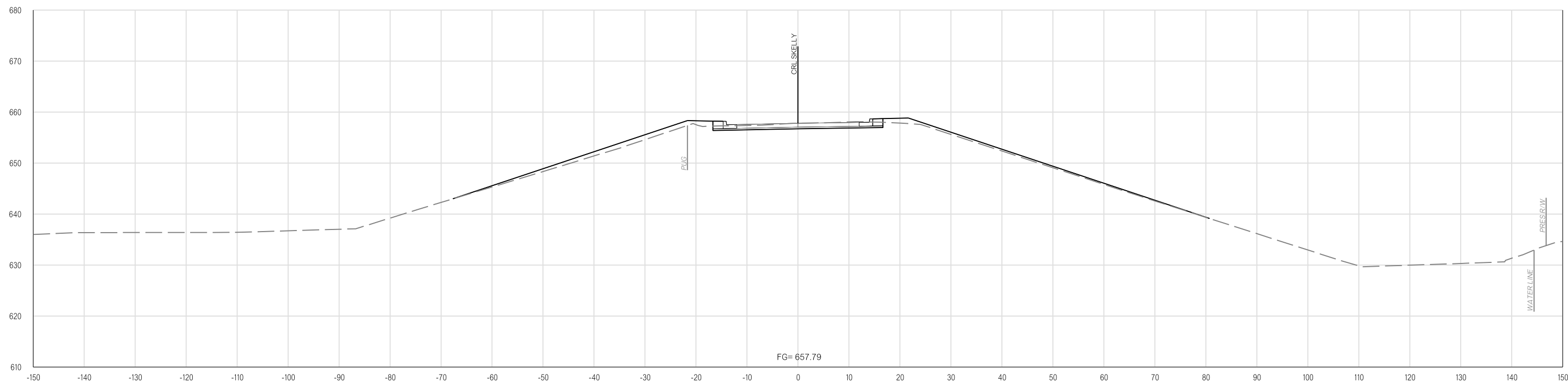


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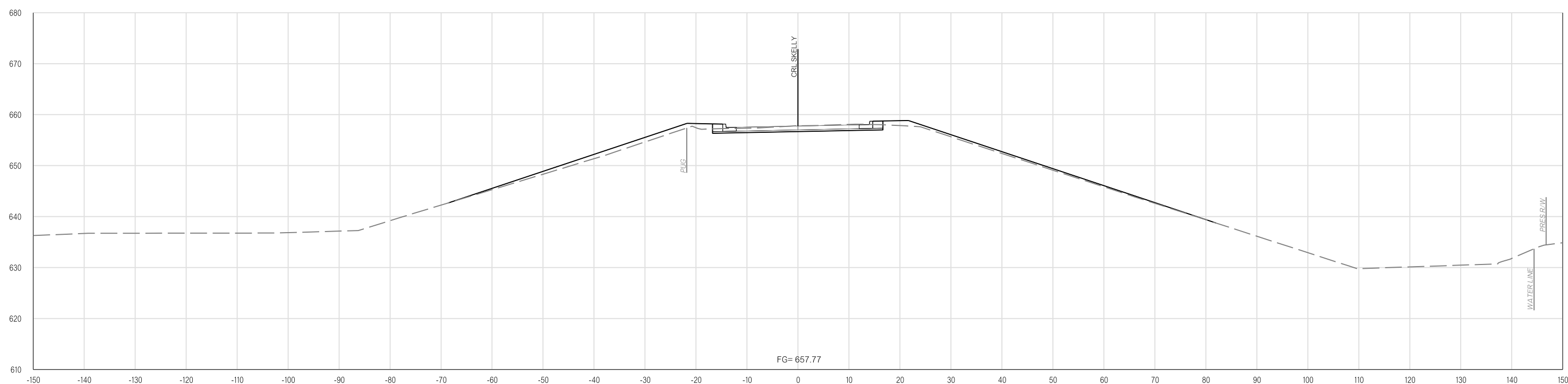
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625+00.00

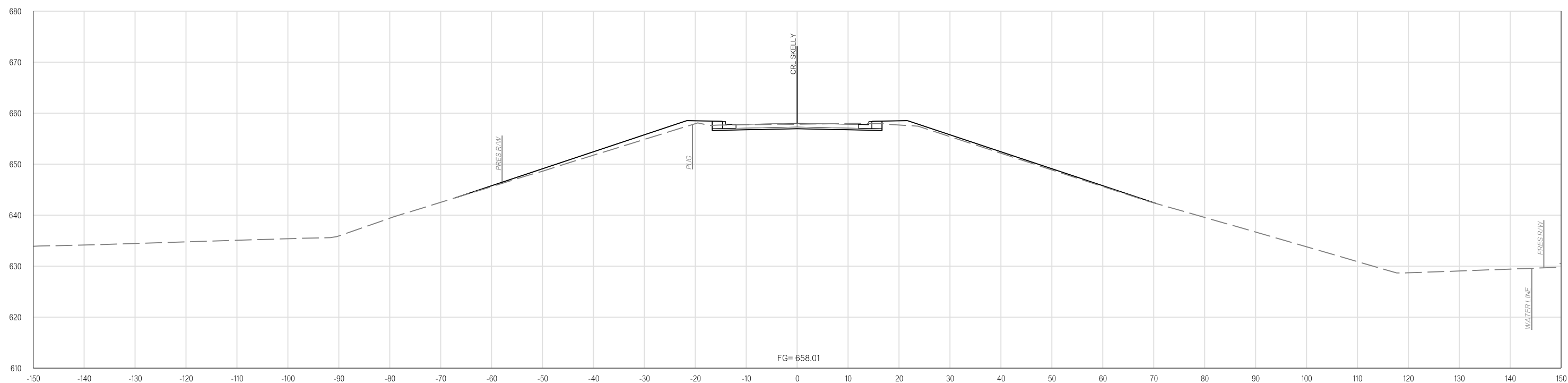


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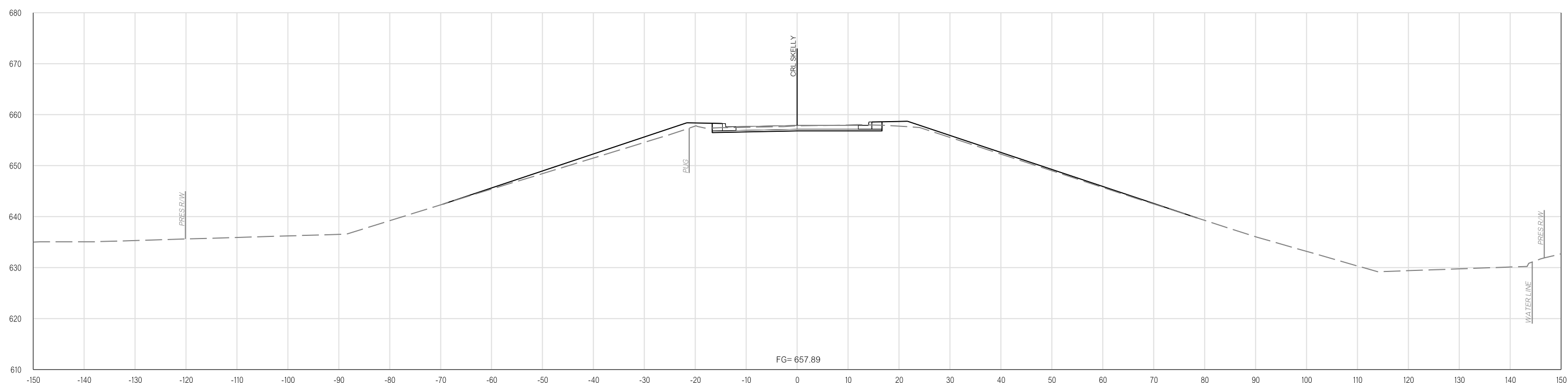
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625+42.68

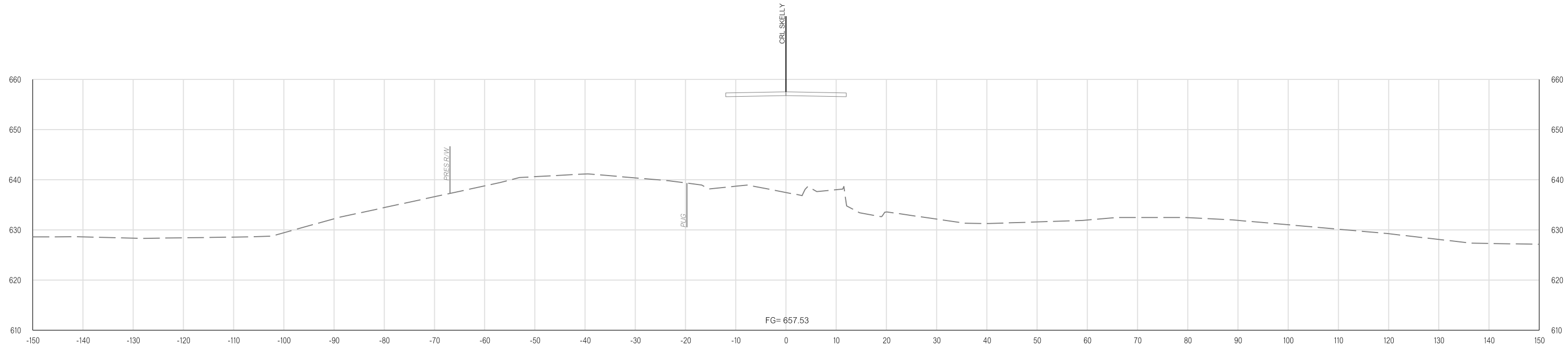


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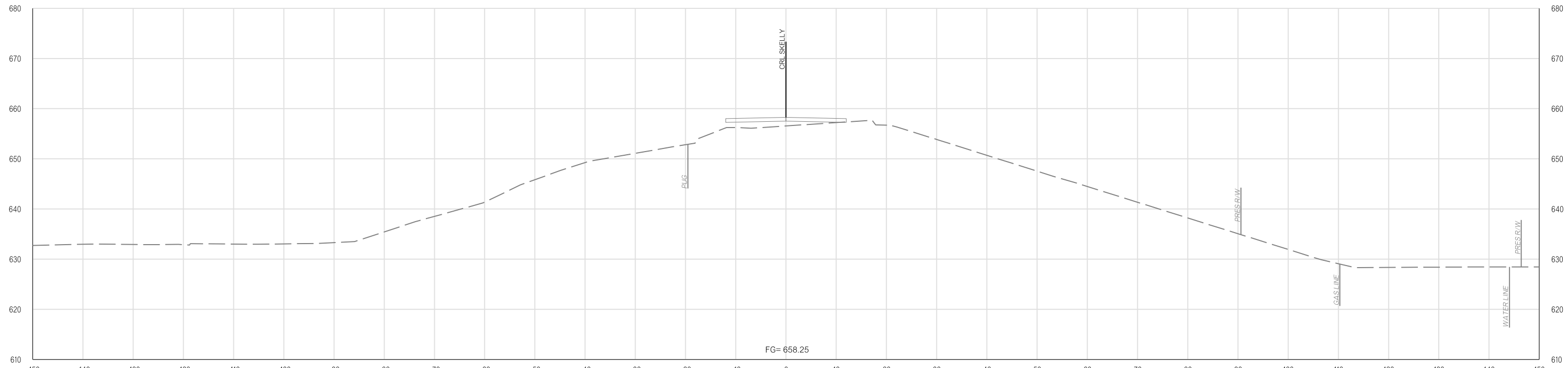
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627+00.00

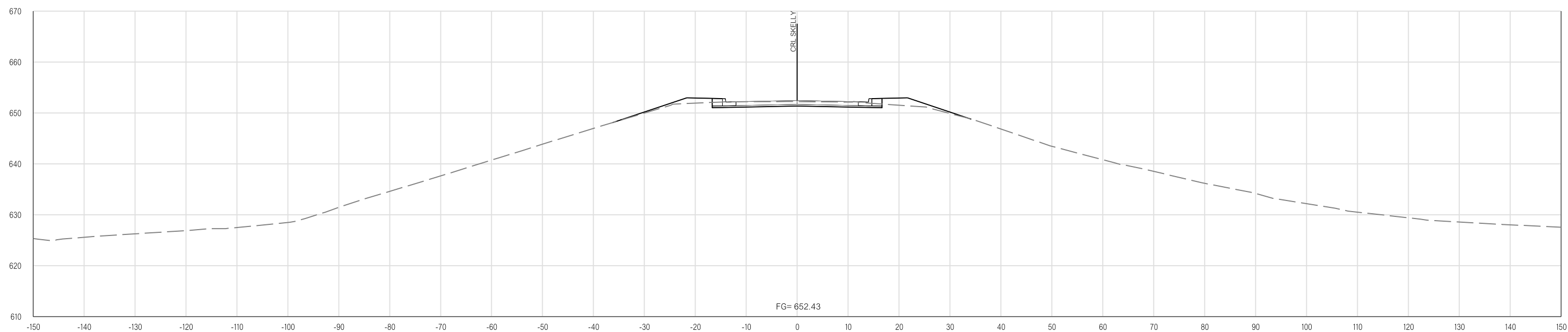


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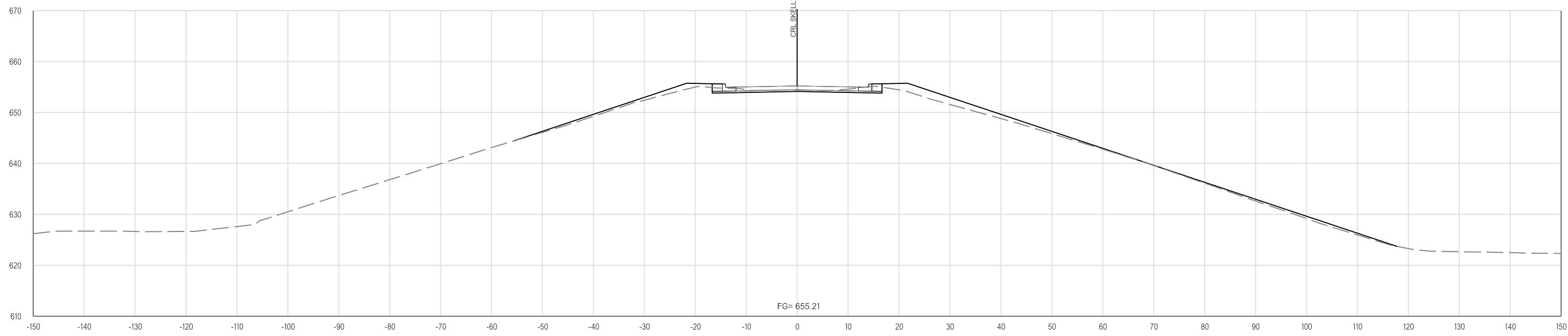
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629+00.00

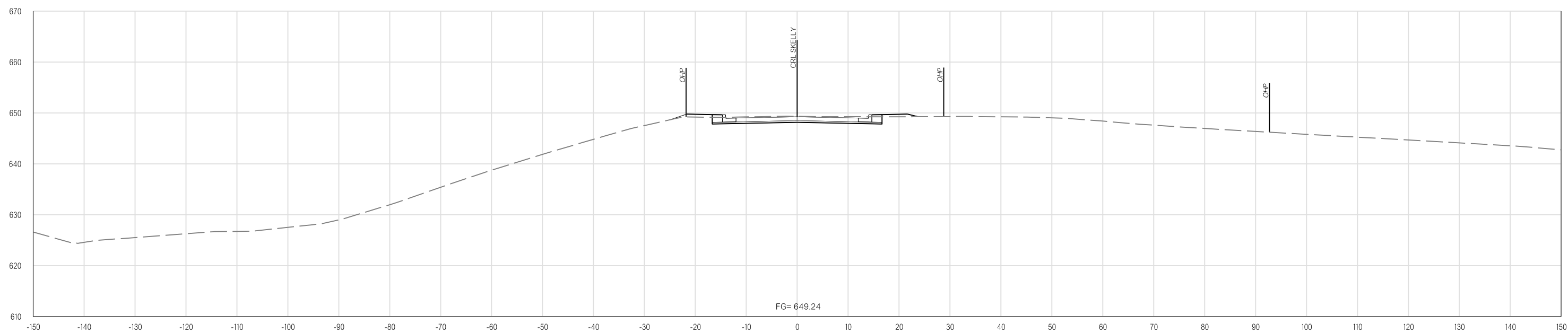


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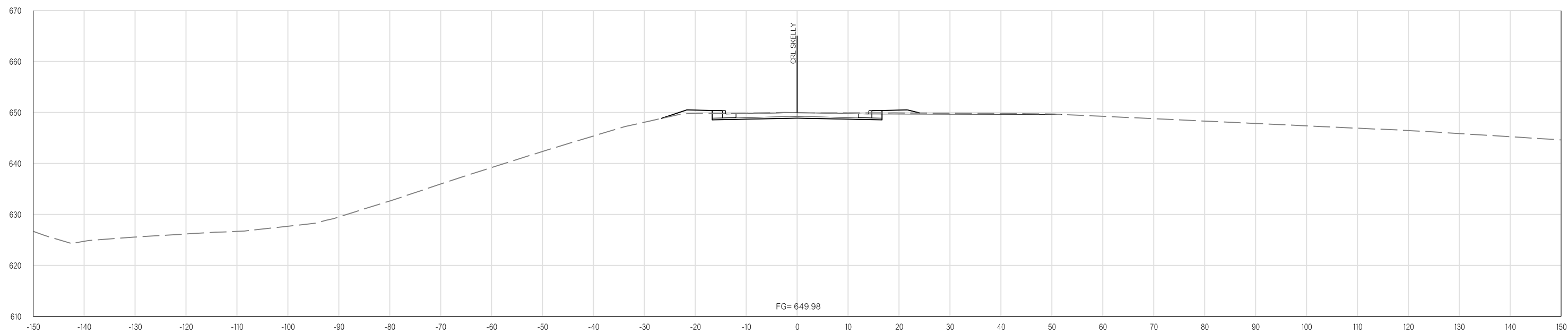
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630+00.00

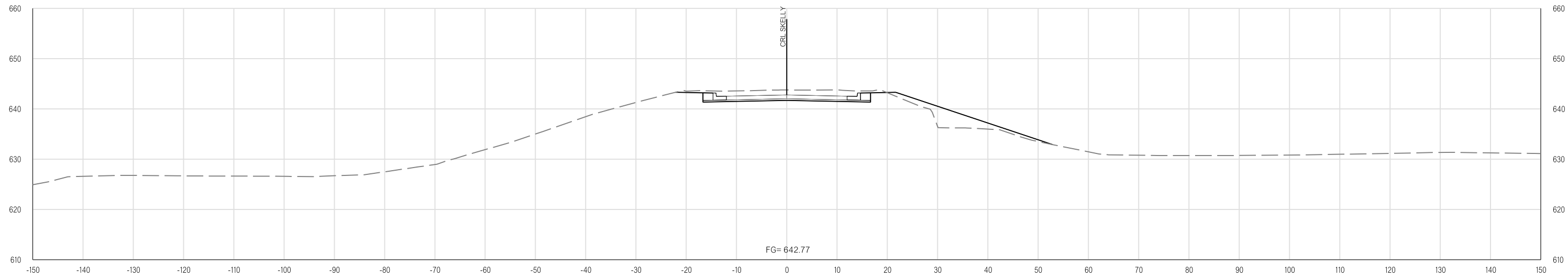


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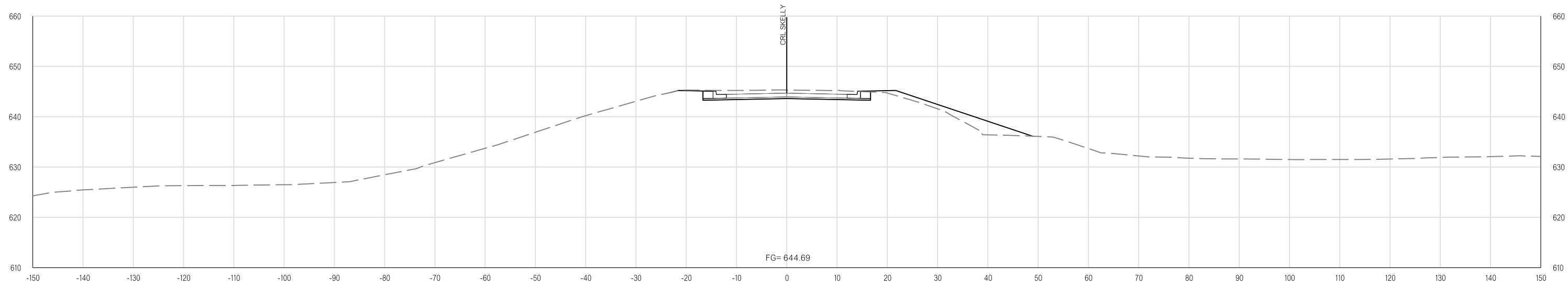
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631+38.85

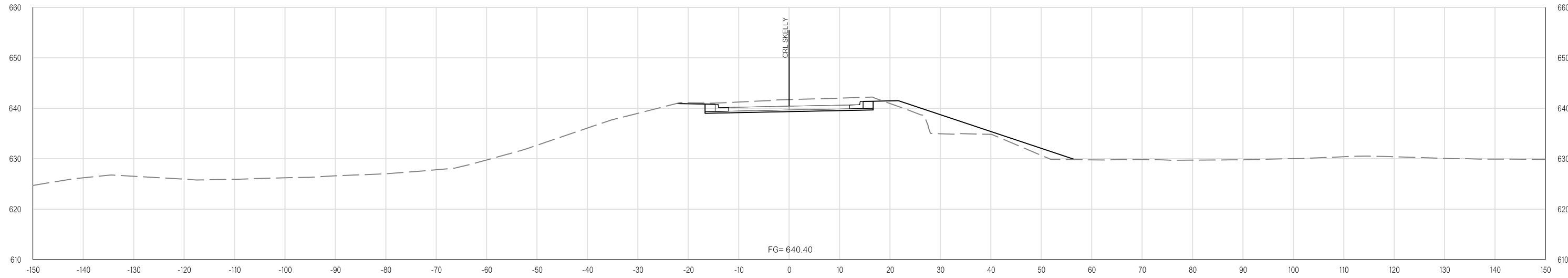


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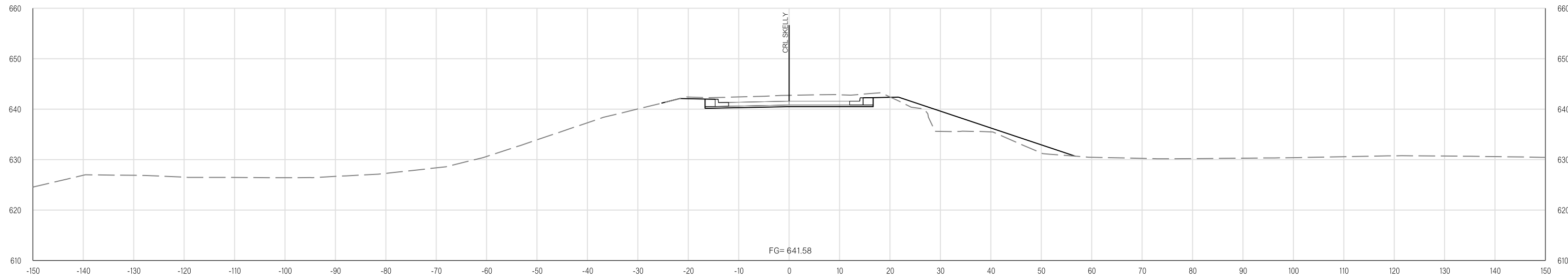
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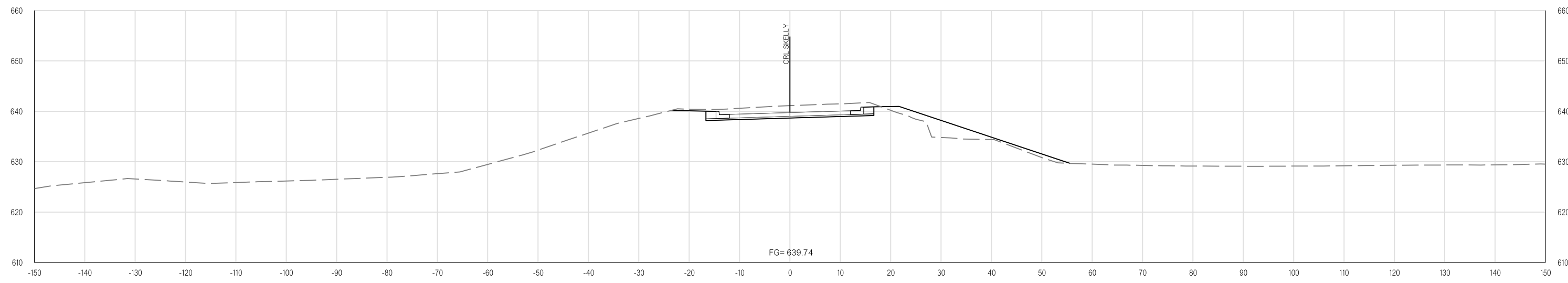


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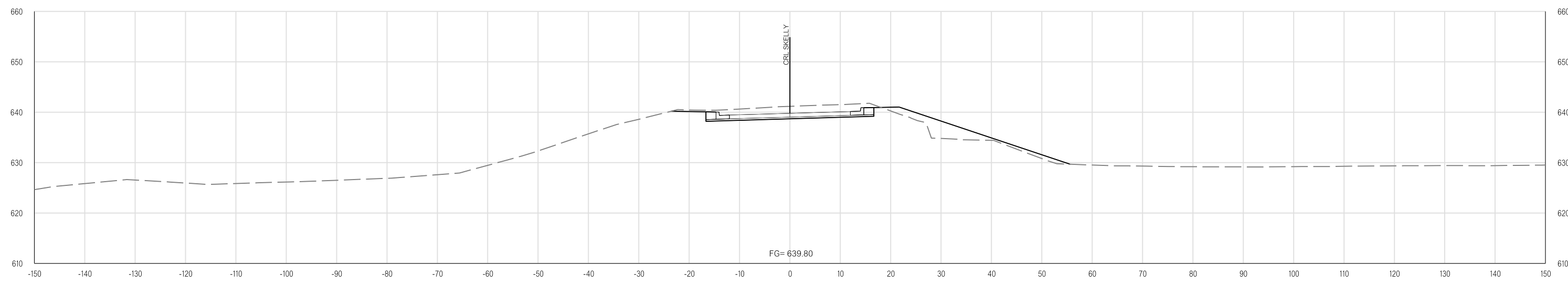


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SKELLY DR.



632+00.00

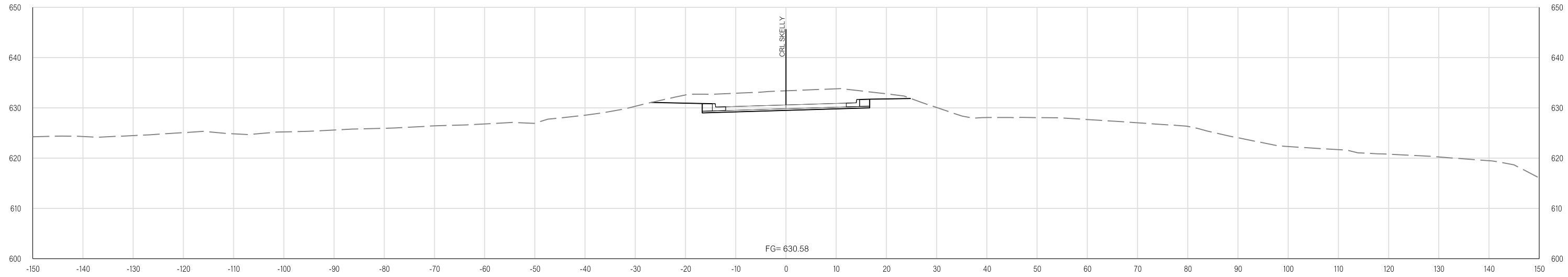


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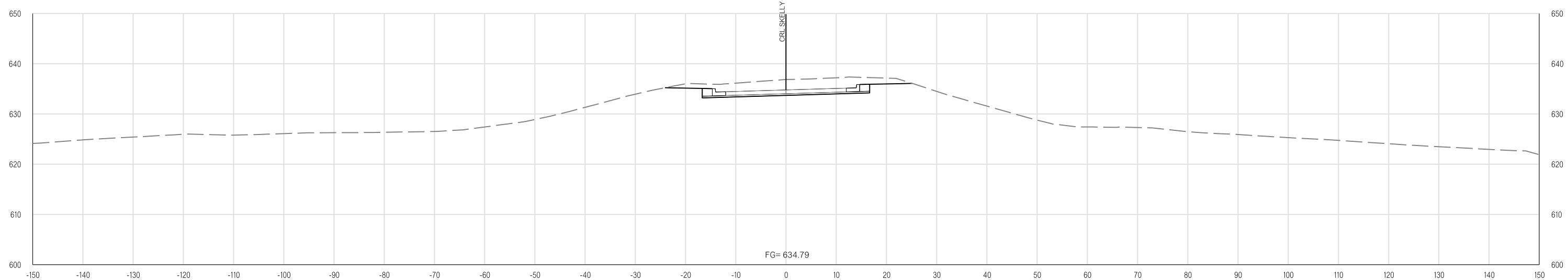
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634+00.00



633+00.00

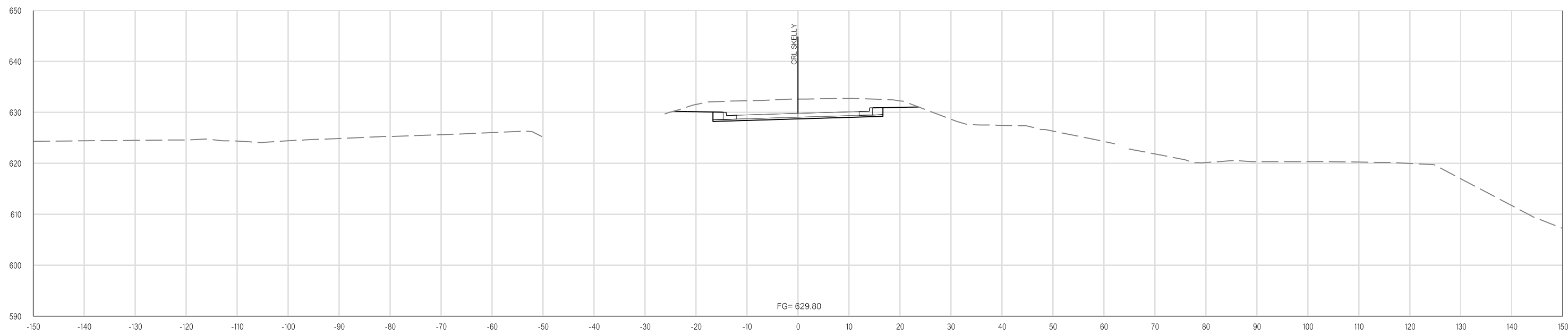
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634+37.36

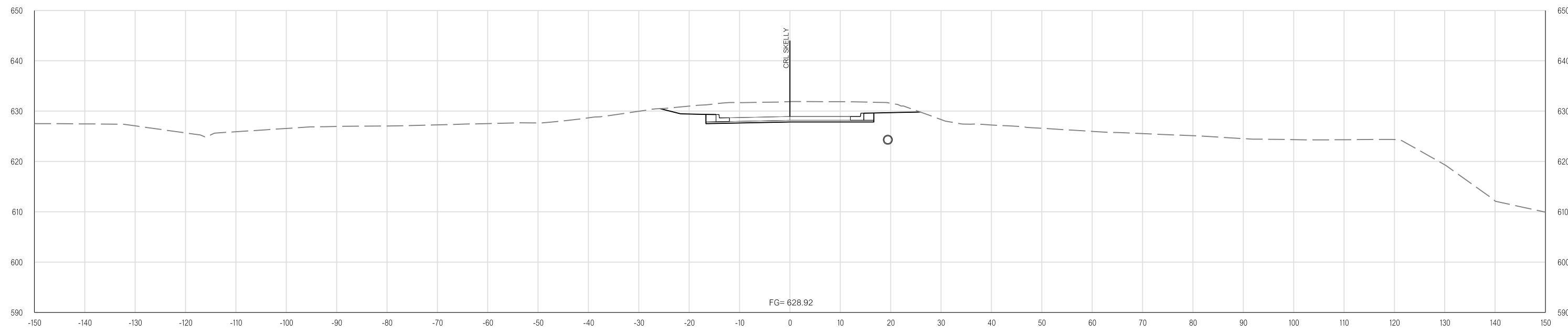
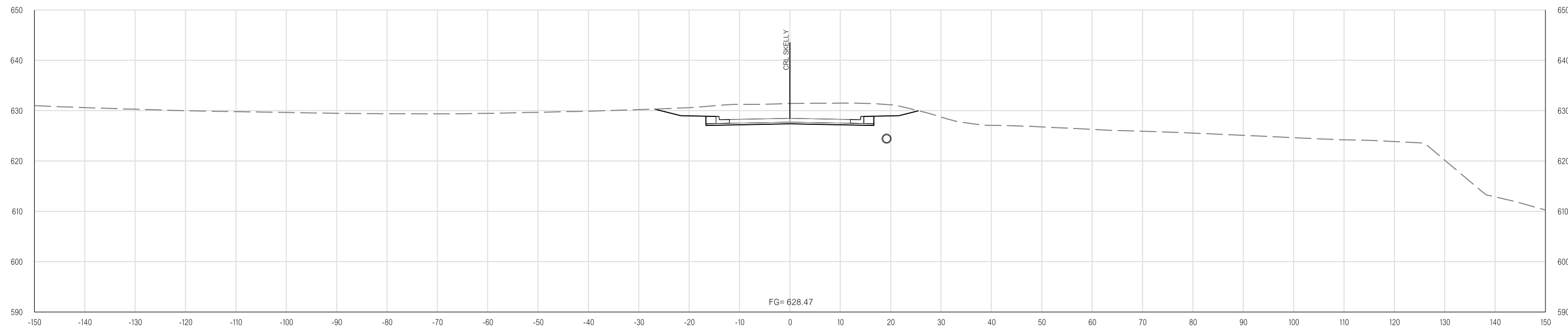


634+25.36

SKELLY DR.

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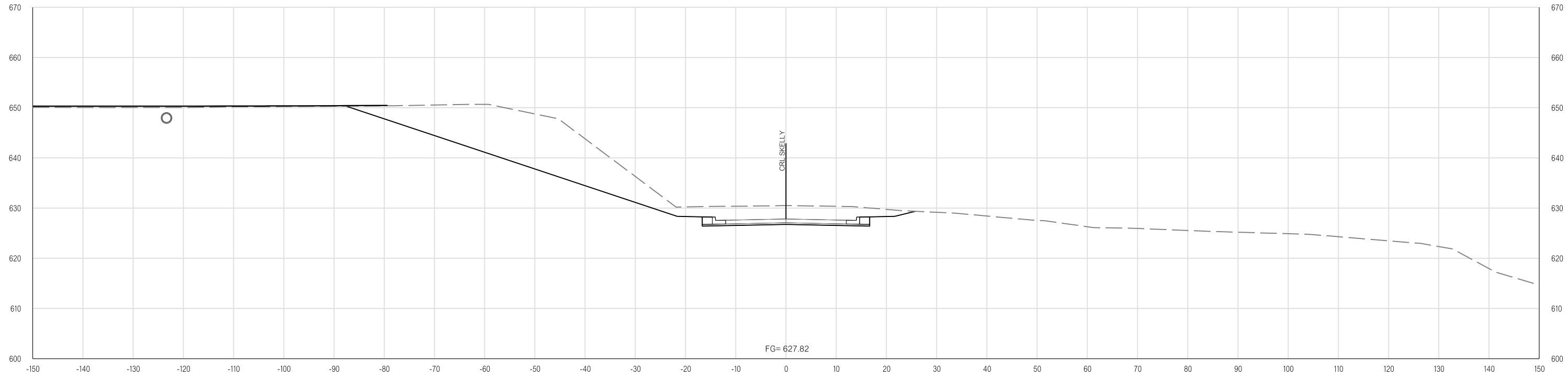


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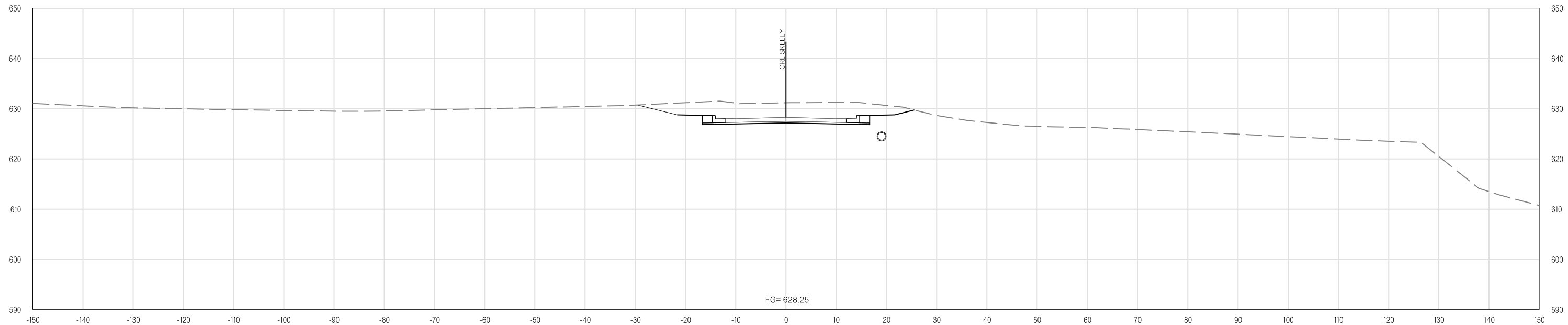
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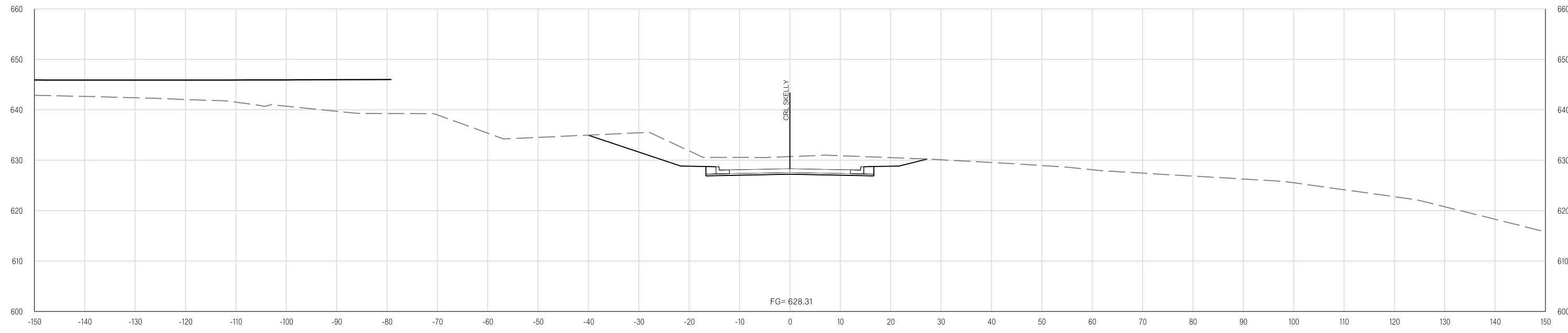


636+00.00

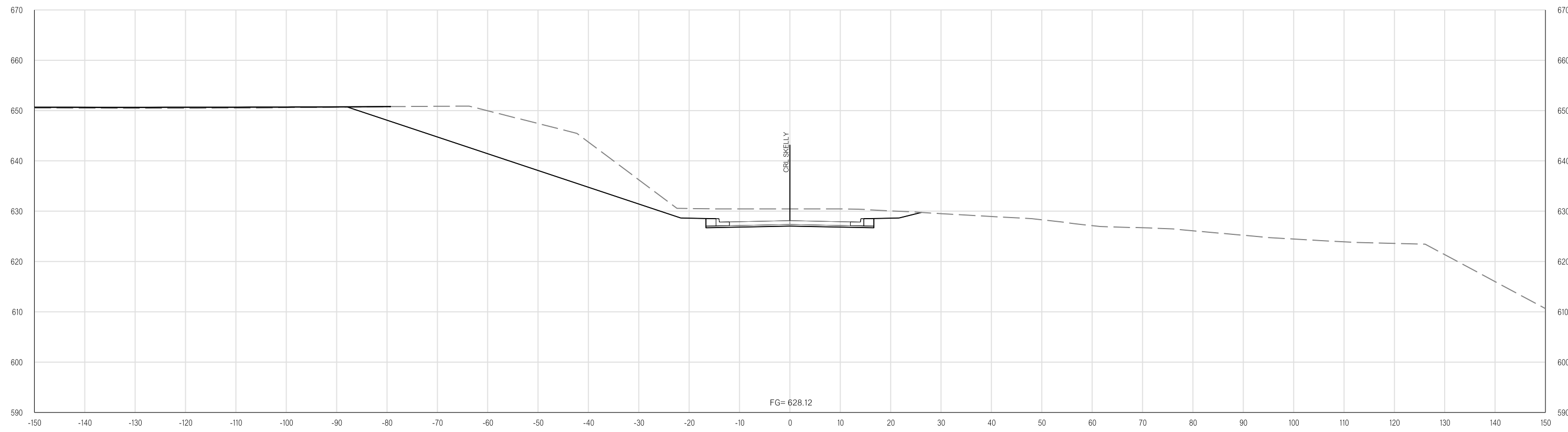


635+00.00

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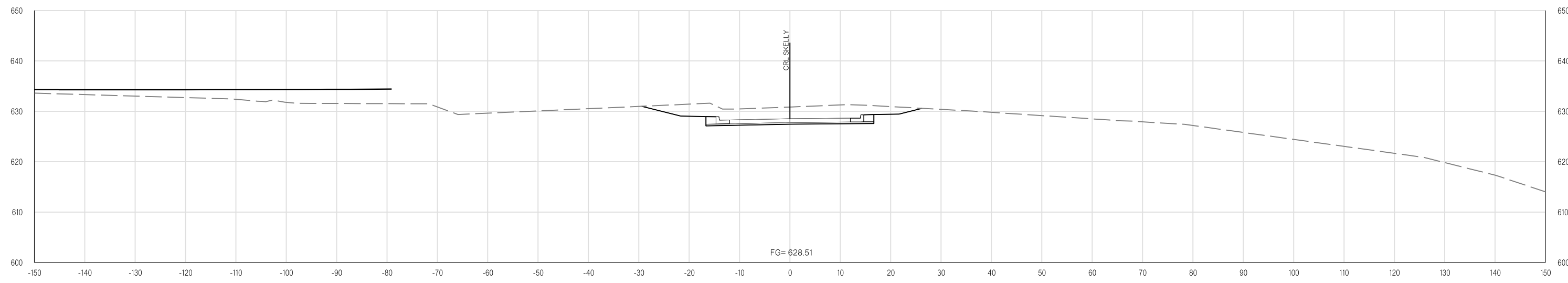


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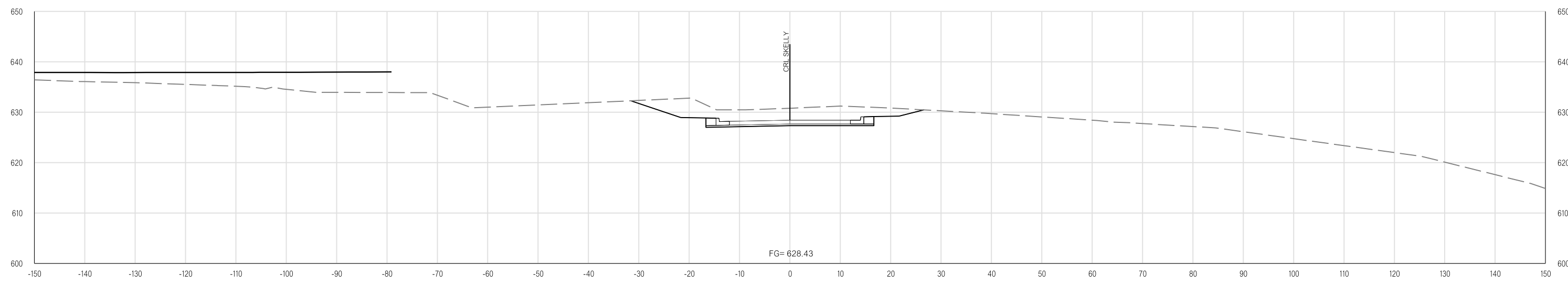


637+00.00

SKELLY DR.



638+00.00

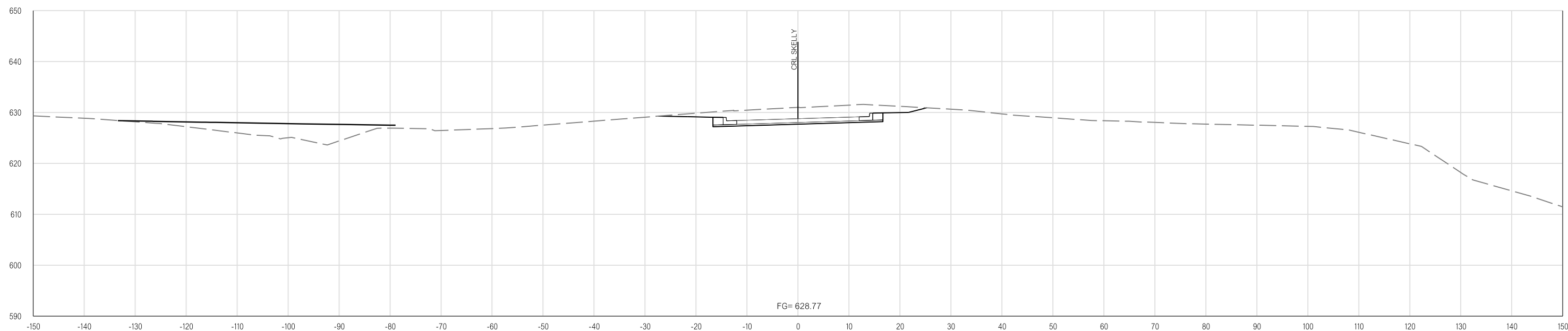


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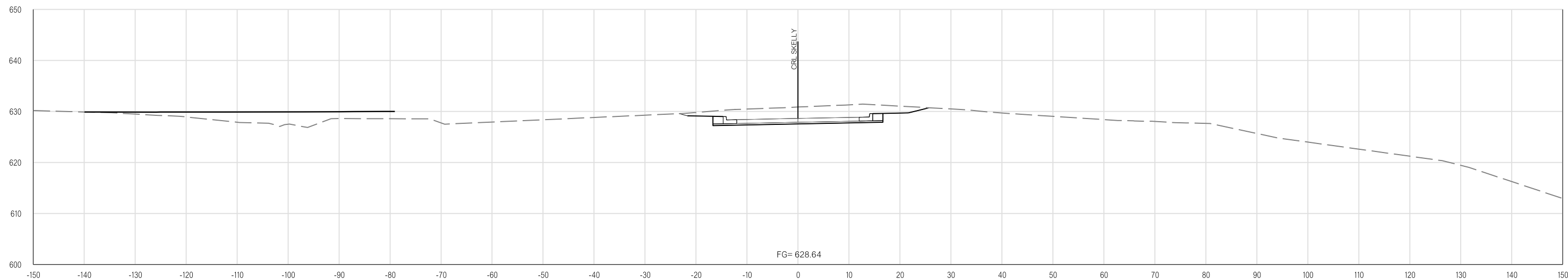
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638+25.17

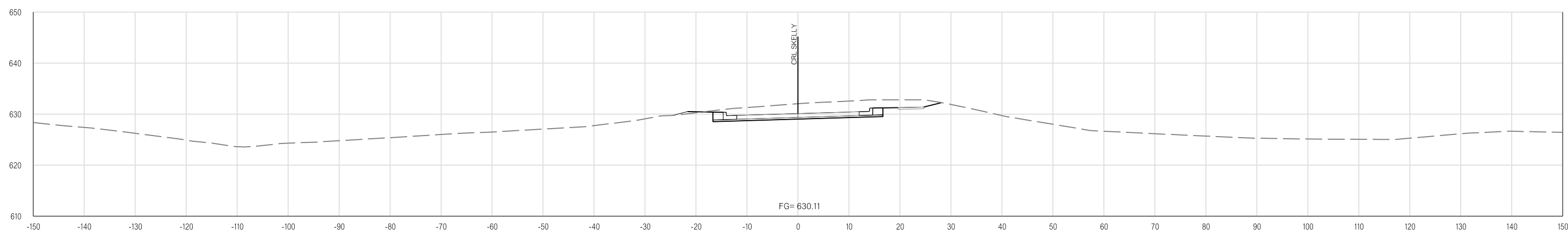
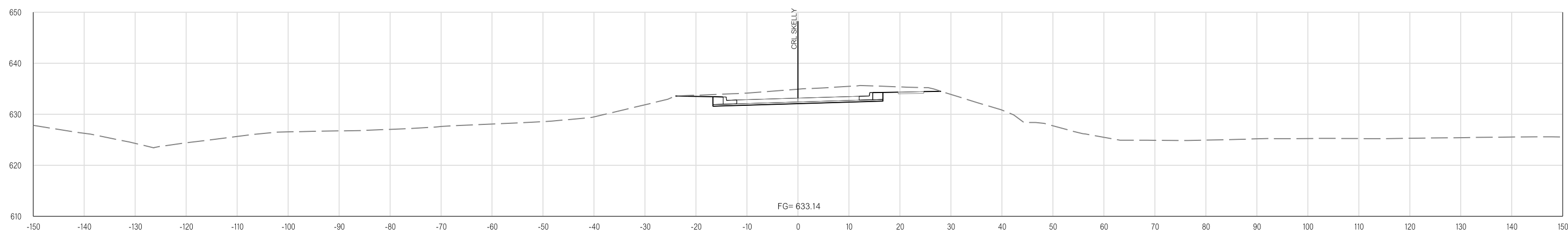
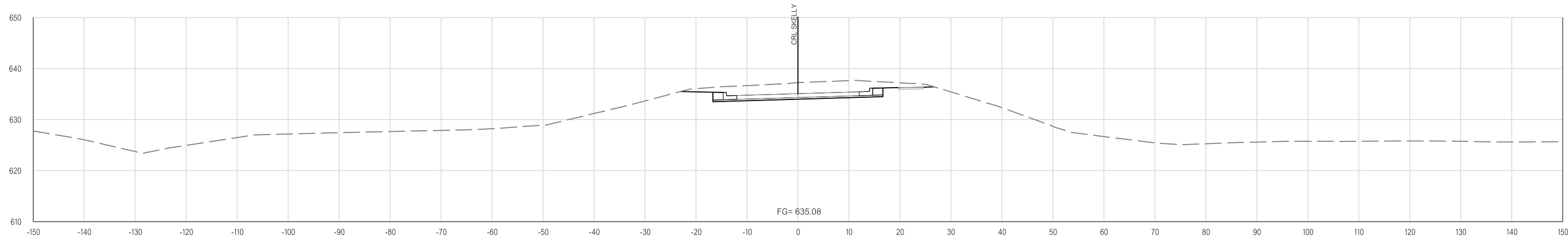


638+13.17

SKELLY DR.

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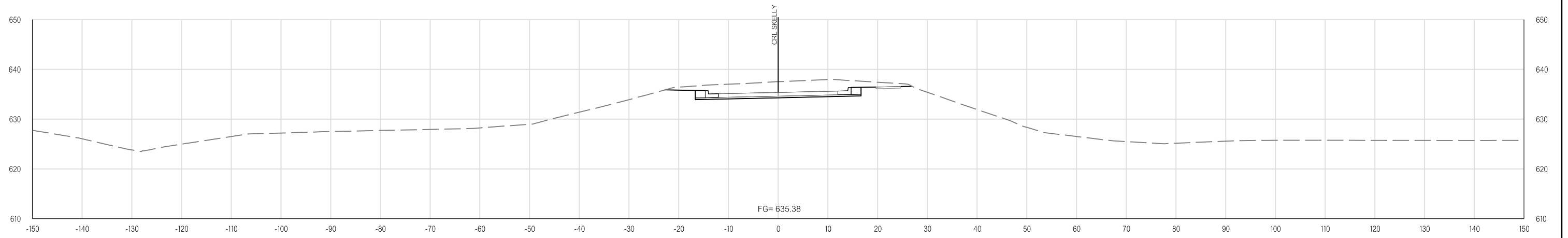
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SKELLY DR.

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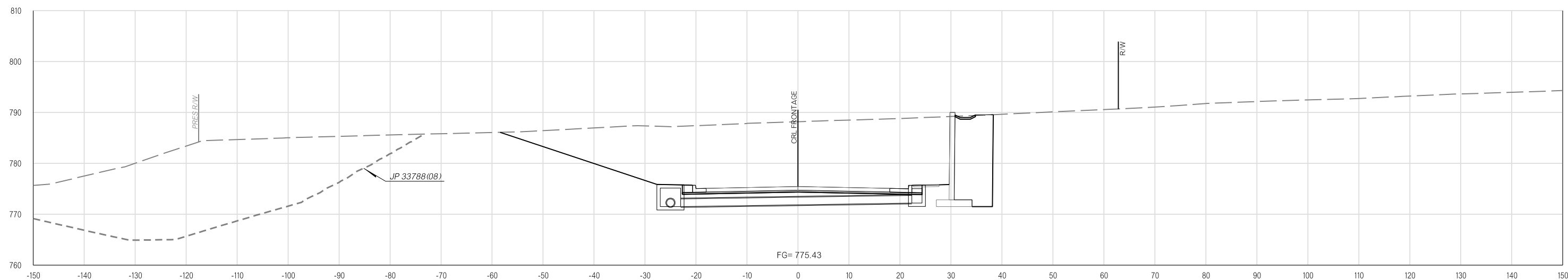


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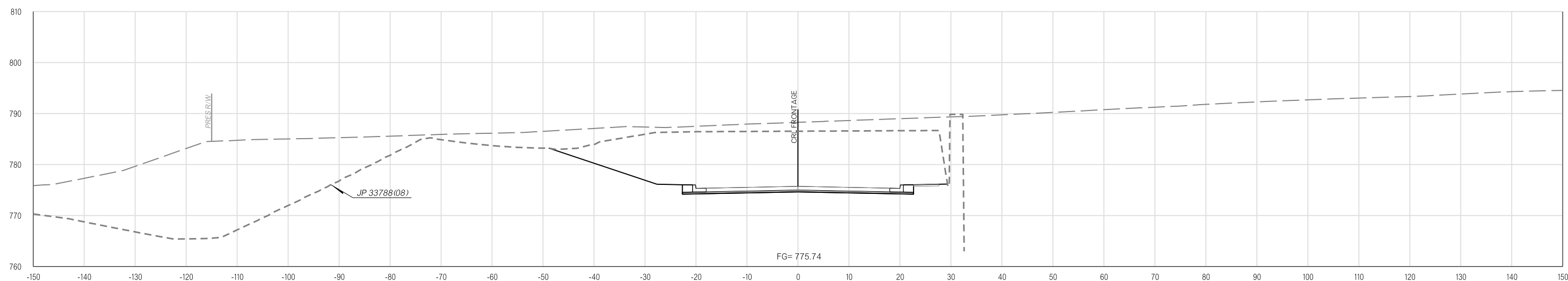
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156+00.00

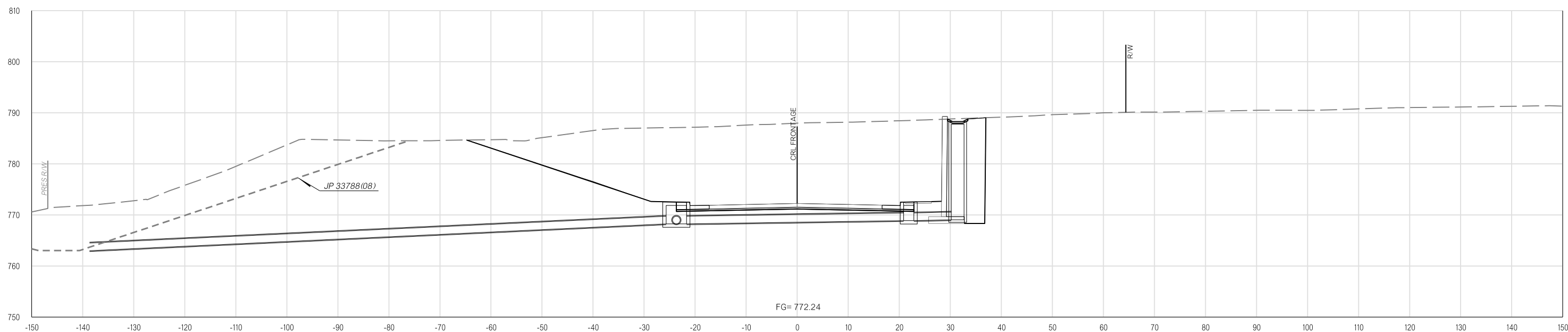


155+85.71

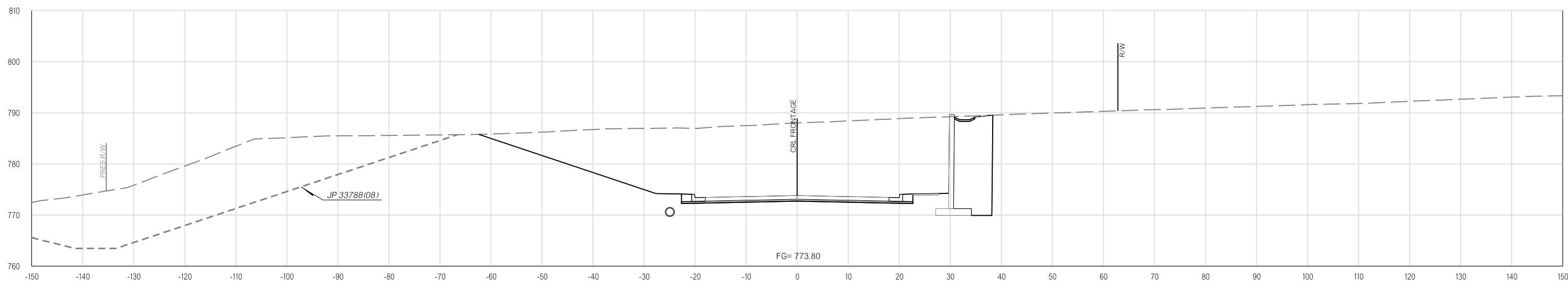
FRONTAGE RD.

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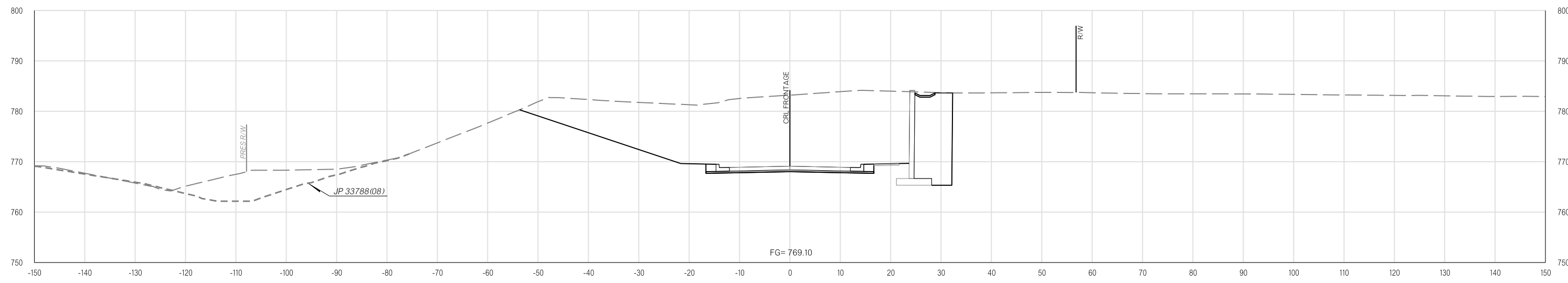


158+00.00

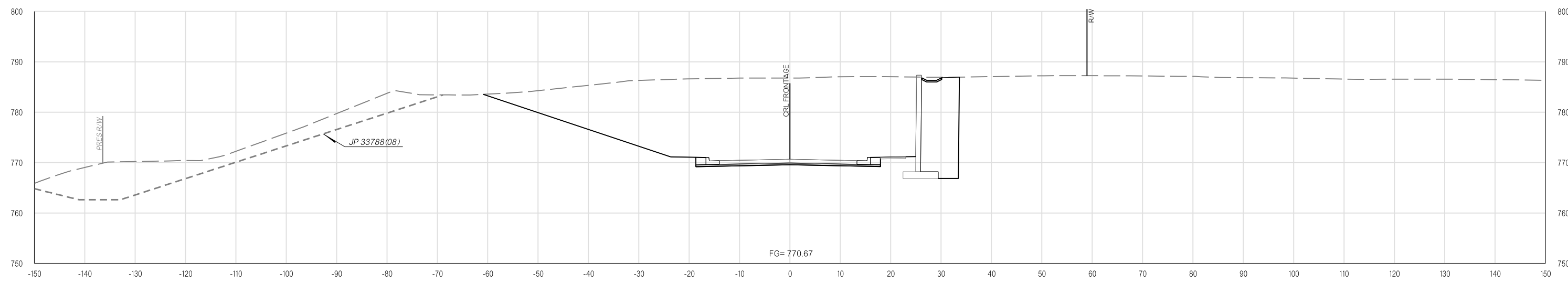


157+00.00

FRONTAGE RD.



160+00.00

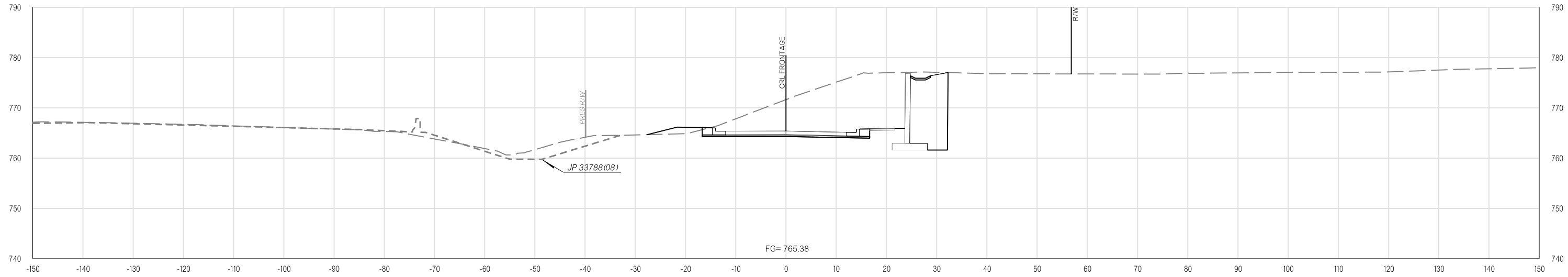


159+00.00

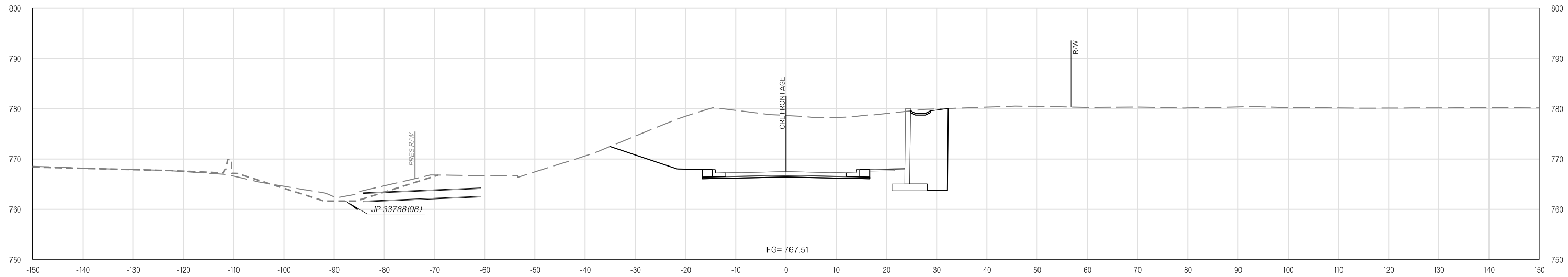
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162+00.00

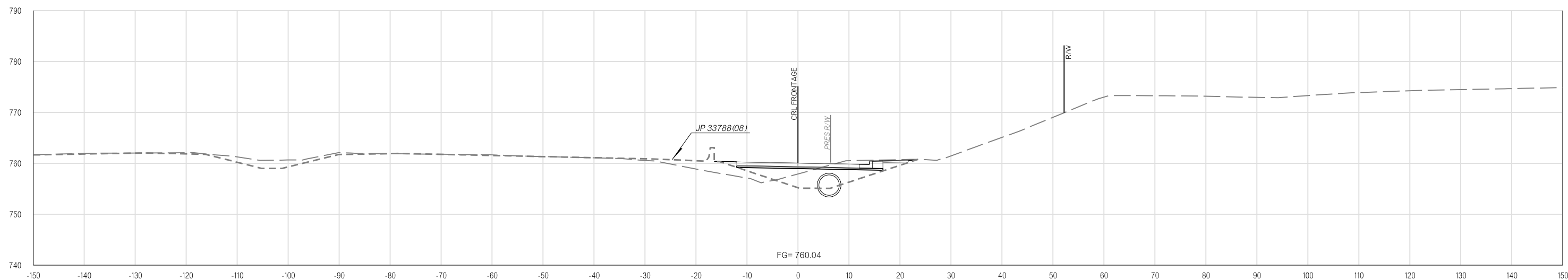


161+00.00

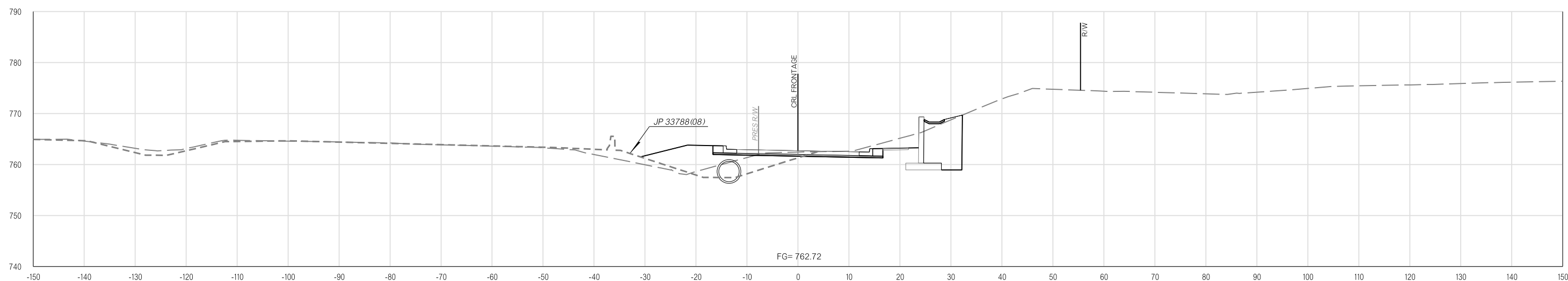
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164+00.00

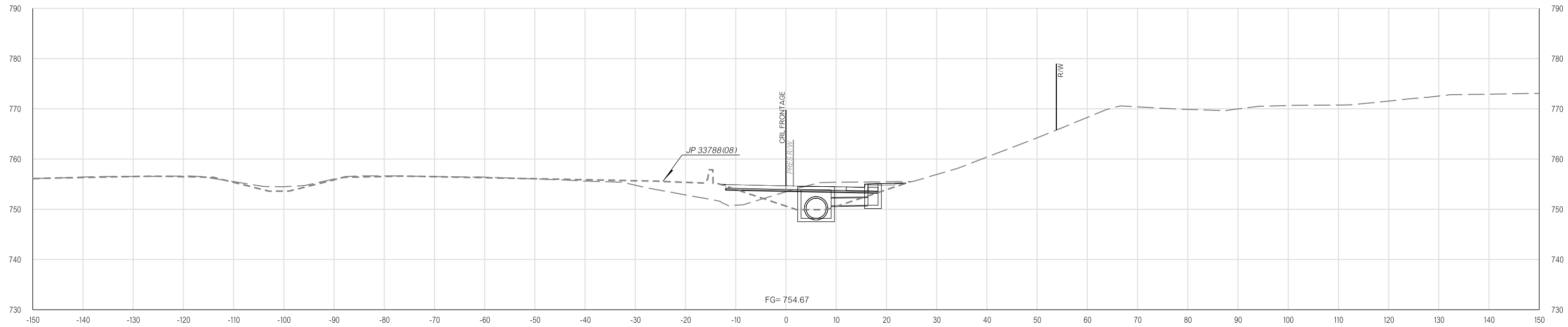


163+00.00

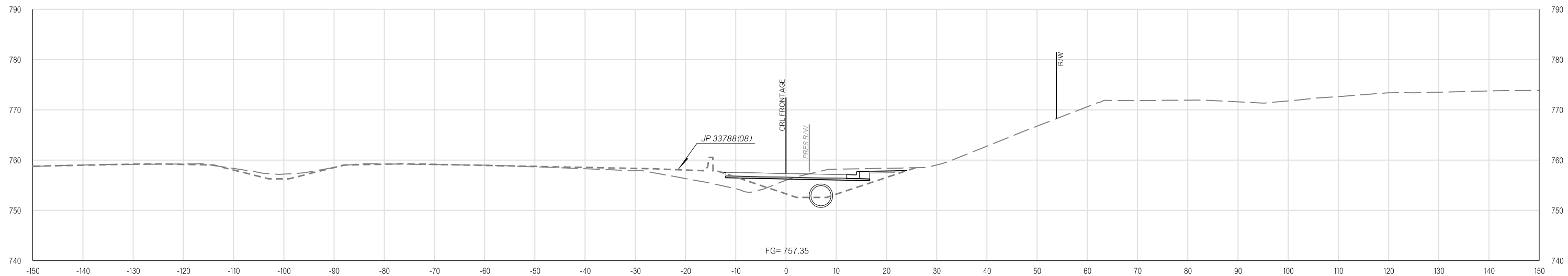
FRONTAGE RD.

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166+00.00

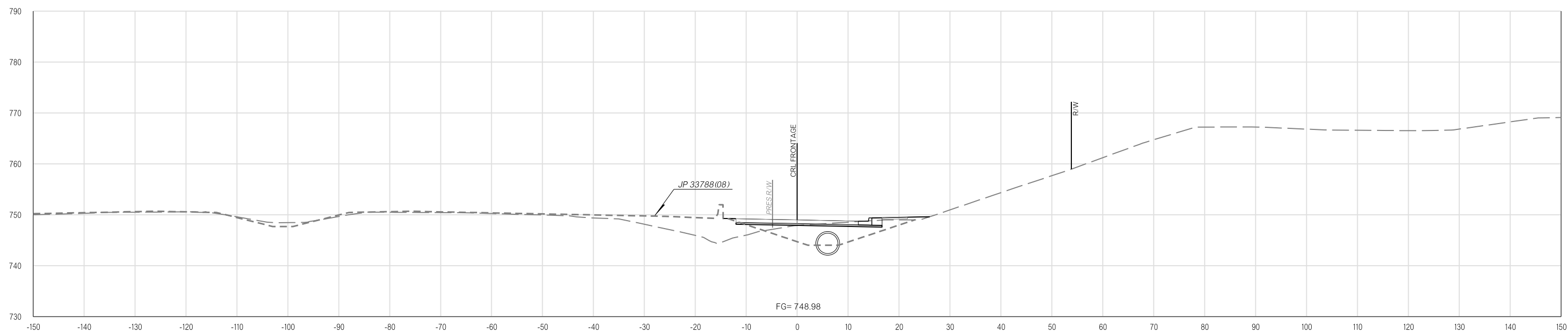


165+00.00

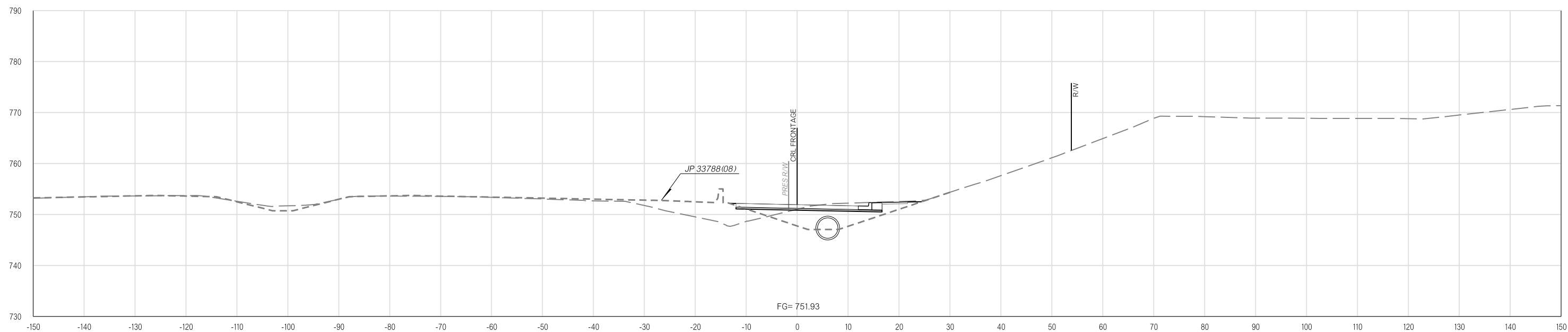
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168+00.00

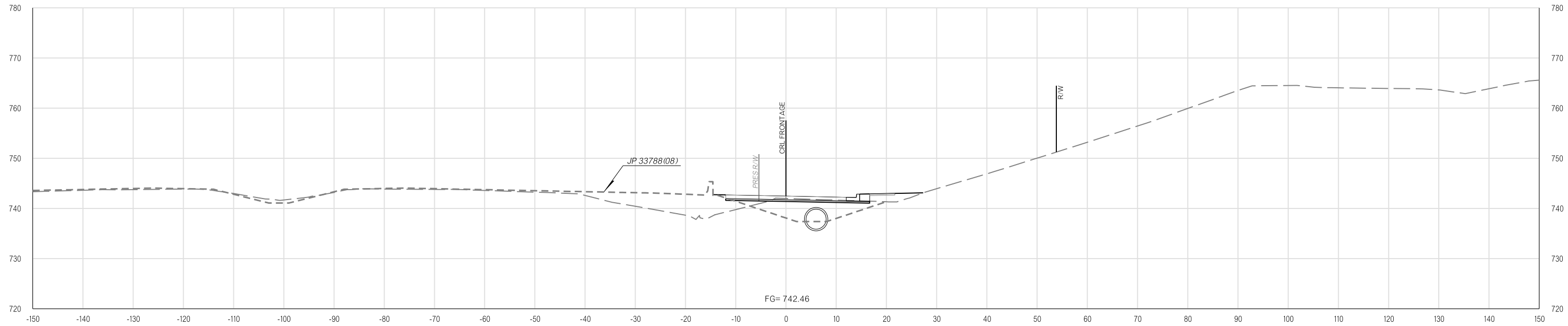


167+00.00

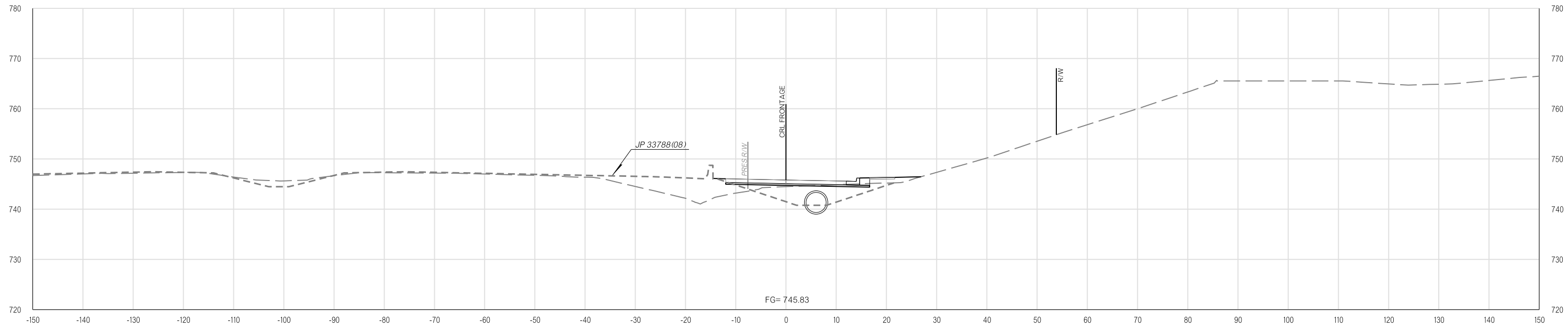
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170+00.00

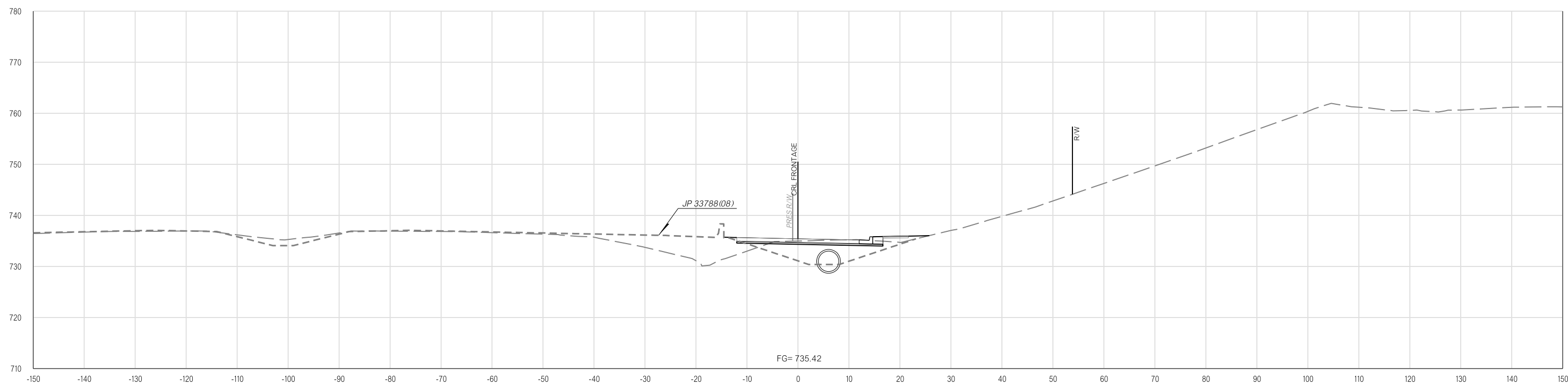


169+00.00

FRONTAGE RD.

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172+00.00

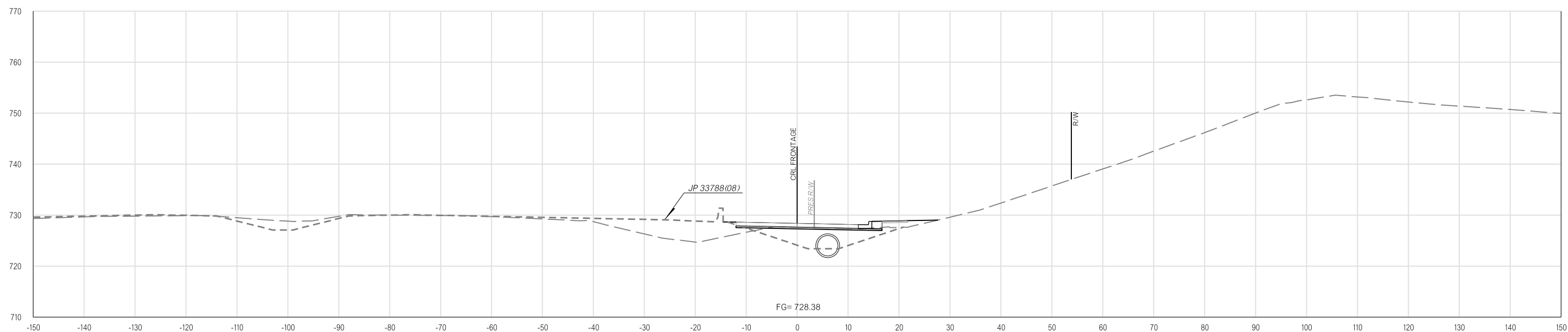


171+00.00

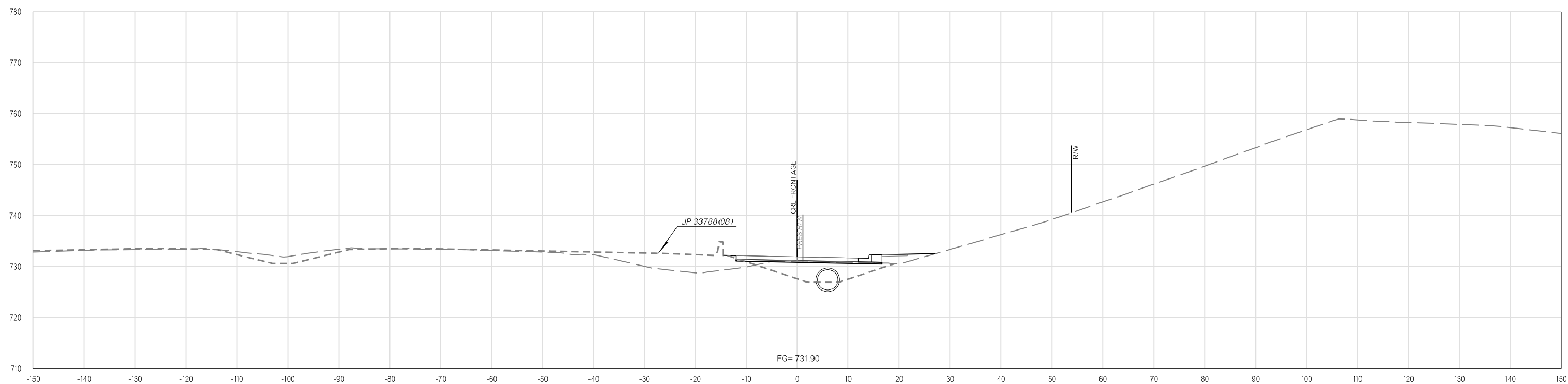
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174+00.00



173+00.00

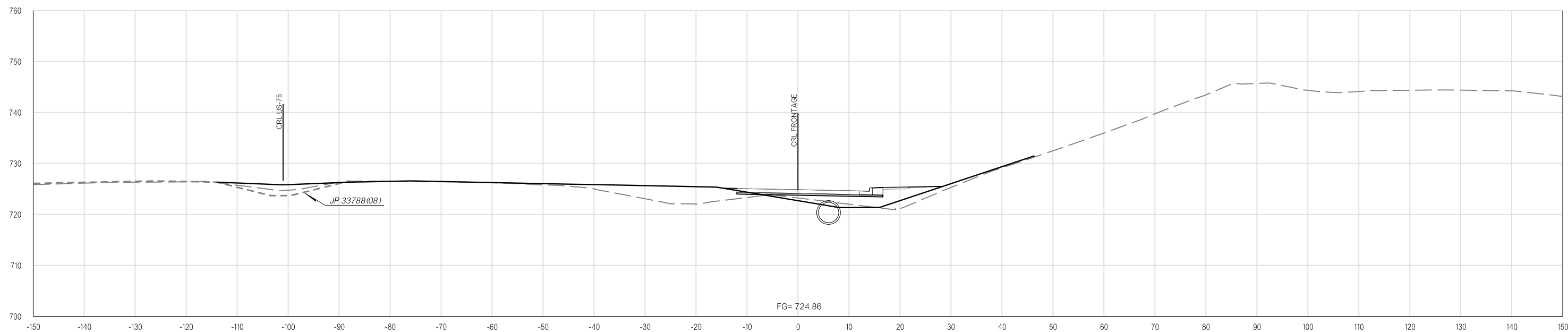
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176+00.00

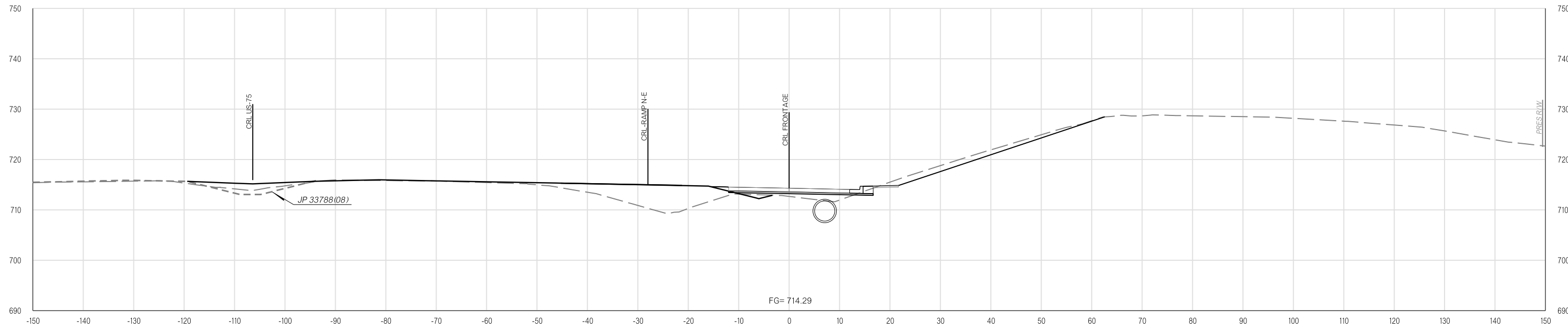


175+00.00

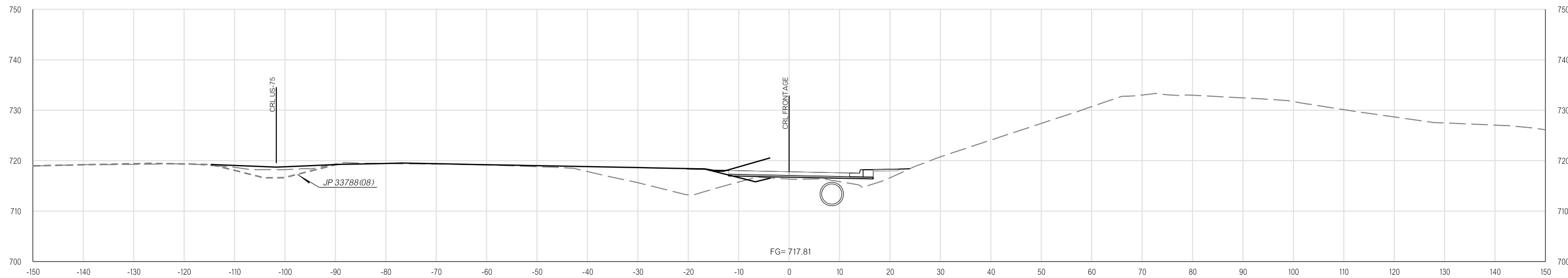
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178+00.00

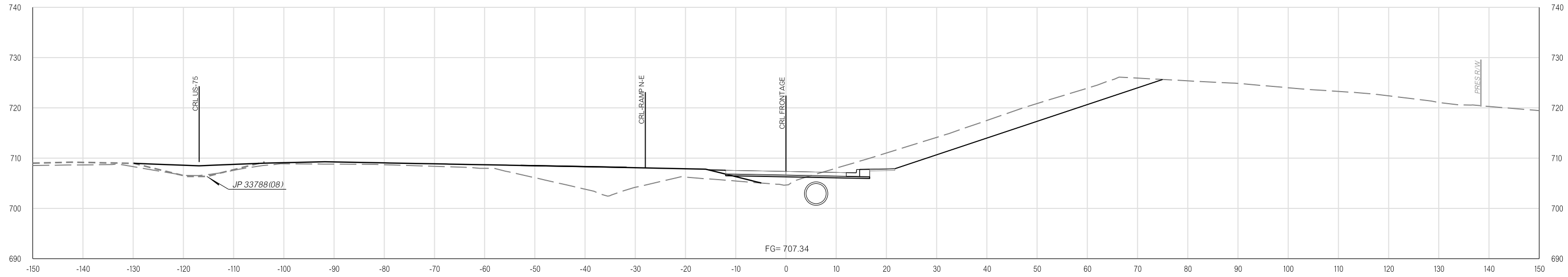


177+00.00

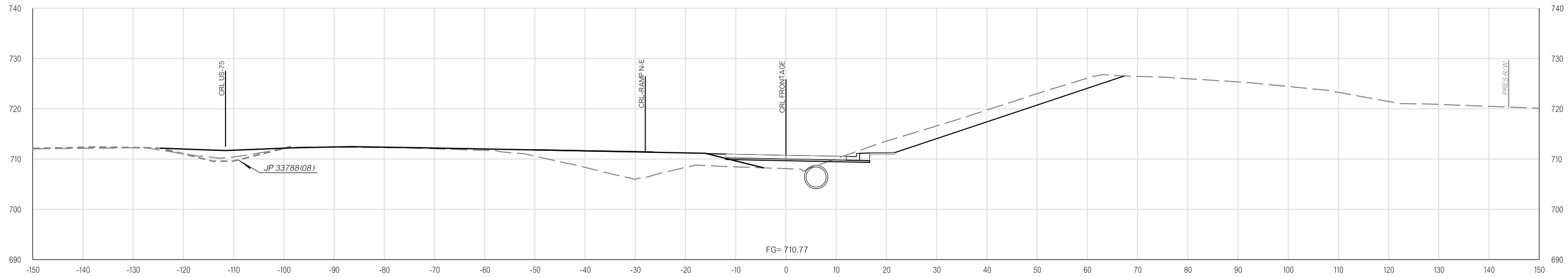
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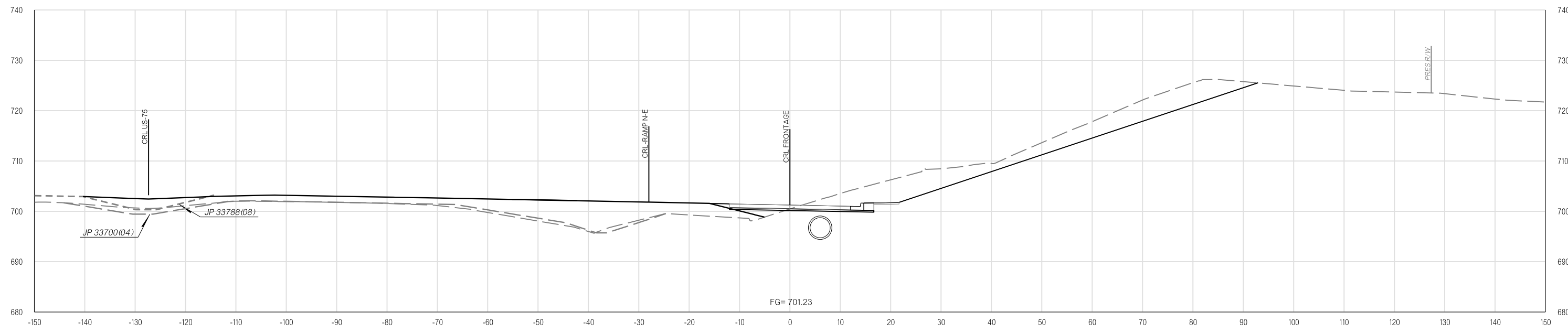


180+00.00

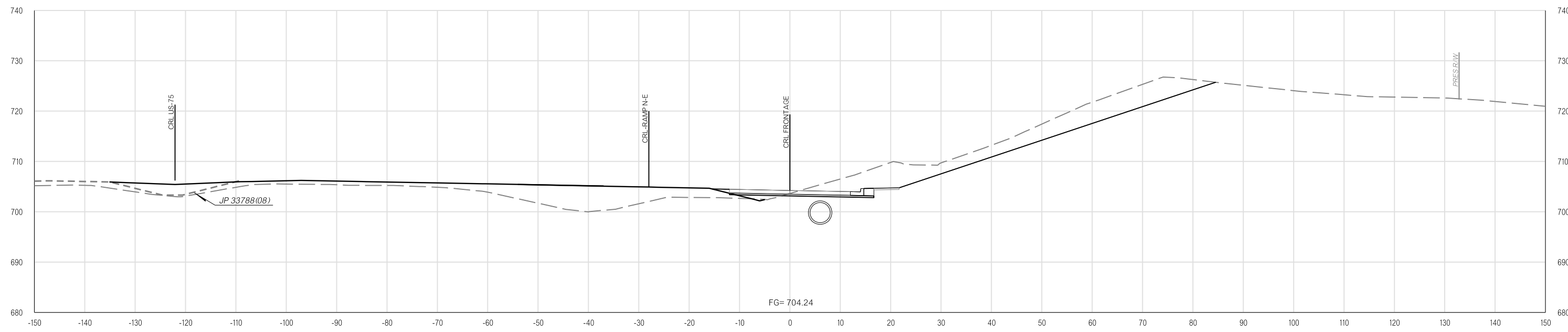


179+00.00

FRONTAGE RD.

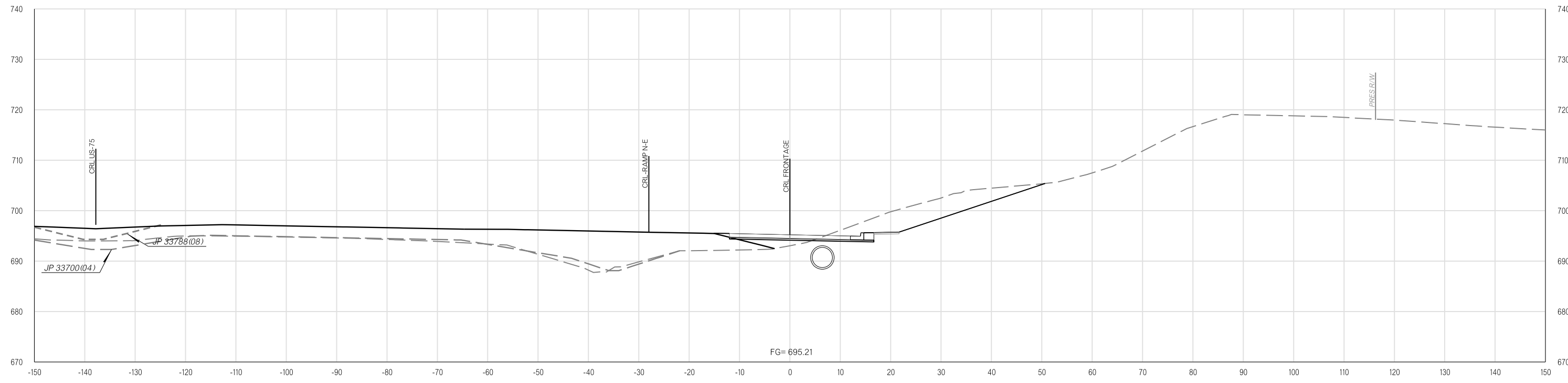


182+00.00

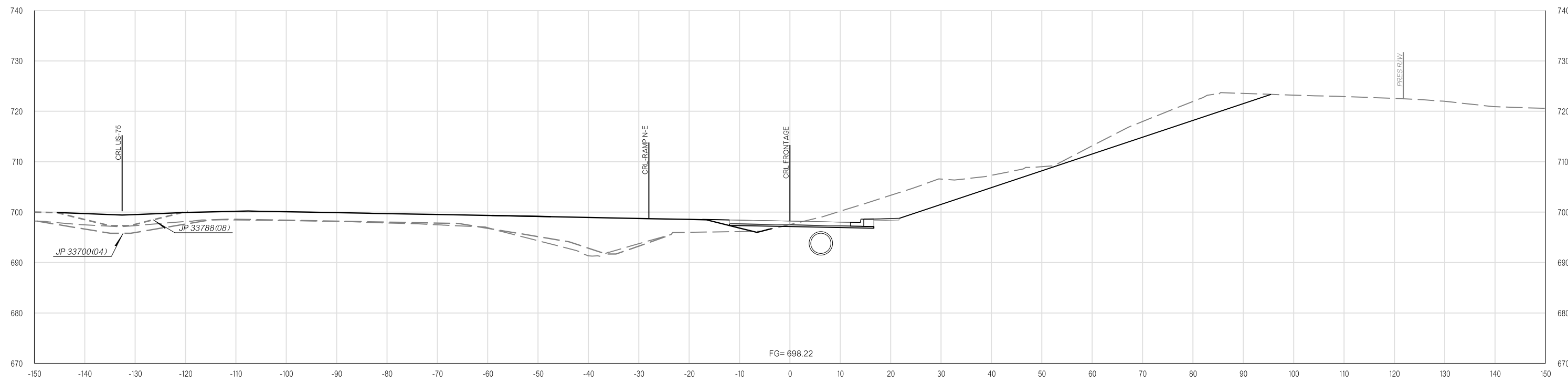


181+00.00

FRONTAGE RD.

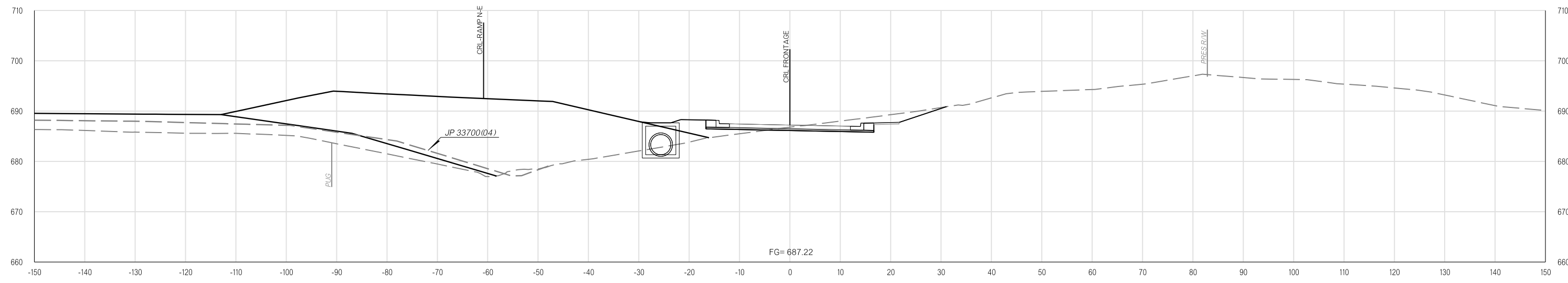


184+00.00

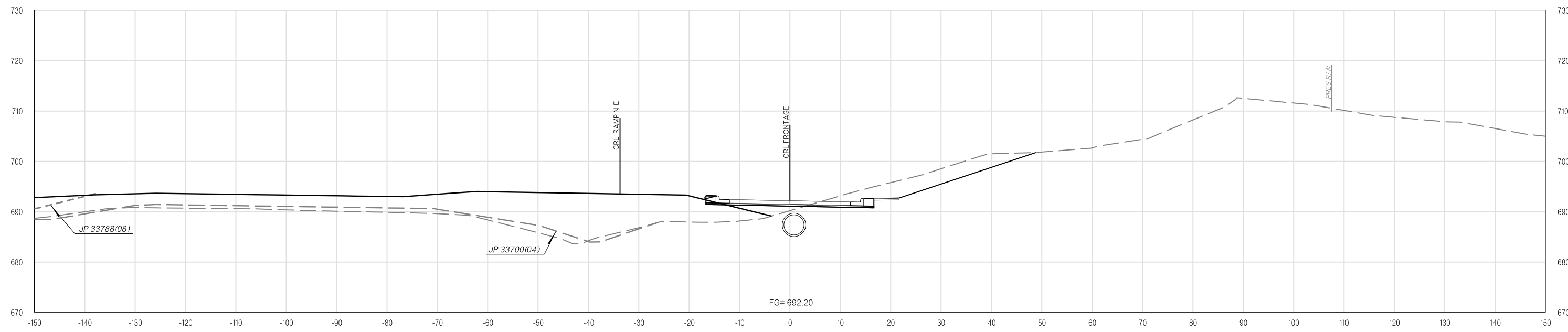


183+00.00

FRONTAGE RD.

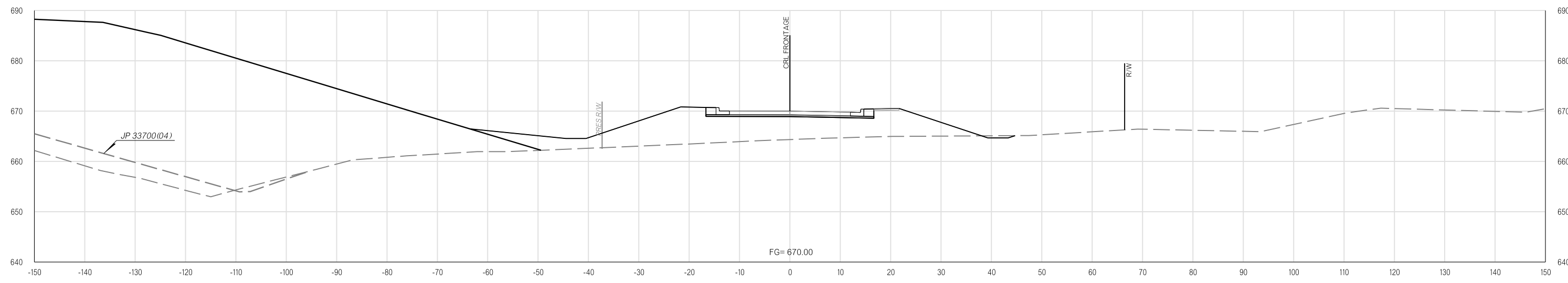


186+00.00

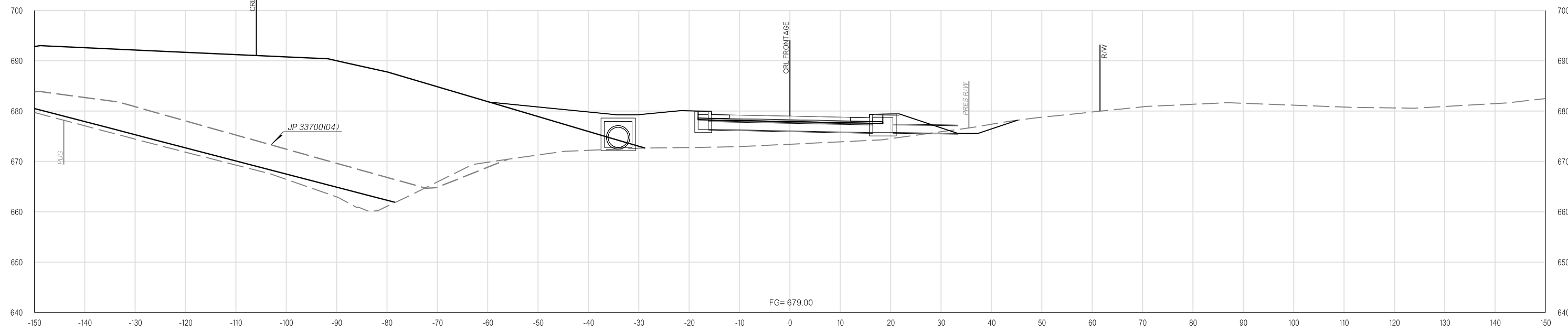


185+00.00

FRONTAGE RD.

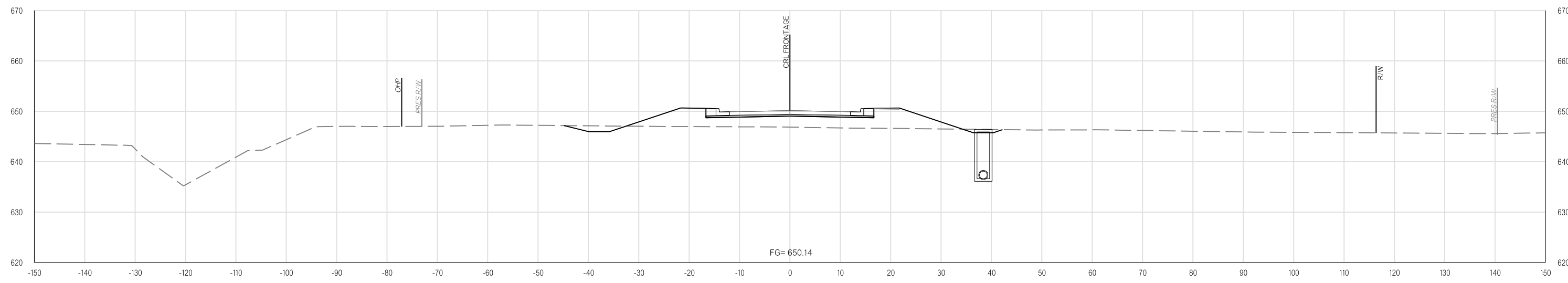


188+00.00

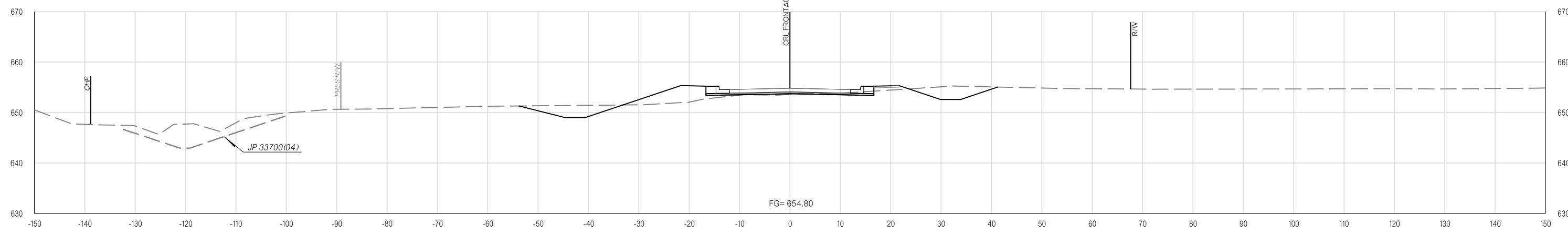


187+00.00

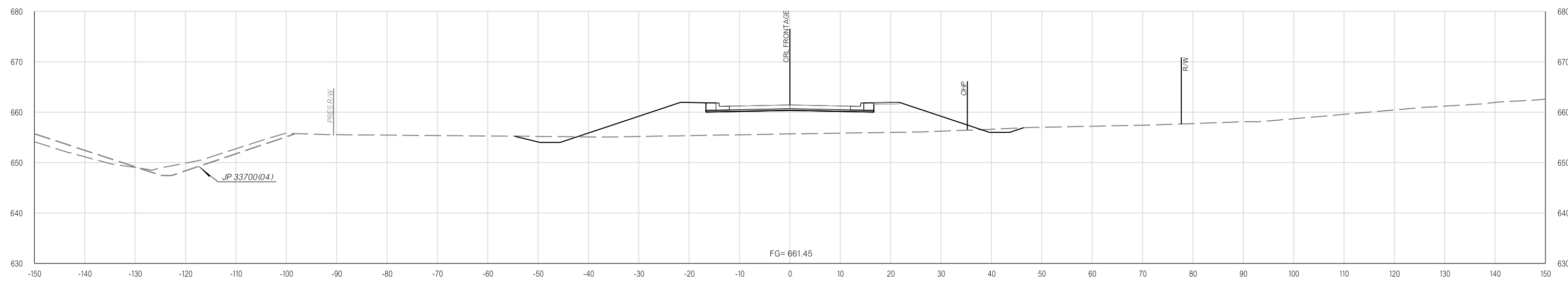
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191+00.00



190+00.00

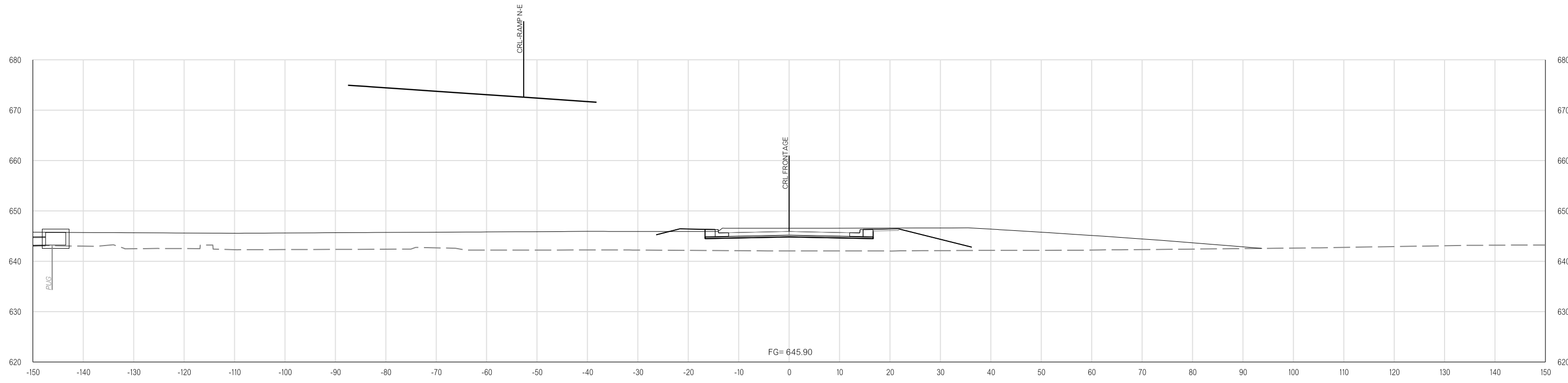


189+00.00

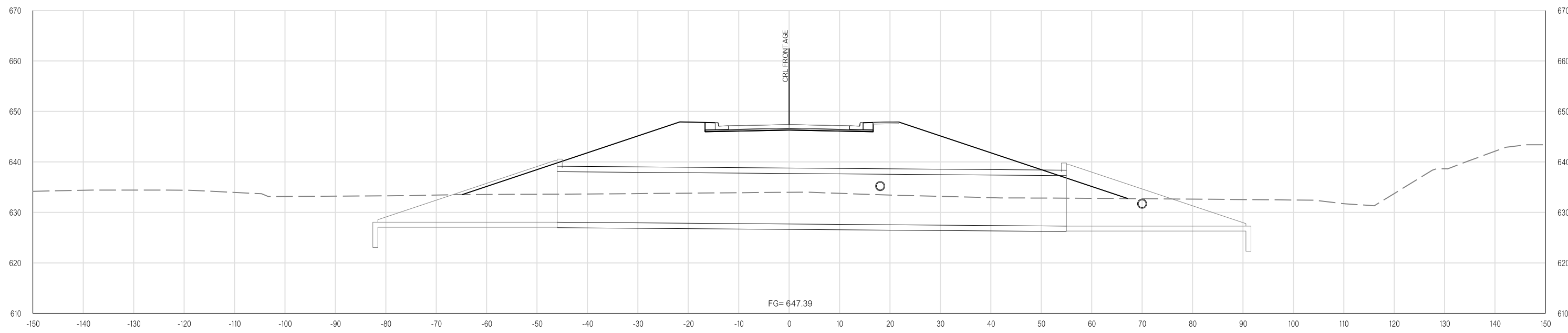
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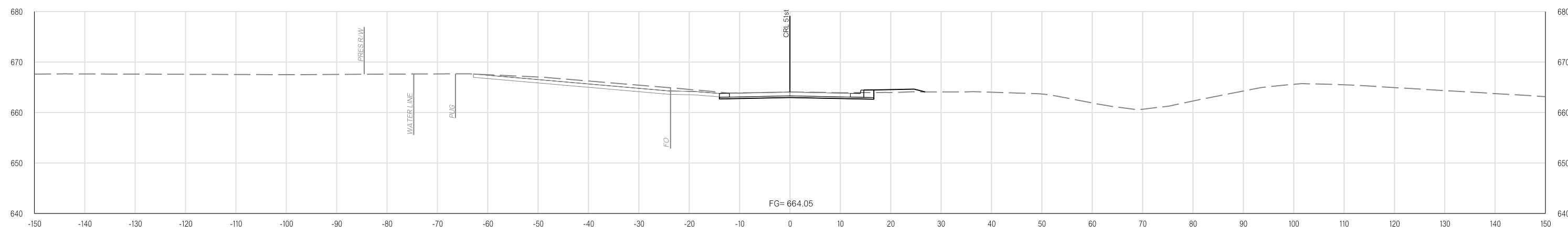


192+65.00

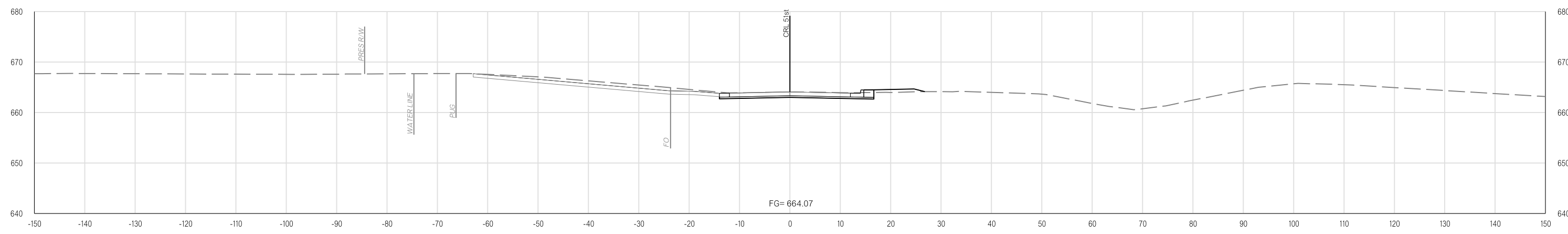


192+00.00

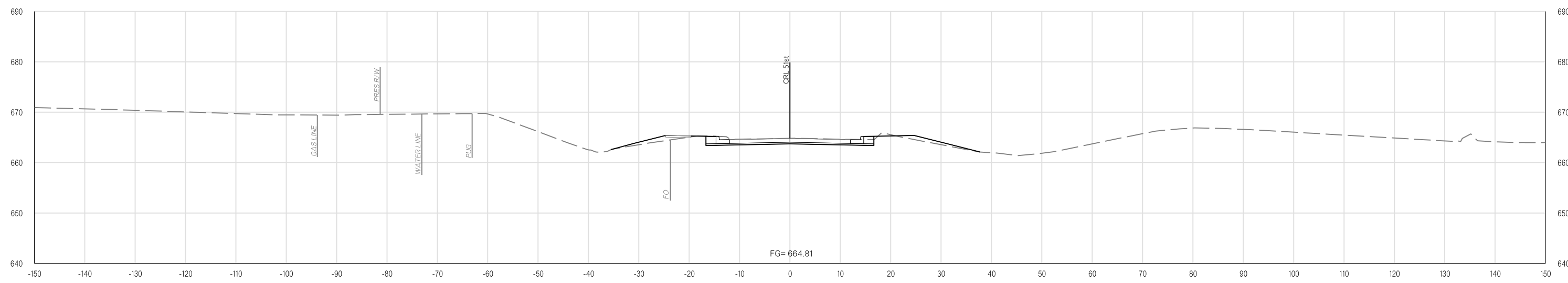
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553+02.73



553+00.00

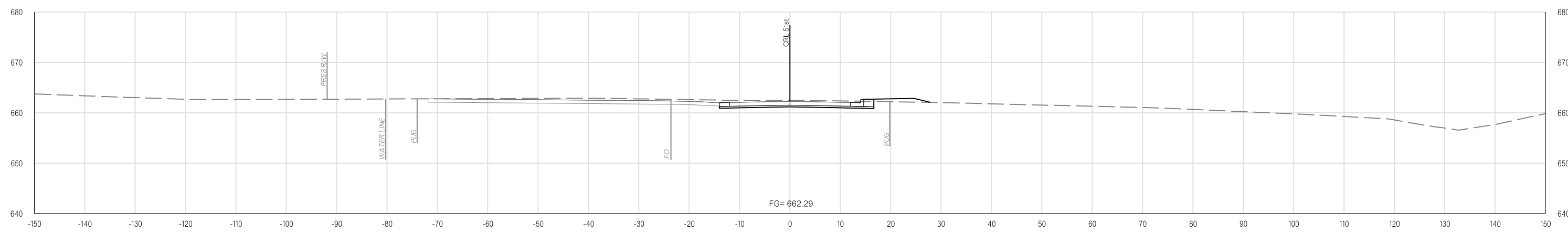


552+01.57

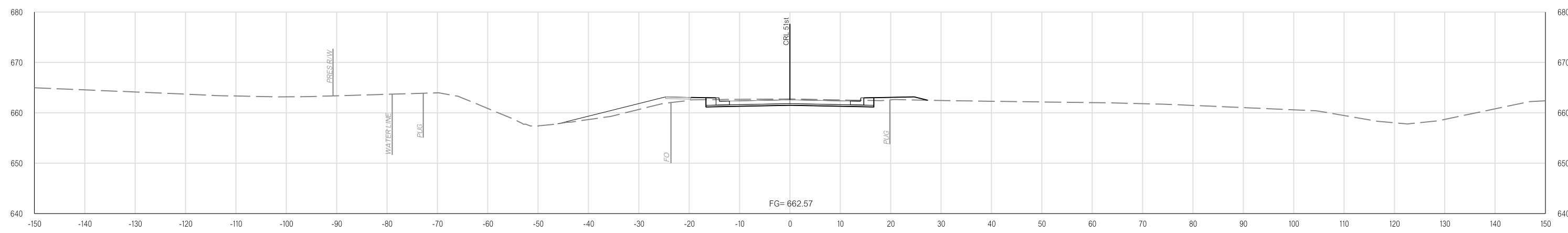
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R/W UTILITY MEETING

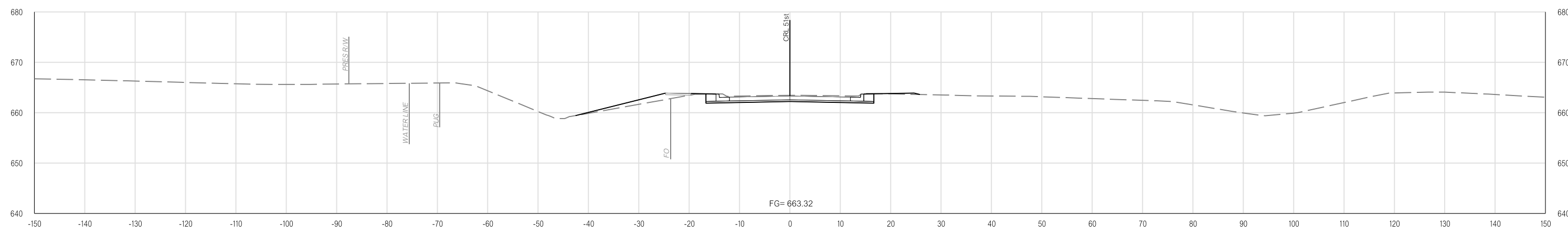
Mar. 17, 2021



555+37.63



555+00.00



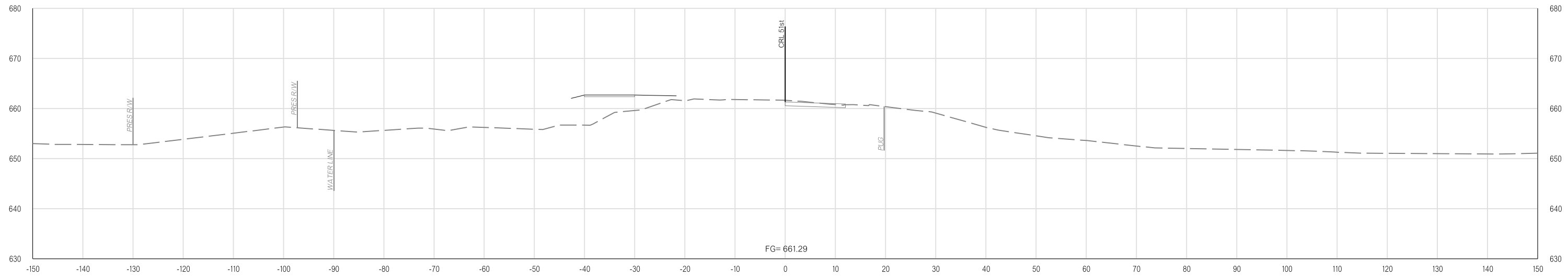
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51st ST.

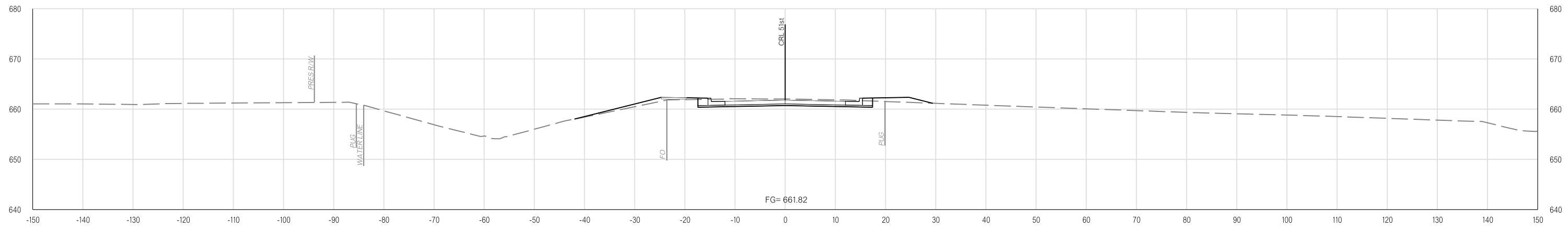
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3/17/2021

R/W UTILITY MEETING

Mar. 17, 2021



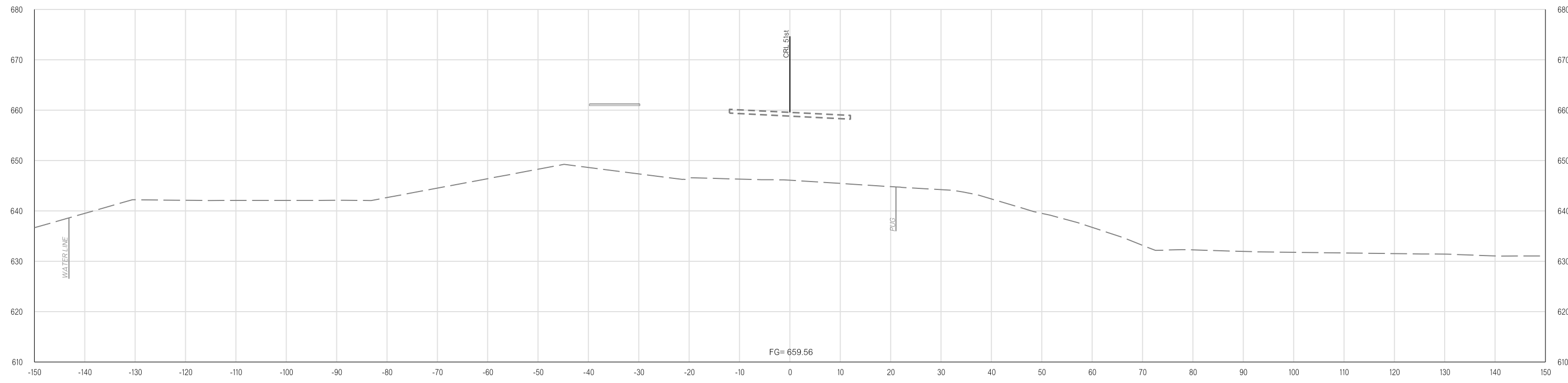
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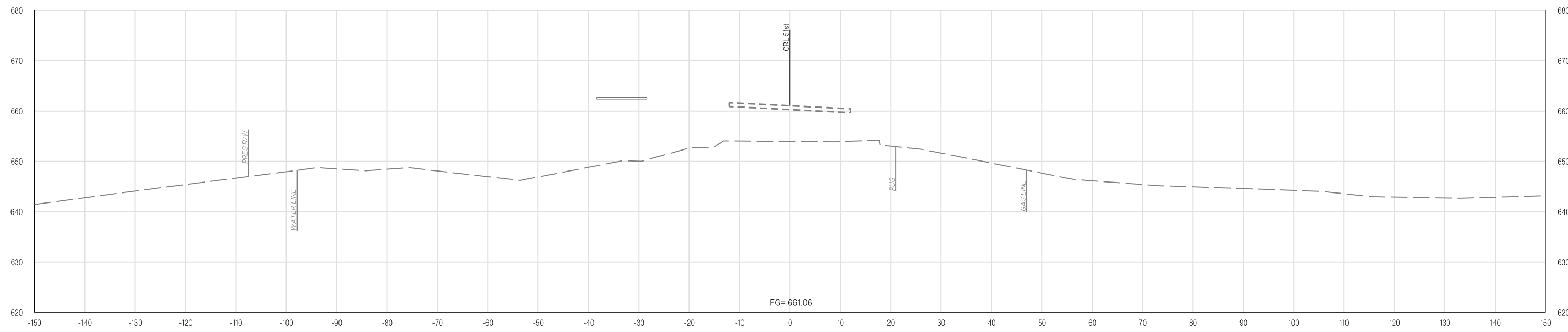
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51st ST.

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3/17/2021



558+00.00

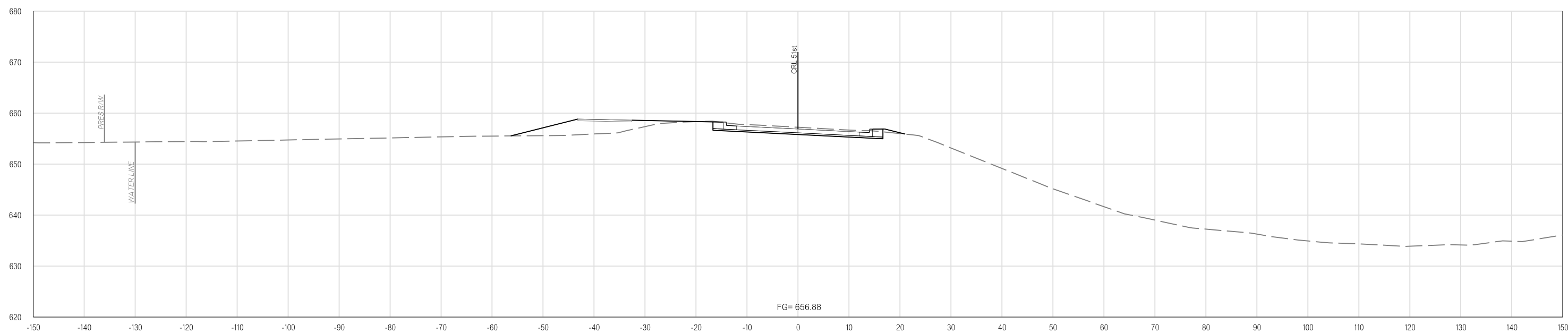


557+00.00

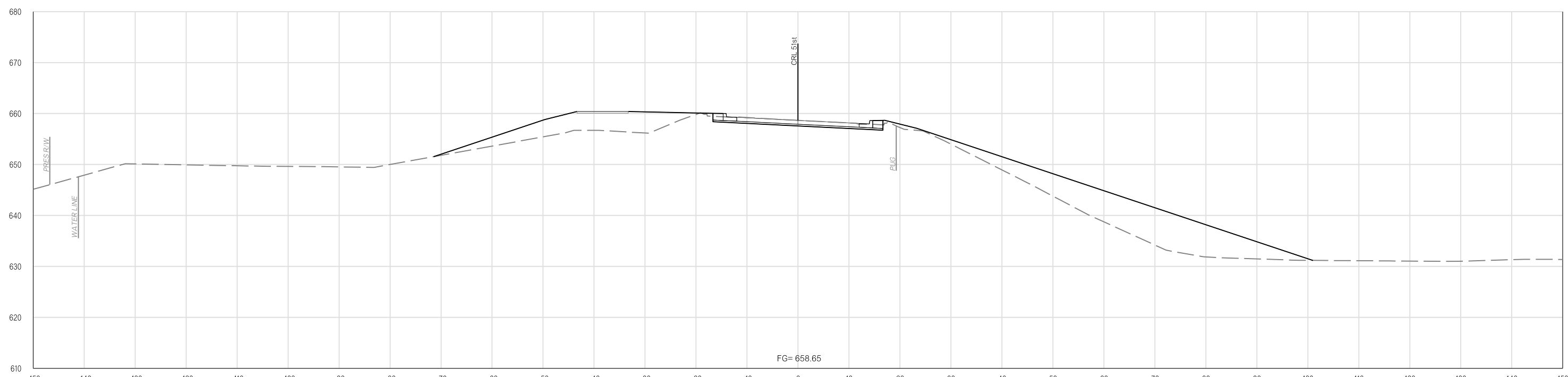
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559+00.00

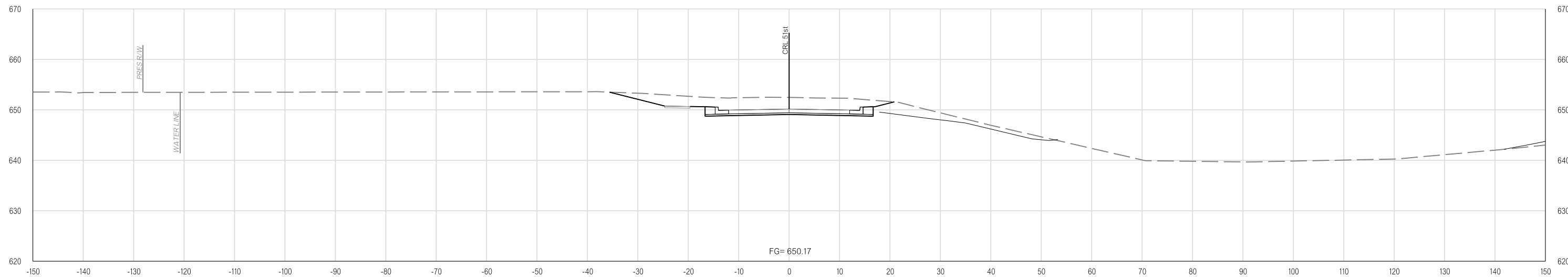


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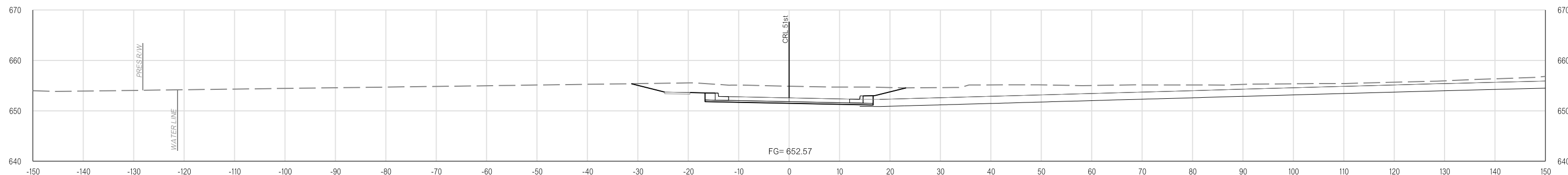
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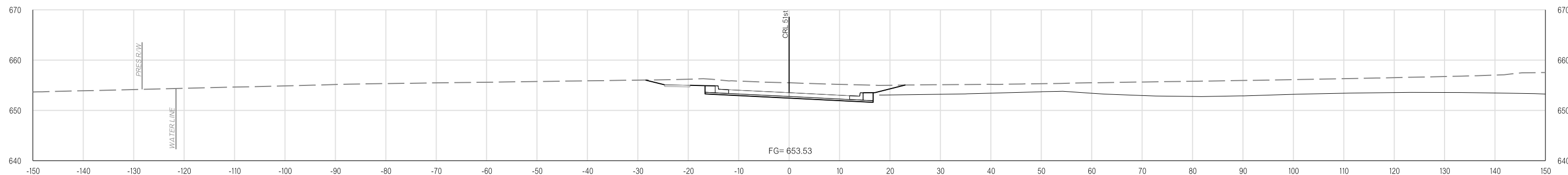
Mar. 17, 2021



561+00.00



560+28.54



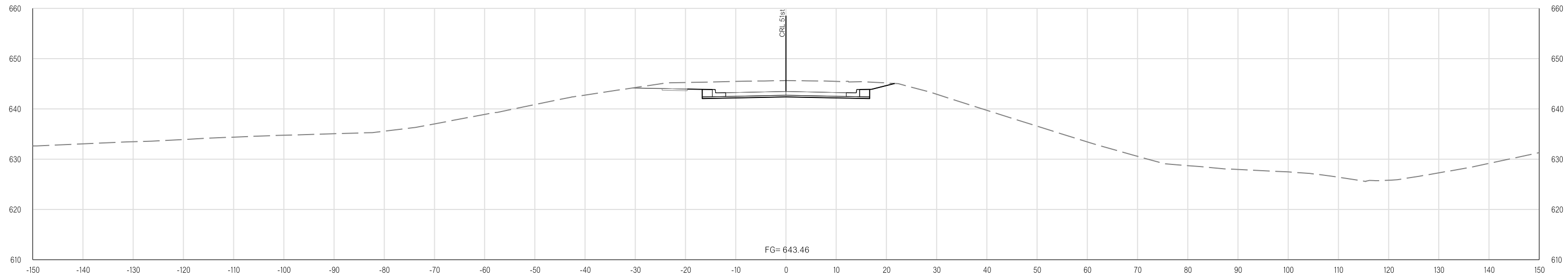
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51st ST.

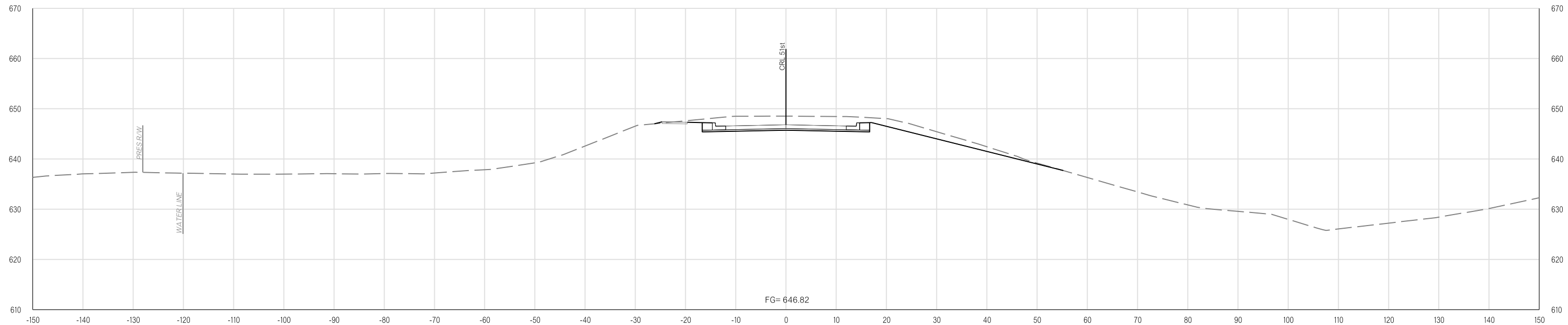
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563+00.00

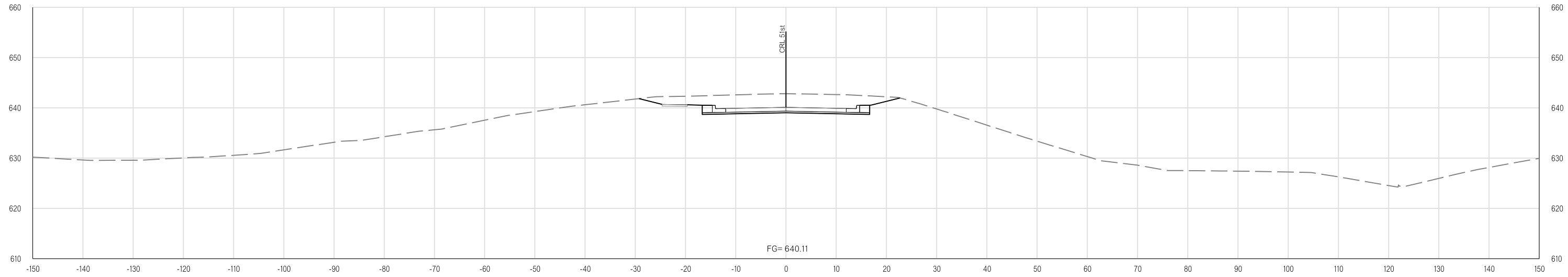


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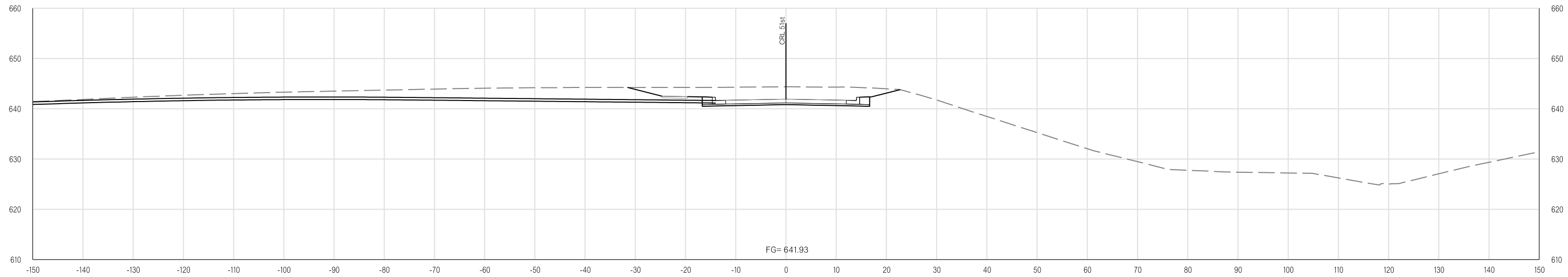
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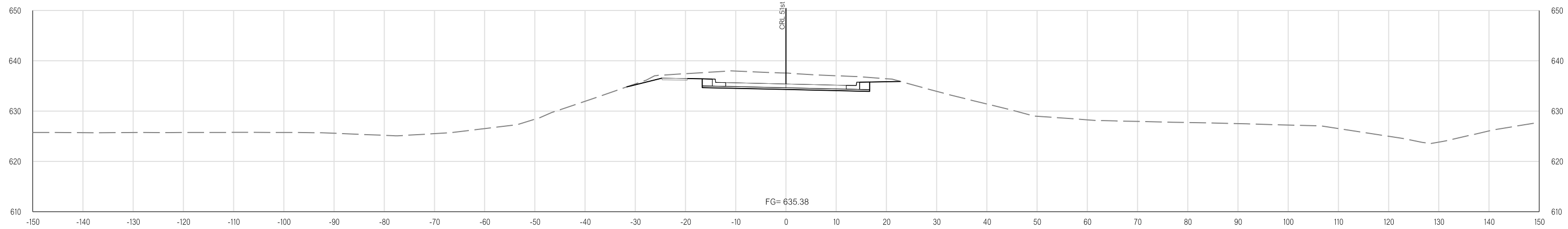


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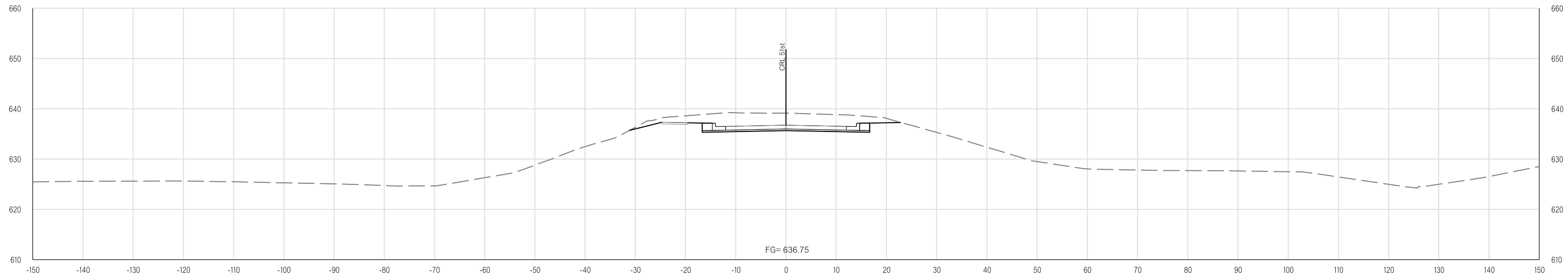
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565+40.87



565+00.00

51st ST.